


**JANUARY 27, 2016 CITY COUNCIL AGENDA
CERTIFICATION**

This certification is given pursuant to Chapter XI, Section 9 of the City Charter for the City Council Agenda dated January 27, 2016. We hereby certify, as to those contracts, agreements, or other obligations on this Agenda authorized by the City Council for which expenditures of money by the City are required, that all of the money required for those contracts, agreements, and other obligations is in the City treasury to the credit of the fund or funds from which the money is to be drawn, as required and permitted by the City Charter, and that the money is not appropriated for any other purpose.



A.C. Gonzalez
City Manager

1-15-16
Date



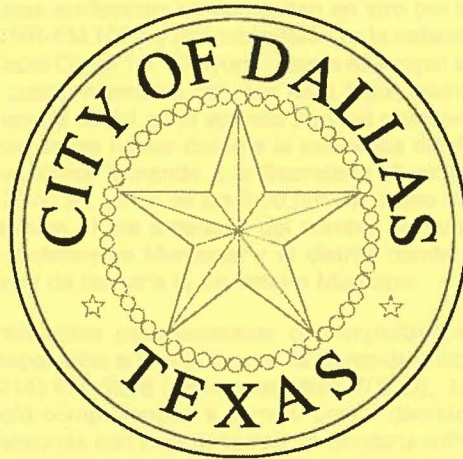
FOR: Jeanne Chipperfield
Chief Financial Officer

1/15/16
Date

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CITY SECRETARY
DALLAS, TEXAS



COUNCIL AGENDA

January 27, 2016

Date

(For General Information and Rules of Courtesy, Please See Opposite Side.)
(La Información General Y Reglas De Cortesía Que Deben Observarse
Durante Las Asambleas Del Consejo Municipal Aparecen En El Lado Opuesto, Favor De Leerlas.)

General Information

The Dallas City Council regularly meets on Wednesdays beginning at 9:00 a.m. in the Council Chambers, 6th floor, City Hall, 1500 Marilla. Council agenda meetings are broadcast live on WRR-FM radio (101.1 FM) and on Time Warner City Cable Channel 16. Briefing meetings are held the first and third Wednesdays of each month. Council agenda (voting) meetings are held on the second and fourth Wednesdays. Anyone wishing to speak at a meeting should sign up with the City Secretary's Office by calling (214) 670-3738 by 5:00 p.m. of the last regular business day preceding the meeting. Citizens can find out the name of their representative and their voting district by calling the City Secretary's Office.

Sign interpreters are available upon request with a 48-hour advance notice by calling (214) 670-5208 V/TDD. The City of Dallas is committed to compliance with the Americans with Disabilities Act.

The Council agenda is available in alternative formats upon request.

If you have any questions about this agenda or comments or complaints about city services, call 311.

Rules of Courtesy

City Council meetings bring together citizens of many varied interests and ideas. To insure fairness and orderly meetings, the Council has adopted rules of courtesy which apply to all members of the Council, administrative staff, news media, citizens and visitors. These procedures provide:

- That no one shall delay or interrupt the proceedings, or refuse to obey the orders of the presiding officer.
- All persons should refrain from private conversation, eating, drinking and smoking while in the Council Chamber.
- Posters or placards must remain outside the Council Chamber.
- No cellular phones or audible beepers allowed in Council Chamber while City Council is in session.

"Citizens and other visitors attending City Council meetings shall observe the same rules of propriety, decorum and good conduct applicable to members of the City Council. Any person making personal, impertinent, profane or slanderous remarks or who becomes boisterous while addressing the City Council or while attending the City Council meeting shall be removed from the room if the sergeant-at-arms is so directed by the presiding officer, and the person shall be barred from further audience before the City Council during that session of the City Council. If the presiding officer fails to act, any member of the City Council may move to require enforcement of the rules, and the affirmative vote of a majority of the City Council shall require the presiding officer to act." Section 3.3(c) of the City Council Rules of Procedure.

Información General

El Ayuntamiento de la Ciudad de Dallas se reúne regularmente los miércoles en la Cámara del Ayuntamiento en el sexto piso de la Alcaldía, 1500 Marilla, a las 9 de la mañana. Las reuniones informativas se llevan a cabo el primer y tercer miércoles del mes. Estas audiencias se transmiten en vivo por la estación de radio WRR-FM 101.1 y por cablevisión en la estación *Time Warner City Cable* Canal 16. El Ayuntamiento Municipal se reúne el segundo y cuarto miércoles del mes para tratar asuntos presentados de manera oficial en la agenda para su aprobación. Toda persona que desee hablar durante la asamblea del Ayuntamiento, debe inscribirse llamando a la Secretaría Municipal al teléfono (214) 670-3738, antes de las 5:00 pm del último día hábil anterior a la reunión. Para enterarse del nombre de su representante en el Ayuntamiento Municipal y el distrito donde usted puede votar, favor de llamar a la Secretaría Municipal.

Intérpretes para personas con impedimentos auditivos están disponibles si lo solicita con 48 horas de anticipación llamando al (214) 670-5208 (aparato auditivo V/TDD). La Ciudad de Dallas está comprometida a cumplir con el decreto que protege a las personas con impedimentos, *Americans with Disabilities Act*. **La agenda del Ayuntamiento está disponible en formatos alternos si lo solicita.**

Si tiene preguntas sobre esta agenda, o si desea hacer comentarios o presentar quejas con respecto a servicios de la Ciudad, llame al 311.

Reglas de Cortesía

Las asambleas del Ayuntamiento Municipal reúnen a ciudadanos de diversos intereses e ideologías. Para asegurar la imparcialidad y el orden durante las asambleas, el Ayuntamiento ha adoptado ciertas reglas de cortesía que aplican a todos los miembros del Ayuntamiento, al personal administrativo, personal de los medios de comunicación, a los ciudadanos, y a visitantes. Estos reglamentos establecen lo siguiente:

- Ninguna persona retrasará o interrumpirá los procedimientos, o se negará a obedecer las órdenes del oficial que preside la asamblea.
- Todas las personas deben abstenerse de entablar conversaciones, comer, beber y fumar dentro de la cámara del Ayuntamiento.
- Anuncios y pancartas deben permanecer fuera de la cámara del Ayuntamiento.
- No se permite usar teléfonos celulares o enlaces electrónicos (*paggers*) audibles en la cámara del Ayuntamiento durante audiencias del Ayuntamiento Municipal.

"Los ciudadanos y visitantes presentes durante las asambleas del Ayuntamiento Municipal deben obedecer las mismas reglas de comportamiento, decoro y buena conducta que se aplican a los miembros del Ayuntamiento Municipal. Cualquier persona que haga comentarios impertinentes, utilice vocabulario obsceno o difamatorio, o que al dirigirse al Ayuntamiento lo haga en forma escandalosa, o si causa disturbio durante la asamblea del Ayuntamiento Municipal, será expulsada de la cámara si el oficial que esté presidiendo la asamblea así lo ordena. Además, se le prohibirá continuar participando en la audiencia ante el Ayuntamiento Municipal. Si el oficial que preside la asamblea no toma acción, cualquier otro miembro del Ayuntamiento Municipal puede tomar medidas para hacer cumplir las reglas establecidas, y el voto afirmativo de la mayoría del Ayuntamiento Municipal precisará al oficial que esté presidiendo la sesión a tomar acción." Según la sección 3.3(c) de las reglas de procedimientos del Ayuntamiento.

"Pursuant to Section 30.06, Penal Code (trespass by license holder with a concealed handgun), a person licensed under Subchapter H, Chapter 411, Government Code (handgun licensing law), may not enter this property with a concealed handgun."

"De acuerdo con la sección 30.06 del código penal (ingreso sin autorización de un titular de una licencia con una pistola oculta), una persona con licencia según el subcapítulo h, capítulo 411, código del gobierno (ley sobre licencias para portar pistolas), no puede ingresar a esta propiedad con una pistola oculta."

"Pursuant to Section 30.07, Penal Code (trespass by license holder with an openly carried handgun), a person licensed under Subchapter H, Chapter 411, Government Code (handgun licensing law), may not enter this property with a handgun that is carried openly."

"De acuerdo con la sección 30.07 del código penal (ingreso sin autorización de un titular de una licencia con una pistola a la vista), una persona con licencia según el subcapítulo h, capítulo 411, código del gobierno (ley sobre licencias para portar pistolas), no puede ingresar a esta propiedad con una pistola a la vista."

**AGENDA
CITY COUNCIL MEETING
WEDNESDAY, JANUARY 27, 2016
ORDER OF BUSINESS**

Agenda items for which individuals have registered to speak will be considered no earlier than the time indicated below:

9:00 a.m. **INVOCATION AND PLEDGE OF ALLEGIANCE**

OPEN MICROPHONE

MINUTES

Item 1

CONSENT AGENDA

Items 2 - 37

ITEMS FOR INDIVIDUAL CONSIDERATION

No earlier
than 9:15 a.m.

Items 38 - 41

PUBLIC HEARINGS AND RELATED ACTIONS

1:00 p.m.

Items 42 - 56

NOTE: A revised order of business may be posted prior to the date of the council meeting if necessary.

AGENDA
CITY COUNCIL MEETING
JANUARY 27, 2016
CITY OF DALLAS
1500 MARILLA
COUNCIL CHAMBERS, CITY HALL
DALLAS, TEXAS 75201
9:00 A. M.

Invocation and Pledge of Allegiance (Council Chambers)

Agenda Item/Open Microphone Speakers

VOTING AGENDA

1. Approval of Minutes of the January 13, 2016 City Council Meeting

CONSENT AGENDA

Aviation

2. Authorize a five-year lease agreement with two five-year renewal options with Viceroy Regal, LP for approximately 2.808 acres of land located at 2225 Burbank Street and 2311 Burbank Street to be used for Dallas Love Field airport operations - Not to exceed \$1,580,640 - Financing: Aviation Current Funds (\$316,128 estimated annually; not to exceed \$1,580,640 over primary term of lease) (subject to annual appropriations)
3. Authorize an early Termination Agreement with Akridge Aircraft Interiors, Inc. regarding its lease at Dallas Executive Airport, for approximately 107,523 square feet; and pay a final termination payment equal to \$374,500 - Not to exceed \$374,500 - Financing: Aviation Current Funds
4. Authorize a First Amendment to the consolidated ground lease with Gulfstream Aerospace Services Corporation at Dallas Love Field to increase the leasehold by approximately 5.842 acres into tract 1 of the leased premise - Financing: Estimated Increased Annual Revenue: \$165,405; Cumulative Rent: \$11,429,691

CONSENT AGENDA (Continued)**Business Development & Procurement Services**

5. Authorize the first twelve-month renewal option to the service contract to provide labor and materials for minor home repairs through the People Helping People Program administered by the Housing/Community Services Department - Miko Trucking, Inc. - Not to exceed \$155,292 - Financing: 2015-16 Community Development Block Grant Funds (subject to appropriations)
6. Authorize a three-year service contract for maintenance, support, parts and hardware for the supervisory control and data acquisition systems at three water treatment plants and two wastewater treatment plants - CDM Smith, Inc., most advantageous proposer of three - Not to exceed \$2,397,350 - Financing: Water Utilities Current Funds (subject to annual appropriations)
7. Authorize a three-year service contract for litter maintenance services for Park and Recreation - Good Earth Corporation in the amount of \$5,504,968 and T. Smith's Lawn Service, LLC in the amount of \$2,608,751, lowest responsible bidders of four - Total not to exceed \$8,113,719 - Financing: Current Funds (subject to annual appropriations)
8. Authorize a three-year service contract for grounds maintenance services at parks, airports and other City owned properties - Good Earth Corporation in the amount of \$2,403,441 and Urban Landscaping & Irrigation in the amount of \$774,908, lowest responsible bidders of three - Total not to exceed \$3,178,349 - Financing: Current Funds (\$2,546,621) and Aviation Current Funds (\$631,728) (subject to annual appropriations)
9. Authorize a three-year service contract for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks - Stericycle Environmental Solutions, Inc. in the amount of \$819,002 and Liquid Environmental Solutions of Texas, LLC in the amount of \$39,895, lowest responsible bidders of two - Total not to exceed \$858,897 - Financing: Current Funds (\$401,591), Aviation Current Funds (\$369,879), Sanitation Current Funds (\$72,584) and Water Utilities Current Funds (\$14,843) (subject to annual appropriations)
10. Authorize the purchase of mobile/folding stage equipment and accessories for the Kay Bailey Hutchinson Convention Center Dallas - Sico America, Inc. through the National Intergovernmental Purchasing Alliance Company - Not to exceed \$1,153,260 - Financing: Convention and Event Services Capital Construction Funds
11. Authorize supplemental agreement no. 6 to increase the service contract for upgrades to the Citizen Request Management System - Motorola Solutions, Inc. - Not to exceed \$114,000, from \$1,912,853 to \$2,026,853 - Financing: Current Funds

CONSENT AGENDA (Continued)**City Attorney's Office**

12. Authorize **(1)** acceptance of the 2015-16 Substance Abuse and Mental Health Services Administration grant from the U.S. Department of Health and Human Services for expansion of the South Dallas Community Court to include the South Dallas Treatment Drug Court for the Adult Treatment Drug Court Expansion project for the period September 30, 2015 through September 29, 2016, a three-year grant with the project period from September 30, 2014 to September 29, 2017; and **(2)** execution of the grant agreement - Not to exceed \$311,290 - Financing: U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration Grant Funds
13. Authorize settlement of the condemnation lawsuit styled City of Dallas v. Pan Coastal Limited Partnership, et al., Cause No. CC-12-06225-C - Not to exceed \$34,303, increased from \$116,997 (\$115,697 plus closing costs and title expenses not to exceed \$1,300) to \$151,300 (total settlement amount) - Financing: 2003 Bond Funds
14. Authorize Supplemental Agreement No. 2 to the professional services contract with Kaplan Kirsch & Rockwell, L.L.P. for additional services on matters relating to a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field - Not to exceed \$25,000, from \$90,000 to \$115,000 - Financing: Aviation Current Funds

Housing/Community Services

15. Authorize an amendment to Resolution No. 13-2115, previously approved on December 11, 2013, to extend the completion date for an affordable housing development conditional grant in the amount of \$180,000 to Builders of Hope CDC for the construction of six single family homes in West Dallas from December 11, 2015 to December 30, 2016 - Financing: No cost consideration to the City
16. Authorize an amendment to Resolution No. 14-1488, previously approved on September 10, 2014, to extend the completion date of each agreement to December 31, 2016, for two separate loan agreements with Builders of Hope CDC for two separate phases of their Prairie Creek project - Financing: No cost consideration to the City
17. Authorize an amendment to Resolution No. 14-1633, previously approved on September 24, 2014, to extend the completion date of each agreement to December 31, 2016, for two separate loan agreements with Builders of Hope CDC for two separate phases of their West Dallas project - Financing: No cost consideration to the City

CONSENT AGENDA (Continued)

Library

18. Authorize **(1)** the acceptance of a grant from the Association for Library Service to Children (ALSC), a division of the American Library Association and funded by the Dollar General Literacy Foundation: the 2016 Building STEAM with Día mini-grant in the amount of \$2,000 for the period January 1, 2016 to May 31, 2016; **(2)** the establishment of appropriations in an amount not to exceed \$2,000; and **(3)** execution of the grant agreement - Total not to exceed \$2,000 - Financing: Association for Library Service to Children

Office of Cultural Affairs

19. Authorize a contract with the Museum of African American Life and Culture for services provided to the City through the Cultural Services Contracts Program for the period October 1, 2015 through September 30, 2016 - Not to exceed \$102,222 - Financing: Current Funds

Park & Recreation

20. Authorize an increase in the contract with DENCO Construction Specialists Corporation for structural repairs and other miscellaneous items at three recreation centers: Arlington Park Recreation Center located at 1505 Record Crossing Road; Janie C. Turner Recreation Center located at 6424 Elam Road; and Marcus Recreation Center located at 3003 Northaven Road - Not to exceed \$97,276, from \$389,600 to \$486,876 - Financing: 2003 Bond Funds (\$77,535) and 2006 Bond Funds (\$19,741)

Planning and Urban Design

21. A resolution adopting the Complete Streets Design Manual as a comprehensive policy guide for all public or private projects that impact the planning, design, construction, and operation of streets - Financing: No cost consideration to the City

Police

22. Authorize a Memorandum of Understanding between the City of Dallas and the Texas Department of Public Safety authorizing the Dallas Police Department to perform commercial motor vehicle inspections and enforcement for the period March 1, 2016 through February 28, 2017 - Financing: No cost consideration to the City
23. Authorize a Memorandum of Understanding between the City of Dallas and the Texas Department of Public Safety authorizing the Dallas Police Department to perform commercial motor vehicle weight enforcement for the period March 1, 2016 through December 31, 2017 - Financing: No cost consideration to the City

CONSENT AGENDA (Continued)**Public Works Department**

24. Authorize a contract with RBR Infrastructure & Road, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage and drive approaches for Alley Reconstruction Group 12-2037 (list attached) - Not to exceed \$1,436,079 - Financing: 2012 Bond Funds
25. Authorize a contract with Ark Contracting Services, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2041 and 12-2043 (list attached) - Not to exceed \$2,871,476 - Financing: 2012 Bond Funds (\$2,566,981), Water Utilities Capital Improvement Funds (\$290,895) and Water Utilities Capital Construction Funds (\$13,600)
26. Authorize a twenty-four month sidewalk and barrier free ramp installation paving services contract at various locations throughout the city - Vescorp Construction, LLC dba Chavez Concrete Cutting, lowest responsible bidder of four - Not to exceed \$3,501,400 - Financing: Capital Assessment Funds (\$351,000), 2003 Bond Funds (\$276,687), 2006 Bond Funds (\$1,489,973), General Obligation Commercial Paper Funds (\$869,150), 2012 Bond Funds (\$439,715) and Water Utilities Capital Construction Funds (\$74,875)
27. Authorize an increase in the contract with North Texas Contracting, Inc. for the removal of unsuitable subgrade material discovered during excavation and replacing the void with select fill material, pavement marking modifications and to provide necessary slope stabilization measures to prevent erosion along the Colorado Boulevard extension from Interstate Highway 30 eastbound service road to Westmoreland Road - Not to exceed \$218,406, from \$4,219,951 to \$4,438,357 - Financing: 2012 Bond Funds
28. Authorize Supplemental Agreement No. 1 to the contract with O'Brien Engineering, Inc. to provide engineering services for design and construction documents for a parking area adjacent to the Tom Braniff Channel Cover project at Dallas Love Field - Not to exceed \$49,874, from \$195,596 to \$245,470 - Financing: Aviation Capital Construction Funds
29. Authorize Supplemental Agreement No. 2 to the professional services contract with HNTB Corporation to provide design, construction documents and bidding services for a Runway Incursion Project identified by the Federal Aviation Administration to be part of the Runway 18-36 Conversion Project at Dallas Love Field - Not to exceed \$287,434, from \$762,421 to \$1,049,855 - Financing: Aviation Capital Construction Funds

CONSENT AGENDA (Continued)**Sustainable Development and Construction**

30. Authorize the quitclaim of 120 properties acquired by the taxing authorities from the Tax Foreclosure Sheriff's Sale to the highest bidders; and authorize the execution of release of liens for any non-tax liens that may have been filed by the City and were included in the foreclosure judgment (list attached) - Revenue: \$798,707
31. A resolution consenting to the sale of 7 tax foreclosed properties by Dallas County, acquired by the taxing authorities from a Sheriff's Sale (list attached) - Financing: No cost consideration to the City
32. A resolution authorizing acceptance of the only bid received from SLF III - The Canyon in Oak Cliff, L.P. for approximately 12.993 acres of land located near the intersection of Pinnacle Park Boulevard and Falls Bluff Drive in exchange for approximately 23.78 acres of unwanted and unneeded City-owned land located near the intersection of Pinnacle Park Boulevard and Pinnacle Point Drive - Revenue: \$7,500
33. A resolution authorizing the conveyance of a tract of City-owned land and a drainage easement containing a total of approximately 5,393 square feet to the State of Texas located near the intersection of Interstate Highway 20 and Haymarket Road - Revenue: \$6,463
34. A resolution authorizing the conveyance of a tract of City-owned land and a drainage easement containing a total of approximately 4,624 square feet to the State of Texas located near the intersection of Interstate Highway 20 and Haymarket Road - Revenue: \$3,730
35. An ordinance granting a private license to DD Dunhill, LLC and DE Design Borrower LLC for the use of approximately 4,350 square feet to install, maintain and use a statue, electrical conduit and wiring, lighting, sensors, paving and landscaping on a portion of Hi Line Drive right-of-way, near its intersection with Oak Lawn Avenue - Revenue: \$2,000 one-time fee, plus the \$20 ordinance publication fee
36. An ordinance abandoning a portion of Levee Street to AR Apartments, LLC, the abutting owner, containing approximately 2,052 square feet of land, located near the intersection of Turtle Creek Boulevard and Levee Street, and authorizing the quitclaim - Revenue: \$65,664, plus the \$20 ordinance publication fee
37. An ordinance abandoning two sight easements to Fairfield Sadler LLC, the abutting owner, containing a total of approximately 226 square feet of land, located near the intersection of Inwood Road and Denton Drive, and authorizing the quitclaim - Revenue: \$5,400, plus the \$20 ordinance publication fee

ITEMS FOR INDIVIDUAL CONSIDERATION

City Secretary's Office

38. Consideration of appointments to boards and commissions and the evaluation and duties of board and commission members (List of nominees is available in the City Secretary's Office)

Sustainable Development and Construction

39. Authorize acquisition, including the exercise of the right of eminent domain, if such becomes necessary, from Jaime Saucedah and Beth Saucedah, of approximately 13,650 square feet of land located near the intersection of Packard and Cadillac Streets for the Cadillac Heights Project Phase I - Not to exceed \$55,000 (\$50,000, plus closing costs and title expenses not to exceed \$5,000) - Financing: 2006 Bond Funds

ITEMS FOR FUTHER CONSIDERATION

Planning and Urban Design

40. Authorize a professional services contract with MIG, Inc. for the preparation of an update to the City's Downtown Dallas 360 Plan adopted by City Council on April 13, 2011 - Not to exceed \$250,000 - Financing: Downtown Connection TIF District Funds

Public Works Department

41. An ordinance amending Chapter 43, Article VIII of the Dallas City Code, relating to improvements in public right-of-way by **(1)** amending registration requirements; **(2)** modifying the permit application requirements; **(3)** amending trench and backfill requirements; **(4)** providing additional signage requirements for vehicles and equipment in the right-of-way; **(5)** providing additional notification requirements; and **(6)** making conforming changes - Financing: No cost consideration to the City

PUBLIC HEARINGS AND RELATED ACTIONS

Sustainable Development and Construction

ZONING CASES - CONSENT

42. A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for a Community service center, Child-care facility, and NO(A) Neighborhood Office District Uses on property zoned an NO(A) Neighborhood Office District and an MF-2(A) Multifamily District, at the southeast corner of Lombardy Lane and Geraldine Drive
Recommendation of Staff and CPC: Approval, subject to a development plan, landscape plan, and conditions
Z145-335(RB)

PUBLIC HEARINGS AND RELATED ACTIONS (Continued)**Sustainable Development and Construction (Continued)**ZONING CASES - CONSENT (Continued)

43. A public hearing to receive comments regarding an application for and an ordinance granting a D-1 Liquor Control Overlay and an ordinance granting a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the southeast corner of Royal Lane and North Stemmons Freeway
Recommendation of Staff and CPC: Approval of a D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and conditions
Z145-344(OTH)
44. A public hearing to receive comments regarding an application for and an ordinance granting an IM Industrial Manufacturing District on property zoned an IR Industrial Research District, on the north line of X Street, west of Tantor Road
Recommendation of Staff and CPC: Approval
Z145-365(OTH)
45. A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for an Open-enrollment charter school on property zoned an IR Industrial/Research District at the northwest corner of Dresser Way and South Walton Walker Boulevard
Recommendation of Staff and CPC: Approval for a five-year period with eligibility for automatic renewals for additional five-year periods, subject to a site/landscape plan, traffic management plan and conditions
Z156-106(RB)
46. A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for IR Industrial/Research District and a library, art gallery or museum uses on property zoned an IR Industrial/Research District on the southeast corner of Royal Lane and Luna Road
Recommendation of Staff and CPC: Approval, subject to a development plan and conditions
Z156-108(WE)
47. A public hearing to receive comments regarding an application for and an ordinance granting an NS(A) Neighborhood Service District and a resolution accepting deed restrictions volunteered by the applicant on property zoned an NO(A) Neighborhood Office District, on the southwest side of North Peak Street, southeast of Gaston Avenue
Recommendation of Staff: Approval
Recommendation of CPC: Approval with deed restrictions volunteered by the applicant
Z156-111(OTH)

PUBLIC HEARINGS AND RELATED ACTIONS (Continued)**Sustainable Development and Construction (Continued)**ZONING CASES - CONSENT (Continued)

48. A public hearing to receive comments regarding an application for and an ordinance granting an amendment and an expansion of Planned Development District No. 805 on property zoned Planned Development District No. 805 with a D-1 Liquor Control Overlay and a CR Community Retail District with a D Liquor Control Overlay, generally on the northeast corner of Lake June Road and North Masters Drive
Recommendation of Staff and CPC: Approval, subject to a revised development plan and conditions
Z156-113(SM)
49. A public hearing to receive comments regarding an application for and an ordinance granting a D-1 Liquor Control Overlay and an ordinance granting a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the south side of Samuell Boulevard, east of North Jim Miller Road
Recommendation of Staff: Approval of a D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and conditions
Recommendation of CPC: Approval of a D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period, subject to a site plan and conditions
Z156-114(OTH)

ZONING CASES - INDIVIDUAL

50. A public hearing to receive comments regarding an application for a Planned Development District for certain CS Commercial Service District Uses, a Specific Use Permit for a Mini-warehouse, and termination of deed restrictions on property zoned a CS Commercial Service District on the north line of East University Boulevard, east of North Central Expressway
Recommendation of Staff: Denial
Recommendation of CPC: Denial without prejudice
Z134-319(RB)
51. A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for a flea market on property zoned Subdistrict 2 of Planned Development District No. 357, the Farmers Market Special Purpose District on the south corner of St. Louis Street and South Harwood Street
Recommendation of Staff: Approval for a three-year period with eligibility for automatic renewals for additional three-year periods, subject to a site plan and conditions
Recommendation of CPC: Approval for a ten-year period with eligibility for automatic renewals for additional ten-year periods, subject to a site plan and conditions
Z145-361(WE)

PUBLIC HEARINGS AND RELATED ACTIONS (Continued)**Sustainable Development and Construction (Continued)**ZONING CASES UNDER ADVISEMENT - INDIVIDUAL

52. A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for an open-enrollment charter school on property zoned an RR Regional Retail District, north of West Camp Wisdom Road and west of Interstate 35E Freeway
Recommendation of Staff and CPC: Approval for a five-year period with eligibility for automatic renewals for additional ten-year periods, subject to a site plan, traffic management plan and conditions
Z145-269(AF)
Note: This item was considered by the City Council at a public hearing on January 13, 2016, and was held under advisement until January 27, 2016, with the public hearing open
53. A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for an Open-enrollment charter school and CR Community Retail District Uses on property zoned an MF-2(A) Multifamily District and a CR Community Retail District with the D-1 Liquor Control Overlay, on the east line of St. Augustine Drive, north of Bruton Road
Recommendation of Staff and CPC: Approval, subject to a development plan, traffic management plan and conditions
Z156-104(RB)
Note: This item was considered by the City Council at a public hearing on January 13, 2016, and was deferred until January 27, 2016

THOROUGHFARE PLAN AMENDMENTS

Planning and Urban Design

54. A public hearing to receive comments to amend the City of Dallas Thoroughfare Plan to change the dimensional classification of Akard Street from IH-30 to Corinth Street from a standard four-lane undivided (S-4-U) roadway within 60-feet of right-of-way to a special two-lane undivided (SPCL 2U) roadway with bicycle facilities within 60-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City
55. A public hearing to receive comments to amend the City of Dallas Thoroughfare Plan to change the dimensional classification of Camp Wisdom Road from FM 1382 to the Grand Prairie City Limits from a standard six-lane divided (S-6-D) roadway within 107-feet of right-of-way to a special four-lane undivided (SPCL 4U) roadway with bicycle facilities within 100-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

PUBLIC HEARINGS AND RELATED ACTIONS (Continued)

MISCELLANEOUS HEARINGS

Planning and Urban Design

56. A public hearing to receive comments to amend the City of Dallas Central Business District Streets and Vehicular Circulation Plan to change the proposed rights-of-way on **(1)** Record Street from Wood Street to Young Street from 80-feet to 64-feet of right-of-way; and **(2)** Market Street from Wood Street to Young Street from 80-feet to 67-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

Alley Reconstruction Group 12-2037
Agenda Item # 24

<u>Alley Reconstruction Group 12-2037</u>	<u>Council District</u>
Alley between Amberwood Road (16101-16239) and Wickerwood Drive (6401-6535) from Shadybank Drive to Shadybank Drive	12
Alley between Caulfield Drive (6701-6805) and Spanky Branch Court (6904-7000) from Brushfield Drive to Caulfield Drive	12
Alley between La Cabeza Drive (7707-7765) and Scotia Drive (7702-7718) from Salado Drive to Spring Creek Road	12
Alley between La Manga Drive (7707-7765) and La Cabeza Drive (7708-7764) from Salado Drive to Spring Creek Road	12
Alley between Rustic Valley Drive (7238-7420) and Echo Bluff Drive (7104-7132) from Hillcrest Road to Meandering Way	12
Alley between Timber Creek Lane (6001-6037) and Gentle Knoll Lane (6002-6030) from Rustic Meadows Drive to Keller Springs Road	12

Alley Reconstruction Groups 12-2041 and 12-2043
Agenda Item # 25

<u>Alley Reconstruction Group 12-2041</u>	<u>Council District</u>
Alley between Cedar Bend Drive (12317-12417) and LBJ Freeway from Marsh Lane to Cedar Bend Drive	13
Alley between Cedarbrush Drive (3809-3951) and Goodfellow Drive (3810-3948) from Cox Lane to Rosser Road	13
Alley between Coral Gables Drive (3608-3660) and Pallos Verdas Drive (3627-3675) from Pallos Verdas Drive to Coral Gables Drive	13
Alley between Cromwell Drive (12112-12132) and High Mesa Drive (3403-3416) from High Meadow Drive and High Bluff Drive to High Mesa Drive	6
Alley between Deep Valley Drive (3920-3890) and Clear Cove Lane (3911-3951) from Rosser Road to Rosser Road	13
<u>Alley Reconstruction Group 12-2043</u>	
Alley behind Beauty Lane (10804-11030) from Royal Lane to Townsend Drive	13
Alley between Catamore Lane (3117-3185) and Flowerdale Lane (3112-3162) from alley between Channel Drive and Dundee Drive to Dundee Drive	13
Alley between Chapel Downs Drive (3108-3256) and Timberview Road (3111-3253) from Dale Crest Drive to Chapel Downs Drive	13

Tax Foreclosed and Seizure Warrant Property Resales**Agenda Item # 30**

Parcel No.	Address	Vac/ Imp	Council District	Zoning	Parcel Size	Struck off Amount	# Bids Rec'd	Min Bid Amount	Highest Bid Amount	DCAD	Highest Bidder
1	2606 52nd	I	4	R-7.5(A)	.1721	\$18,961.00	3	\$8,000.00	\$9,999.99	\$146,190.00	Anthony Boyd
2	1706 Alaska	V	4	R-7.5(A)	.1790	\$13,000.00	9	\$1,000.00	\$10,000.00	\$13,000.00	Camden Homes, LLC
3	3835 Atlanta	I	7	PD-595	.0851	\$19,538.00	7	\$5,000.00	\$13,000.00	\$26,780.00	Demond Thomas
4	506 Avenue	V	4	D(A)	.1261	\$9,500.00	2	\$500.00	\$1,451.00	\$9,500.00	Rolando Cobos
5	3302 Beauchamp	V	4	R-5(A)	.2517	\$11,500.00	1	\$1,000.00	\$1,350.00	\$11,500.00	Nextlots Now L.L.C.
6	3042 S. Beckley	V	4	R-7.5(A)	.1640	\$13,500.00	1	\$1,000.00	\$10,000.00	\$13,500.00	Camden Homes, LLC
7	3614 Bertrand	V	7	PD-595	.1492	\$1,724.00	1	\$500.00	\$608.00	\$3,250.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
8	4431 Bonnie View	V	4	R-5(A)	.3523	\$5,920.00	1	\$250.00	\$795.00	\$6,000.00	Nextlots Now L.L.C.
9	3508 Booker	V	7	PD-595	.1030	\$17,310.00	1	\$250.00	\$449.00	\$6,000.00	Anthony Boyd
10	2702 Brigham	V	7	PD-595	.1180	\$1,940.00	2	\$250.00	\$901.00	\$4,000.00	DaPorscha Kelley
11	15319 Budeudy	V	8	MH-(A)	.0601	\$5,000.00	6	\$500.00	\$8,475.00	\$4,500.00	Nextlots Now L.L.C.
12	1210 Cabot	V	8	R-7.5(A)	.3233	\$20,226.00	2	\$1,000.00	\$4,275.00	\$17,500.00	Nextlots Now L.L.C.
13	1613 Caldwell	V	7	D(A)	.0876	\$3,820.00	2	\$500.00	\$2,175.00	\$3,820.00	Nextlots Now L.L.C.
14	3837 Canada	V	6	R-5(A)	.2509	\$12,000.00	10	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
15	1527 Carson	V	4	R-7.5(A)	.1592	\$1,535.00	2	\$1,000.00	\$2,161.80	\$11,500.00	Luis Ramirez
16	5815 Cary	V	7	R-7.5(A)	.1380	\$15,000.00	4	\$1,000.00	\$5,660.99	\$15,000.00	Abraham Galdian
17	315 E. Cherry Point	V	3	R-7.5(A)	.4713	\$3,500.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
18	307 E. Cherry Point	V	3	R-7.5(A)	.4589	\$3,500.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
19	339 E. Cherry Point	V	3	R-7.5(A)	.4572	\$3,500.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
20	323 E. Cherry Point	V	3	R-7.5(A)	.4564	\$4,000.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
21	6404 Cinnamon Oaks	V	8	R-5(A)	.1640	\$11,334.00	1	\$1,000.00	\$1,508.00	\$8,500.00	Devan Earle
22	1403 E. Clarendon	V	4	PD-388	.0910	\$4,000.00	2	\$500.00	\$562.00	\$4,000.00	William Baker
23	3513 Cleveland	V	7	PD-595	.1917	\$12,530.00	1	\$1,000.00	\$2,475.00	\$12,530.00	Nextlots Now L.L.C.
24	3600 Colonial	V	7	PD-595	.1756	\$6,890.00	3	\$1,000.00	\$2,851.00	\$6,890.00	Rolando Cobos
25	5003 Colonial	V	7	PD-595	.0990	\$5,470.00	1	\$250.00	\$899.00	\$3,570.00	Devan Earle
26	2046 Cool Mist	V	8	R-7.5(A)	.1706	\$15,750.00	2	\$1,000.00	\$1,788.00	\$8,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
27	3943 Coolidge	V	7	PD-595	.1046	\$1,662.00	1	\$500.00	\$608.00	\$5,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
28	4006 Coolidge	V	7	PD-595	.0932	\$1,283.00	1	\$500.00	\$1,088.00	\$5,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
29	3918 Coolidge	V	7	PD-595	.1072	\$1,034.00	1	\$500.00	\$788.00	\$5,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
30	1104 Coston	V	5	R-7.5(A)	.1675	\$15,000.00	3	\$1,000.00	\$7,110.00	\$15,000.00	David Menn
31	4419 Cranfill	V	4	R-7.5(A)	.4685	\$12,000.00	3	\$1,000.00	\$6,510.00	\$12,000.00	David Menn
32	2238 Dathe	V	7	PD-595	.1459	\$1,646.00	1	\$500.00	\$1,288.00	\$6,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick

Tax Foreclosed and Seizure Warrant Property Resales
Agenda Item # 30 (Continued)

<u>Parcel No.</u>	<u>Address</u>	<u>Vac/ Imp</u>	<u>Council District</u>	<u>Zoning</u>	<u>Parcel Size</u>	<u>Struck off Amount</u>	<u># Bids Rec'd</u>	<u>Min Bid Amount</u>	<u>Highest Bid Amount</u>	<u>DCAD</u>	<u>Highest Bidder</u>
33	2403 Dathe	V	7	R-5(A)	.1171	\$2,930.00	1	\$250.00	\$688.00	\$2,930.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
34	4019 S. Denley	V	4	R-7.5(A)	.7244	\$15,750.00	1	\$1,000.00	\$3,361.00	\$15,750.00	Anthony Boyd
35	2194 Echo Lake	V	8	R-7.5(A)	.1648	\$8,000.00	1	\$500.00	\$1,000.00	\$8,000.00	JCRB Bucker Realty, L.P.
36	2111 Edd	V	8	R-7.5(A)	.3473	\$12,000.00	6	\$1,000.00	\$4,795.00	\$12,000.00	Nextlots Now L.L.C.
37	4105 Elk Horn	V	4	R-7.5(A)	.2080	\$12,000.00	5	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
38	659 Ella	V	5	R-7.5(A)	.1646	\$15,000.00	4	\$1,000.00	\$3,575.00	\$15,000.00	Nextlots Now L.L.C.
39	231 Elmore	V	4	R-7.5(A)	.1335	\$15,000.00	10	\$1,000.00	\$10,000.00	\$15,000.00	Camden Homes, LLC
40	4107 Esmalda	V	6	R-5(A)	.1434	\$10,000.00	8	\$1,000.00	\$8,100.00	\$12,000.00	J. Santos Coria
41	2131 S. Ewing	V	4	R-7.5(A)	.1940	\$13,000.00	5	\$1,000.00	\$7,510.00	\$12,500.00	David Menn
42	2810 Farragut	V	7	PD-595	.0811	\$2,019.00	1	\$500.00	\$901.00	\$3,200.00	DaPorscha Kelley
43	3217 Fordham	V	4	R-7.5(A)	.3812	\$10,000.00	3	\$1,000.00	\$4,575.00	\$10,000.00	Nextlots Now L.L.C.
44	1726 Fordham	V	4	R-7.5(A)	.1382	\$9,000.00	1	\$1,000.00	\$1,099.00	\$9,000.00	Anthony Boyd
45	2910 Gay	V	7	PD-595	.0785	\$9,750.00	1	\$500.00	\$1,000.00	\$5,000.00	Benita Beltran
46	3206 Goldspier	V	7	PD-595	.1721	\$1,540.00	1	\$500.00	\$1,499.00	\$6,000.00	Devan Earle
47	3201 S. Haskell	V	7	PD-305	.1358	\$17,448.00	4	\$3,000.00	\$8,200.00	\$17,750.00	Benito Mojica
48	4410 Helen	V	7	CS	.1080	\$4,710.00	1	\$500.00	\$2,050.00	\$4,710.00	Nextlots Now L.L.C.
49	3623 Humphrey	V	4	R-5(A)	.3298	\$3,147.00	1	\$1,000.00	\$1,250.00	\$11,500.00	Nextlots Now L.L.C.
50	1230 Hutchins	V	4	R-7.5(A)	.1236	\$9,500.00	1	\$1,000.00	\$10,000.00	\$9,500.00	Camden Homes, LLC
51	3114 Indianola	V	7	R-7.5(A)	.2321	\$9,318.00	4	\$1,000.00	\$6,600.00	\$15,000.00	Abraham Galdian
52	2434 Ingersoll	V	6	R-5(A)	.1721	\$12,000.00	9	\$1,000.00	\$12,500.00	\$12,000.00	Omar Correa
53	704 Jeran	V	5	R-7.5(A)	.1919	\$12,500.00	3	\$1,000.00	\$7,110.00	\$12,500.00	David Menn
54	3050 Kinkaid	V	6	R-7.5(A)	.1757	\$29,565.00	7	\$4,000.00	\$41,900.00	\$33,750.00	Aubrey Quarles
55	8322 Lake Anna	V	8	R-7.5(A)	.2367	\$15,500.00	3	\$1,000.00	\$3,875.00	\$17,500.00	Nextlots Now L.L.C.
56	4506 Leland	V	7	PD-595	.1148	\$1,944.00	1	\$250.00	\$308.00	\$6,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
57	2262 Lolita	V	5	R-7.5(A)	.4224	\$15,000.00	6	\$1,000.00	\$8,375.00	\$15,000.00	Nextlots Now L.L.C.
58	1311 Lotus	V	4	LI	.0681	\$4,460.00	1	\$500.00	\$1,050.00	\$4,460.00	Rolando Cobos
59	1222 E. Louisiana	I	4	R-7.5(A)	.1973	\$31,240.00	22	\$5,000.00	\$30,000.00	\$31,240.00	Edgar Milton Pineda
60	2211 Marburg	V	7	PD-595	.1699	\$9,996.00	1	\$1,250.00	\$2,345.00	\$6,000.00	County Land & Water LLC
61	2627 Marburg	V	7	PD-595	.1723	\$6,000.00	1	\$500.00	\$600.00	\$6,000.00	Constance Armstrong
62	2823 S. Marsalis	V	4	R-7.5(A)	.1881	\$13,500.00	3	\$1,000.00	\$8,100.00	\$13,500.00	King Home Builders, LLC
63	2923 S. Marsalis	V	4	R-7.5(A)	.1886	\$13,500.00	4	\$1,000.00	\$8,100.00	\$13,500.00	King Home Builders, LLC
64	3723 Mcbroom	V	6	R-5(A)	.1194	\$2,500.00	7	\$250.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
65	4921 Meadow View	V	7	CA-1(A)	.1120	\$1,221.00	4	\$250.00	\$1,005.62	\$2,440.00	Nicholas A. Barnett Sr.
66	2515 Meyers	V	7	PD-363	.1721	\$3,750.00	2	\$500.00	\$800.00	\$3,750.00	Jose Alfredo Ramirez
67	11135 Midway	I	13	R-16(A)	.3673	\$311,580.00	17	\$40,000.00	\$231,100.00	\$367,890.00	SDE Texas LLC
68	3539 Mingo	V	7	R-7.5(A)	.0771	\$4,200.00	1	\$500.00	\$2,475.00	\$4,200.00	Nextlots Now L.L.C.
69	2724 Mojave	V	8	R-7.5(A)	.4617	\$18,500.00	5	\$1,000.00	\$7,250.00	\$18,500.00	Nextlots Now L.L.C.
70	1522 Montague	V	4	R-7.5(A)	.1731	\$6,000.00	2	\$1,000.00	\$1,475.00	\$6,000.00	Nextlots Now L.L.C.
71	3107 Morgan	V	8	R-7.5(A)	.3091	\$17,550.00	1	\$1,000.00	\$3,675.00	\$9,000.00	Nextlots Now L.L.C.
72	3602 Morris	V	6	R-5(A)	.1579	\$2,130.00	9	\$500.00	\$10,000.00	\$10,200.00	Camden Homes, LLC
73	1106 Muncie	V	6	TH-3(A)	.1147	\$11,695.00	10	\$1,000.00	\$10,600.00	\$8,000.00	William Baker
74	609 Murdock	V	8	R-7.5(A)	.1675	\$14,866.00	4	\$1,000.00	\$10,000.00	\$17,500.00	Camden Homes, LLC
75	605 Murdock	V	8	R-7.5(A)	.1649	\$11,314.00	5	\$1,000.00	\$10,000.00	\$17,500.00	Camden Homes, LLC
76	1624 Murdock	V	8	R-7.5(A)	.3046	\$17,500.00	2	\$1,000.00	\$1,775.00	\$17,500.00	Nextlots Now L.L.C.
77	3411 Myrtle	V	7	PD-595	.1615	\$12,489.00	1	\$250.00	\$703.00	\$6,000.00	Friew Zerihun
78	1334 Oakley	V	4	R-7.5(A)	.1754	\$10,000.00	2	\$1,000.00	\$1,875.00	\$10,000.00	Nextlots Now L.L.C.
79	3431 Odessa	V	6	R-5(A)	.1917	\$12,000.00	6	\$1,000.00	\$13,050.00	\$12,000.00	Albert Almanza
80	4507 N. Ottawa	V	6	R-5(A)	.1511	\$12,000.00	5	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC

Tax Foreclosed and Seizure Warrant Property Resales
Agenda Item # 30 (Continued)

<u>Parcel No.</u>	<u>Address</u>	<u>Vac/ Imp</u>	<u>Council District</u>	<u>Zoning</u>	<u>Parcel Size</u>	<u>Struck off Amount</u>	<u># Bids Rec'd</u>	<u>Min Bid Amount</u>	<u>Highest Bid Amount</u>	<u>DCAD</u>	<u>Highest Bidder</u>
81	1446 Owega	V	4	R-7.5(A)	.1763	\$6,000.00	2	\$1,000.00	\$3,175.00	\$6,000.00	Nextlots Now L.L.C.
82	1409 Park Row	V	7	PD-314	.0918	\$6,000.00	3	\$1,000.00	\$2,810.00	\$10,000.00	William Baker
83	2504 Peabody	V	7	PD-595	.1377	\$6,000.00	2	\$1,000.00	\$2,050.00	\$6,000.00	Kimiaki Itamura
84	2724 Peabody	V	7	PD-595	.1442	\$18,296.00	2	\$500.00	\$2,801.00	\$33,790.00	Lilia Jimenez
85	2603 Pine	V	7	PD-595	.1128	\$12,770.00	1	\$500.00	\$1,300.00	\$6,000.00	Kimiaki Itamura
86	3319 Pine	V	7	PD-595	.1779	\$6,000.00	1	\$1,000.00	\$2,004.00	\$6,000.00	Temesgen Wukaye
87	1725 Pine	V	7	PD-595	.1652	\$6,480.00	4	\$1,000.00	\$3,201.00	\$6,480.00	Lilia Jimenez
88	2616 Pine	V	7	R-5(A)	.1530	\$21,250.00	2	\$250.00	\$500.00	\$6,000.00	Benita Beltran
89	4483 N. Polk	V	3	R-7.5(A)	3.7290	\$115,200.00	2	\$5,000.00	\$35,500.00	\$129,960.00	Maria Ospina
90	3915 Polly	V	7	PD-595	.1007	\$10,993.00	1	\$250.00	\$308.00	\$2,190.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
91	3220 Reed	V	7	PD-595	.1916	\$1,944.00	2	\$500.00	\$1,002.00	\$6,000.00	Keith Marshall
92	3335 Reed	V	7	PD-595	.1721	\$1,665.00	2	\$500.00	\$2,560.00	\$6,000.00	Temesgen Wukaye
93	3228 Reed	V	7	PD-595	.2721	\$1,667.00	3	\$500.00	\$2,501.00	\$6,000.00	Temesgen Wukaye
94	3600 Reese	V	7	PD-595	.1730	\$1,692.00	1	\$250.00	\$308.00	\$3,770.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
95	1126 Ridgewood	V	5	R-7.5(A)	.1823	\$51,705.00	3	\$1,000.00	\$4,375.00	\$12,000.00	Nextlots Now L.L.C.
96	2637 Ripple	V	8	R-7.5(A)	.1414	\$4,000.00	1	\$500.00	\$849.00	\$4,000.00	Anthony Boyd
97	3310 Rutledge	V	7	PD-595	.1721	\$5,630.00	2	\$250.00	\$400.00	\$5,630.00	Saul Vargas
98	3054 Seever	V	4	R-7.5(A)	.1688	\$15,000.00	10	\$1,000.00	\$10,000.00	\$15,000.00	Camden Homes, LLC
99	2518 Seever	V	4	R-7.5(A)	.1672	\$14,000.00	8	\$1,000.00	\$10,000.00	\$14,000.00	Camden Homes, LLC
100	1703 Smoke Tree	V	8	PD-258	.1871	\$9,000.00	4	\$1,000.00	\$2,575.00	\$9,000.00	Nextlots Now L.L.C.
101	1526 Southernland	V	4	R-7.5(A)	.1080	\$1,846.00	3	\$500.00	\$3,195.00	\$11,900.00	Nextlots Now L.L.C.
102	3022 Southland	V	7	PD-595	.1399	\$14,758.00	1	\$500.00	\$600.00	\$6,000.00	Saul Vargas
103	3835 Spence	V	7	PD-595	.1377	\$1,611.00	2	\$500.00	\$600.00	\$5,400.00	Constance Armstrong
104	3809 Spence	V	7	PD-595	.1377	\$1,572.00	2	\$500.00	\$508.00	\$5,400.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
105	4304 Spring	V	7	PD-595	.1678	\$1,617.00	2	\$500.00	\$1,800.00	\$5,000.00	Benita Beltran
106	3240 Springview	V	4	R-7.5(A)	.2503	\$29,670.00	1	\$500.00	\$576.00	\$10,000.00	Sabrina Sutton
107	1406 Stella	V	4	R-7.5(A)	.1743	\$11,500.00	3	\$1,000.00	\$4,110.00	\$11,500.00	Yesenia Villela
108	2521 Stephenson	V	7	PD-595	.1607	\$6,310.00	1	\$500.00	\$600.00	\$6,260.00	Saul Vargas
109	1444 Stirling	V	4	TH-3(A)	.5498	\$2,838.00	1	\$1,500.00	\$4,219.00	\$17,250.00	Devan Earle
110	2802 Swanson	V	7	PD-595	.7940	\$10,414.00	1	\$500.00	\$1,000.00	\$3,750.00	County Land & Water LLC
111	3418 Toronto	V	6	R-5(A)	.1370	\$12,000.00	14	\$1,000.00	\$7,100.00	\$12,000.00	J. Santos Coria
112	3540 Toronto	V	6	R-5(A)	.1421	\$24,630.00	7	\$1,000.00	\$7,600.00	\$10,200.00	J. Santos Coria
113	6214 Tracy	V	8	R-5(A)	.1434	\$8,000.00	1	\$250.00	\$651.00	\$8,000.00	Anthony Boyd
114	9627 Travis	V	8	A(A)	.1780	\$8,481.00	1	\$1,000.00	\$1,475.00	\$12,680.00	Nextlots Now L.L.C.
115	1110 S. Tyler	V	4	R-7.5(A)	.1807	\$19,111.00	3	\$2,000.00	\$7,150.00	\$20,000.00	Juan Roberto Leon
116	3007 Urban	V	5	R-7.5(A)	.2978	\$65,870.00	2	\$1,000.00	\$3,001.99	\$12,000.00	Abraham Galdian
117	9652 Valley Mills	V	7	PD-415	.0891	\$13,000.00	3	\$1,000.00	\$2,804.00	\$13,000.00	Devan Earle
118	3922 Vineyard	V	6	R-5(A)	.1588	\$12,000.00	6	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
119	3317 Wendelkin	V	7	PD-595	.1056	\$4,500.00	2	\$500.00	\$606.00	\$4,500.00	County Land & Water LLC
120	1627 E. Woodin	V	4	R-7.5(A)	.1684	\$11,500.00	1	\$1,000.00	\$2,709.00	\$11,500.00	Devan Earle

**Tax Foreclosure Properties Struck Off to Wilmer Hutchins I.S.D.
Dallas County as Trustee for City of Dallas and Dallas I.S.D.
Agenda Item # 31**

<u>Street Address</u>	<u>Cause#/Judgment Date</u>	<u>Improved or Unimproved</u>	<u>Tax Years Included in Judgment County/City/School</u>	<u>Date of Sheriff's Sale</u>
7615 S. Central Expy, Dallas, Texas	TX-92-40409-TK consolidated with TX-95-30537-TD 10/28/99 (Tr. 2)	Unimproved	WHISD: 1990-1998 County: 1990-1998 City: 1990-1998	7/1/03
7623 S. Central Expy, Dallas, Texas	TX-92-40409-TK consolidated with TX-95-30537-TD 10/28/99 (Tr. 1)	Unimproved	WHISD: 1990-1998 County: 1990-1998 City: 1990-1998	7/1/03
4836 Fellows Ln., Dallas, Texas	TX-91-40715 8/6/94	Unimproved	WHISD: 1984-1995 County: 1982-1995 City: 1984-1995	10/5/04
7715 Hull Ave., Dallas, Texas	TX-90-40973 6/27/91	Unimproved	WHISD: 1982-1990 County: 1982-1990 City: 1980-1990	1/4/05
4234 Memory Ln., Dallas, Texas	TX-98-41126-T-G 10/9/03	Unimproved	WHISD: 1987-2002 County: 1987-2002 City: 1987-2002	6/1/04
4020 Silverhill Dr., Dallas, Texas	TX-88-40708-T/E 2/6/90	Unimproved	WHISD: 1982-1989 County: 1984-1989 City: 1982-1989	10/5/04
3623 Softcloud Dr., Dallas, Texas	TX-94-40485-T/C 7/14/97	Unimproved	WHISD: 1990-1995 County: 1988-1995 City: 1986-1987 1989-1995	6/1/04

A closed executive session may be held if the discussion of any of the above agenda items concerns one of the following:

1. Contemplated or pending litigation, or matters where legal advice is requested of the City Attorney. Section 551.071 of the Texas Open Meetings Act.
2. The purchase, exchange, lease or value of real property, if the deliberation in an open meeting would have a detrimental effect on the position of the City in negotiations with a third person. Section 551.072 of the Texas Open Meetings Act.
3. A contract for a prospective gift or donation to the City, if the deliberation in an open meeting would have a detrimental effect on the position of the City in negotiations with a third person. Section 551.073 of the Texas Open Meetings Act.
4. Personnel matters involving the appointment, employment, evaluation, reassignment, duties, discipline or dismissal of a public officer or employee or to hear a complaint against an officer or employee. Section 551.074 of the Texas Open Meetings Act.
5. The deployment, or specific occasions for implementation of security personnel or devices. Section 551.076 of the Texas Open Meetings Act.
6. Deliberations regarding Economic Development negotiations. Section 551.087 of the Texas Open Meetings Act.

AGENDA DATE January 27, 2016

ITEM	IND								DESCRIPTION
#	OK	DEF	DISTRICT	TYPE	DEPT.	DOLLARS	LOCAL	MWBE	
1			All	V	NA	NA	NA	NA	Approval of Minutes of the January 13, 2016 City Council Meeting
2			2	C	AVI	\$1,580,640.00	NA	NA	Authorize a five-year lease agreement with two five-year renewal options with Viceroy Regal, LP for approximately 2.808 acres of land located at 2225 Burbank Street and 2311 Burbank Street to be used for Dallas Love Field airport operations - Not to exceed \$1,580,640 - Financing: Aviation Current Funds (\$316,128 estimated annually; not to exceed \$1,580,640 over primary term of lease) (subject to annual appropriations)
3			3	C	AVI	\$374,500.00	NA	NA	Authorize an early Termination Agreement with Akridge Aircraft Interiors, Inc. regarding its lease at Dallas Executive Airport, for approximately 107,523 square feet; and pay a final termination payment equal to \$374,500 - Not to exceed \$374,500 - Financing: Aviation Current Funds
4			2	C	AVI	REV \$165,405	NA	NA	Authorize a First Amendment to the consolidated ground lease with Gulfstream Aerospace Services Corporation at Dallas Love Field to increase the leasehold by approximately 5.842 acres into tract 1 of the leased premise - Financing: Estimated Increased Annual Revenue: \$165,405; Cumulative Rent: \$11,429,691
5			All	C	PBD, HOU	GT	100.00%	0.00%	Authorize the first twelve-month renewal option to the service contract to provide labor and materials for minor home repairs through the People Helping People Program administered by the Housing/Community Services Department - Miko Trucking, Inc. - Not to exceed \$155,292 - Financing: 2015-16 Community Development Block Grant Funds (subject to appropriations)
6			All	C	PBD, WTR	\$2,397,350.00	100.00%	0.00%	Authorize a three-year service contract for maintenance, support, parts and hardware for the supervisory control and data acquisition systems at three water treatment plants and two wastewater treatment plants - CDM Smith, Inc., most advantageous proposer of three - Not to exceed \$2,397,350 - Financing: Water Utilities Current Funds (subject to annual appropriations)
7			All	C	PBD, PKR	\$8,113,718.68	100.00%	23.80%	Authorize a three-year service contract for litter maintenance services for Park and Recreation – Good Earth Corporation in the amount of \$5,504,968 and T. Smith's Lawn Service, LLC in the amount of \$2,608,751, lowest responsible bidders of four - Total not to exceed \$8,113,719 - Financing: Current Funds (subject to annual appropriations)
8			All	C	PBD, AVI, EBS, PKR, PBW	\$3,178,348.56	99.06%	42.38%	Authorize a three-year service contract for grounds maintenance services at parks, airports and other City owned properties - Good Earth Corporation in the amount of \$2,403,441 and Urban Landscaping & Irrigation in the amount of \$774,908, lowest responsible bidders of three - Total not to exceed \$3,178,349 - Financing: Current Funds (\$2,546,621) and Aviation Current Funds (\$631,728) (subject to annual appropriations)
9			All	C	PBD, AVI, EBS, PKR, POL, SAN, WTR	\$858,897.00	100.00%	0.00%	Authorize a three-year service contract for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks - Stericycle Environmental Solutions, Inc. in the amount of \$819,002 and Liquid Environmental Solutions of Texas, LLC in the amount of \$39,895, lowest responsible bidders of two - Total not to exceed \$858,897 - Financing: Current Funds (\$401,591), Aviation Current Funds (\$369,879), Sanitation Current Funds (\$72,584) and Water Utilities Current Funds (\$14,843) (subject to annual appropriations)
10			2	C	PBD, CES	\$1,153,260.00	NA	NA	Authorize the purchase of mobile/folding stage equipment and accessories for the Kay Bailey Hutchinson Convention Center Dallas - Sico America, Inc. through the National Intergovernmental Purchasing Alliance Company - Not to exceed \$1,153,260 - Financing: Convention and Event Services Capital Construction Funds
11			All	C	PBD, CIS	\$114,000.00	NA	NA	Authorize supplemental agreement no. 6 to increase the service contract for upgrades to the Citizen Request Management System - Motorola Solutions, Inc. - Not to exceed \$114,000, from \$1,912,853 to \$2,026,853 - Financing: Current Funds
12			N/A	C	ATT	GT	NA	NA	Authorize (1) acceptance of the 2015-16 Substance Abuse and Mental Health Services Administration grant from the U.S. Department of Health and Human Services for expansion of the South Dallas Community Court to include the South Dallas Treatment Drug Court for the Adult Treatment Drug Court Expansion project for the period September 30, 2015 through September 29, 2016, a three-year grant with the project period from September 30, 2014 to September 29, 2017; and (2) execution of the grant agreement - Not to exceed \$311,290 - Financing: U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration Grant Funds
13			14	C	ATT, PBW	\$34,303.54	NA	NA	Authorize settlement of the condemnation lawsuit styled City of Dallas v. Pan Coastal Limited Partnership, et al., Cause No. CC-12-06225-C - Not to exceed \$34,303, increased from \$116,997 (\$115,697 plus closing costs and title expenses not to exceed \$1,300) to \$151,300 (total settlement amount) - Financing: 2003 Bond Funds

AGENDA DATE January 27, 2016

ITEM	IND								DESCRIPTION
#	OK	DEF	DISTRICT	TYPE	DEPT.	DOLLARS	LOCAL	MWBE	
14			N/A	C	ATT, AVI	\$25,000.00	0.00%	0.00%	Authorize Supplemental Agreement No. 2 to the professional services contract with Kaplan Kirsch & Rockwell, L.L.P. for additional services on matters relating to a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field - Not to exceed \$25,000, from \$90,000 to \$115,000 - Financing: Aviation Current Funds
15			6	C	HOU	NC	NA	NA	Authorize an amendment to Resolution No. 13-2115, previously approved on December 11, 2013, to extend the completion date for an affordable housing development conditional grant in the amount of \$180,000 to Builders of Hope CDC for the construction of six single family homes in West Dallas from December 11, 2015 to December 30, 2016 - Financing: No cost consideration to the City
16			7	C	HOU	NC	NA	NA	Authorize an amendment to Resolution No. 14-1488, previously approved on September 10, 2014, to extend the completion date of each agreement to December 31, 2016, for two separate loan agreements with Builders of Hope CDC for two separate phases of their Prairie Creek project - Financing: No cost consideration to the City
17			6	C	HOU	NC	NA	NA	Authorize an amendment to Resolution No. 14-1633, previously approved on September 24, 2014, to extend the completion date of each agreement to December 31, 2016, for two separate loan agreements with Builders of Hope CDC for two separate phases of their West Dallas project - Financing: No cost consideration to the City
18			All	C	LIB	GT	NA	NA	Authorize (1) the acceptance of a grant from the Association for Library Service to Children (ALSC), a division of the American Library Association and funded by the Dollar General Literacy Foundation: the 2016 Building STEAM with Dia mini-grant in the amount of \$2,000 for the period January 1, 2016 to May 31, 2016; (2) the establishment of appropriations in an amount not to exceed \$2,000; and (3) execution of the grant agreement - Total not to exceed \$2,000 - Financing: Association for Library Service to Children
19			7	C	OCA	\$102,222.00	NA	NA	Authorize a contract with the Museum of African American Life and Culture for services provided to the City through the Cultural Services Contracts Program for the period October 1, 2015 through September 30, 2016 - Not to exceed \$102,222 - Financing: Current Funds
20			2, 8, 13	C	PKR	\$97,275.80	82.10%	36.61%	Authorize an increase in the contract with DENCO Construction Specialists Corporation for structural repairs and other miscellaneous items at three recreation centers: Arlington Park Recreation Center located at 1505 Record Crossing Road; Janie C. Turner Recreation Center located at 6424 Elam Road; and Marcus Recreation Center located at 3003 Northaven Road - Not to exceed \$97,276, from \$389,600 to \$486,876 - Financing: 2003 Bond Funds (\$77,535) and 2006 Bond Funds (\$19,741)
21			All	C	PNV	NC	NA	NA	A resolution adopting the Complete Streets Design Manual as a comprehensive policy guide for all public or private projects that impact the planning, design, construction, and operation of streets - Financing: No cost consideration to the City
22			All	C	POL	NC	NA	NA	Authorize a Memorandum of Understanding between the City of Dallas and the Texas Department of Public Safety authorizing the Dallas Police Department to perform commercial motor vehicle inspections and enforcement for the period March 1, 2016 through February 28, 2017 - Financing: No cost consideration to the City
23			All	C	POL	NC	NA	NA	Authorize a Memorandum of Understanding between the City of Dallas and the Texas Department of Public Safety authorizing the Dallas Police Department to perform commercial motor vehicle weight enforcement for the period March 1, 2016 through December 31, 2017 - Financing: No cost consideration to the City
24			12	C	PBW	\$1,436,079.00	95.47%	100.00%	Authorize a contract with RBR Infrastructure & Road, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage and drive approaches for Alley Reconstruction Group 12-2037 - Not to exceed \$1,436,079 - Financing: 2012 Bond Funds
25			6, 13	C	PBW, WTR	\$2,871,476.00	24.35%	25.59%	Authorize a contract with Ark Contracting Services, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2041 and 12-2043 - Not to exceed \$2,871,476 - Financing: 2012 Bond Funds (\$2,566,981), Water Utilities Capital Improvement Funds (\$290,895) and Water Utilities Capital Construction Funds (\$13,600)
26			All	C	PBW, WTR	\$3,501,400.00	100.00%	100.00%	Authorize a twenty-four month sidewalk and barrier free ramp installation paving services contract at various locations throughout the city - Vescorp Construction, LLC dba Chavez Concrete Cutting, lowest responsible bidder of four - Not to exceed \$3,501,400 - Financing: Capital Assessment Funds (\$351,000), 2003 Bond Funds (\$276,687), 2006 Bond Funds (\$1,489,973), General Obligation Commercial Paper Funds (\$869,150), 2012 Bond Funds (\$439,715) and Water Utilities Capital Construction Funds (\$74,875)

AGENDA DATE January 27, 2016

ITEM	IND								DESCRIPTION
#	OK	DEF	DISTRICT	TYPE	DEPT.	DOLLARS	LOCAL	MWBE	
27			3	C	PBW, ECO	\$218,406.00	0.00%	24.45%	Authorize an increase in the contract with North Texas Contracting, Inc. for the removal of unsuitable subgrade material discovered during excavation and replacing the void with select fill material, pavement marking modifications and to provide necessary slope stabilization measures to prevent erosion along the Colorado Boulevard extension from Interstate Highway 30 eastbound service road to Westmoreland Road - Not to exceed \$218,406, from \$4,219,951 to \$4,438,357 - Financing: 2012 Bond Funds
28			2	C	PBW, AVI	\$49,874.00	100.00%	30.64%	Authorize Supplemental Agreement No. 1 to the contract with O'Brien Engineering, Inc. to provide engineering services for design and construction documents for a parking area adjacent to the Tom Braniff Channel Cover project at Dallas Love Field - Not to exceed \$49,874, from \$195,596 to \$245,470 - Financing: Aviation Capital Construction Funds
29			2	C	PBW, AVI	\$287,434.00	72.68%	32.18%	Authorize Supplemental Agreement No. 2 to the professional services contract with HNTB Corporation to provide design, construction documents and bidding services for a Runway Incursion Project identified by the Federal Aviation Administration to be part of the Runway 18-36 Conversion Project at Dallas Love Field - Not to exceed \$287,434, from \$762,421 to \$1,049,855 - Financing: Aviation Capital Construction Funds
30			1, 2, 3, 4, 5, 6, 7, 8, 13	C	DEV	REV \$798,706.39	NA	NA	Authorize the quitclaim of 120 properties acquired by the taxing authorities from the Tax Foreclosure Sheriff's Sale to the highest bidders; and authorize the execution of release of liens for any non-tax liens that may have been filed by the City and were included in the foreclosure judgment - Revenue: \$798,707
31			7, 8	C	DEV	NC	NA	NA	A resolution consenting to the sale of 7 tax foreclosed properties by Dallas County, acquired by the taxing authorities from a Sheriff's Sale - Financing: No cost consideration to the City
32			3	C	DEV	REV \$7,500	NA	NA	A resolution authorizing acceptance of the only bid received from SLF III - The Canyon in Oak Cliff, L.P. for approximately 12.993 acres of land located near the intersection of Pinnacle Park Boulevard and Falls Bluff Drive in exchange for approximately 23.78 acres of unwanted and unneeded City-owned land located near the intersection of Pinnacle Park Boulevard and Pinnacle Point Drive - Revenue: \$7,500
33			8	C	DEV	REV \$6,463	NA	NA	A resolution authorizing the conveyance of a tract of City-owned land and a drainage easement containing a total of approximately 5,393 square feet to the State of Texas located near the intersection of Interstate Highway 20 and Haymarket Road - Revenue: \$6,463
34			8	C	DEV	REV \$3,730	NA	NA	A resolution authorizing the conveyance of a tract of City-owned land and a drainage easement containing a total of approximately 4,624 square feet to the State of Texas located near the intersection of Interstate Highway 20 and Haymarket Road - Revenue: \$3,730
35			6	C	DEV	REV \$2,000	NA	NA	An ordinance granting a private license to DD Dunhill, LLC and DE Design Borrower LLC for the use of approximately 4,350 square feet to install, maintain and use a statue, electrical conduit and wiring, lighting, sensors, paving and landscaping on a portion of Hi Line Drive right-of-way, near its intersection with Oak Lawn Avenue - Revenue: \$2,000 one-time fee, plus the \$20 ordinance publication fee
36			6	C	DEV	REV \$65,664	NA	NA	An ordinance abandoning a portion of Levee Street to AR Apartments, LLC, the abutting owner, containing approximately 2,052 square feet of land, located near the intersection of Turtle Creek Boulevard and Levee Street, and authorizing the quitclaim - Revenue: \$65,664, plus the \$20 ordinance publication fee
37			2	C	DEV	REV \$5,400	NA	NA	An ordinance abandoning two sight easements to Fairfield Sadler LLC, the abutting owner, containing a total of approximately 226 square feet of land, located near the intersection of Inwood Road and Denton Drive, and authorizing the quitclaim - Revenue: \$5,400, plus the \$20 ordinance publication fee
38			N/A	I	SEC	NC	N/A	N/A	Consideration of appointments to boards and commissions and the evaluation and duties of board and commission members (List of nominees is available in the City Secretary's Office)
39			4	I	DEV, PBW	\$55,000.00	NA	NA	Authorize acquisition, including the exercise of the right of eminent domain, if such becomes necessary, from Jaime Saucedah and Beth Saucedah, of approximately 13,650 square feet of land located near the intersection of Packard and Cadillac Streets for the Cadillac Heights Project Phase I - Not to exceed \$55,000 (\$50,000, plus closing costs and title expenses not to exceed \$5,000) - Financing: 2006 Bond Funds
40			2, 14	I	PNV	\$250,000.00	NA	NA	Authorize a professional services contract with MIG, Inc. for the preparation of an update to the City's Downtown Dallas 360 Plan adopted by City Council on April 13, 2011 - Not to exceed \$250,000 - Financing: Downtown Connection TIF District Funds

AGENDA DATE January 27, 2016

ITEM	IND								DESCRIPTION
#	OK	DEF	DISTRICT	TYPE	DEPT.	DOLLARS	LOCAL	MWBE	
41			N/A	I	PBW	NC	NA	NA	An ordinance amending Chapter 43, Article VIII of the Dallas City Code, relating to improvements in public right-of-way by (1) amending registration requirements; (2) modifying the permit application requirements; (3) amending trench and backfill requirements; (4) providing additional signage requirements for vehicles and equipment in the right-of-way; (5) providing additional notification requirements; and (6) making conforming changes - Financing: No cost consideration to the City
42			6	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for a Community service center, Child-care facility, and NO(A) Neighborhood Office District Uses on property zoned an NO(A) Neighborhood Office District and an MF-2(A) Multifamily District, at the southeast corner of Lombardy Lane and Geraldine Drive
43			6	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a D-1 Liquor Control Overlay and an ordinance granting a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the southeast corner of Royal Lane and North Stemmons Freeway
44			6	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting an IM Industrial Manufacturing District on property zoned an IR Industrial Research District, on the north line of X Street, west of Tantor Road
45			3	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for an Open-enrollment charter school on property zoned an IR Industrial/Research District at the northwest corner of Dresser Way and South Walton Walker Boulevard
46			6	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for IR Industrial/Research District and a library, art gallery or museum uses on property zoned an IR Industrial/Research District on the southeast corner of Royal Lane and Luna Road
47			2	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting an NS(A) Neighborhood Service District and a resolution accepting deed restrictions volunteered by the applicant on property zoned an NO(A) Neighborhood Office District, on the southwest side of North Peak Street, southeast of Gaston Avenue
48			5	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting an amendment and an expansion of Planned Development District No. 805 on property zoned Planned Development District No. 805 with a D-1 Liquor Control Overlay and a CR Community Retail District with a D Liquor Control Overlay, generally on the northeast corner of Lake June Road and North Masters Drive
49			7	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a D-1 Liquor Control Overlay and an ordinance granting a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the south side of Samuell Boulevard, east of North Jim Miller Road
50			14	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for a Planned Development District for certain CS Commercial Service District Uses, a Specific Use Permit for a Mini-warehouse, and termination of deed restrictions on property zoned a CS Commercial Service District on the north line of East University Boulevard, east of North Central Expressway
51			2	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for a flea market on property zoned Subdistrict 2 of Planned Development District No. 357, the Farmers Market Special Purpose District on the south corner of St. Louis Street and South Harwood Street
52			3	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for an open-enrollment charter school on property zoned an RR Regional Retail District, north of West Camp Wisdom Road and west of Interstate 35E Freeway
53			7	PH	DEV	NC	NA	NA	A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for an Open-enrollment charter school and CR Community Retail District Uses on property zoned an MF-2(A) Multifamily District and a CR Community Retail District with the D-1 Liquor Control Overlay, on the east line of St. Augustine Drive, north of Bruton Road

AGENDA DATE January 27, 2016

ITEM	IND								DESCRIPTION
#	OK	DEF	DISTRICT	TYPE	DEPT.	DOLLARS	LOCAL	MWBE	
54			2	PH	PNV	NC	NA	NA	A public hearing to receive comments to amend the City of Dallas Thoroughfare Plan to change the dimensional classification of Akard Street from IH-30 to Corinth Street from a standard four-lane undivided (S-4-U) roadway within 60-feet of right-of-way to a special two-lane undivided (SPCL 2U) roadway with bicycle facilities within 60-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City
55			3	PH	PNV	NC	NA	NA	A public hearing to receive comments to amend the City of Dallas Thoroughfare Plan to change the dimensional classification of Camp Wisdom Road from FM 1382 to the Grand Prairie City Limits from a standard six-lane divided (S-6-D) roadway within 107-feet of right-of-way to a special four-lane undivided (SPCL 4U) roadway with bicycle facilities within 100-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City
56			2	PH	PNV	NC	NA	NA	A public hearing to receive comments to amend the City of Dallas Central Business District Streets and Vehicular Circulation Plan to change the proposed rights-of-way on (1) Record Street from Wood Street to Young Street from 80-feet to 64-feet of right-of-way; and (2) Market Street from Wood Street to Young Street from 80-feet to 67-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

TOTAL \$26,699,184.58

AGENDA ITEM # 2

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: January 27, 2016
COUNCIL DISTRICT(S): 2
DEPARTMENT: Aviation
CMO: Ryan S. Evans, 671-9837
MAPSCO: 33H

SUBJECT

Authorize a five-year lease agreement with two five-year renewal options with Viceroy Regal, LP for approximately 2.808 acres of land located at 2225 Burbank Street and 2311 Burbank Street to be used for Dallas Love Field airport operations - Not to exceed \$1,580,640 - Financing: Aviation Current Funds (\$316,128 estimated annually; not to exceed \$1,580,640 over primary term of lease) (subject to annual appropriations)

BACKGROUND

Since the expiration of the Wright Amendment on October 13, 2014, Dallas Love Field ("Airport") has experienced tremendous increases in the number of daily enplanements and amount of passenger traffic. With this increase in passenger traffic, the Airport has also experienced a major increase in customer demand for parking and rental cars. The Department of Aviation ("DOA") currently has limited land to help meet these increasing demands. The Airport experienced a high volume of additional parking requests during to 2015 holiday season. The acquisition of this vacant lot will insure that any increased demand; and emergency parking needs would be accommodated during the parking garage construction.

As a result of this need, DOA has been in the process of identifying potential off airport sites for use as additional parking for Airport operations. Viceroy Regal, LP offered to lease approximately 2.808 acres of land to the City of Dallas under a lease to help alleviate the need for more land.

The City shall pay an estimated annual amount of \$316,128.00 in rent and tax payments. The City will also be responsible for the utilities and maintenance of the premises.

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

\$1,580,640.00 over primary term of lease - Aviation Current Funds (subject to annual appropriations)

OWNER

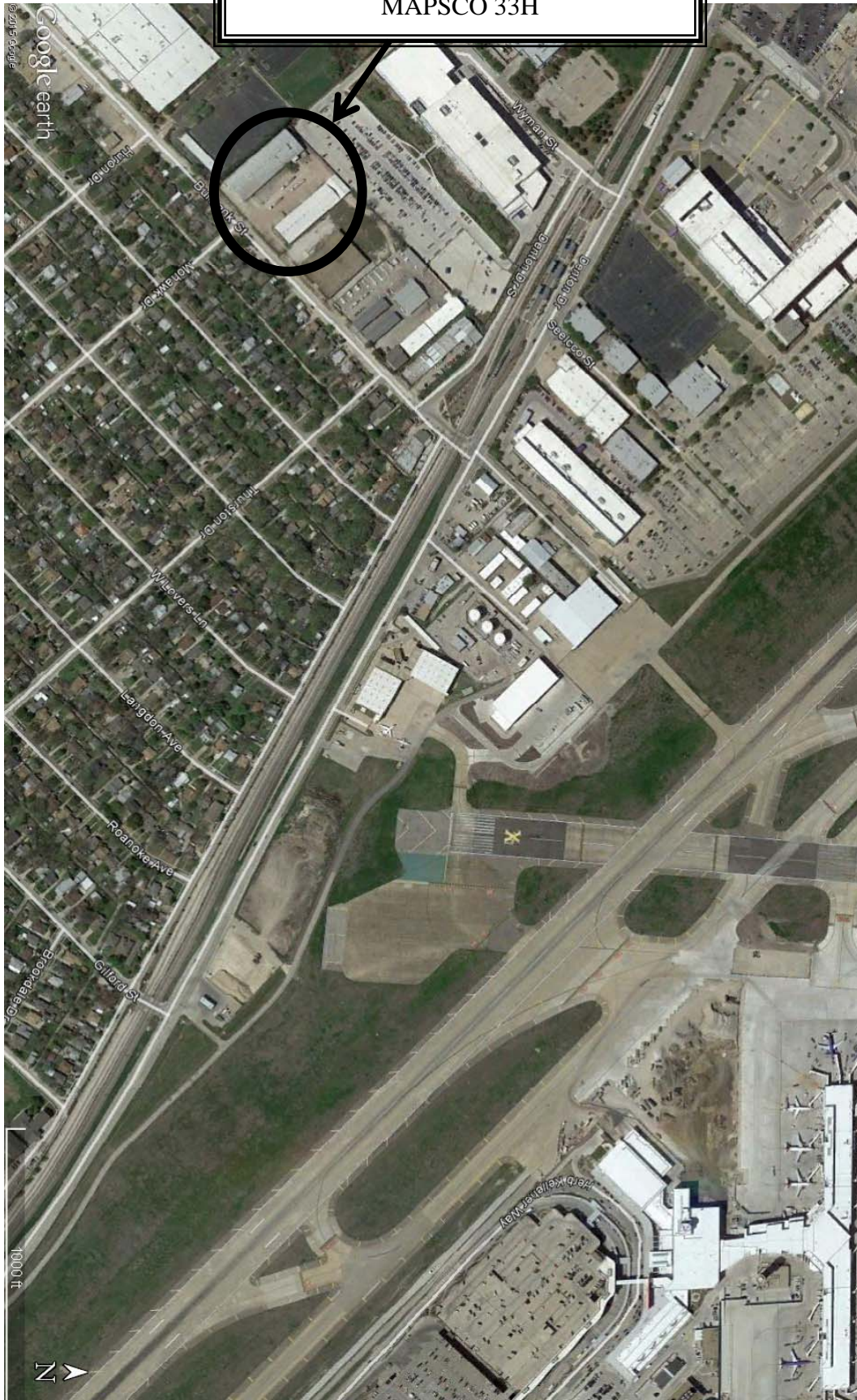
Viceroy Regal, LP

Stephen J. Rogers, President and General Partner

MAP

Attached.

DALLAS LOVE FIELD
Viceroy Regal, LP
Lease Premises
MAPSCO 33H



January 27, 2016

WHEREAS, the present parking lots and parking garages at Dallas Love Field Airport are insufficient to meet current and future airport parking needs year-round; and,

WHEREAS, Viceroy Regal, LP ("Lessor") is the owner of approximately 2.808 acres of land located at 2225 Burbank Street and 2311 Burbank Street, Dallas, Texas 75235 and being in the vicinity of Dallas Love Field Airport; and,

WHEREAS, Lessor has agreed to lease the entire 2.808 acres of land to the City of Dallas ("City" or "Lessee") under a five-year lease agreement with two five-year renewal options.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager, upon approval as to form by the City Attorney, is hereby authorized to execute a lease agreement with Viceroy Regal, LP, a Texas Limited Partnership, as Lessor, and the City of Dallas, as Lessee, for approximately 2.808 acres of land located at 2225 Burbank Street and 2311 Burbank Street, Dallas, Texas 75235 to be used primarily as vehicular parking and for other lawful uses at the discretion of the City for Dallas Love Field Airport.

Section 2. That the special terms and conditions of the lease are:

- a) The lease is for a term of five (5) years with two (2) five-year (5-yr) renewal options. The estimated monthly rental shall be \$26,344.00 and shall consist of a base monthly rental of \$24,780.00 and a monthly tax payment.
- b) The Lessor will deliver the leased premises to the City as an asphalt covered grade level and lighted parking lot in a good and workmanlike manner to a standard consistent with other asphalt covered grade level parking lots in the City of Dallas.
- c) The City shall pay all operating expenses for the Leased Premises, including utilities, insurance, (self-insurance being allowed), repair and maintenance in addition to the monthly rental.

Section 3. That subject to appropriations, the Chief Financial Officer is hereby authorized to draw warrants payable to Viceroy Regal, LP the first day of each month in advance during the lease term and charge the same to AVI Operating Fund 0130, Unit 7751, Agency AVI, Object 3330, Vendor No. VS90043, through service contract number MASCAVIVRLP016 in an estimated amount of \$316,128.00 annually, not to exceed \$1,580,640.00 for the primary term of the lease.

January 27, 2016

Section 4. That the Chief Financial Officer is hereby authorized to draw warrants payable to the respective utility, communication, and security companies upon the receipt of a bill for charges throughout the term of the lease.

Section 5. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 3

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: January 27, 2016
COUNCIL DISTRICT(S): 3
DEPARTMENT: Aviation
CMO: Ryan S. Evans, 671-9837
MAPSCO: 63L

SUBJECT

Authorize an early Termination Agreement with Akridge Aircraft Interiors, Inc. regarding its lease at Dallas Executive Airport, for approximately 107,523 square feet; and pay a final termination payment equal to \$374,500 - Not to exceed \$374,500 - Financing: Aviation Current Funds

BACKGROUND

Akridge Aircraft Interiors, Inc. currently holds one lease for improved and unimproved land, offices, and hangars on the subject property. The runway re-construction project has caused a decrease in tenant operations and revenue during this period. The lease buy-back option would allow the Department of Aviation the option to buy back this property, which is in need of improvement and repair. This hangar has been identified by several potential tenants, whose business model is in-line with input received from the community which will result in job creation and improvement to the hangar. In exchange for the lease buy-back of the property, the City desires to pay Akridge Aircraft Interiors, Inc. an agreed-upon final termination payment of \$374,500.00 based upon independent aviation appraisals acquired by the Department of Aviation.

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized the lease of land and improvements on February 23, 2000, by Resolution No. 00-0695.

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

\$374,500.00 - Aviation Current Funds

OWNER

Akridge Aircraft Interiors, Inc.

Leo Akridge, Owner and Chief Executive Officer

MAP

Attached.

DALLAS EXECUTIVE
Akridge Aircraft Interiors, Inc.
Lease Premises
MAPSCO 63L



January 27, 2016

WHEREAS, the City Council approved Resolution No. 00-0695 on February 23, 2000 which authorized the City Manager to execute a 20-year lease agreement with two (2) five (5) year option periods, of approximately 107,523 square feet of improved and unimproved land, an aircraft hangar, ramp and automobile parking, providing annual rent to the City in the approximate amount of \$18,278.91; and,

WHEREAS, the City now desires to execute an early Termination Agreement with Akridge Aircraft Interiors, Inc. concerning the Lease at Dallas Executive Airport and pay Akridge Aircraft Interiors Inc. a termination payment equal to \$374,500.00.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager, upon approval as to form by the City Attorney, is hereby authorized to execute the early Termination Agreement with Akridge Aircraft Interiors, Inc. concerning the Lease at Dallas Executive Airport.

Section 2. That the special terms and conditions of the early Termination Agreement are:

(a) Akridge Aircraft Interiors, Inc. shall forever release, quitclaim, and surrender to the City and its successors and assigns the Lease and all rights it has therein, together with all of its rights, interests, and title in all improvements, furniture, personal property, equipment, and fixtures therein contained, and all of the estate and rights of Akridge Aircraft Interiors, Inc. in and to the Lease.

(b) Within thirty (30) days from the date of City Council approval of the early Termination Agreement, City will deliver a termination payment of \$374,500.00 to Akridge Aircraft Interiors, Inc., which shall be the final termination payment due Akridge Aircraft Interiors, Inc.

Section 3. That the Chief Financial Officer is hereby authorized to draw warrants payable to Akridge Aircraft Interiors, Inc. upon execution of the early Termination Agreement and charge same in the amount of \$374,500.00 to AVI Operation Fund 0130, Dept. AVI, Unit 7731, Object 3899, Activity AV09, Vendor No. VS90306.

Section 4. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 4

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: January 27, 2016
COUNCIL DISTRICT(S): 2
DEPARTMENT: Aviation
CMO: Ryan S. Evans, 671-9837
MAPSCO: 34E

SUBJECT

Authorize a First Amendment to the consolidated ground lease with Gulfstream Aerospace Services Corporation at Dallas Love Field to increase the leasehold by approximately 5.842 acres into tract 1 of the leased premise - Financing: Estimated Increased Annual Revenue: \$165,405; Cumulative Rent: \$11,429,691

BACKGROUND

On January 25, 1978 the City Council approved the original lease with a 10 year primary term with 4, five-year renewal options for approximately 243,800 square feet. The lease was effective February 1, 1978. On October 28, 1981, the City Council approved the 1st amendment with Gulfstream exercising the right of 1st refusal on approximately 71,533 square feet (Parcels C & E), additional annual rent of \$18,598.58. On June 1, 1989, the City Council approved the 2nd amendment with Gulfstream exercising the right of 1st refusal on approximately 41,070 square feet (deleted Parcel E), additional annual rent of \$152,264.52. On June 25, 2008 the City Council approved a Consolidation Lease and Termination of the 1978 lease and the 1989 sub-lease. This amendment will increase the leasehold by approximately 5.842 acres (254,470 square feet) into tract 1 of the leased premises, as depicted in Exhibit B.

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Approved the original lease, on January 25, 1978, by Resolution No. 78-0243.

Approved 1st lease amendment, on October 28, 1981, by Resolution No. 81-3170.

Approved 2nd lease amendment, on June 1, 1989, by Resolution No. 89-1113.

Approved consolidation lease and termination of the 1978 & the 1989 sub-lease, on June 25, 2008, by Resolution No. 08-1874.

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS) (Continued)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

\$165,405.48 - Estimated Increased Annual Revenue

\$11,429,691.00 - Cumulative Rent

OWNER

Gulfstream Aerospace Services Corporation, a wholly-owned subsidiary of General Dynamics

Mark Burns, President

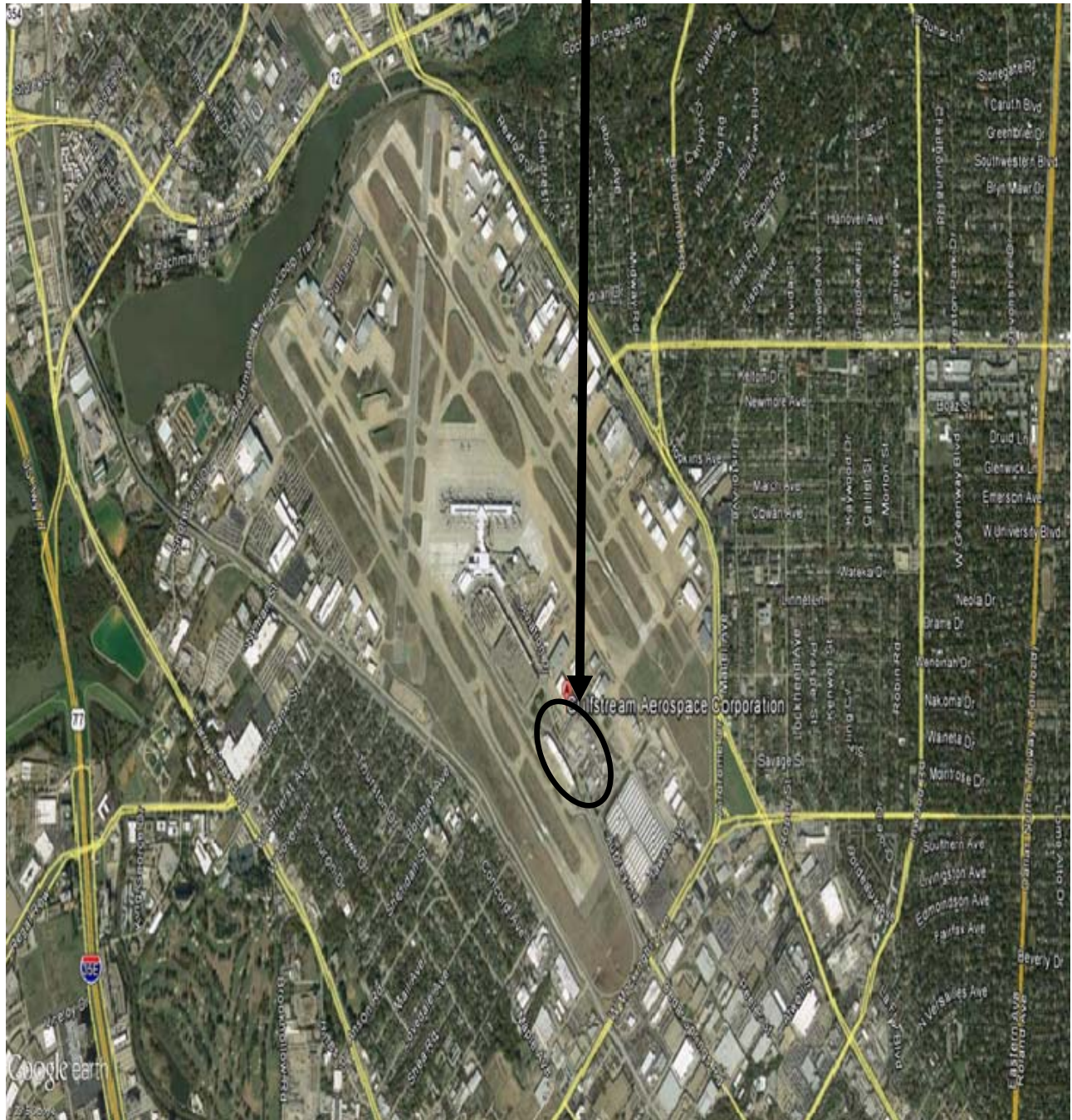
Daniel G. Clare, Vice President

Ira P. Berman, Secretary

MAP

Attached.

GULFSTREAM AEROSPACE
SERVICES CORPORATION
Existing Ground Lease
34E



January 27, 2016

WHEREAS, on January 25, 1978 the City Council approved the original lease with Gulfstream Aerospace Services Corporation for a 10 year primary term with 4 to 5 year options for approximately 243,800 square feet; and,

WHEREAS, on October 28, 1981, the City Council approved the 1st amendment to the original lease with Gulfstream Aerospace Services Corporation exercising the right of 1st refusal on approximately 71,533 square feet (Parcels C & E); and,

WHEREAS, on June 1, 1989, the City Council approved the 2nd amendment to the original lease with Gulfstream Aerospace Services Corporation exercising the right of 1st refusal on approximately 41,070 square feet (deleted Parcel E); and,

WHEREAS, on June 25, 2008 the City Council approved a Consolidation Lease and Termination of the 1978 lease and the 1989 sub-lease; and,

WHEREAS, the City now desires to authorize a First Amendment to the consolidated ground lease with Gulfstream Aerospace Services Corporation at Dallas Love Field to increase the leasehold by approximately 5.842 acres (254,470 square feet) into Tract 1 of the Leased Premises.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the proposed First Amendment to the Consolidated Lease, is hereby approved and the City Manager is authorized to execute, on behalf of the Lessor, this First Amendment to the Consolidated Lease after approval as to form by the City Attorney.

Section 2. That the capitalized terms used in this First Amendment and not defined herein shall have the meanings given to such terms in the Consolidated Lease. From and after the date hereof, all references to the "Consolidated Lease" shall mean the Consolidated Lease as amended by this First Amendment.

Section 3. That the parties desire to modify and amend the Exhibits to the Consolidated Lease as follows:

(a) Exhibit A - See map attached.

(b) Exhibit B - "Survey of the Leased Premises" is hereby deleted in its entirety and replaced with the attached Exhibit B.

January 27, 2016

Section 4. That the parties desire to amend the Leased Premises to add approximately 5.842 acres (254,470 square feet) into Tract 1 of the Leased Premises, as more fully described and as depicted in Exhibit B & B-2, both attached hereto and made a part hereof. As of the date of this First Amendment, the term "Leased Premises" shall be deemed to include this additional 5.842 acres.

Section 5. That this First Amendment shall increase the square footage of Tract 1 of the Leased Premises by approximately 254,470 square feet, for a total of 684,195 square feet as more fully described and as shown on Exhibit B attached hereto and providing an increased annual rent of \$165,405.48; and increased monthly rent of \$13,783.79 for a cumulative rent of \$11,429,691.84 (twelve years remaining on initial lease period); annual rent of \$952,474.32; and monthly rent of \$79,372.86.

Section 6. That the Chief Financial Officer is hereby authorized to deposit all revenues received under the Consolidated Lease, as amended, to: Aviation Operating Fund 0130; Dept. AVI; Unit 7725; Revenue Source 7814.

Section 7. That except as hereby amended, all other provisions of the Consolidated Lease shall continue in full force and effect. This First Amendment along with the Consolidated Lease shall hereinafter be considered a single agreement.

Section 8. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

EXHIBIT "A"

GULFSTREAM AEROSPACE
SERVICES CORPORATION
Existing Ground Lease
34E

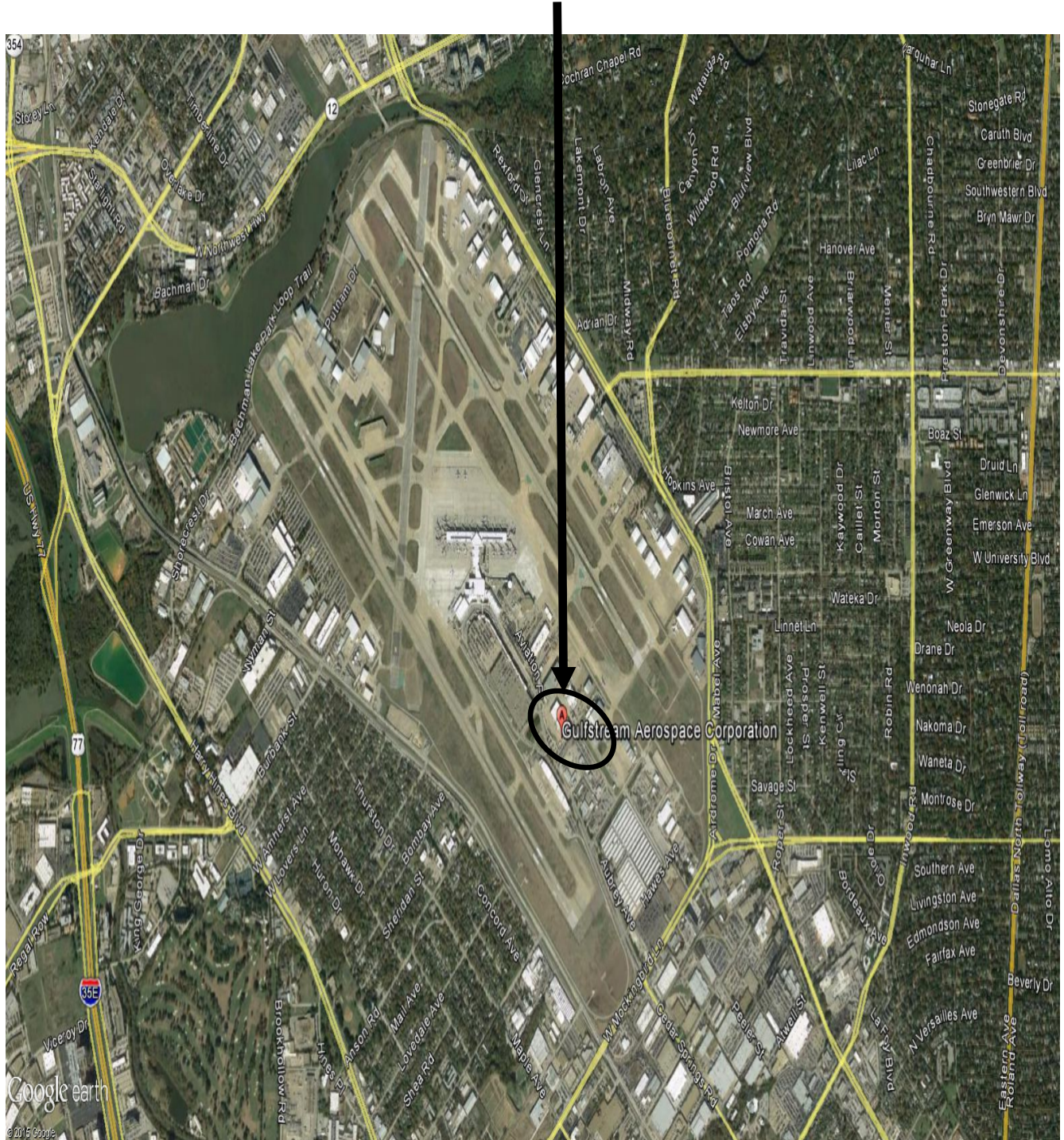
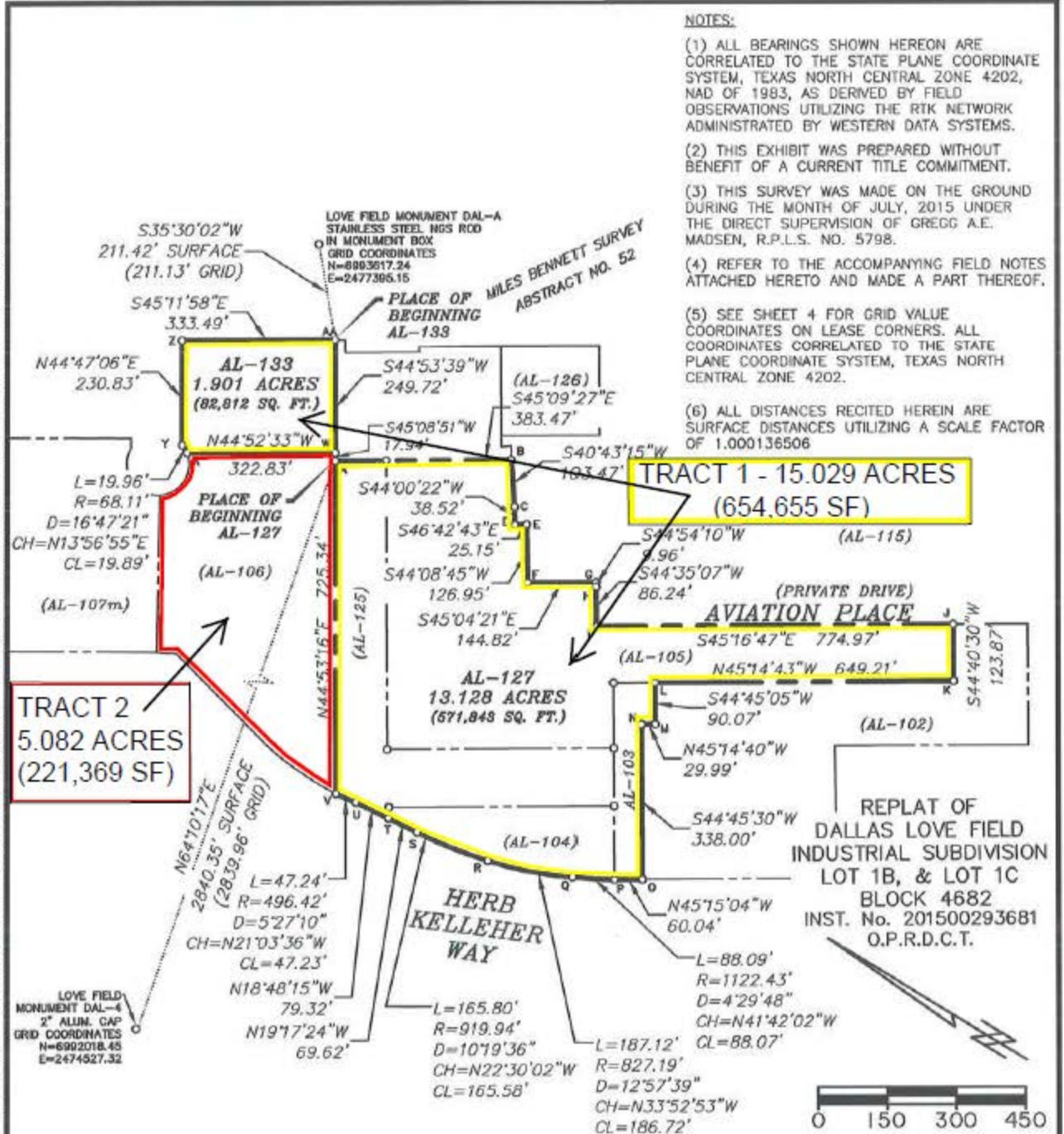


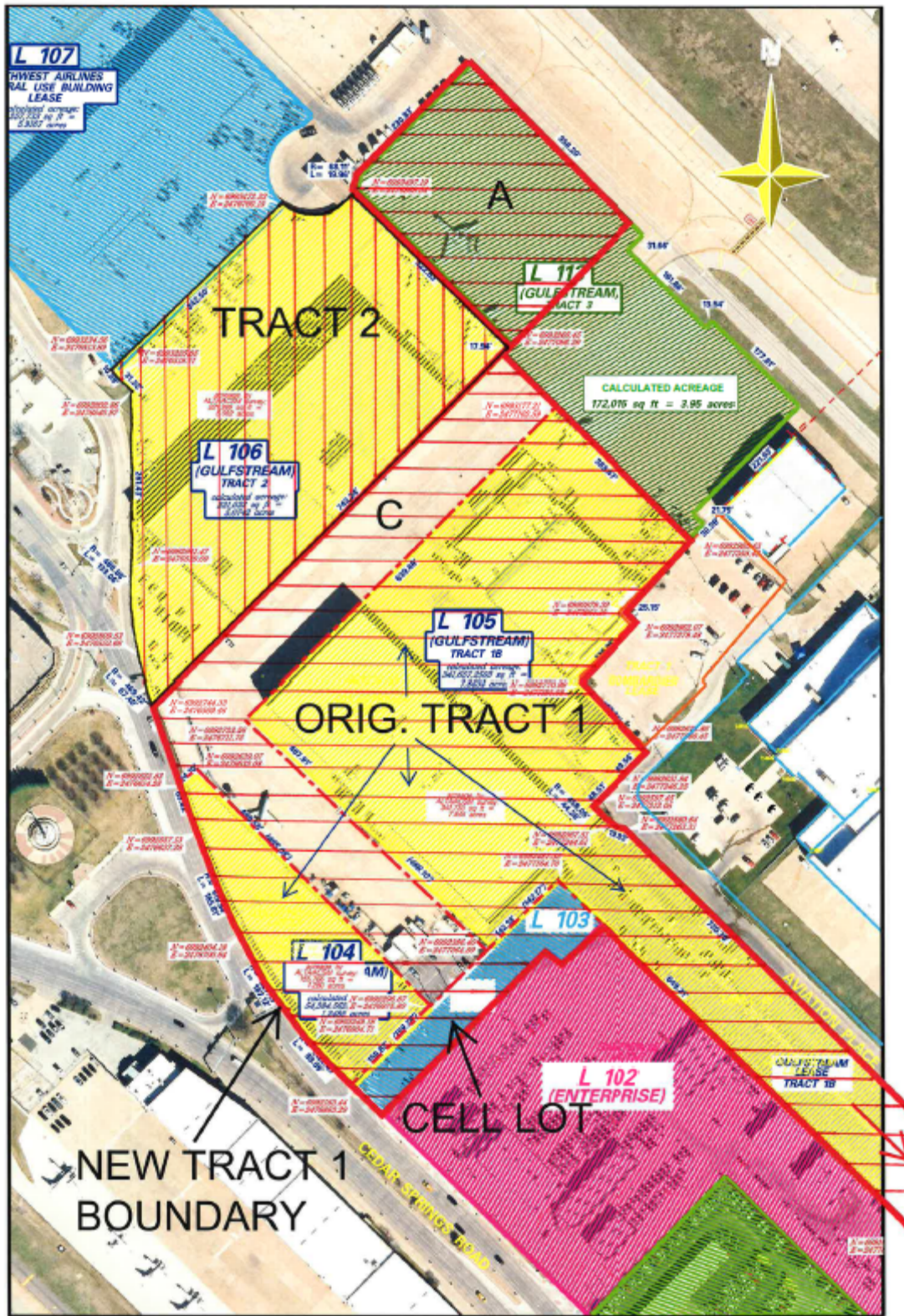
EXHIBIT "B"

WA# 07062.02 PRINTED: 11-24-2015 LAST SAVED: 11-24-2015 10:37 AM SAVED BY: MATTHEW FILE: COMBINED LEASE AREA EXHIBIT.DWG



GULFSTREAM PROPOSED LEASE AREAS AL-127 AND AL-133				
CITY OF DALLAS (LOVE FIELD AIRPORT) DEPT. OF PUBLIC WORKS & TRANSPORTATION SURVEY DIVISION CITY OF DALLAS, TEXAS				
REV.	DATE	BY	DESCRIPTION	CHK.
WIA SURVEYOR PREPARING THIS EXHIBIT: WIER & ASSOCIATES, INC. 2201 E. LAMAR BLVD., SUITE 200E ARLINGTON, TEXAS 76006 METRO (817)467-7700 Texas Firm Registration No. F-2776 www.WierAssociates.com Texas Board of Land Surveying Registration No. 10033900				
DRAWN BY: MTJ		DATE: 11-24-2015		DWG. NO.
CHECKED BY: GAM		DATE: 11-24-2015		REV.
SCALE: 1"=300'		APP: <i>GAM</i>		5 OF 7

EXHIBIT "B-2"



AGENDA ITEM # 5

KEY FOCUS AREA: E-Gov

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Business Development & Procurement Services
Housing/Community Services

CMO: Jeanne Chipperfield, 670-7804
Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: N/A

SUBJECT

Authorize the first twelve-month renewal option to the service contract to provide labor and materials for minor home repairs through the People Helping People Program administered by the Housing/Community Services Department – Miko Trucking, Inc. – Not to exceed \$155,292 – Financing: 2015-16 Community Development Block Grant Funds (subject to appropriations)

BACKGROUND

This action does not encumber funds; the purpose of a service contract is to establish firm pricing for services, for a specific term, which are ordered on an as needed basis.

This service contract will continue to provide labor and materials for minor home repairs through the People Helping People (PHP) Program. The PHP Program is designed to help low-income, elderly (62 or older) and disabled residents receive minor exterior home repairs. Available funds will serve households that meet low-income level guidelines established by the U.S. Department of Housing and Urban Development (HUD). It is estimated that each repair will average \$900.00 per household based on previous history of the program, however each household is allowed a maximum of \$5,000.00. The contracted vendor will be a supplement to volunteers, and will be used when special expertise is required. The goal of this year's program is to assist approximately 200 households utilizing grant funds.

BACKGROUND (Continued)

Once a home is verified as meeting the guidelines of the program, a Housing/Community Services Inspector is sent out to the home to assess the work to be done. The inspector then provides a work write up to the contractor to execute the repairs. Some examples of repairs provided under this program are:

- Roofing install/repair
- Repair porches and steps
- Painting exterior
- Install/repair ramps and handrails
- Remove unsafe garages and sheds

This renewal option meets the wage floor rate of \$10.37 approved by City Council on November 10, 2015 by Resolution No. 15-2141.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On January 26, 2011, City Council authorized the twelve month renewal option to provide labor for minor home repairs for the People Helping People Program for the period January 27, 2011 through January 26, 2012, by Resolution No. 11-0261.

On April 25, 2012, City Council authorized a one-year service contract, with a one-year renewal option, for general home repairs under the People Helping People Program administered by Housing/Community Services by Resolution No. 12-1167.

On April 24, 2013, City Council authorized Supplemental Agreement No. 1 to exercise the one-year contract renewal for general home repairs under the People Helping People Program administered by Housing/Community Services by Resolution No. 13-0698.

On November 12, 2014, City Council authorized a one-year service contract, with two one-year renewal options, to provide labor and materials for minor home repairs through the People Helping People Program administered by the Housing/Community Services Department by Resolution No. 14-1874.

Information about this item will be provided to the Budget, Finance and Audit Committee on January 19, 2016.

FISCAL INFORMATION

\$155,292.00 – 2015-16 Community Development Block Grant Funds (subject to appropriations)

ETHNIC COMPOSITION

Miko Trucking, Inc.

White Female	0	White Male	0
Black Female	0	Black Male	11
Hispanic Female	3	Hispanic Male	21
Other Female	0	Other Male	0

OWNER

Miko Trucking, Inc.

Michael Robinson, President

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize the first twelve-month renewal option to the service contract to provide labor and materials for minor home repairs through the People Helping People Program administered by the Housing/Community Services Department – Miko Trucking, Inc. – Not to exceed \$155,292 – Financing: 2015-16 Community Development Block Grant Funds (subject to appropriations)

Miko Trucking, Inc. is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use their own workforce.

PROJECT CATEGORY: Other Services

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$155,292.00	100.00%
Total non-local contracts	\$0.00	0.00%
TOTAL CONTRACT	\$155,292.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

None

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$0.00	0.00%
Total	\$0.00	0.00%	\$0.00	0.00%

January 27, 2016

WHEREAS, on January 26, 2011, City Council authorized the twelve month renewal option to provide labor for minor home repairs for the People Helping People Program for the period January 27, 2011 through January 26, 2012, by Resolution No. 11-0261; and,

WHEREAS, on April 25, 2012, City Council authorized a one-year service contract, with a one-year renewal option, for general home repairs under the People Helping People Program administered by Housing/Community Services by Resolution No. 12-1167; and,

WHEREAS, on April 24, 2013, City Council authorized Supplemental Agreement No. 1 to exercise the one-year contract renewal for general home repairs under the People Helping People Program administered by Housing/Community Services by Resolution No. 13-0698; and,

WHEREAS, on November 12, 2014, City Council authorized a one-year service contract, with two one-year renewal options, to provide labor and materials for minor home repairs through the People Helping People Program administered by the Housing/Community Services Department by Resolution No. 14-1874;

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is authorized to execute the first of two twelve month renewal options to the service contract with Miko Trucking, Inc. (VS0000010476) to provide labor and materials for minor home repairs through the People Helping People Program administered by the Housing/Community Services Department, in an amount not to exceed \$155,292.00, upon approval as to form by the City Attorney. If the service was bid or proposed on an as needed, unit price basis for performance of specified tasks, payment to Miko Trucking, Inc. shall be based only on the amount of the services directed to be performed by the City and properly performed by Miko Trucking, Inc. under the contract.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$155,292.00 (subject to appropriations) from Service Contract number BE1604.

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 6

KEY FOCUS AREA: E-Gov

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Business Development & Procurement Services
Water Utilities

CMO: Jeanne Chipperfield, 670-7804
Mark McDaniel, 670-3256

MAPSCO: N/A

SUBJECT

Authorize a three-year service contract for maintenance, support, parts and hardware for the supervisory control and data acquisition systems at three water treatment plants and two wastewater treatment plants - CDM Smith, Inc., most advantageous proposer of three - Not to exceed \$2,397,350 - Financing: Water Utilities Current Funds (subject to annual appropriations)

BACKGROUND

This action does not encumber funds; the purpose of a service contract is to establish firm pricing for services, for a specific term, which are ordered on an as needed basis.

This service contract will provide as needed maintenance, support, parts and hardware for the supervisory control and data acquisition (SCADA) systems at Water Utilities' three water treatment plants and two wastewater treatment plants. The SCADA systems located at these five locations allow for automated control of plant functions, data logging and regulatory reporting of plant operations. These systems have been installed over the past ten years and require periodic maintenance. Maintenance includes patches, updates, technical and customer service support to ensure compliance with state or federal requirements.

Water Utilities provides water and wastewater service to approximately 2.4 million customers in the Dallas service area of approximately 700 square miles.

BACKGROUND (Continued)

A seven member committee from the following departments reviewed and evaluated the proposals:

- Trinity Watershed Management (1)
- Water Utilities (2)
- Communication and Information Services (2)
- Business Development and Procurement Services (2)*

*Business Development and Procurement Services evaluated cost and Business Inclusion and Development Plan.

The successful proposer was selected by the committee on the basis of demonstrated competence and qualifications under the following criteria:

- Cost 30%
- Capability and Expertise 20%
- Technical Match 20%
- Business Inclusion and Development Plan 15%
- Overall Approach 15%

As part of the solicitation process and in an effort to increase competition, Business Development and Procurement Services (BDPS) used its procurement system to send out 623 email bid notifications to vendors registered under respective commodities. To further increase competition, BDPS uses historical solicitation information, the internet, and vendor contact information obtained from user departments to contact additional vendors by phone. Additionally, in an effort to secure more bids, notifications were sent by the BDPS' ResourceLink Team (RLT) to 25 chambers of commerce, the DFW Minority Business Council and the Women's Business Council – Southwest, to ensure maximum vendor outreach.

The recommended vendor meets the wage floor rate of \$10.37 approved by City Council on November 10, 2015 by Resolution No. 15-2141.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On August 8, 2012, City Council authorized a three-year service contract for maintenance and support for the supervisory control and data acquisition systems at five water and wastewater treatment plants by Resolution No. 12-1881.

On January 14, 2015, City Council authorized an increase to the service contract for maintenance and support for the supervisory control and data acquisition systems at five water and wastewater treatment plants by Resolution No. 15-0110.

Information about this item will be provided to the Budget, Finance and Audit Committee on January 19, 2016.

FISCAL INFORMATION

\$2,397,350.00 - Water Utilities Current Funds (subject to annual appropriations)

M/WBE INFORMATION

95 - Vendors contacted
95 - No response
0 - Response (Bid)
0 - Response (No bid)
0 - Successful

623 - M/WBE and Non-M/WBE vendors were contacted

The recommended awardee has fulfilled the good faith requirements set forth in the Business Inclusion and Development (BID) Plan adopted by Council Resolution No. 08-2826 as amended.

ETHNIC COMPOSITION

CDM Smith, Inc.

White Male	42	White Female	10
Black Male	3	Black Female	4
Hispanic Male	4	Hispanic Female	1
Other Male	19	Other Female	4

PROPOSAL INFORMATION

The following proposals were received from solicitation number BUZ1513 and opened on July 22, 2015. This service contract is being awarded in its entirety to the most advantageous proposer.

*Denotes successful proposer

<u>Proposers</u>	<u>Address</u>	<u>Score</u>	<u>Amount</u>
*CDM Smith, Inc.	8140 Walnut Hill Lane Suite 1000 Dallas, TX 75231	92%	\$2,397,350.00
Signature Automation, LLC	14679 Midway Road Suite 205 Addison, TX 75001	87%	\$2,544,200.00

PROPOSAL INFORMATION (Continued)

<u>Proposers</u>	<u>Address</u>	<u>Score</u>	<u>Amount</u>
Prime Controls, LP	1725 Lakepointe Drive Lewisville, TX 75057	81%	\$2,437,380.00

OWNER**CDM Smith, Inc.**

Tim Wall, President
Douglas Varner, Vice President

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a three-year service contract for maintenance, support, parts and hardware for the supervisory control and data acquisition systems at three water treatment plants and two wastewater treatment plants - CDM Smith, Inc., most advantageous proposer of three - Not to exceed \$2,397,350 - Financing: Water Utilities Current Funds (subject to annual appropriations)

CDM Smith, Inc. is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use their own workforce.

PROJECT CATEGORY: Other Services

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$2,397,350.00	100.00%
Total non-local contracts	\$0.00	0.00%
TOTAL CONTRACT	\$2,397,350.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

None

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$0.00	0.00%
Total	\$0.00	0.00%	\$0.00	0.00%

January 27, 2016

WHEREAS, on August 8, 2012, City Council authorized a three-year service contract for maintenance and support for the supervisory control and data acquisition systems at five water and wastewater treatment plants by Resolution No. 12-1881; and,

WHEREAS, on January 14, 2015, City Council authorized an increase to the service contract for maintenance and support for the supervisory control and data acquisition systems at five water and wastewater treatment plants in the amount of \$388,750.00, from \$1,555,000.00 to \$1,943,750.00, by Resolution No. 15-0110;

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is authorized to execute a service contract with CDM Smith, Inc. (089614) for maintenance, support, parts and hardware for the supervisory control and data acquisition systems at three water treatment plants and two wastewater treatment plants for a term of three years in an amount not to exceed \$2,397,350.00, upon approval as to form by the City Attorney. If the service was bid or proposed on an as needed, unit price basis for performance of specified tasks, payment to CDM Smith, Inc. shall be based only on the amount of the services directed to be performed by the City and properly performed by CDM Smith, Inc. under the contract.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$2,397,350.00 (subject to annual appropriations) from Service Contract number BUZ1513.

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 7

KEY FOCUS AREA: Clean, Healthy Environment

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Business Development & Procurement Services
Park & Recreation

CMO: Jeanne Chipperfield, 670-7804
Willis Winters, 670-4071

MAPSCO: N/A

SUBJECT

Authorize a three-year service contract for litter maintenance services for Park and Recreation – Good Earth Corporation in the amount of \$5,504,968 and T. Smith's Lawn Service, LLC in the amount of \$2,608,751, lowest responsible bidders of four - Total not to exceed \$8,113,719 - Financing: Current Funds (subject to annual appropriations)

BACKGROUND

This action does not encumber funds; the purpose of a service contract is to establish firm pricing for services, for a specific term, which are ordered on an as needed basis.

This service contract will be used for regular litter maintenance service in virtually every park within the six park maintenance districts. Contractors will perform services on a predetermined maintenance frequency established by the City. Maintenance frequencies vary in each park based on the amount of traffic and the time of year. Adjustments to the schedule can be made based on variable needs, such as special events and spray ground activity. The service contract also includes a provision for litter maintenance performed on an emergency response basis.

As part of the solicitation process and in an effort to increase competition, Business Development and Procurement Services used its procurement system to send out 397 email bid notifications to vendors registered under respective commodities. To further increase competition, Business Development and Procurement Services uses historical solicitation information, the internet, and vendor contact information obtained from user departments to contact additional vendors by phone. Additionally, in an effort to secure more bids, notifications were sent by the Business Development and Procurement Services' ResourceLink Team (RLT) to 25 chambers of commerce, the DFW Minority Business Council and the Women's Business Council – Southwest, to ensure maximum vendor outreach.

BACKGROUND (Continued)

This solicitation closed prior to the wage floor policy approved by City Council on November 10, 2015 by Resolution No. 15-2141. Prior to Council approval of the wage floor, City contracts did not include language that required contractors to identify their rate of pay or the number of employees assigned on City contracts. T. Smith's Lawn Service, LLC has agreed to pay their employees a minimum wage of \$10.37 per hour. Good Earth Corporation will pay their employees a minimum of \$9.00 per hour.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On February 22, 2012, City Council authorized a three-year service contract for litter pick up and refuse collection services by Resolution No. 12-0527.

On January 28, 2015, City Council authorized supplemental agreement no. 1 to the service contract for litter pick up and refuse collection services at City parks, to extend the term from March 31, 2015 through March 31, 2016, by Resolution No. 15-0164.

Information about this item will be provided to the Budget, Finance and Audit Committee on January 19, 2016.

The Park and Recreation Board will be briefed on January 21, 2016.

FISCAL INFORMATION

\$8,113,718.68 - Current Funds (subject to annual appropriations)

M/WBE INFORMATION

55 - Vendors contacted
55 - No response
0 - Response (Bid)
0 - Response (No Bid)
0 - Successful

397 M/WBE and Non-M/WBE vendors were contacted

The recommended awardees have fulfilled the good faith requirements set forth in the Business Inclusion and Development (BID) Plan adopted by Council Resolution No. 08-2826 as amended.

ETHNIC COMPOSITION

Good Earth Corporation

White Male	16	White Female	2
Black Male	19	Black Female	1
Hispanic Male	117	Hispanic Female	10
Other Male	0	Other Female	0

T. Smith's Lawn Service, LLC

White Male	0	White Female	0
Black Male	16	Black Female	2
Hispanic Male	2	Hispanic Female	0
Other Male	0	Other Female	0

BID INFORMATION

The following bids were received from solicitation number BF1516 and were opened on September 25, 2015. This service contract is being awarded to the lowest responsive and responsible bidders by group. Information related to this solicitation is available upon request.

*Denotes successful bidders

<u>Bidders</u>	<u>Address</u>	<u>Amount</u>
*Good Earth Corporation	8020 Heinen Drive Dallas, TX 75227	Multiple Groups
*T. Smith's Lawn Service LLC	3315 Daniieldale Lancaster, TX 75146	Multiple Groups
CBMAA, LLC	225 Wolf Drive Forney, TX 75126	Multiple Groups
Evans Mobile Detail	311 Sorrel St Duncanville, TX 75137	Non-responsive**

**Evans Mobile Detail was deemed non-responsive due to not meeting the specifications.

OWNERS

Good Earth Corporation

Ron Points, President
George Belk, Vice President
Emily Points, Secretary

T. Smith's Lawn Service, LLC

Terrence Smith, President
Teresa Smith, Vice President

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a three-year service contract for litter maintenance services for Park and Recreation – Good Earth Corporation in the amount of \$5,504,968 and T. Smith's Lawn Service, LLC in the amount of \$2,608,751, lowest responsible bidders of four - Total not to exceed \$8,113,719 - Financing: Current Funds (subject to annual appropriations)

Good Earth Corporation and T. Smith's Lawn Service, LLC are local, non-minority firms, have signed the "Business Inclusion & Development" documentation, and propose to use the following sub-contractors.

PROJECT CATEGORY: Other Services

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$8,113,718.68	100.00%
Total non-local contracts	\$0.00	0.00%
TOTAL CONTRACT	\$8,113,718.68	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Urban Landscaping & Irrigation	BMMB63836N1216	\$620,882.75	7.65%
Sun Coast Resources	WFWB63943N0117	\$1,310,182.00	16.15%
Total Minority - Local		\$1,931,064.75	23.80%

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$620,882.75	7.65%	\$620,882.75	7.65%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$1,310,182.00	16.15%	\$1,310,182.00	16.15%
Total	\$1,931,064.75	23.80%	\$1,931,064.75	23.80%

January 27, 2016

WHEREAS, on February 22, 2012, City Council authorized a three-year service contract for litter pick up and refuse collection services by Resolution No. 12-0527; and,

WHEREAS, on January 28, 2015, City Council authorized supplemental agreement no. 1 to the service contract for litter pick up and refuse collection services at City parks, to extend the term from March 31, 2015 through March 31, 2016, by Resolution No. 15-0164;

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is authorized to execute a contract with Good Earth Corporation (510006) in the amount of \$5,504,967.63 and T. Smith's Lawn Service, LLC (VS0000052924) in the amount of \$2,608,751.05 to provide litter maintenance services for Park and Recreation for a term of three years in a total amount not to exceed \$8,113,718.68, upon approval as to form by the City Attorney. If the service was bid or proposed on an as needed, unit price basis for performance of specified tasks, payment to Good Earth Corporation and T. Smith's Lawn Service, LLC shall be based only upon the amount of the services directed to be performed by the City and properly performed by Good Earth Corporation and T. Smith's Lawn Service, LLC under the contract.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$8,113,718.68 (subject to annual appropriations) from Service Contract number BF1516.

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 8

KEY FOCUS AREA: Clean, Healthy Environment

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Business Development & Procurement Services
Aviation
Equipment & Building Services
Park & Recreation
Public Works Department

CMO: Jeanne Chipperfield, 670-7804
Ryan S. Evans, 671-9837
Jill A. Jordan, P.E., 670-5299
Willis Winters, 670-4071

MAPSCO: N/A

SUBJECT

Authorize a three-year service contract for grounds maintenance services at parks, airports and other City owned properties - Good Earth Corporation in the amount of \$2,403,441 and Urban Landscaping & Irrigation in the amount of \$774,908, lowest responsible bidders of three - Total not to exceed \$3,178,349 - Financing: Current Funds (\$2,546,621) and Aviation Current Funds (\$631,728) (subject to annual appropriations)

BACKGROUND

This action does not encumber funds; the purpose of a service contract is to establish firm pricing for services, for a specific term, which are ordered on an as needed basis.

This service contract will be used for grounds maintenance service for parks, airports and other City owned properties. This contract will allow for services on a predetermined maintenance schedule established by the City. The service contract will also have a provision for emergency responses.

BACKGROUND (Continued)

This service contract will provide grounds maintenance service in 130 parks located in all six park maintenance districts, Fair Park, Bahama Beach Water Park, Love Field, Love Hub Parking Garage and Executive Airport. The contractors will perform these services on a predetermined maintenance frequency established by the City. Fleet and Building Service centers are serviced on an as needed basis. Maintenance frequencies vary for each property based on the type of turf area and the time of year. Adjustments to the schedule can be made based on variable needs, such as special events and weather conditions.

The properties in the first six groups were each divided into three sub-groups. Park and Recreation compared the costs for Park maintenance staff to perform grounds maintenance service against the bid prices for each group and sub-group and based this award on those results.

This solicitation was structured in a manner which required bidders to submit a response using unit pricing. This bid resulted in a 14.3% increase over comparable unit prices for the bid awarded in 2012.

As part of the solicitation process and in an effort to increase competition, Business Development and Procurement Services (BDPS) used its procurement system to send out 382 email bid notifications to vendors registered under respective commodities. To further increase competition, BDPS uses historical solicitation information, the internet, and vendor contact information obtained from user departments to contact additional vendors by phone. Additionally, in an effort to secure more bids, notifications were sent by the BDPS' ResourceLINK Team (RLT) to 25 chambers of commerce, the DFW Minority Business Council and the Women's Business Council – Southwest, to ensure maximum vendor outreach.

This solicitation closed prior to the wage floor policy approved by City Council on November 10, 2015 by Resolution No. 15-2141. Prior to Council approval of the wage floor, City contracts did not include language that required contractors to identify their rate of pay or the number of employees assigned on City contracts. Urban Landscaping & Irrigation has agreed to pay their employees a minimum wage of \$10.37 per hour. Good Earth Corporation will pay their employees a minimum of \$9.50 per hour.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On February 22, 2012, City Council authorized a three-year service contract for grounds maintenance services at parks, airports and other City owned properties by Resolution No. 12-0525.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS) (Continued)

Information about this item will be provided to the Budget, Finance and Audit Committee on January 19, 2016.

The Park and Recreation Board will be briefed on January 21, 2016.

FISCAL INFORMATION

\$2,546,620.56 - Current Funds (subject to annual appropriations)

\$ 631,728.00 - Aviation Current Funds (subject to annual appropriations)

M/WBE INFORMATION

51 - Vendors contacted

49 - No response

2 - Response (Bid)

0 - Response (No bid)

1 - Successful

382 - M/WBE and Non-M/WBE vendors were contacted

The recommended awardees have fulfilled the requirements set forth in the Business Inclusion and Development (BID) Plan adopted by Council Resolution No. 08-2826 as amended.

ETHNIC COMPOSITION

Good Earth Corporation

White Male	16	White Female	2
Black Male	24	Black Female	2
Hispanic Male	117	Hispanic Female	11
Other Male	0	Other Female	0

Urban Landscaping & Irrigation

White Male	0	White Female	0
Black Male	5	Black Female	0
Hispanic Male	8	Hispanic Female	1
Other Male	0	Other Female	0

BID INFORMATION

The following bids were received from solicitation number BF1517 and were opened on November 6, 2015. This service contract is being awarded to the lowest responsive and responsible bidders by group. Information related to this solicitation is available upon request.

*Denotes successful bidders

<u>Bidders</u>	<u>Address</u>	<u>Amount of Bid</u>
*Good Earth Corporation	8020 Heinen Dr. Dallas, TX 75227	Multiple Groups
*Urban Landscaping & Irrigation	700 S. Dallas Ave. Lancaster, TX 75146	Multiple Groups
Jba Land Management, LLC	10875 Jupiter Rd. Dallas, TX 75218	Non-responsive**

**Jba Land Management, LLC was deemed non-responsive due to not meeting specifications.

OWNERS

Good Earth Corporation

Ron Points, President
George Belk, Vice President
Emily Points, Secretary

Urban Landscaping & Irrigation

Dexter Payne, President

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a three-year service contract for grounds maintenance services at parks, airports and other City owned properties - Good Earth Corporation in the amount of \$2,403,441 and Urban Landscaping & Irrigation in the amount of \$774,908, lowest responsible bidders of three - Total not to exceed \$3,178,349 - Financing: Current Funds (\$2,546,621) and Aviation Current Funds (\$631,728) (subject to annual appropriations)

Good Earth Corporation is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and propose to use the following sub-contractors. Urban Landscaping & Irrigation is a local, minority firm, has signed the "Business Inclusion & Development" documentation, and propose to use the following sub-contractors.

PROJECT CATEGORY: Other Services

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$3,148,348.56	99.06%
Total non-local contracts	\$30,000.00	0.94%
TOTAL CONTRACT	\$3,178,348.56	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Urban Landscaping & Irrigation	BMMB63836N1216	\$744,907.56	23.66%
Sun Coast Resources	WFWB63943N0117	\$572,019.00	18.17%
Total Minority - Local		\$1,316,926.56	41.83%

Non-Local Contractors / Sub-Contractors

<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Irrigators Supply, Inc.	WFWB61642Y0516	\$30,000.00	100.00%
Total Minority - Non-local		\$30,000.00	100.00%

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$744,907.56	23.66%	\$744,907.56	23.44%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$572,019.00	18.17%	\$602,019.00	18.94%
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Total	\$1,316,926.56	41.83%	\$1,346,926.56	42.38%

January 27, 2016

WHEREAS, on February 22, 2012, City Council authorized a three-year service contract for grounds maintenance services at parks, airports and other City owned properties by Resolution No. 12-0525;

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is authorized to execute a service contract with Good Earth Corporation (510006) in the amount of \$2,403,441.00 and Urban Landscaping & Irrigation (514920) in the amount of \$774,907.56 for grounds maintenance services at parks, airports and other City owned properties for a term of three years in an amount not to exceed \$3,178,348.56, upon approval as to form by the City Attorney. If the service was bid or proposed on an as needed, unit price basis for performance of specified tasks, payment to Good Earth Corporation and Urban Landscaping & Irrigation shall be based only on the amount of the services directed to be performed by the City and properly performed by Good Earth Corporation and Urban Landscaping & Irrigation under the contract.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$3,178,348.56 (subject to annual appropriations) from Service Contract number BF1517.

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 9

KEY FOCUS AREA: Clean, Healthy Environment

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Business Development & Procurement Services
Aviation
Equipment & Building Services
Park & Recreation
Police
Sanitation Services
Water Utilities

CMO: Jeanne Chipperfield, 670-7804
Ryan S. Evans, 671-9837
Jill A. Jordan, P.E., 670-5299
Willis Winters, 670-4071
Eric Campbell, 670-3255
Joey Zapata, 670-3009
Mark McDaniel, 670-3256

MAPSCO: N/A

SUBJECT

Authorize a three-year service contract for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks - Stericycle Environmental Solutions, Inc. in the amount of \$819,002 and Liquid Environmental Solutions of Texas, LLC in the amount of \$39,895, lowest responsible bidders of two - Total not to exceed \$858,897 - Financing: Current Funds (\$401,591), Aviation Current Funds (\$369,879), Sanitation Current Funds (\$72,584) and Water Utilities Current Funds (\$14,843) (subject to annual appropriations)

BACKGROUND

This action does not encumber funds; the purpose of a service contract is to establish firm pricing for services, for a specific term, which are ordered on an as needed basis.

This service contract will provide for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks throughout the City. State law requires the City to operate holding tanks or "traps" to collect contaminated water, grease run-off and other debris from specific drains citywide. The traps range in size from 25 gallons to 14,000 gallons. The waste collected in these traps is prohibited from being discharged into the sanitary sewer system.

BACKGROUND (Continued)

This contract provides for servicing of sand traps found in vehicle and equipment wash bays throughout the City, grease traps associated with commercial kitchens such as Bahama Beach Water Park, Love Field restaurant facilities, and Fair Park venues, interceptors for the pre-treatment of stormwater found at Sanitation transfer stations, and septic tanks found at older City facilities.

This contract also provides for servicing as needed and emergency cleaning of the sand traps, grease traps, interceptors and septic tanks. They are cleaned and the waste is properly disposed of per State laws and regulations.

As part of the solicitation process and in an effort to increase competition, Business Development and Procurement Services used its procurement system to send out 758 email bid notifications to vendors registered under respective commodities. To further increase competition, Business Development and Procurement Services uses historical solicitation information, the internet, and vendor contact information obtained from user departments to contact additional vendors by phone. Additionally, in an effort to secure more bids, notifications were sent by the Business Development and Procurement Services' ResourceLINK Team (RLT) to 25 chambers of commerce, the DFW Minority Business Council and the Women's Business Council – Southwest, to ensure maximum vendor outreach.

The recommended vendor meets the wage floor rate of \$10.37 approved by City Council on November 10, 2015 by Resolution No. 15-2141.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On December 12, 2012, City Council authorized a three-year service contract for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks by Resolution No. 12-2967.

Information about this item will be provided to the Budget, Finance and Audit Committee on January 19, 2016.

FISCAL INFORMATION

\$401,591.00 - Current Funds (subject to annual appropriations)

\$369,879.00 - Aviation Current Funds (subject to annual appropriations)

\$ 72,584.00 - Sanitation Current Funds (subject to annual appropriations)

\$ 14,843.00 - Water Utilities Current Funds (subject to annual appropriations)

M/WBE INFORMATION

183 - Vendors contacted

183 - No response

0 - Response (Bid)

0 - Response (No bid)

0 - Successful

758 M/WBE and Non-M/WBE vendors were contacted

The recommended awardees have fulfilled the requirements set forth in the Business Inclusion and Development (BID) Plan adopted by Council Resolution No. 08-2826 as amended.

ETHNIC COMPOSITION

Stericycle Environmental Solutions, Inc.

White Male	4	White Female	1
Black Male	8	Black Female	0
Hispanic Male	4	Hispanic Female	0
Other Male	1	Other Female	1

Liquid Environmental Solutions of Texas, LLC

White Male	191	White Female	43
Black Male	131	Black Female	21
Hispanic Male	114	Hispanic Female	25
Other Male	20	Other Female	14

BID INFORMATION

The following bids were received for solicitation number BP1514 and were opened on October 30, 2015. This service contract is being awarded to the lowest responsive and responsible bidders by group.

*Denotes successful bidders

<u>Bidders</u>	<u>Address</u>	<u>Amount of Bid</u>
*Stericycle Environmental Solutions, Inc.	2131 Progressive Dr. Dallas, TX 75212	Group 1 - \$352,931.00 Group 2 - \$466,071.00 Group 3 - \$ 53,552.00
*Liquid Environmental Solutions of Texas, LLC	7651 Esters Blvd. Suite 200 Dallas, TX 75063	Group 1 - No Bid Group 2 - \$488,911.00 Group 3 - \$ 39,895.00

OWNERS

Stericycle Environmental Solutions, Inc.

Charles A. Alutto, President
John Simpson, Vice President

Liquid Environmental Solutions of Texas, LLC

Alan Viterbi, President
Patrick J. Reilly, Vice President
William Bergastrom, Treasurer

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a three-year service contract for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks - Stericycle Environmental Solutions, Inc. in the amount of \$819,002 and Liquid Environmental Solutions of Texas, LLC in the amount of \$39,895, lowest responsible bidders of two - Total not to exceed \$858,897 - Financing: Current Funds (\$401,591), Aviation Current Funds (\$369,879), Sanitation Current Funds (\$72,584) and Water Utilities Current Funds (\$14,843) (subject to annual appropriations)

Stericycle Environmental Solutions, Inc. and Liquid Environmental Solutions of Texas, LLC., are local, non-minority firms have signed the "Business Inclusion & Development" documentation, and propose to use their own workforces.

PROJECT CATEGORY: Other Services

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$858,897.00	100.00%
Total non-local contracts	\$0.00	0.00%
TOTAL CONTRACT	\$858,897.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

None

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$0.00	0.00%
Total	\$0.00	0.00%	\$0.00	0.00%

January 27, 2016

WHEREAS, on December 12, 2012, City Council authorized a three-year service contract for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks by Resolution No. 12-2967; and,

WHEREAS, on December 7, 2015, Administrative Action Nos. 15-7246 and 15-7334 authorized extension of the service contract for three months from December 12, 2015 to March 10, 2016;

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS

Section 1. That the City Manager is authorized to execute a service contract with Stericycle Environmental Solutions, Inc. (VS90420) in the amount of \$819,002.00 and Liquid Environmental Solutions of Texas, LLC (246988) in the amount of \$39,895.00 for the removal and disposal of waste collected in sand traps, grease traps, interceptors and septic tanks for a term of three years in a total amount not to exceed \$858,897.00, upon approval as to form by the City Attorney. If the service was bid or proposed on an as needed, unit price basis for performance of specified tasks, payment to Stericycle Environmental Solutions, Inc. and Liquid Environmental Solutions of Texas, LLC shall be based only on the amount of the services directed to be performed by the City and properly performed by Stericycle Environmental Solutions, Inc. and Liquid Environmental Solutions of Texas, LLC under the contract.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$858,897.00 (subject to annual appropriations) from Service Contract number BP1514.

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 10

KEY FOCUS AREA: Culture, Arts and Recreation and Educational Enhancements

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2

DEPARTMENT: Business Development & Procurement Services
Convention and Event Services

CMO: Jeanne Chipperfield, 670-7804
Ryan S. Evans, 671-9837

MAPSCO: 45P

SUBJECT

Authorize the purchase of mobile/folding stage equipment and accessories for the Kay Bailey Hutchinson Convention Center Dallas - Sico America, Inc. through the National Intergovernmental Purchasing Alliance Company - Not to exceed \$1,153,260 - Financing: Convention and Event Services Capital Construction Funds

BACKGROUND

This action will allow for the purchase of mobile/folding stage equipment and accessories for the Kay Bailey Hutchinson Convention Center Dallas (KBHCCD). This equipment will expand and upgrade the existing 20 year old stage equipment which serves as an essential function for events and conventions being held at the KBHCCD. This brand of stage riser will be used in conjunction with the existing stage risers and will allow for the purchase of additional mobile/folding stage equipment such as:

- 680 - 4' x 8' stage decking
- 50 - 2 and 3 step risers
- 20 - 6 step articulated risers with wheels and guardrails
- 430 - various height drapes
- 270 - various height guard rails

The National Intergovernmental Purchasing Alliance Company (NIPA) conforms to the requirements of Texas Statutes that are applicable for competitive bids and proposals, in accordance with the Interlocal Cooperation Act, Chapter 791, Texas Government Code. In addition, NIPA receives bids from manufacturers and dealers throughout the United States.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Budget, Finance and Audit Committee on January 19, 2016.

FISCAL INFORMATION

\$1,153,260.00 - Convention and Event Services Capital Construction Funds

ETHNIC COMPOSITION

Sico America, Inc.

White Male	104	White Female	30
Black Male	16	Black Female	0
Hispanic Male	6	Hispanic Female	1
Other Male	22	Other Female	2

OWNER

Sico America, Inc.

Ken Steinbauer, President
Pam Johnson, Vice President
Keith Dahlen, Secretary

January 27, 2016**BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:**

Section 1. That the purchase of mobile/folding stage equipment and accessories for the Kay Bailey Hutchison Convention Center Dallas is authorized with Sico America, Inc. (VS86700) through the National Intergovernmental Purchasing Alliance in an amount not to exceed \$1,153,260.00.

Section 2. That the Purchasing Agent is authorized, upon appropriate requisition, to issue a purchase order for mobile/folding stage equipment and accessories for the Kay Bailey Hutchison Convention Center Dallas. If a formal contract is required for this purchase instead of a purchase order, the City Manager is authorized to execute the contract upon approval as to form by the City Attorney.

Section 3. That the Chief Financial Officer is hereby authorized to disburse funds from the following appropriation in an amount not to exceed \$1,153,260.00:

<u>Fund</u>	<u>Dept</u>	<u>Unit</u>	<u>Object</u>	<u>Amount</u>	<u>Encumbrance</u>
0082	CCT	W056	4710	\$1,153,260.00	POCCT123283

Section 4. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 11

KEY FOCUS AREA: E-Gov

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Business Development & Procurement Services
Communication and Information Services

CMO: Jeanne Chipperfield, 670-7804
Mark McDaniel, 670-3256

MAPSCO: N/A

SUBJECT

Authorize supplemental agreement no. 6 to increase the service contract for upgrades to the Citizen Request Management System - Motorola Solutions, Inc. - Not to exceed \$114,000, from \$1,912,853 to \$2,026,853 - Financing: Current Funds

BACKGROUND

This increase will allow for upgrades to the existing Citizen Request Management System (CRMS). CRMS is used to log, document, route and track service requests for non-emergency services submitted to 311 by citizens via telephone or the City's website.

The updated version of CRMS will include a number of features and functions which significantly improves ease of use and enhanced ad-hoc reporting capabilities.

Improvements provided by this upgrade include:

- Improved functionality in electronically submitting service requests, photographs and service request status for mobile users
- Enhanced data file administration
- Allow the City to send service outage announcements via social media networks such as Facebook and Twitter

Upgrading the current CRMS software will enhance the manner in which service requests are logged, documented and routed. Most importantly, this upgrade will provide significant enhancements to the City's efforts in effectively responding to and resolving service requests.

During FY 14-15, approximately 52,000 service requests were entered via the City's website and approximately 28,000 service requests were entered via the City's mobile application.

BACKGROUND (Continued)

This supplemental meets the wage floor rate of \$10.37 approved by City Council on November 10, 2015 by Resolution No. 15-2141.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On June 27, 2012, City Council authorized a five-year service contract for maintenance, support and upgrades for the Citizen Request Management System by Resolution No. 12-1713.

Information about this item will be provided to the Budget, Finance and Audit Committee on January 19, 2016.

FISCAL INFORMATION

\$114,000.00 - Current Funds

ETHNIC COMPOSITION

Motorola Solutions, Inc.

White Male	3,239	White Female	962
Black Male	227	Black Female	120
Hispanic Male	349	Hispanic Female	161
Other Male	706	Other Female	297

OWNER

Motorola Solutions, Inc.

Gregory Q. Brown, Chairman
Gino A. Bonanotte, Executive Vice President

January 27, 2016

WHEREAS, on June 27, 2012, City Council authorized a five-year service contract for maintenance, support and upgrades for the Citizen Request Management System by Resolution No. 12-1713; and,

WHEREAS, on January 23, 2013, Administrative Action No. 13-5122 authorized supplemental agreement no.1 to the service contract with Motorola Solutions, Inc. to expand the original scope of effort for the service request purge process to include a requirement; and,

WHEREAS, on May 15, 2014, Administrative Action No. 14-6101 authorized supplemental agreement no. 2 to the service contract with Motorola Solutions, Inc. to add photo blocking in the citizen 311 mobile application; and,

WHEREAS, on December 15, 2014, Administrative Action No. 14-7246 authorized supplemental agreement no.3 to the service contract with Motorola Solutions, Inc. to add professional services to create an interface between the Citizen Request Management System (CRMS) and the chameleon software used at the Dallas Animal Shelter; and,

WHEREAS, on March 23, 2015, Administrative Action No. 15-5622 authorized supplemental agreement no.4 to the service contract with Motorola Solutions, Inc. to add Cognos reports and training for the Citizen Request Management System (CRMS); and,

WHEREAS, on April 23, 2015, Administrative Action No. 15-5970 authorized supplemental agreement no.5 to the service contract with Motorola Solutions, Inc. to add application enhancements to the Citizen Request Management System (CRMS);

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That following approval as to form by the City Attorney, the City Manager is hereby authorized to execute supplemental agreement no. 6 to increase the service contract for upgrades to the Citizen Request Management System with Motorola Solutions, Inc. (033581) in the amount of \$114,000.00, increasing the contract from \$1,912,853.00 to \$2,026,853.00.

January 27, 2016

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds from the following appropriation in an amount not to exceed \$114,000.00:

<u>Fund</u>	<u>Dept</u>	<u>Unit</u>	<u>Object</u>	<u>Amount</u>	<u>FY</u>	<u>Encumbrance</u>
0198	DSV	1665	3070	\$114,000.00	2016	CTDSV16CRMSV4

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 12

KEY FOCUS AREA: E-Gov

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): N/A

DEPARTMENT: City Attorney's Office

CMO: Warren M.S. Ernst, 670-3491

MAPSCO: N/A

SUBJECT

Authorize **(1)** acceptance of the 2015-16 Substance Abuse and Mental Health Services Administration grant from the U.S. Department of Health and Human Services for expansion of the South Dallas Community Court to include the South Dallas Treatment Drug Court for the Adult Treatment Drug Court Expansion project for the period September 30, 2015 through September 29, 2016, a three-year grant with the project period from September 30, 2014 to September 29, 2017; and **(2)** execution of the grant agreement - Not to exceed \$311,290 - Financing: U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration Grant Funds

BACKGROUND

This item will authorize the second year of the Substance Abuse and Mental Health Services Administration grant from the U.S. Department of Health and Human Services. The Substance Abuse and Mental Health Services Administration of the U.S. Department of Health and Human Services awarded the City a three-year grant with the project period from September 30, 2014 to September 29, 2017. The South Dallas Treatment Drug Court (SDTDC) seeks to identify adults with mental health issues and substance abuse addictions who have been referred to the South Dallas Community Court for adjudication and processing, and to offer those individuals alcohol/drug treatment, recovery support services, screening, assessment, case management and related services in lieu of jail time. The SDTDC is located in the Martin Luther King, Jr. Community Center at 2922 Martin Luther King, Jr. Boulevard, and serves South Dallas/Fair Park, Jubilee Park, and Pleasant Grove.

The U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration, has awarded the City \$311,290.00 in support of the SDTDC program for the second year.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

City Council authorized the acceptance of the 2014-15 Substance Abuse and Mental Health Services Administration grant from the U.S. Department of Health and Human Services for expansion of the South Dallas Community Court to include the South Dallas Treatment Drug Court for the Adult Treatment Drug Expansion project for the period September 30, 2014 through September 29, 2015, on January 14, 2015, by Resolution No. 15-0142.

FISCAL INFORMATION

\$311,290.00 - U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration Grant Funds

January 27, 2016

WHEREAS, the City of Dallas established an Adult Treatment Drug Court Expansion project that seeks to expand the South Dallas Community Drug Court into a Treatment Drug Court that will incorporate alcohol/drug treatment, recovery support services, screening, assessment, case management, and program coordination; and

WHEREAS, the Substance Abuse and Mental Health Services Administration of the U.S. Department of Health and Human Services awarded the City a three-year grant with the project period from September 30, 2014 to September 29, 2017; and

WHEREAS, on January 14, 2015, the City Council authorized the acceptance of the 2014-15 Substance Abuse and Mental Health Services Administration grant from the U.S. Department of Health and Human Services for expansion of the South Dallas Community Court to include the South Dallas Treatment Drug Court for the Adult Treatment Drug Expansion project for the period September 30, 2014 through September 29, 2015, by Resolution No. 15-0142; and

WHEREAS, the U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration, has made grant funds available during the 2015-16 fiscal year to offer individuals facing criminal charges for substance abuse and possession an opportunity to enter a substance abuse recovery program in lieu of jail time; and

WHEREAS, such grant funding was awarded to the City on September 30, 2015; and

WHEREAS, the grant funding would benefit the City in its endeavor to reduce crime and help affected individuals become drug free; and

WHEREAS, the City finds it in the best interest of the City's residents that the grant funds be accepted for the 2015-16 fiscal year; and

WHEREAS, the City agrees that in the event of loss or misuse of the grant funds, the City will return the funds identified as ineligible to the U.S. Department of Health and Human Services; and

WHEREAS, the City designates the City Manager or his designee as the grantee's authorized official, who has the power to apply for, accept, reject, alter, or terminate the grant on behalf of the City; **Now, Therefore,**

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

January 27, 2016

Section 1. That the City Manager is hereby authorized to accept the 2015-16 U.S. Department of Health and Human Services Administration Grant No. 5H79T1025045-02, CFDA No. 93.243, to be used in the Adult Treatment Drug Court Expansion project for the period September 30, 2015, through September 29, 2016, in an amount not to exceed \$311,290.00 and execute the grant agreement.

Section 2. That the City Manager is hereby authorized to establish appropriations in the Adult Treatment Drug Court Expansion Project fund in the amount of \$311,290.00 in Fund F447, Department ATT, Unit 1827.

Section 3. That the Chief Financial Officer is authorized to deposit grant funds in an amount not to exceed \$311,290.00 into Fund F447, Department ATT, Unit 1827, Revenue Source 6506.

Section 4. That the Chief Financial Officer is authorized to disburse grant funds from Fund F447, Department ATT, Unit 1827, in accordance to the attached schedule, in an amount not to exceed \$311,290.00.

Section 5. That the City Manager is hereby authorized to reimburse the U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration any expenditure identified as ineligible. The City Manager shall notify the appropriate City Council Committee of expenditures identified as ineligible not later than 30 days after the reimbursement.

Section 6. That the City Manager shall keep the appropriate City Council Committee informed of all final U.S. Department of Health and Human Services, Substance Abuse and Mental Health Services Administration monitoring reports not later than 30 days after the receipt of the report.

Section 7. That this resolution shall take effect immediately from and after its passage in accordance with the Charter of the City of Dallas, and it is accordingly so resolved.

SAMHSA - Adult Drug Court		
Fund F447		
Unit 1827		
FY 2015-2016		
ATT		BUDGET FY 15-16
<i>Obj.</i>		311,290
1101	Salaries	100,400
1301	Pension	13,072
1304	Health Insurance	10,172
1306	FICA	1,456
2110	Office Supplies	3,600
2252	Metered Postage	300
2280	Other Supplies	400
3070	Professional Services	153,848
3092	Security Services	13,300
3361	Professional Development	8,718
3851	Pension Bond Debt	6,024

AGENDA ITEM # 13

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 14

DEPARTMENT: City Attorney's Office
Public Works Department

CMO: Warren M.S. Ernst, 670-3491
Jill A. Jordan, P.E., 670-5299

MAPSCO: N/A

SUBJECT

Authorize settlement of the condemnation lawsuit styled City of Dallas v. Pan Coastal Limited Partnership, et al., Cause No. CC-12-06225-C - Not to exceed \$34,303, increased from \$116,997 (\$115,697 plus closing costs and title expenses not to exceed \$1,300) to \$151,300 (total settlement amount) - Financing: 2003 Bond Funds

BACKGROUND

This item authorizes the settlement of a lawsuit for the acquisition of approximately 2,736 square feet of land out of an approximately 42,435 square foot tract for the Central Expressway from Commerce to Live Oak Realignment Project. A final offer of \$110,000 was made based on a written appraisal from an independent certified appraiser. The offer was not accepted by the owners and the City filed an eminent domain proceeding to acquire the land. The City's appraiser updated his report for the hearing and testified that the City owed \$115,000 for the property. The owners did not attend the hearing. After the hearing, the Special Commissioners awarded the owners \$115,696.46, which the City deposited into the registry of the Court. The owners filed objections to the award of the Special Commissioners.

The City obtained a new appraisal as of the date of the taking, which opined that the City owed \$116,000 for the property. After settlement discussions, the parties agreed to the settlement of the lawsuit at \$151,300.00, subject to City Council approval. The settlement will require the payment of an additional \$34,303.54 to acquire the property.

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS)

City Council authorized the acquisition of real property and condemnation on June 27, 2012, by Resolution No. 12-1672.

City Council authorized deposit of the amount awarded by the Special Commissioners for the acquisition of the property on August 14, 2013, by Resolution No. 13-1311.

Council will be briefed by memorandum regarding this item.

FISCAL INFORMATION

2003 Bond Funds - \$34,303.54

Resolution No. 13-1311	\$116,996.46
Additional Amount (this action)	<u>\$ 34,303.54</u>
Total Settlement Amount	\$151,300.00

OWNER

Pan Coastal Limited Partnership

Southstate Management Corporation

Don Shenoy, President

January 27, 2016

A RESOLUTION AUTHORIZING SETTLEMENT OF A CONDEMNATION SUIT.

IN THIS RESOLUTION THE FOLLOWING DEFINITIONS SHALL APPLY:

CONDEMNATION SUIT: Cause No. CC-12-06225-C, in Dallas County Court at Law No. 3, and styled City of Dallas v. Pan Coastal Limited Partnership, et al., filed in accordance with City Council Resolution No. 12-1672.

PROPERTY: Approximately 2,736 square feet of land located in Dallas County, as described in the CONDEMNATION SUIT.

PROJECT: Central Expressway from Commerce to Live Oak Realignment Project.

OFFER: \$110,000.00.

COMMISSIONERS' AWARD: \$115,696.46 (Res. No. 13-1311).

CLOSING COSTS AND TITLE EXPENSES: Not to exceed \$1,300.00.

ORIGINAL AUTHORIZED AMOUNT: \$116,996.46 (Res. No. 12-1672).

SETTLEMENT AMOUNT: \$151,300.00 which includes the ORIGINAL AUTHORIZED AMOUNT and the ADDITIONAL AMOUNT.

ADDITIONAL AMOUNT: \$34,303.54 which is the difference between the SETTLEMENT AMOUNT and the ORIGINAL AUTHORIZED AMOUNT.

DESIGNATED FUNDS: Street and Transportation Improvements Fund, Fund No. 4R22, Unit U779, Department PBW, Activity No. THRF, Program No. PB06U779, Object 4210, and Encumbrance No. CT-SUSU779PM23.

WHEREAS, Defendants have agreed to settle the CONDEMNATION SUIT for the SETTLEMENT AMOUNT; and

WHEREAS, the City Council desires to authorize the City Attorney to settle the CONDEMNATION SUIT; **Now, Therefore**,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Attorney and the City Manager are authorized to settle the CONDEMNATION SUIT for the SETTLEMENT AMOUNT.

January 27, 2016

Section 2. That the City Attorney and the City Manager are authorized to prepare and execute such documents as may be necessary to effect the settlement described herein.

Section 3. That the Chief Financial Officer is hereby authorized and directed to issue a check, paid out of and charged to the DESIGNATED FUNDS, for the ADDITIONAL AMOUNT payable to the County Clerk of Dallas County, Texas to be deposited by the City Attorney with the Clerk.

Section 4. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

APPROVED AS TO FORM
WARREN M.S. ERNST, City Attorney

By Christopher C. Ernst
Assistant City Attorney

AGENDA ITEM # 14

KEY FOCUS AREA: E-Gov

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): N/A

DEPARTMENT: City Attorney's Office
Aviation

CMO: Warren M.S. Ernst, 670-3491
Ryan S. Evans, 671-9837

MAPSCO: N/A

SUBJECT

Authorize Supplemental Agreement No. 2 to the professional services contract with Kaplan Kirsch & Rockwell, L.L.P. for additional services on matters relating to a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field - Not to exceed \$25,000, from \$90,000 to \$115,000 - Financing: Aviation Current Funds

BACKGROUND

Supplemental Agreement No. 2 will authorize Kaplan Kirsch & Rockwell, L.L.P. to continue providing legal services to the City in a Federal Aviation Administration Part 16 Investigation.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Council was briefed in Closed Session on September 9, 2015.

Council was briefed by memorandum on November 3, 2015 regarding this item.

Council authorized Supplemental Agreement No. 1 with Kaplan Kirsch & Rockwell, L.L.P. for additional services on matters relating to a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field on November 10, 2015, by Resolution No. 15-2135.

Council will be briefed by memorandum regarding this item.

FISCAL INFORMATION

\$25,000.00 - Aviation Current Funds

M/WBE INFORMATION

The recommended awardee has fulfilled the good faith requirements set forth in the Business Inclusion and Development (BID) Plan adopted by Resolution No. 08-2826, as amended.

OWNER

Kaplan Kirsch & Rockwell, L.L.P.

Peter J. Kirsch, Partner

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize Supplemental Agreement No. 2 to the professional services contract with Kaplan Kirsch & Rockwell, L.L.P. for additional services on matters relating to a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field - Not to exceed \$25,000, from \$90,000 to \$115,000 - Financing: Aviation Current Funds

Kaplan Kirsch & Rockwell, L.L.P. is a non-local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use their own workforce.

PROJECT CATEGORY: Professional Services

LOCAL/NON-LOCAL CONTRACT SUMMARY - THIS ACTION ONLY

	<u>Amount</u>	<u>Percent</u>
Local contracts	\$0.00	0.00%
Non-local contracts	\$25,000.00	100.00%
TOTAL THIS ACTION	\$25,000.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION THIS ACTION

Local Contractors / Sub-Contractors

None

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE PARTICIPATION

	<u>This Action</u>		<u>Participation to Date</u>	
	<u>Amount</u>	<u>Percent</u>	<u>Amount</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$0.00	0.00%
Total	\$0.00	0.00%	\$0.00	0.00%

January 27, 2016

WHEREAS, on October 27, 2015, pursuant to Administrative Action No. 15-6990, the City authorized a professional services contract with Kaplan Kirsch & Rockwell, L.L.P., in an amount not to exceed \$50,000.00 to represent the City in a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field; and,

WHEREAS, on November 10, 2015, the City Council authorized Supplemental Agreement No. 1, in the amount of \$40,000.00, with Kaplan Kirsch & Rockwell, L.L.P. by Resolution No. 15-2135, for additional services on matters relating to a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field; and,

WHEREAS, the professional services of Kaplan Kirsch & Rockwell, L.L.P. continue to be necessary; **Now, Therefore,**

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That, following approval as to form by the City Attorney, the City Manager is hereby authorized to enter into Supplemental Agreement No. 2 to the professional services contract with Kaplan Kirsch & Rockwell, L.L.P. for additional services on matters relating to a Federal Aviation Administration Part 16 Investigation regarding the City's gate allocation policy at Dallas Love Field in an amount not to exceed \$25,000.00, increasing the contract amount from \$90,000.00 to \$115,000.00.

Section 2. That the Chief Financial Officer is authorized to disburse, in periodic payments to Kaplan Kirsch & Rockwell, L.L.P., an amount not to exceed \$25,000.00 from Fund 0130, Department AVI, Unit 7710, Obj. 3033, Encumbrance No. CTAVIKKR7710FY16, Vendor No. VC0000012035.

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 6

DEPARTMENT: Housing/Community Services

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: 43F 43M

SUBJECT

Authorize an amendment to Resolution No. 13-2115, previously approved on December 11, 2013, to extend the completion date for an affordable housing development conditional grant in the amount of \$180,000 to Builders of Hope CDC for the construction of six single family homes in West Dallas from December 11, 2015 to December 30, 2016 – Financing: No cost consideration to the City

BACKGROUND

In September 2013, Builders of Hope CDC (BOH) submitted a proposal to the City of Dallas for the construction of six (6) affordable housing units located on scattered sites throughout West Dallas. The proposal requested \$30,000 per home in construction financing leveraged with \$85,000 per home from the Rees-Jones Foundation for the construction of the homes to be built on the lots and sold by December 11, 2015.

In August 2015, BOH had a change in executive staff that resulted in some delays in the completion of this project. To-date, BOH has built and sold five of the six units to low-moderate income families at or below 80% of Area Median Family Income. BOH has completed 95% of the construction of the remaining unit. When completed, BOH may proceed with the sale of the unit to an approved homebuyer.

The extension of the maturity date in the loan agreement will allow BOH to continue its efforts to complete construction and sell the remaining unit.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On June 27, 2001, City Council approved the CHDO Loan Program Statement that authorized the Housing Director to approve development funding for projects with fewer than 100 units, by Resolution No. 01-2049.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS) (Continued)

On June 26, 2013, City Council approved the City of Dallas Consolidated Plan Budget for FY 2013-14 federal funds which included the HOME Investment Partnership Funds, by Resolution No. 13-1142.

On December 11, 2013, City Council approved an affordable housing development conditional grant in the amount of \$180,000 to Builders of Hope CDC for the construction of six (6) single family homes in West Dallas through December 11, 2015, by Resolution No. 13-2115.

Information about this item will be provided to the Housing Committee on January 19, 2016.

FISCAL INFORMATION

No cost consideration to the City

OWNER

Builders of Hope CDC

Damon Polk, Interim President

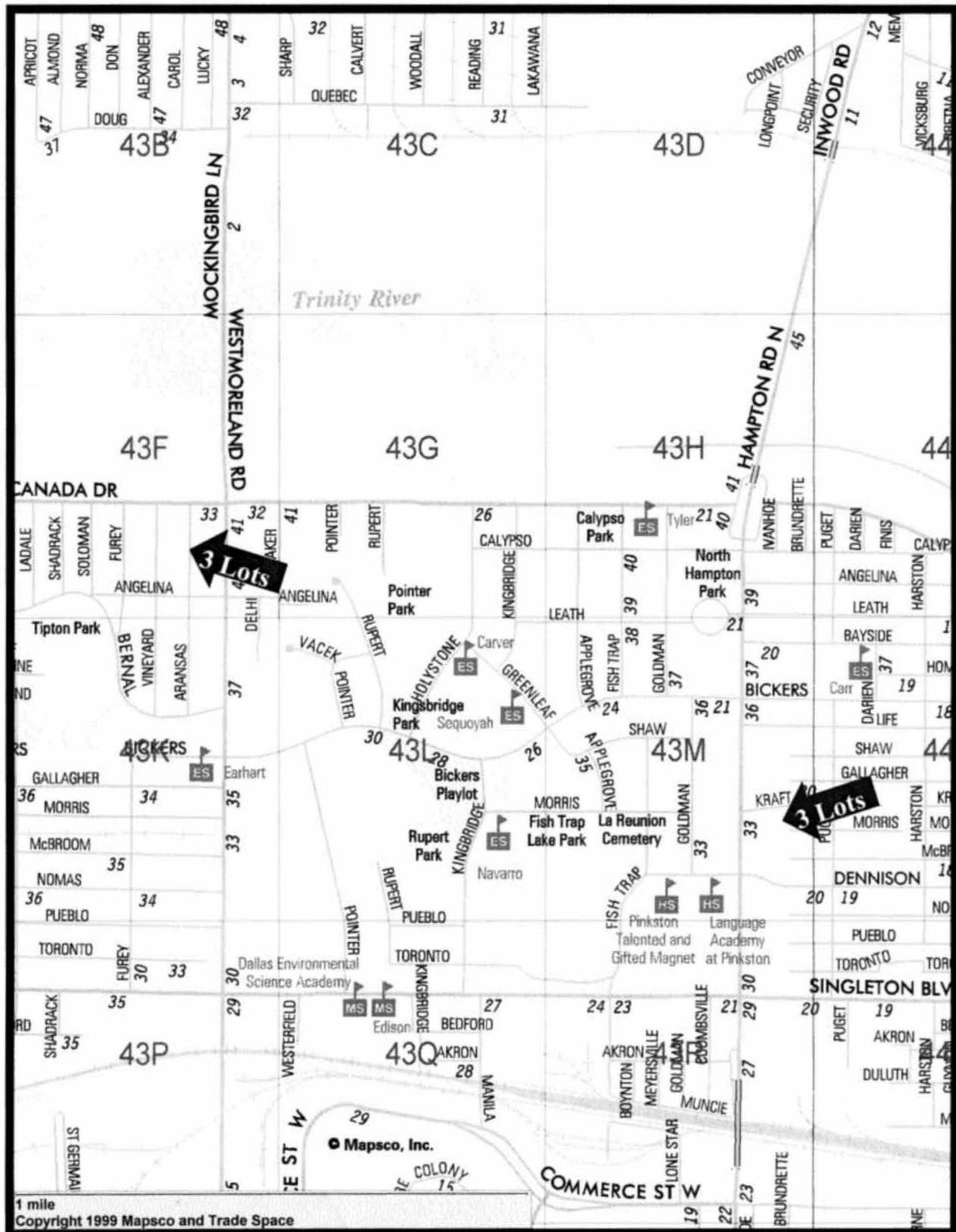
DEVELOPER

Builders of Hope CDC

Damon Polk, Interim President

MAP

Attached



MAPSCO 43F & 43M

January 27, 2016

WHEREAS, affordable housing for low and moderate income families is a high priority of the City of Dallas; and

WHEREAS, on June 27, 2001, City Council approved the CHDO Loan Program Statement that authorized the Housing Director to approve development funding for projects with fewer than 100 units, by Resolution No. 01-2049; and

WHEREAS, on June 26, 2013, City Council approved the City of Dallas Consolidated Plan Budget for FY 2013-14 federal funds which included the HOME Investment Partnership Funds, by Resolution No. 13-1142; and

WHEREAS, on December 11, 2013, City Council approved an affordable housing development conditional grant in the amount of \$180,000 to Builders of Hope CDC for the construction of six (6) single family homes in West Dallas through December 11, 2015, by Resolution No. 13-2115; and

WHEREAS, Builders of Hope CDC has requested an extension of time to the development grant agreement to allow them to continue their efforts to construct the final unit and complete the sale of the unit to low-to-moderate income family at or below 80% AMFI; **NOW, THEREFORE**,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That following approval as to form by the City Attorney, the City Manager is authorized to amend the grant agreement with Builders of Hope CDC (BOH) to extend the completion date from December 11, 2015 to December 30, 2016.

Section 2. That the terms of the agreement will include:

- (a) Borrower must execute a Notes Payable and Deed Restrictions.
- (b) BOH must complete and sell or rent the remaining homes to low-to-moderate income families whose incomes are 80% or less of area median family income by the maturity date.
- (c) Borrower will be released from liability on the Note at the same time the low-to-moderate income family closes the purchase on the home.
- (d) Properties will be deed restricted for affordability at 80% or less of Area Median Family Income for sale and resale to eligible homebuyers for a period of fifteen years.
- (e) The City will subordinate to the interim construction lender for the construction of each unit.

January 27, 2016

Section 3. That the City Manager, upon approval as to form by the City Attorney, is authorized to execute the agreement described herein, as well as releases of liens and terminations of deed restrictions on the property upon compliance with the loan terms and deed restrictions for forgiveness of debt.

Section 4. That this resolution does not constitute a binding agreement upon the City or subject the City to any liability or obligation with respect to the loan, until such time as the loan documents are duly approved by all parties and executed.

Section 5. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 7

DEPARTMENT: Housing/Community Services

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: 48V

SUBJECT

Authorize an amendment to Resolution No. 14-1488, previously approved on September 10, 2014, to extend the completion date of each agreement to December 31, 2016, for two separate loan agreements with Builders of Hope CDC for two separate phases of their Prairie Creek project – Financing: No cost consideration to the City

BACKGROUND

In 2009, this project received HOME funds in the amount of \$500,000 for acquisition of 40 lots for the Prairie Creek project. This loan agreement, as amended, calls for 40 single family homes to be built on the lots and sold by December 31, 2015. Additionally, by Resolution No.14-1488, this project also received \$480,000 as gap financing for the construction of sixteen of the 40 homes. This loan agreement required completion and sale of the sixteen units by December 31, 2015.

In August 2015, Builders of Hope CDC (BOH) had a change in executive staff that resulted in some delay in the completion of this project. To-date, BOH has built and sold 24 of the 40 units to low-moderate income families at or below 80% of Area Median Family Income. BOH has obtained interim financing from Benchmark Bank, Inwood Bank and American National Bank for construction of the remaining sixteen units and has pulled permits and began construction of nine of the sixteen units

The extension of the maturity dates of the two loan agreements will allow BOH to continue its efforts to complete construction and sell the remaining sixteen units.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On June 27, 2001, City Council approved the CHDO Loan Program Statement that authorized the Housing Director to approve development funding for projects with fewer than 100 units, by Resolution No. 01-2049.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS) (Continued)

On September 10, 2014, City Council approved (1) an extension of the development loan agreement with Builders of Hope CDC, Prairie Creek Project for acquisition and related acquisition costs of 40 unimproved properties to December 31, 2015; and (2) a development loan in the amount of \$480,000 for the construction of the final sixteen homes, by Resolution No. 14-1488.

Information about this item will be provided to the Housing Committee on January 19, 2016.

FISCAL INFORMATION

No cost consideration to the city

OWNER

Builders of Hope CDC

Damon Polk, Interim President

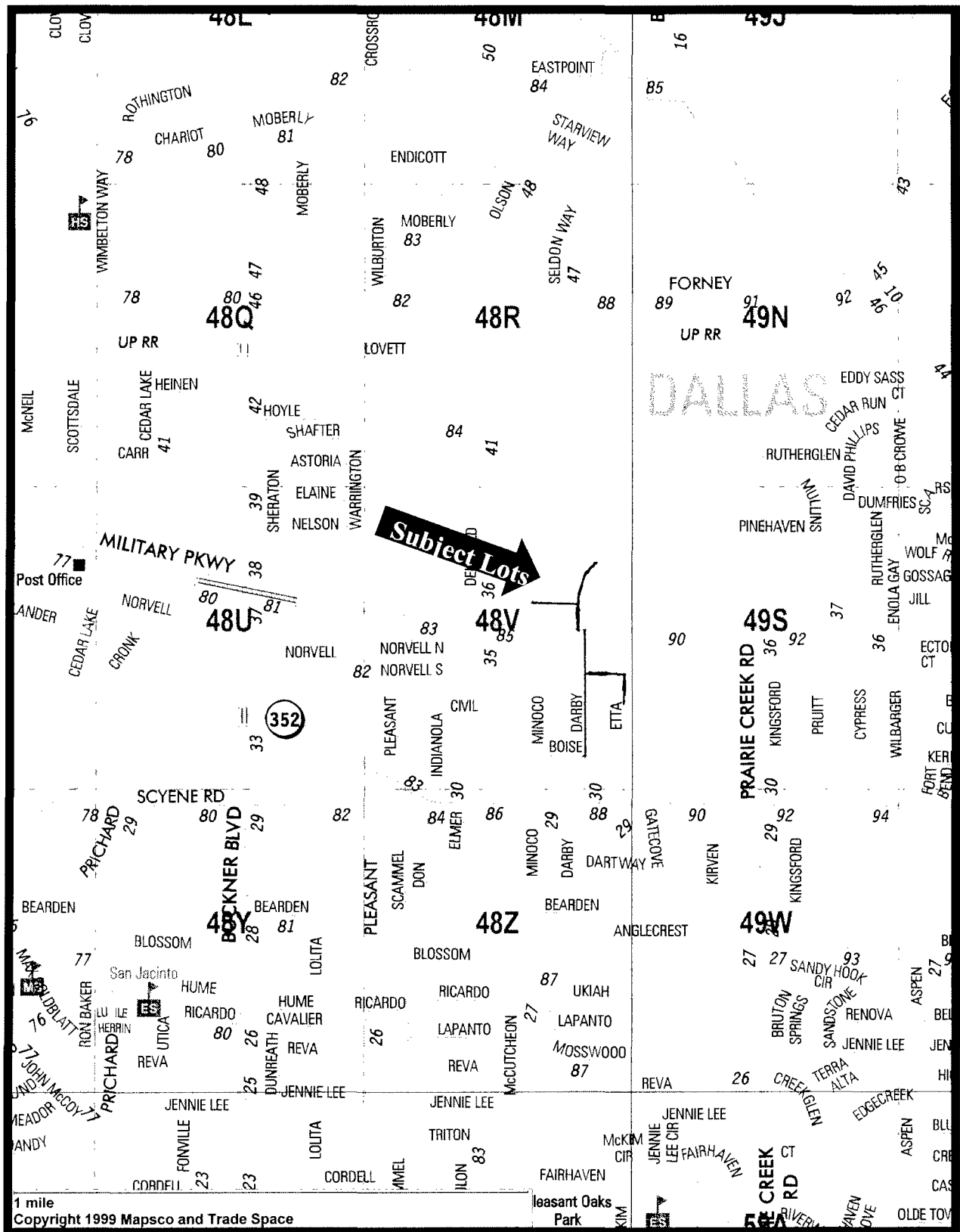
DEVELOPER

Builders of Hope CDC

Damon Polk, Interim President

MAP

Attached



MAPSCO 48V

January 27, 2016

WHEREAS, affordable housing for low and moderate income families is a high priority of the City of Dallas; and

WHEREAS, on September 10, 2014, the City Council approved (1) an extension of the development loan agreement with Builders of Hope CDC, Prairie Creek Project for acquisition and related acquisition costs of 40 unimproved properties to December 31, 2015; and (2) a development loan in the amount of \$480,000 for the construction of the final sixteen homes, by Resolution No. 14-1488; and

WHEREAS, Builders of Hope CDC has requested an extension of time to the development loan agreement to allow them to continue their efforts to construct the final 16 units and complete the sale of the units to low-to-moderate income families at or below 80% AMFI; **NOW, THEREFORE**,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That following approval as to form by the City Attorney, the City Manager is authorized to amend two separate loan agreements with Builders of Hope CDC (BOH), for two separate phases of their Prairie Creek project, to extend the completion dates of each agreement to December 31, 2016.

Section 2. That the terms of the agreement will include:

- (a) Borrower must execute a Notes Payable and Deed Restrictions.
- (b) BOH must complete and sell or rent the remaining homes to low-to-moderate income families whose incomes are 80% or less of area median family income by the maturity date.
- (c) Borrower will be released from liability on the Note at the same time the low-to-moderate income family closes the purchase on the home.
- (d) Properties will be deed restricted for affordability at 80% or less of Area Median Family Income for sale and resale to eligible homebuyers for a period of fifteen years.
- (e) The City will subordinate to the interim construction lender for the construction of each unit.

Section 3. That the City Manager, upon approval as to form by the City Attorney, is authorized to execute the agreement described herein, as well as releases of liens and terminations of deed restrictions on the property upon compliance with the loan terms and deed restrictions for forgiveness of debt.

Section 4. That this resolution does not constitute a binding agreement upon the City or subject the City to any liability or obligation with respect to the loan, until such time as the loan documents are duly approved by all parties and executed.

January 27, 2016

Section 5. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 17

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 6

DEPARTMENT: Housing/Community Services

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: 43E 43F 43K 43P 44J 44K 44N

SUBJECT

Authorize an amendment to Resolution No. 14-1633, previously approved on September 24, 2014, to extend the completion date of each agreement to December 31, 2016, for two separate loan agreements with Builders of Hope CDC for two separate phases of their West Dallas project - Financing: No cost consideration to the City

BACKGROUND

In 2009 this project received HOME funds in the amount of \$547,000 for acquisition of 22 scattered sites lots in West Dallas. This loan agreement, as amended, calls for 22 single family homes to be built on the lots and sold by December 31, 2015. By Resolution No. 14-1633, this project also received \$521,400 as gap financing for the construction eleven of the 22 homes. This loan agreement requires completion and sale of its eleven units by December 31, 2015.

In August 2015, Builders of Hope CDC (BOH) had a change in executive staff that resulted in some delays in the completion of this project. To-date, BOH has built and sold eleven of the 22 units to low-moderate income families at or below 80% of Area Median Family Income. BOH has obtained interim financing from Inwood Bank for construction of the remaining eleven units and has pulled permits for eight units. Seven units are already under construction.

The extension of the maturity dates in the two loan agreements will allow BOH to continue its efforts to complete construction and sell the remaining eleven units.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On June 27, 2001, City Council approved the CHDO Loan Program Statement that authorized the Housing Director to approve development funding for projects with fewer than 100 units, by Resolution No. 01-2049.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS) (continued)

On September 24, 2014, City Council approved **(1)** an extension of the development loan agreement with Builders of Hope CDC, West Dallas Project for acquisition and related acquisition costs of 22 unimproved properties to December 31, 2015; and **(2)** a development loan in the amount of \$521,400 for the construction of the final eleven homes, by Resolution No. 14-1633.

Information about this item will be provided to the Housing Committee on January 19, 2016.

FISCAL INFORMATION

No cost consideration to the City

OWNER

Builders of Hope CDC

Damon Polk, Interim President

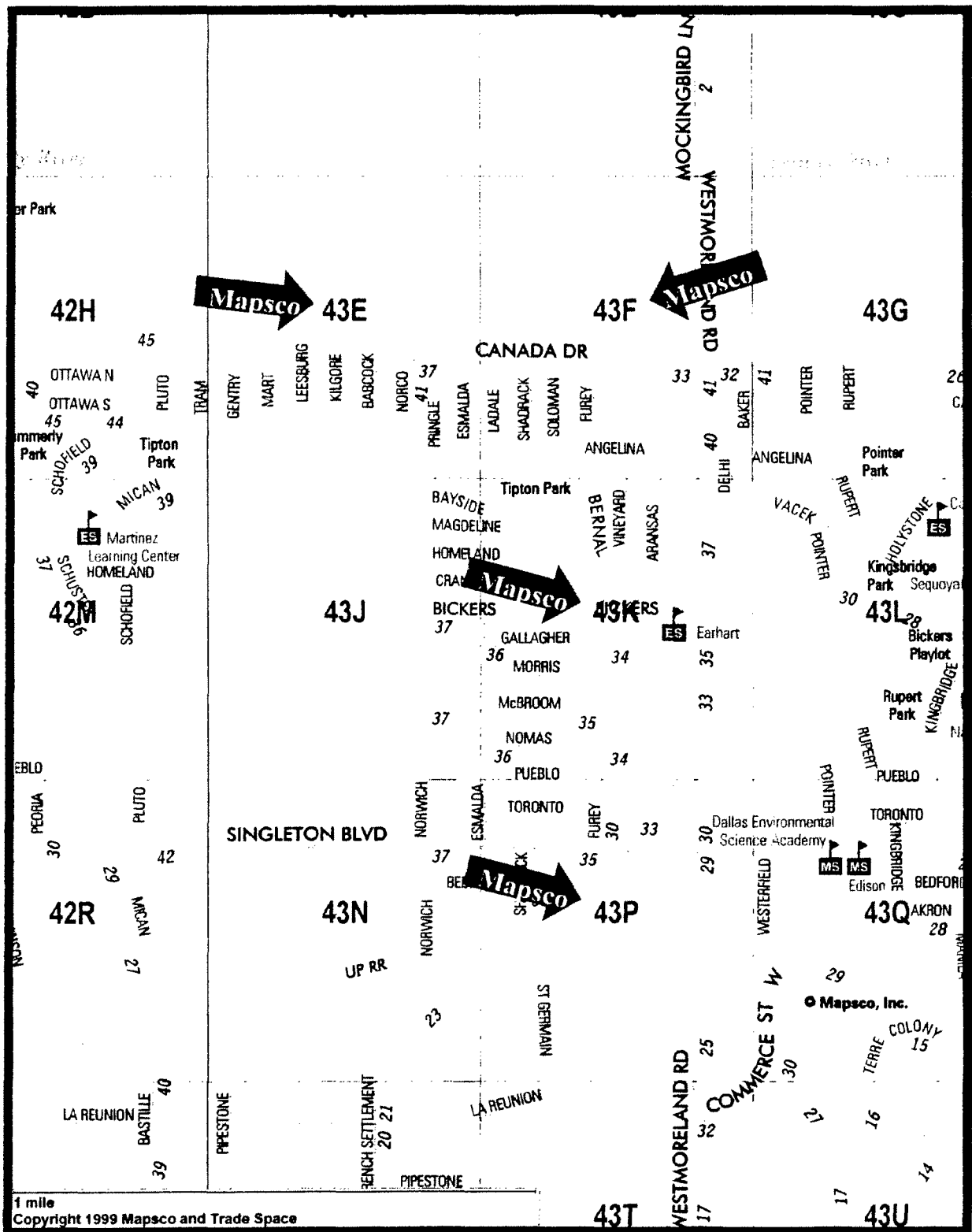
DEVELOPER

Builders of Hope CDC

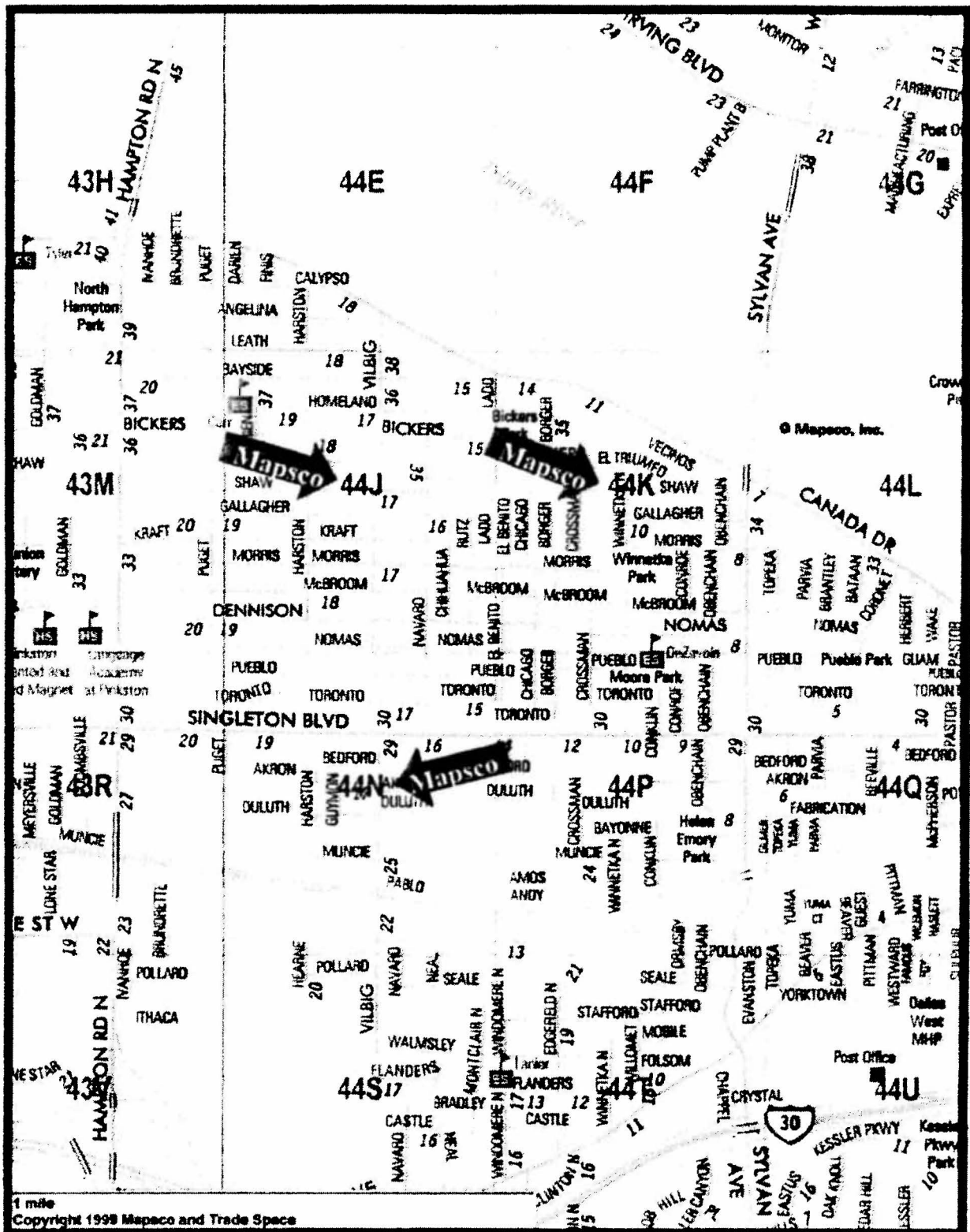
Damon Polk, Interim President

MAP

Attached



MAPSCO 43E, F, K & P



MAPSCO 44J, K & N

January 27, 2016

WHEREAS, affordable housing for low and moderate income families is a high priority of the City of Dallas; and

WHEREAS, on June 27, 2001, City Council approved the CHDO Loan Program Statement that authorized the Housing Director to approve development funding for projects with fewer than 100 units, by Resolution No. 01-2049; and

WHEREAS, on September 24, 2014, City Council approved (1) an extension of the development loan agreement with Builders of Hope CDC (BOH), West Dallas Project for acquisition and related acquisition costs of 22 unimproved properties to December 31, 2015; and (2) a development loan in the amount of \$521,400 for the construction of the final eleven homes, by Resolution No. 14-1633; and

WHEREAS, the City desires for BOH to develop affordable units for low and moderate income families; **NOW, THEREFORE**,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That following approval as to form by the City Attorney, the City Manager is authorized to amend two separate loan agreements with Builders of Hope CDC, for two separate phases of their West Dallas project, to extend completion dates of each agreement to December 31, 2016.

Section 2. The terms of the agreement will include:

- (a) Borrower must execute a Notes Payable and Deed Restrictions.
- (b) BOH must complete and sell or rent the remaining homes to low-to-moderate income families whose incomes are 80% or less of area median family income by the maturity date.
- (c) Borrower will be released from liability on the Note at the same time the low-to-moderate income family closes the purchase on the home.
- (d) Properties will be deed restricted for affordability at 80% or less of Area Median Family Income for sale and resale to eligible homebuyers for a period of fifteen years.
- (e) The City will subordinate to the interim construction lender for the construction of each unit.

Section 3. That the City Manager, upon approval as to form by the City Attorney, is authorized to execute the agreement described herein, as well as releases of liens and terminations of deed restrictions on the property upon compliance with the loan terms and deed restrictions for forgiveness of debt.

Section 4. That this resolution does not constitute a binding agreement upon the City or subject the City to any liability or obligation with respect to the loan, until such time as the loan documents are duly approved by all parties and executed.

January 27, 2016

Section 5. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 18

KEY FOCUS AREA: Culture, Arts and Recreation and Educational Enhancements

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Library

CMO: Joey Zapata, 670-1204

MAPSCO: N/A

SUBJECT

Authorize **(1)** the acceptance of a grant from the Association for Library Service to Children (ALSC), a division of the American Library Association and funded by the Dollar General Literacy Foundation: the 2016 Building STEAM with Día mini-grant in the amount of \$2,000 for the period January 1, 2016 to May 31, 2016; **(2)** the establishment of appropriations in an amount not to exceed \$2,000; and **(3)** execution of the grant agreement - Total not to exceed \$2,000 - Financing: Association for Library Service to Children

BACKGROUND

Building STEAM with Día, also part of the El día de los niños/El día de los libros (Children's Day/Book Day), is a nationally recognized initiative that emphasizes the importance of literacy for all children from all backgrounds. The purpose of this mini-grant is to encourage libraries to offer Building STEAM with Día programs in their community that focus on culturally diverse and appropriate STEAM (Science, Technology, Engineering, Art, and Math) activities and to strengthen the opportunity to link children and their families to diverse books, languages and cultures.

The Dallas Public Library's Arcadia Park Branch was one of ten libraries awarded the mini-grant to implement this Día program. The Arcadia Park Branch will provide programs to the community that will link children and their families to diverse books, languages and cultures. Funding granted from the American Library Association-ALSC that was funded by the Dollar General Literacy Foundation will cover costs associated with the purchase of books to be given to participants, purchase program supplies for program outreach and activities.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Arts, Culture and Libraries Committee on January 19, 2016.

FISCAL INFORMATION

\$2,000.00 - Association for Library Service to Children

January 27, 2016

WHEREAS, the Association for Library Service to Children (ALSC) has awarded the City of Dallas grant funds to enhance the Dallas Public Library's children's literacy efforts; and

WHEREAS, funds have been awarded to the City of Dallas for the Dallas Public Library for such programs and initiatives that will promote literacy for all children from all backgrounds; and

WHEREAS, it is recommended that the City Manager be authorized to accept the grant funds in an amount not to exceed \$2,000.00 for the period January 1, 2016 through May 31, 2016.

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to accept grant funds from the Association for Library Service to Children (ALSC), a division of the American Library Association and funded by the Dollar General Literacy Foundation in an amount not to exceed \$2,000.00 for the period January 1, 2016 through May 31, 2016 and execute the grant agreement.

Section 2. That the Chief Financial Officer is hereby authorized to receive and deposit award funds into Fund P116, Department LIB, Unit 1826, various object codes in an amount not to exceed \$2,000.00.

Section 3. That the City Manager is hereby authorized to establish appropriations in Fund P116, Department LIB, Unit 1826, various object codes in an amount not to exceed \$2,000.00 per attached Schedule.

Section 4. That the Chief Financial Officer is hereby authorized to disburse funds from Fund P116, Department LIB, Unit 1826, various object codes, not to exceed \$2,000.00 per attached Schedule.

Section 5. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

Schedule

2016 Building STEAM

Fund: P116, Unit 1826

Object Code	Description	Amount
2110	Office Supplies	\$100
2232	Food Supplies	\$100
2280	Other Materials (Program Supplies)	\$1,400
2860	Give Away books	\$300
3030	Printing (Promotion)	\$100
Total		\$2,000

AGENDA ITEM # 19

KEY FOCUS AREA: Culture, Arts and Recreation and Educational Enhancements

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 7

DEPARTMENT: Office of Cultural Affairs

CMO: Joey Zapata, 670-1204

MAPSCO: 46-P

SUBJECT

Authorize a contract with the Museum of African American Life and Culture for services provided to the City through the Cultural Services Contracts Program for the period October 1, 2015 through September 30, 2016 - Not to exceed \$102,222 - Financing: Current Funds

BACKGROUND

On October 28, 2015, Resolution No. 15-1949 approved funds in the FY 2015-16 operating budget for services procured from cultural organizations. The Office of Cultural Affairs manages the Cultural Services Contracts Program that provides operating support to eligible organizations in exchange for cultural services to the citizens of Dallas. Organizations submit annual proposals for funding, which are reviewed by the Office of Cultural Affairs and the Cultural Affairs Commission. Funding recommendations are made to City Council by the Cultural Affairs Commission.

At their September 17, 2015 meeting, the Cultural Affairs Commission reviewed and recommended the Museum for African American Life and Culture for a FY 2015-16 cultural services contract in the amount of \$102,222 for cultural services to the citizens of Dallas, pending completion of their FY 2014-15 contract and submission of the Museum's FY 2014 audited financial statements. The Museum submitted the FY 2014 audited financial statements on December 10, 2015 and has completed the FY 2014-15 contract. The Museum is now eligible and compliant to be paid for the FY 2015-16 services through the cultural services contract authorized by this action.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSION)

On September 17, 2015, the Cultural Affairs Commission approved FY 2015-16 Cultural Organization Program funding.

Information about this item will be provided to the Arts, Culture and Libraries Committee on January 19, 2016.

FISCAL INFORMATION

\$102,222.00 - Current Funds

ETHNIC COMPOSITION

Museum of African American Life and Culture

Staff

African American Male	11	African American Female	16
Asian Male	0	Asian Female	0
Hispanic Male	3	Hispanic Female	1
White Male	3	White Female	0
Other Male	0		

Board

African American Male	23	African American Female	25
Asian Male	0	Asian Female	0
Hispanic Male	0	Hispanic Female	0
White Male	1	White Female	0
Native Hawaiian Male	1		

OWNER

Museum of African American Life and Culture

Wright L. Lassiter, Jr., Board Chair

January 27, 2016

WHEREAS, on October 28, 2015, by Resolution No. 15-1949, City Council approved funds in the FY 2015-16 Operating Budget for the procurement of services from cultural organizations; and

WHEREAS, the Cultural Services Contracts Program provides operating support to eligible organizations in exchange for cultural services to the citizens of Dallas; and

WHEREAS, the Museum of African American Life and Culture was approved for funding through the Cultural Services Program for services provided during FY 2015-16; and

WHEREAS, it is necessary for the City to define services to be received through the Cultural Services Program and authorize payment of these funding allocations for the FY 2015-16 fiscal year.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That following approval as to form by the City Attorney, the City Manager is hereby authorized to enter into a contract with the Museum of African American Life and Culture for the provision of services to the City through the Cultural Services Contracts Program in an amount not to exceed \$102,222.

Section 2. That the Chief Financial Officer is authorized to disburse funds to the Museum of African American Life and Culture in the following amount:

<u>FUND</u>	<u>DEPT</u>	<u>UNIT</u>	<u>OBJECT</u>	<u>VENDOR</u>	<u>ENCUMBRANCE</u>	<u>AMOUNT</u>
0001	OCA	4836	3089	219251	MOCA4836A034	\$102,222

Section 3. That the following services, at a minimum, are to be performed by the Cultural Agency for the period October 1, 2015 through September 30, 2016:

Scope of Services

The cultural agency will serve 198,000 people by operating the Museum of African American Life and Culture in Fair Park. The museum will host three exhibitions, five lectures, four workshops and a museum summer camp for youth. The museum will also present the following programs: "Music under the Dome" concerts, African American History Bowl, Preservation Workshop, the Carol Harris Simms Competition and present the exhibitions, two popular exhibitions Facing the Rising Sun and Sister Gertrude: New Orleans Folk Artist, and two major exhibitions; African Vessels and Witness: The Art of Jerry Pinkney.

January 27, 2016

Section 3. (Continued)

Also provided is a Distinguished Lecture series and two celebrations that provide opportunities to collaborate with the public and school libraries of DFW.

Section 4. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 20

KEY FOCUS AREA: Culture, Arts and Recreation and Educational Enhancements

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2, 8, 13

DEPARTMENT: Park & Recreation

CMO: Willis Winters, 670-4071

MAPSCO: 23-B 33-V 57-V

SUBJECT

Authorize an increase in the contract with DENCO Construction Specialists Corporation for structural repairs and other miscellaneous items at three recreation centers: Arlington Park Recreation Center located at 1505 Record Crossing Road; Janie C. Turner Recreation Center located at 6424 Elam Road; and Marcus Recreation Center located at 3003 Northaven Road - Not to exceed \$97,276, from \$389,600 to \$486,876 - Financing: 2003 Bond Funds (\$77,535) and 2006 Bond Funds (\$19,741)

BACKGROUND

The original contract was awarded to DENCO Construction Specialists Corporation by City Council on November 12, 2014, by Resolution No. 14-1978 for structural repair and miscellaneous work at Arlington Park Recreation Center; Janie C. Turner Recreation Center; and Marcus Recreation Center, in an amount not to exceed \$389,600.

This action is to authorize Change Order No. 1 to the contract with DENCO Construction Specialists Corporation, in an amount not to exceed \$97,275.80, increasing the original contract amount from \$389,600.00 to \$486,875.80 with DENCO Construction Specialists Corporation for structural repairs and other miscellaneous items at three recreation centers to include: installation of retaining wall, asbestos and lead based paint abatement, add joint sealants and installation of new floor tile at Arlington Park Recreation Center; reroute French drain line, installation of cleanouts for existing sewer line and installation of irrigation system branch line to stabilize gym foundation at Janie C. Turner Recreation Center; underpin plumbing drains, installation of new downspout and paint a portion of gym interior wall at Marcus Recreation Center.

ESTIMATED SCHEDULE OF PROJECT

Began Construction	February 2015
Complete Construction	August 2016

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

The Park and Recreation Board authorized proceeding with advertisement for construction procurement on January 23, 2014.

The Park and Recreation Board authorized award of the contract on November 6, 2014.

City Council authorized award of the contract on November 12, 2014, by Resolution No. 14-1978.

The Park and Recreation Board authorized Change Order No. 1 on January 7, 2016.

Information about this item will be provided to the Quality of Life & Environment Committee on January 25, 2016.

FISCAL INFORMATION

2003 Bond Funds - \$77,534.51

2006 Bond Funds - \$19,741.29

Construction Contract	\$389,600.00
Change Order No. 1 (this action)	<u>\$ 97,275.80</u>

Total amount not to exceed	\$486,875.80
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<u>Council District</u>	<u>Amount</u>
2	\$77,534.51
8	\$ 7,549.53
13	<u>\$12,191.76</u>
Total	\$97,275.80

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

DENCO Construction Specialists Corporation

White Male	2	White Female	10
Black Male	0	Black Female	0
Hispanic Male	0	Hispanic Female	0
Other Male	0	Other Female	0

OWNER

DENCO Construction Specialists Corporation

Mark Boland, President

MAPS

Attached

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize an increase in the contract with DENCO Construction Specialists Corporation for structural repairs and other miscellaneous items at three recreation centers: Arlington Park Recreation Center located at 1505 Record Crossing Road; Janie C. Turner Recreation Center located at 6424 Elam Road; and Marcus Recreation Center located at 3003 Northaven Road - Not to exceed \$97,276, from \$389,600 to \$486,876 - Financing: 2003 Bond Funds (\$77,535) and 2006 Bond Funds (\$19,741)

DENCO Construction Specialist Corporation is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Construction

LOCAL/NON-LOCAL CONTRACT SUMMARY - THIS ACTION ONLY

	<u>Amount</u>	<u>Percent</u>
Local contracts	\$79,858.87	82.10%
Non-local contracts	\$17,416.93	17.90%
TOTAL THIS ACTION	<u>\$97,275.80</u>	<u>100.00%</u>

LOCAL/NON-LOCAL M/WBE PARTICIPATION THIS ACTION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
OAC Construction	HMMB64794N0417	\$4,663.78	5.84%
Total Minority - Local		<u>\$4,663.78</u>	<u>5.84%</u>

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE PARTICIPATION

	This Action		Participation to Date	
	<u>Amount</u>	<u>Percent</u>	<u>Amount</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$4,663.78	4.79%	\$178,260.55	36.61%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$0.00	0.00%
	-----	-----	-----	-----
Total	\$4,663.78	4.79%	\$178,260.55	36.61%



0 45 90 180 270 Feet

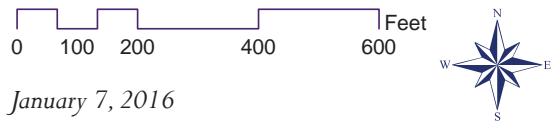


January 7, 2016

Arlington Park Recreation Center (1505 Record Crossing)

Mapsco
33 V

District
2



January 7, 2016

Janie C. Turner Recreation Center
(6424 Elam Rd)

Mapsco
57V

District
8



0 65 130 260 390 Feet



January 7, 2016

Marcus Recreation Center (3003 Northaven)

Mapsco
23 B

District
13

January 27, 2016

WHEREAS, on November 12, 2014, by Resolution No. 14-1978, DENCO Construction Specialists Corporation was awarded a contract for structural repair and miscellaneous work at Arlington Park Recreation Center located at 1505 Record Crossing Road; Janie C. Turner Recreation Center located at 6424 Elam Road; and Marcus Recreation Center located at 3003 Northaven Road, in an amount not to exceed \$389,600; and

WHEREAS, this action will authorize Change Order No. 1 to the contract with DENCO Construction Specialists Corporation for structural repairs and other miscellaneous items at three recreation centers: Arlington Park Recreation Center; Janie C. Turner Recreation Center; and Marcus Recreation Center, in an amount not to exceed \$97,275.80, increasing the original contract amount from \$389,600.00 to \$486,875.80.

Now, Therefore,

BE IT RESOLVED BY THE PARK AND RECREATION BOARD AND THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That the City Manager is hereby authorized to execute Change Order No. 1 to the contract with DENCO Construction Specialists Corporation for structural repairs and other miscellaneous items at three recreation centers: Arlington Park Recreation Center; Janie C. Turner Recreation Center; and Marcus Recreation Center, in an amount not to exceed \$97,275.80, increasing the original contract amount from \$389,600.00 to \$486,875.80.

SECTION 2. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$97,275.80, as follows:

Arlington Park Recreation Center

(2003) Neighborhood Park and Recreation Facilities Fund	
Fund 6R05, Department PKR, Unit T007, Object 4310	
Activity RFSI, Program PK06T007, CT-PKR14019471	
Commodity 91200, Vendor VS0000062071	\$77,534.51

Janie C. Turner Recreation Center

(2006) Park and Recreation Facilities Improvement Fund	
Fund BT00, Department PKR, Unit T176, Object 4310	
Activity RFSI, Program PK06T177, CT-PKR14019471	
Commodity 91200, Vendor VS0000062071	\$7,549.53

January 27, 2016

SECTION 2. (Continued)

Marcus Recreation Center

(2006) Park and Recreation Facilities Improvement Fund
Fund BT00, Department PKR, Unit T223, Object 4310
Activity RFSI, Program PK06T223, CT-PKR14019471
Commodity 91200, Vendor VS0000062071

\$12,191.76

Total amount not to exceed

\$97,275.80

SECTION 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 21

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Planning and Urban Design

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: N/A

SUBJECT

A resolution adopting the Complete Streets Design Manual as a comprehensive policy guide for all public or private projects that impact the planning, design, construction, and operation of streets - Financing: No cost consideration to the City

BACKGROUND

In June 2010, the City of Dallas received a planning grant award of \$400,000 from the Federal Highway Administration through the Transportation, Community and System Preservation grant program to develop a Complete Streets Design Manual for citywide use. The City of Dallas also obtained \$400,000 of Regional Toll Revenue (RTR) funds from the Regional Transportation Council to match this federal grant.

In June 2011, the City of Dallas launched the Complete Streets Initiative with the goal of instituting a new approach to designing and building streets. The Complete Streets Design Manual was developed through an extensive community engagement process that included community workshops, community surveys, pilot projects, and briefings to City boards, commissions, Council committees, and the City Council. The Complete Streets vision is to design and build streets that are safe for all modes of travel and comfortable for everyone, regardless of age or ability; motorists and bicyclists, pedestrians and wheelchair users; bus and train riders alike; and to be sensitive to the surrounding neighborhood character based on public input.

Complete Streets improve safety for all users. Complete Streets connect employment, education, residential, recreation, retail centers, and public facilities; promotes healthy lifestyles; creates more livable communities; and reduces traffic congestion and greenhouse gas emissions. Complete Streets have economic benefits as well. They improve conditions for existing businesses, and are a method for revitalizing areas and attracting new development.

BACKGROUND (Continued)

The Complete Streets Design Manual will serve as a comprehensive design policy guide for professionals who are involved in street-related project planning, design, construction, and operations. It will also serve as a policy guide for the review of off-street development projects seeking City approvals that impact the design of the streets adjoining them. The manual can be used immediately to inform public improvement projects and for review of zoning requests that involve street design elements. It will also provide a basis for future amendments to regulations and standards affecting street design in the Thoroughfare Plan, the Dallas City Code and various engineering manuals.

The City is in the process of initiating an Urban Design Program aimed at engaging stakeholders involved in the development process and the public at large to promote awareness of the value of urban design and to develop citywide urban design principles, policies and guidelines to enhance the quality of the public realm. This initiative will build on and integrate the content of the Complete Streets Design Manual and upon completion of this process, the Complete Streets Design Manual and the broader urban design principles, policies, and guidelines will be incorporated into the *forwardDallas!* Comprehensive Plan.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item was provided to the Transportation and Environment Committee on December 13, 2010, February 28, 2011, June 13, 2011 and September 26, 2011.

Information about this item was provided to the City Plan Commission Transportation Committee on September 20, 2012.

Information about this item was provided to the City Plan Commission on September 12, 2013.

Information about this item was provided to the Transportation and Trinity River Project Committee on September 24, 2013 and September 15, 2015.

The Dallas City Council was briefed on the Complete Streets Initiative and Design Manual on October 5, 2011 and December 2, 2015.

FISCAL INFORMATION

No cost consideration to the City

January 27, 2016

WHEREAS, the City seeks to become a more livable city that can continue to promote a healthy population and a robust economy; and recognizes that streets play a vital part in achieving this goal; and

WHEREAS, the City recognizes the need for its transportation infrastructure to evolve and the importance of providing an integrated transportation system that balances the needs of all users regardless of age or ability, and that meets multi-modal needs including walking, bicycling, and mass transit, as well as automobiles, trucks, and emergency vehicles; and

WHEREAS, the City does not control all roads or modes of transportation within its corporate limits and therefore depends on partner agencies to enhance the transportation options offered within the city; and

WHEREAS, the Transportation Element of the *forwardDallas!* Comprehensive Plan, adopted by City Council in June 2006, calls for a context sensitive approach to the design of streets throughout the City to meet the needs of the neighborhoods through which they pass, and to build a multi-modal system that serves all users; and

WHEREAS, Complete Streets principles offer inclusive, context sensitive framework for planning, design, construction, operation, and maintenance of a multi-modal transportation infrastructure system that can ensure safe and convenient access to community destinations for all users and modes of transportation; and

WHEREAS, Complete Streets principles have been, and continue to be, adopted nationwide at state, county, and city levels in the interest of proactive planning and adherence to federal regulations that guide transportation planning organizations to promote multi-modal transportation options and accessibility for all users; and

WHEREAS, in June 2010, the City received a planning grant award from the Federal Highway Administration through the Transportation and Community and System Preservation grant program as well as matching funds from the Regional Transportation Council to develop a Complete Streets Design Manual for citywide use; and

WHEREAS, following an extensive community engagement process that included community workshops, community surveys, pilot projects, and briefings to City boards, commissions, Council committees, and the City Council, the Complete Streets Design Manual has been completed and contains policies and guidelines for all aspects of street design to promote a comprehensive, connected, and integrated transportation network; and

January 27, 2016

WHEREAS, the City has recently initiated an Urban Design Program to develop citywide comprehensive urban design principles, policies, and guidelines to enhance the quality of the public realm, including but not limited to streets.

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That the City Council adopts the Complete Streets Design Manual as a comprehensive policy guide for all public or private projects that impact the planning, design, construction, and operation of streets, and hereby directs the City Manager to initiate the following actions:

1. Coordinate and integrate the content of the Complete Streets Design Manual with the broader urban design principles, policies, and guidelines to be developed through the City's Urban Design Program and thereafter amend the *forwardDallas!* Comprehensive Plan to incorporate the resulting policy framework;
2. Amend the Dallas Thoroughfare Plan to incorporate references to the content of the Complete Street Design Manual;
3. Amend the Paving Design Manual and the Drainage Design Manual to incorporate and reflect Complete Street design principles;
4. Review the Dallas City Code, including Chapters 28, 43, and 51A, to identify specific amendments necessary to provide references to and ensure consistency with the Complete Streets Design Manual relating to considerations such as minor street design, sidewalk and streetscape design, curb cuts, licensing and abandonment of street rights-of-way, and street operations;
5. Communicate to partner agencies the City's desire that they utilize Complete Streets principles when designing and operating projects within the City's limits;
6. Continue ongoing implementation of funded Complete Streets projects and bike lane striping and coordinate complete street project implementation with ongoing streetcar planning, DART bus route planning and on-street parking improvements;
7. Identify funding opportunities for additional Complete Streets projects through the next bond program;
8. Measure the progress of implementation of the Complete Street Design Manual by developing performance measures to be reported in the City's Sustainability Plan. These measures must include total miles of bike lanes (including marked shared lanes), number of new ADA compliant curb ramps built along city streets, total linear feet of sidewalks with pedestrian lighting, and linear feet of sidewalk waivers approved for that year.

January 27, 2016

SECTION 2. That the City of Dallas affirms that all road projects should be designed to comfortably accommodate all users, regardless of age or ability, to the fullest extent possible within the context of the adjacent community, in a manner that balances multi-modal transportation needs, including pedestrians, bicycles, mass transit, automobiles, trucks, and emergency vehicles, and that these considerations shall be a routine part of all road planning, design, construction, operation, and maintenance activities.

SECTION 3. The City acknowledges that not all work activities associated with streets may be appropriate for accommodating all modes of transportation. Ordinary maintenance activities such as mowing, sweeping, and spot repairs will not be subject to this policy. Exceptions to this policy for any other projects must be reviewed and approved by the Director of Public Works, the Director of Planning and Urban Design and the City's Traffic Engineer (Streets Department). If the application of Complete Streets principles is found to be unnecessary, unduly cost prohibitive, contrary to public safety, or prohibited by law, the project may receive an exception.

SECTION 4. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.



City of



COMPLETE STREETS DESIGN MANUAL
~JANUARY 2016 DRAFT

ACKNOWLEDGEMENTS

Dallas City Council

- Mayor Mike Rawlings
- Mayor Pro Tem Tennell Atkins*, District 8
- Mayor Pro Tem Erik Wilson, District 8
- Deputy Mayor Pro Tem Monica Alonzo, District 6
- Scott Griggs, District 1
- Adam Medrano, District 2
- Vonciel Jones Hill*, District 3
- Casey Thomas II, District 3
- Dwaine R. Caraway*, District 4
- Rick Calahan, District 5
- Carolyn R. Davis*, District 7
- Tiffinni A. Young, District 7
- Sheffie Kadane*, District 9
- Mark Clayton, District 9
- Jerry R. Allen*, District 10
- B. Adam McGough, District 10
- Lee M. Kleinman, District 11
- Sandy Greyson, District 12
- Jennifer S. Gates, District 13
- Philip T. Kingston, District 14

Council Transportation and Trinity River Project Committee

- Linda Koop*, Chair
- Sheffie Kadane*, Vice Chair
- Mayor Pro Tem Pauline Medrano*
- Delia Jasso*
- Vonciel Jones Hill*
- Sandy Greyson
- Lee M. Kleinman
- Deputy Mayor Pro Tem Erik Wilson, Vice Chair
- Monica Alonzo, Mayor Pro Tem
- Adam Medrano, Councilmember
- Casey Thomas II, Councilmember

Consultant Team

- Kimley-Horn and Associates, Inc.
- Toole Design Group (TDG)
- Moore Iacofano Goltsman (MIG)
- Collective Strength
- Team Better Block

Dallas City Plan Commission

- Joe Alcantar, Chair, District 15
- Richard Davis, District 1
- Liz Wally, District 2
- Michael Anglin, District 3
- Robert Abtahi, District 4
- Emma Rodgers, District 5
- Tony Hinojosa, Jr., District 6
- Ann Bagley, District 7
- Myrtl M. Lavallaisaa, District 8
- Gloria Tarpley, District 9
- John Shellene, District 10
- Bruce Bernbaum, District 11
- Sally Wolfish, District 12
- Michael Schwartz, District 13
- Paul E. Ridley, District 14

City Plan Commission Transportation Committee

- Ann Bagley, Chair
- Richard Davis, Vice Chair
- Myrtl Lavallaisaa
- Steve Stoner
- Gary Lawler
- Terry Watson

City Management

- A.C. Gonzalez, City Manager
- Ryan S. Evans, First Assistant City Manager
- Jill A. Jordan, P.E., Assistant City Manager
- Eric Campbell, Assistant City Manager
- Mark McDaniel, Assistant City Manager
- Joey Zapata, Assistant City Manager

Planning and Urban Design

- Theresa O'Donnell, Chief Resiliency Officer
- Alan Simms, Chief Planning Officer
- Peer Chacko, Director
- Keith Manoy, Former Assistant Director, Mobility Planning
- Tanya Brooks, Interim Assistant Director, Mobility Planning/Project Manager
- Luis Tamayo, Chief Planner
- Jared White, Bike Program Manager
- Mark R. Brown, Senior Planner
- Kimberly Smith, Senior Planner

Note: This Manual is dedicated to Ross Cravens, with appreciation for his work.

* Former Councilmember

Funding Partner Agencies

Texas Department of Transportation

- Wes McClure, Dallas District Program Engineer
- Polita Fleming, Transportation Funding Specialist

North Central Texas Council of Governments

- Karla Weaver, Principal Planner
- Deborah Humphreys, Transportation Planner

Technical Committee

City Staff

- Alan Hendrix, Public Works
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- Thomas Morris, Public Works
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Portions of Chapter 4, 5, and 6 were derived from the Boston Complete Streets Guidelines, prepared by the City of Boston Transportation Department, with permission.

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INTRODUCTION



A successful Complete Street creates an environment in which diners, pedestrians, bicyclists and transit users feel safe sharing the corridor right-of-way with cars, transit, goods and services vehicles.

INTRODUCTION



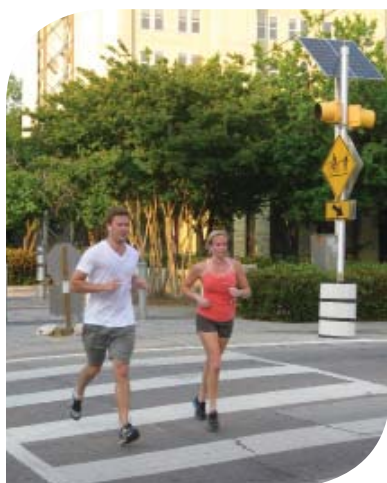
Sidewalk cafe in West Village

WHAT ARE COMPLETE STREETS?

The vision of the Dallas Complete Streets Initiative is to build streets that are safe and comfortable for everyone: young and old; motorists and bicyclists; walker and wheelchair users; bus and train riders alike.

The City launched the Complete Streets Initiative in June 2011 with the goal of instituting a new approach to designing and building streets. Complete Streets is a relatively new term for an idea from decades past. Long before regulations and requirements promoting rapid automobile movement began dictating street design, streets were built and developed to serve the destinations surrounding them. Some of the greatest streets in America still maintain this centuries-old character. New great streets – built to evolving standards – are being built throughout the country through Complete Streets programs.

Complete Streets make it easy to cross the street, walk to shops, and bicycle to work. They help buses run on time and make it safe for people to walk to and from train stations.



Runners on the Katy Trail



DART transit center



Vibrant street life



Complete Streets are designed with all users in mind, not just drivers.

DALLAS COMPLETE STREETS GOALS AND OBJECTIVES

Dallas Complete Streets Initiative has been guided by goals embraced in past City Council policy directives. The goal of Dallas Complete Streets is to design streets that

- Enhance the public realm rather than serve as mere traffic conduits
- Provide for multiple transportation modes—pedestrian, bicycle, transit, and automobile—and include environmentally sustainable solutions appropriate for the situations
- Reflect that all streets are not the same
- Use design solutions that are specific to the context
- Support flexibility to accommodate changing needs, and allow change to occur incrementally

The following objectives have driven the development of this manual as a means towards achieving the above-stated goals:

- Establish a new street design process, policies, and standards that integrate Complete Streets and Integrated Stormwater Management (iSWM) principles
- Provide effective and timely opportunities for community stakeholder input on the design priorities, costs, benefits, and trade-offs of proposed street improvements
- Develop a strategy for systematic and phased implementation over time through both public and private improvements

Benefits of Complete Streets

Complete Streets drive both infrastructure and cultural changes. Shifting demographic trends show more people are choosing to live in walkable urban areas and desire access to several modes of transportation. This gives an increasing number of people the option not to use automobiles for everyday travel.

With over 40 percent of the trips made in the U.S. being less than two miles, and roadway congestion growing despite a continual investment in infrastructure, the demand for alternative ways to move around the City is increasing. In a random phone survey (discussed later in this chapter) of Dallas citizens' perception and use of transportation, 54 percent of respondents expressed interest in using transit; 12 percent indicated they would be interested in walking or biking, if it was more convenient; 68 percent of residents believed that being able to walk or bike to destinations would be better for the economy; and 88 percent would accept a five-minute longer drive time if it meant more convenient walking and biking. Given these responses from Dallas residents, it is obvious that providing safe and healthy alternatives to our current transportation system is critical.

The Complete Streets design also encourages a shift in the City's modal split, increasing the use of transit, biking, and walking. By diversifying modes of transportation, like transit and rail, more people can move through a corridor other than by solely using motor vehicles. Another critical benefit is the incorporation of green features. These design elements can improve the visual impact of the roadway, assist in stormwater management, combat pollution from emissions, reduce exposed pavement, and lead to a decrease in the City's heat island effect.

Complete Streets benefit the city in the following ways:

- Promote public safety
- Create increased social, civic, and economic activity on streets
- Increase the overall capacity of the transportation network while offering options to avoid traffic
- Provide incentives for economic revitalization by reducing transportation costs and travel time while increasing property values and job growth
- Reduce the demand on existing infrastructure by incorporating stormwater management into street designs
- Improve the return on infrastructure investments by integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project, sparing the expense of later retrofits
- Improve the quality of place by creating vibrant, livable centers through increased walking and bicycling, and by promoting suitable denser development patterns
- Improve safety by designing and accommodating for all modes—bicyclists, pedestrians, drivers, and transit users—thus reducing the incidence of crashes
- Create more walking and bicycling opportunities to address our City's obesity epidemic; streets that provide room for bicycling and walking can help encourage children to be more physically active and to gain independence
- Provide environmental benefits from reduced congestion, alternative transportation options, and water quality improvements
- Enhance the everyday quality of life for Dallas residents by providing safe, comfortable space for public activities



Complete Streets is not a new idea—Dallas' streets once hosted greater diversity in modes of transportation.



Community input was sought throughout all stages of the Complete Streets Initiative.



Complete streets foster economic development.

Community Aspirations

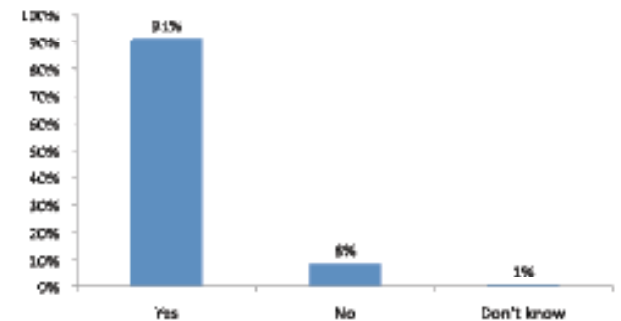
Citizens from across the City participated in a random phone survey about their perception and use of transportation in Dallas. Residents also attended workshops and participated in focus groups and interviews to discuss how they would like to see their streets designed in the future. The following statements reflect the desires expressed by Dallas citizens:

- Dallas citizens broadly support sustainable economic development that fosters neighborhoods that have more transportation choices; are closer to shops, schools, and jobs; and are more energy efficient
- Complete Streets encourage citizen safety, health, and economic viability by promoting pedestrian safety, maintaining existing streets, and providing public transportation options
- Both the public and private sectors are encouraged to participate in promoting Complete Streets throughout the City through public-private partnerships and by aggressively implementing Complete Streets
- While the primary form of transportation continues to be personal vehicles, the Complete Streets Initiative recognizes that safer streets, an improved economic environment, and enhanced walking and biking conditions are essential considerations in future roadway investments.

Survey Results*

The following are highlights of the survey questions:

1. A sustainable community is defined as a neighborhood that has more transportation choices; is it closer to shops, schools, and jobs; is it more energy efficient; and helps protect our air and water. Do you want your elected officials to work towards more sustainable communities?

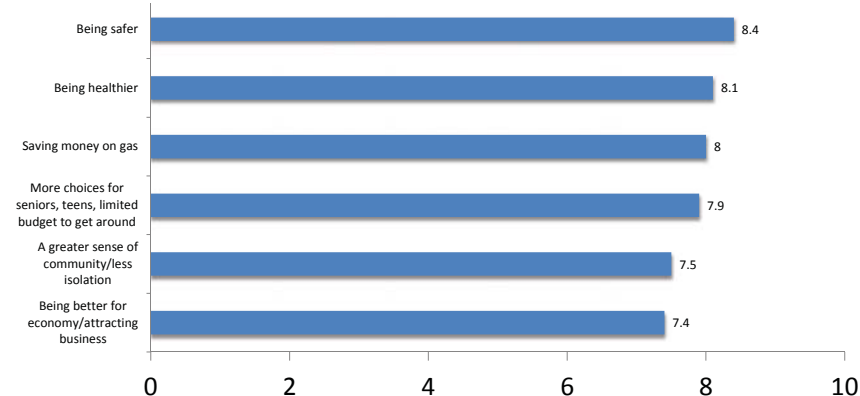


The vast majority of respondents want elected officials to work towards sustainable communities.

* Questionnaire was designed by Collective Strength, with input provided by Kimley-Horn and Associates and City of Dallas. 518 interviews were conducted using standard market research industry protocol for telephone polling by Promark Research in August of 2011. The results are calibrated to mirror 2010 Census estimates for age, race, income, gender, and region.

2. On a one to ten scale, where one is not at all important and ten is extremely important, how important to you are the following reasons for giving up some street space for walking and biking?

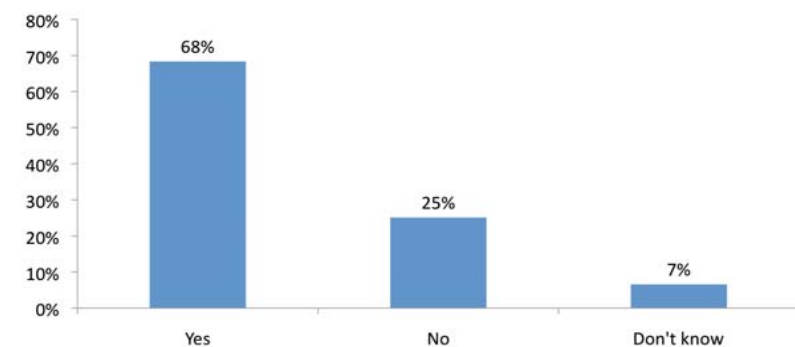
According to Dallas citizens, safety, health, and saving money are all strong motivators for



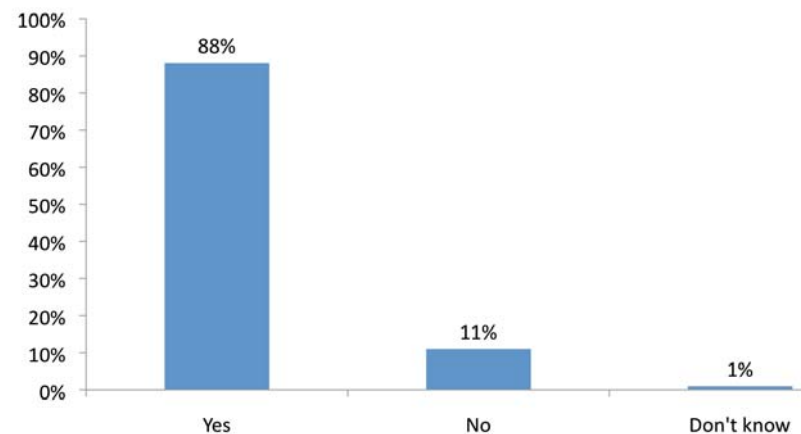
implementing Complete Streets.

3. Do you feel that kids being able to walk or bike to school, and adults being able to walk or bike to places to shop and eat would be better for the Dallas economy than it is now?

Most residents agree that being able to walk or bike would be better for the economy.



4. Would you be willing to accept your drive time taking five more minutes than it does now on City streets if it meant more convenient walking and biking?



A majority of residents agree that the benefit gained from Complete Streets by having more convenient opportunities to walk and bike to destinations is worth a slightly longer drive time.



The Complete Streets concept takes development patterns and the community's needs and desires into account and applies these considerations to the design, construction, reconstruction, or rehabilitation process to create streets that are multifaceted rather than single purpose. Complete Streets programs use many features like the context and character of an area, future goals for a corridor, and the future need for different modes of transportation to create a realistic and compatible roadway design.

FOUNDATION

forwardDallas!

The forwardDallas! Comprehensive Plan adopted by the City Council in 2006 envisions a new multimodal Dallas that encourages a thriving urban downtown, employment opportunities, and a more visually-pleasing urban design for thoroughfares.

The Vision for Dallas, mapped out by Dallas citizens during the forwardDallas! process, discusses how the City of Dallas will look, function, and feel over the next quarter century. The policies in the forwardDallas! Guiding Vision and Comprehensive Plan are the starting point for creating the economy, housing, transportation choices, parks, and open spaces that the citizens of Dallas desire. The forwardDallas! Strategic Plan establishes the first steps for implementing Complete Streets and recommends the development of policies and guidelines for Dallas.

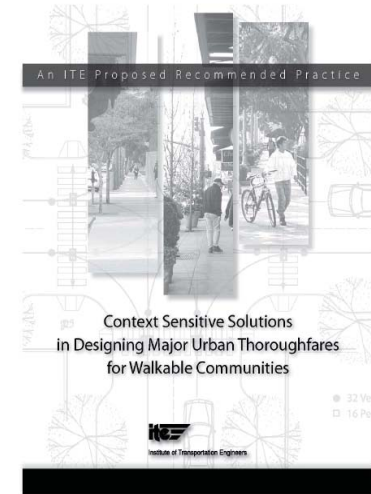
The forwardDallas! Vision places an emphasis on coordinating transportation facility design with the land uses and the context they serve. Like many American cities, Dallas' transportation system has historically been built to support automobile-oriented land development transportation. Looking forward, cars will likely remain the predominant mode of transportation for Dallas citizens. However, during the extensive public input process conducted while developing this Complete Streets Initiative, significant support was expressed for expanding the range of transportation options, as well as for land development forms that are walk- and bike-friendly and easily served by transit.

National Trends

Locally and across the nation recent trends in development have changed the approach to roadway planning, allowing for greater flexibility in thoroughfare design which better complements surrounding land uses. This emerging practice is based upon the principles of context sensitive roadway design. The Context Sensitive Solutions (CSS) Design Manual, written by the Institute of Transportation Engineers and the Congress for the New Urbanism, provides a guide on how this emerging practice can be implemented during the thoroughfare planning process. Opportunities for multimodal corridors that advance economic development and create a safer, more efficient transportation system arise when the context of a roadway is taken into account during the planning and design processes.

People who live in cities and towns throughout the country have a strong interest in ensuring that transportation investments provide for the safe travel of everyone using the road. Across the country, Complete Streets policies have been gaining traction as more communities have realized the benefits of safe, accessible, and healthy streets. Of all the trips within the U.S., 40 percent are less than two miles; of these, 99 percent are made by automobile. Nationwide, people are open to using viable transportation alternatives, if available. Trends also show that vibrant bike- and transit-friendly cities attract youth and the creative class—those people who are integral to building tomorrow's workforce.

In 2011, 125 jurisdictions adopted a Complete Streets policy, up from 80 that committed to Complete Streets in 2010. In total, as of 2011, 330 regional and local jurisdictions, 26 states, the Commonwealth of Puerto Rico, and the District of Columbia have made a commitment to Complete Streets implementation.



Context Sensitive Solutions (CSS) Design Manual



McKinney Avenue streetcar



Lively, vibrant street life enhances the livability of a city for all citizens.

County, NCTCOG, and State Initiatives

Dallas County recently celebrated the completion of a sustainable solution for a complicated five-leg approach intersection (Belt Line Road, Eastgate Drive, and Pioneer Road) in Balch Springs. Instead of a complicated, traditional at-grade intersection with a multiphase traffic signal, the County, the City, and adjacent property owners worked together to implement the first modern multi-lane roundabout in Dallas County.

The North Central Texas Council of Governments (NCTCOG) is in the process of developing a Regional Complete Streets Policy Statement, which when adopted, will provide guidance to implement Complete Streets. The NCTCOG's policy statement will:

- Apply to both new and retrofit projects as identified in the Transportation Improvement Program and the Metropolitan Transportation Plan
- Provide local governments assistance with the implementation of the NCTCOG Complete Streets Policy Statement

On March 23, 2011, the Texas Department of Transportation (TxDOT) issued a memorandum stating the following: "TxDOT is committed to proactively plan, design, and construct facilities to safely accommodate bicyclists and pedestrians. It is critical that bicycle and pedestrian accommodations be considered and discussed as the need and purpose of a project is defined during the National Environmental Policy Act (NEPA) process, taking into consideration existing and anticipated bicycle and pedestrian facility systems and needs." The memo provides guidance for both urbanized and rural settings, and shows that TxDOT is committed to a Complete Streets policy.

With such a large portion of Dallas' land area developed as roadways, these public spaces provide one of the greatest opportunities to enhance the City's urban design and livability for all citizens.

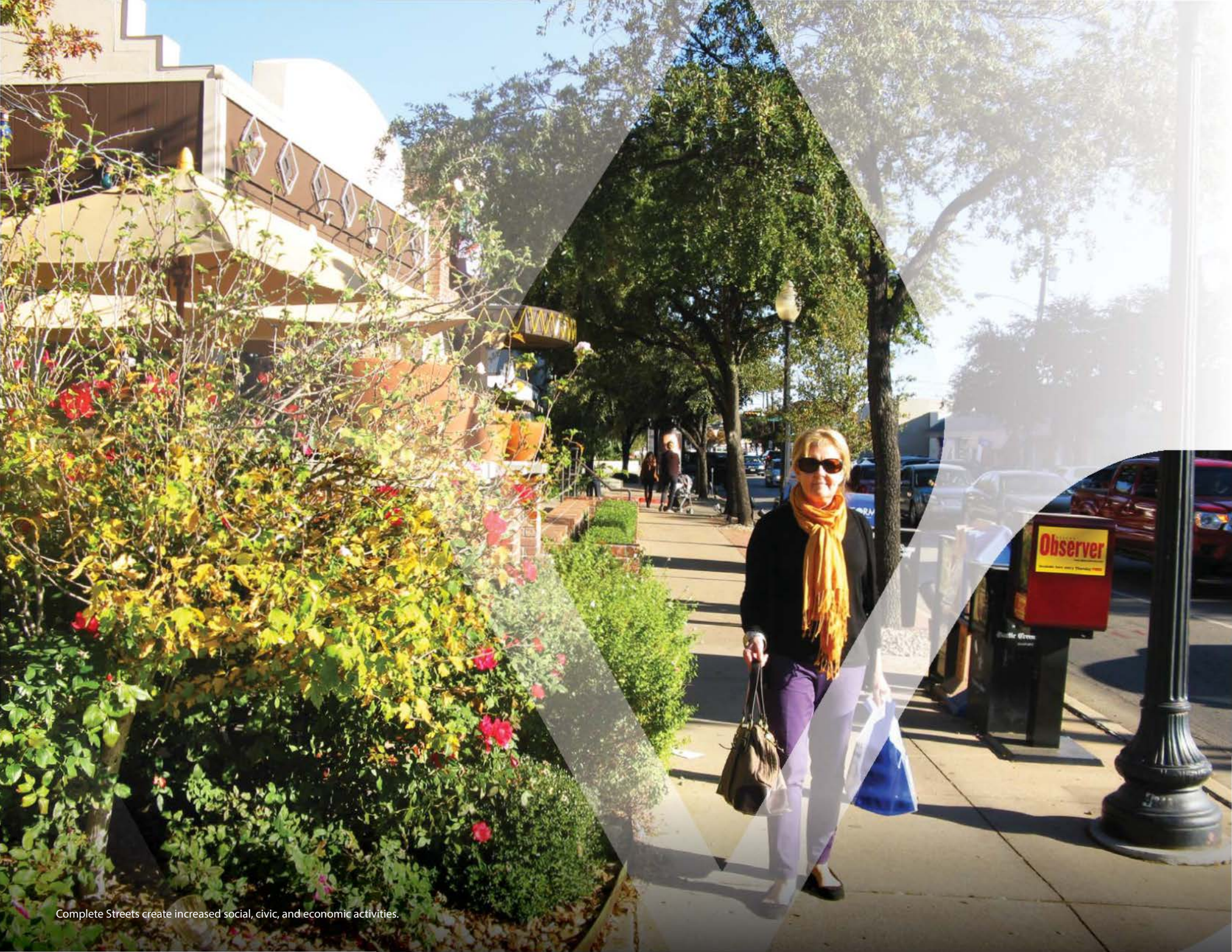
Fortunately, Dallas has a robust transit system and a street network. In many areas, there is a significant amount of available capacity which will allow streets to be transformed into high performance, multimodal transportation environments. In essence, the notion of Complete Streets offers the promise of an economically vibrant, safe, multimodal, and sustainable Dallas. This manual lays out a process and policy framework to lead Dallas along a pathway from our existing auto-oriented roadway system towards the pedestrian-, bicyclist-, and transit-sensitive roadway system of Dallas' future.



Provides a variety of experiences.

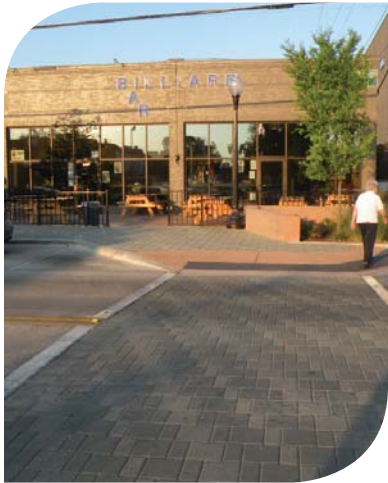


1. HOW TO USE THE MANUAL



Complete Streets create increased social, civic, and economic activities.

1. HOW TO USE THIS MANUAL



Greenville Avenue



Outdoor dining area

This Complete Streets Design Manual provides policies and design best practice guidelines to City agencies, design professionals, private developers, and community groups for the improvement of streets and pedestrian areas throughout Dallas. The manual promotes higher quality street designs that create safe, multimodal streets for all users. This manual is intended to direct transportation planners and engineers to routinely design and operate the entire right-of-way to enable safe access for all users, regardless of age, ability, or mode of transportation.

This manual is intended to work alongside the Dallas Thoroughfare Plan and the Dallas Development Code to provide the policy framework for the design and use of Dallas' roadway network. Through the use of this manual early in the design process, street improvement plans will consider the context of the roadway, community design priorities, and the roadway's functional classification. This manual will also serve as a policy guide for private development projects and community-driven initiatives that involve physical improvements within the public right-of-way.



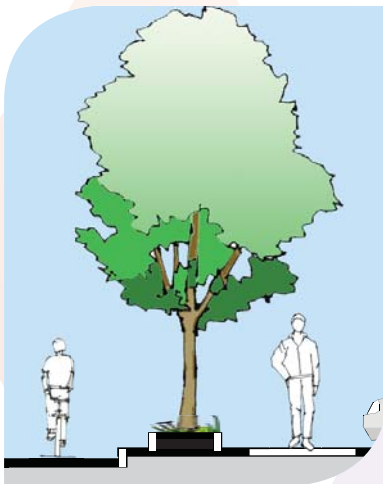
Urban reserve



West End Station



Complete Streets vision maps are displayed in Chapter 2.



Example cross sections are illustrated in Chapter 3.

COMPLETE STREETS TOOLS

The Complete Streets Initiative provides a series of new guidelines and best practices to use in planning for roadway improvements. The following tools are provided to support implementation of Complete Streets in Dallas.

Complete Streets Typology:

A new classification system for streets that takes into account the street context and the future vision for accommodating all modes of travel—including pedestrians, cyclists, public transit, the movement of services and goods, and private vehicles—is provided. These street types include mixed use, commercial, residential, industrial, and parkway and are described starting on page 46 of Chapter 2.

Complete Streets Vision Maps

- **Contextual Overlay:** The vision map designates the City's thoroughfares as one of five new Complete Streets types. This map provides a way to envision the future role of Dallas' street network in serving their adjoining neighborhoods, and is a starting point in determining how a particular street should be designed in the future. The vision map is provided on page 67 in Chapter 2.
- **Bike and Transit Network Overlay:** This overlay map shows streets that are designated on the 2011 Dallas Bike Plan, as well as transit streets, which have enhanced bus service, street cars, and passenger rail or transit stations. This overlay map shows streets that should be designed with particular consideration given to bike or transit facilities. The overlay map is provided on page 68 in Chapter 2.

Green Streets and Stormwater Management Coordination:

Chapter 7 promotes coordination between the City's integrated stormwater management and this Complete Streets Manual.

Flexible Requirements: Page 83 in Chapter 3 provides flexible lane widths for a variety of Complete Streets elements. These are shown by street type, so that flexibility is maintained when balancing competing goals in areas with limited right-of-way.

Complete Streets Design Elements:

The charts provided in Chapter 3 offer policy guidance on design priorities. The "Design Element Priorities Chart" on page 85 provides a quick reference for prioritization of key design elements. The "Bike and Transit Network Facility Priorities Chart" on page 87 provides general policy guidance for selection of appropriate bike and transit facility types for bike and transit network streets.

Example Cross Sections:

Chapter 3 also shows how to apply varying elements in corridors with limited right-of-ways.

Best Practices for All Parts of the Street and Sidewalk:

- Sidewalk zone best practices are provided in Chapter 4
- Street zone best practices are provided in Chapter 5
- Intersection best practices are provided in Chapter 6

Pilot Projects: As part of the Complete Streets Initiative, the City produced 15 pilot projects throughout the City.

GETTING STARTED

Initial assumptions driving the implementation program include the following:

- The Complete Streets Initiative reflects a shifting mind-set for some citizens in favor of gradual transition of streets to accommodate multiple users
- Not all streets are appropriate or feasible candidates for transformation—many streets will not change significantly, or will change slowly over time.
- Cost of enhancements and maintenance of new amenities as well as trade-offs are key factors that must be considered when weighing the benefits of Complete Streets

Street improvement projects are initiated in many ways, and have a wide range of purposes and a variety of stakeholders. Street projects may be initiated by either the public or private sector. Projects may include new streets, or street reconstruction projects. Reconstruction projects may be resurfacing (or rehabilitation), underground utility upgrades that require replacing the pavement surface afterwards, or complete reconstruction projects. Projects may be funded 100 percent by a capital improvement program, financed from the enterprise fund of the particular utility, or a combination of both public funding sources. Projects may be the result of a public-private partnership in which private funds are matched by state, federal, or local funds. Finally, projects may be financed 100 percent by the private sector.

Who initiates and sponsors a Complete Streets project determines the initial review and approval process by the City of Dallas. Not all street projects will be appropriate for Complete Streets treatment, and those that are can vary greatly in the type and scope of improvement that may be suitable at any given time. For example, short segments of streets that are reconstructed as part of a utility repair process would most likely not be appropriate for an application of Complete Streets elements. Streets that need to

be reconstructed because of utility emergencies are not practical for Complete Streets implementation. Similarly, a number of Dallas roadways are functioning well within their physical context and do not need special enhancements to be high-performing, safe streets. In addition, highways do not fall under the purview of the City's thoroughfare plan or this Complete Streets Manual. Complete Streets improvements may occur as large capital projects along an entire roadway segment, incrementally on portions of a roadway along with private developments, or through small neighborhood initiatives.

Regardless of the manner in which a project is initiated, we would encourage professional planners and designers, private developers and property owners, and community and neighborhood organizations to consider this Design Manual a primary guide to a successful project.



Complete Streets foster economic development, not just transportation.



Our roadways belong to all of us, no matter how we choose to travel them.

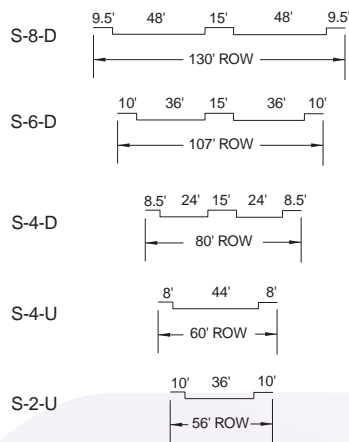
THOROUGHFARE PLAN FUNCTIONAL CLASSIFICATION

The Dallas Thoroughfare Plan provides the basis for classifying streets based on projected traffic volumes on the road. This functional classification has guided the design of most of Dallas' thoroughfares, resulting in conventional, separate-use building blocks. This type of street design does not account for other types of non-motorist facilities on the road or sidewalk, and favors automobile mobility over the convenience and safety of other modes of transportation. The Dallas Thoroughfare Plan generally defines roadways based on motorist needs as follows:

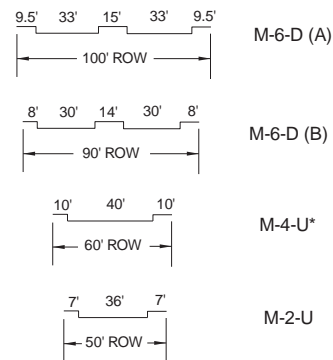
Arterial Roads

Arterial roads deliver traffic from collector roads to freeways, and between urban centers, with minimal delay to motor vehicles. They typically carry high volumes of motor vehicle traffic operating at higher speeds. As a result, arterial roads are unfriendly to pedestrians and bicyclists.

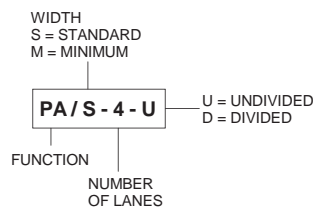
STANDARD ROADWAY SECTIONS



MINIMUM ROADWAY SECTIONS



*M-4-U can be striped and operated as 2 or 4 lanes.



PROPOSED FUNCTION

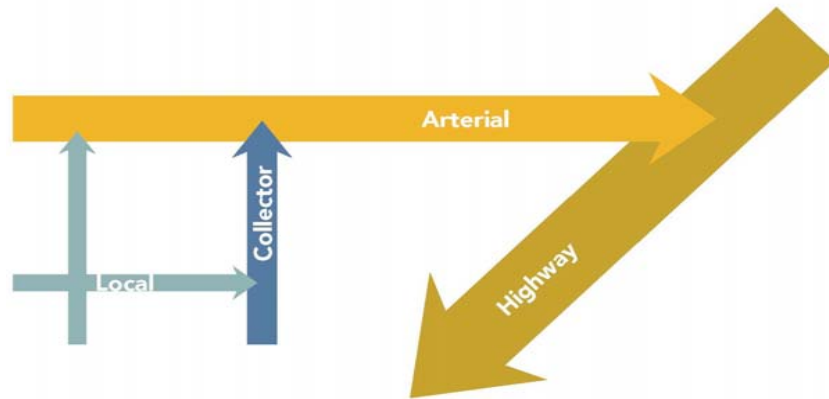
- PA - PRINCIPAL ARTERIAL
- MA - MINOR ARTERIAL
- C - COMMUNITY COLLECTOR
- RC - RESIDENTIAL COLLECTOR
- FURTHER STUDY
- FUTURE OPTIONS

Map by Transportation GIS
Revised: July 2004

Dallas Thoroughfare Plan minimum and standard roadway sections

Collector Roads

Collector roads have a moderate capacity and serve to move traffic from arterial roads to local streets. Unlike arterial roads, collector roads provide access to residential properties, parks, schools, and other neighborhood amenities. They often have limited consideration for surrounding neighborhood character.



Functional hierarchy of roadways

Local or Minor Streets

Local streets, which are not designated on the Thoroughfare Plan, carry low volumes of motor vehicle traffic at lower speeds and provide direct access to residential property. Chapter 51A of the Dallas Development Code, as amended, and the resulting applied zoning districts, establish the right-of-way—and in some instances, design criteria—for minor streets.

	Street Classification	Pavement Width	ROW Width	Minimum Alley Required	Center line Radius
Zoning					
R-1 thru R- 7.5	L-2-U(B)	26'	50'	YES	150'
	L-2-U(A)	33'	53'	NO	200'
	S-2-U	36'	56'	NO	230'
R-5,MH, D,TH-1, TH-2	L-2-U(A)	33'	53'	YES	200'
	S-2-U	36'	56'	NO	230'
TH-3, CH, Multifamily	S-2-U	36'	56'	NO	230'
All non-residential districts except PDDs, and WR Districts in Article XIII	S-2-U	36'	56'	NO	280'

Chapter 51A of the Dallas Development Code:
Requirements for Minor Streets

COMPLETE STREETS INITIATIVE IMPROVEMENT PROCESS

The Complete Streets Process uses the current public and private processes as a base and enhances them with specific coordination, planning, and conceptual design in order to improve the efficiency of the process, balance user needs up front, and anticipate short- and long-term associated costs. This process produces improved outcomes that create an environment that addresses the needs of all users. This manual is intended to supplement and reference rather than replace existing engineering and environmental standards and requirements, including but not limited to the City's design manuals, Texas Manual on Uniform Traffic Control Devices (TMUTCD), and AASHTO Policy on Geometric Design of Highways and Streets ("Green Book"). In a city with many varied and complex conditions, designs must be tailored for the particular needs and opportunities created by the local context, uses, and dimensions of streets. This section describes the Complete Streets Process for public street projects as well as for development-related projects.

The Complete Streets Roadway Design Process requires that both traditional information as well as the contextual environment inform the roadway design team. Designers of Complete Streets must balance roadway characteristics, the context and land use of an area, priority elements, and the construction, operation, and maintenance costs associated with the improvements. Some factors to consider in roadway design include:

Roadway Characteristics and Needs

- Thoroughfare plan designation and the roadway's functional classification
- Existing and proposed right-of-way
- Multimodal traffic counts
- Auto speeds
- Congestion and capacity
- Planned infrastructure projects

Context and Land Use

- forwardDallas! Vision
- Local area plans
- Existing land uses and form
- Existing zoning
- Development proposals
- Complete Streets Vision

Priority Elements

- Design priorities chart - Page 85
- Variable lane width chart
- Community input and user needs and preferences
- Major destinations
- Desired connections
- Under-performing districts
- Regional plans

Funding Sources

- Construction
- Operation and maintenance

PROJECT TYPES

There are several ways in which Complete Streets can be implemented but the nature and scope of the improvements will depend on the type of project. There are three broad types of projects that can result in implementation of Complete Streets: Street Improvement Projects, Development Related Projects and Interim Complete Streets Projects.

Street Improvement Projects

Street Improvement Projects are typically initiated by a City department or other public agency, such as Dallas County. These public projects are primarily focused on the public right-of-way and may be one of the following types, depending on their scope or impetus:

- Roadway construction/reconstruction projects
- Utility replacement projects
- Street resurfacing and restriping projects

The "Complete Streets Improvement Process" later in this chapter describes the recommended approach to ensure integration of Complete Streets Design Standards in the typical street improvement process.

Roadway Construction and Reconstruction Projects

Roadway projects may be public works reconstruction of existing facilities or new roadway construction. These projects are typically publicly initiated by being placed on the City's need inventory for prioritization. The scope of these projects are usually large enough to provide the opportunity for more comprehensive implementation of Complete Streets elements, and may provide for changing the number and width of lanes, expanding sidewalks, or introducing new features.

Utility Replacement Projects

Utility replacement projects are large scale infrastructure projects that are initiated to replace water, sewer, and utility lines. In these cases, entire segments of roadways are torn up and replaced. These projects are placed on the utility needs inventory and coordinated with City departments. Utility replacement projects are prioritized based on the service levels and operational criteria and are funded through the utility funds. In some instances, the repairs are in small areas and would not trigger a Complete Streets evaluation. Other projects cover multiple blocks and allow for consideration of planning and potential implementation of Complete Streets elements.

Street Resurfacing and Restriping Projects

The Streets or Public Works Departments initiate resurfacing and restriping projects to maintain existing streets within their current lane configurations. Since these improvement programs may be discontinuous segments based on the current condition of the roadways, they may not always be appropriate for consideration for Complete Streets elements. In some situations these projects may provide an opportunity to make changes in lane configuration based on Complete Streets considerations, such as implementing the bike plan or introducing new multimodal features. The most likely of these types of projects to be implemented as Complete Streets include resurfacing or restriping projects that connect significant origins and destinations and have a source of funding such as a Tax Increment Financing District (TIF) or Public Improvement District (PID), or other public-private mechanism.



Lancaster corridor - existing condition

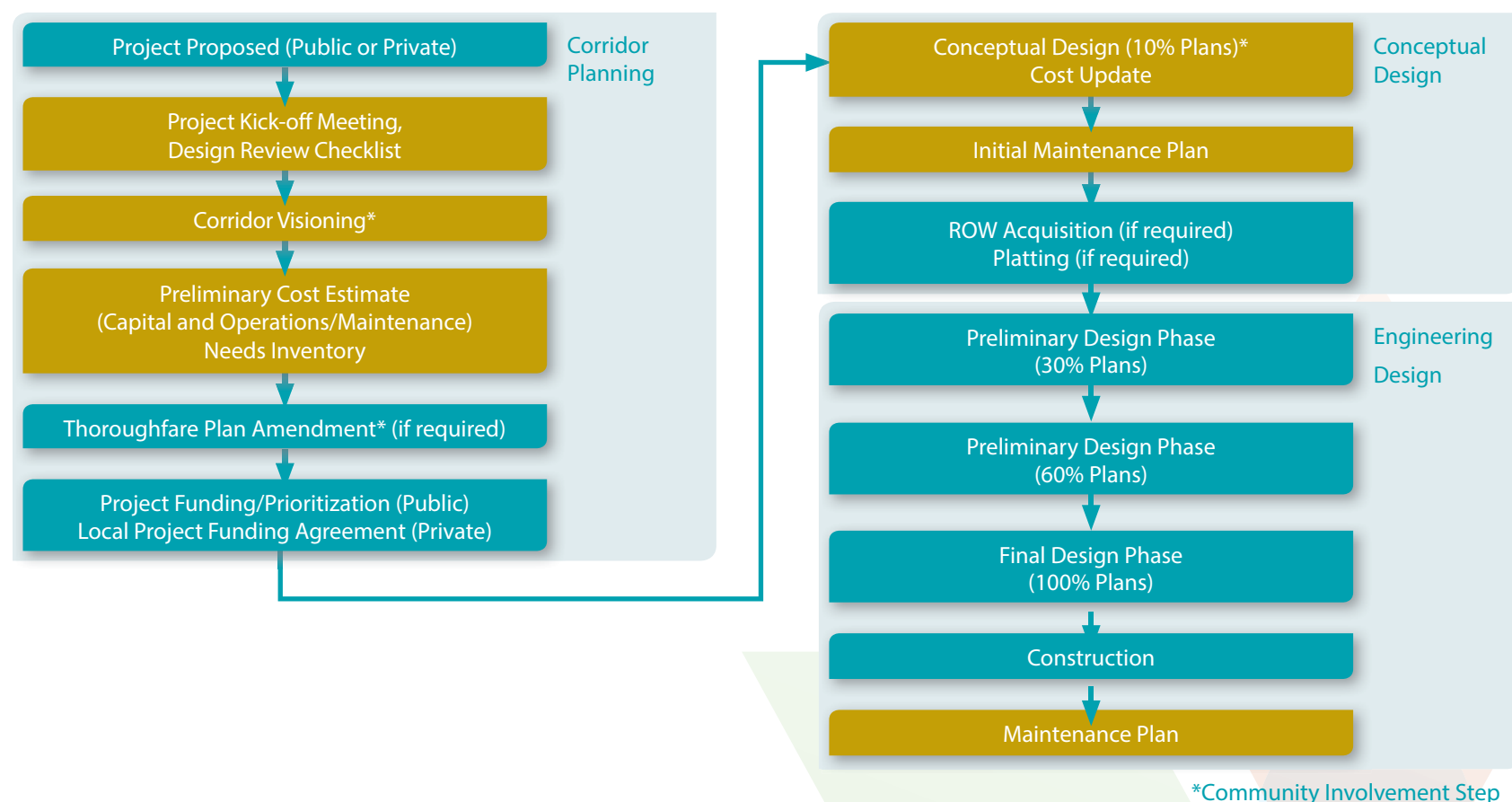


Photo simulation of Complete Streets improvements on Lancaster

Complete Streets Improvement Process

The following chart shows the typical street improvement process in blue, with steps that are critical for integration of Complete Streets considerations highlighted in gold. This process also highlights key steps for community involvement during planning and design so that projects progress efficiently within a transparent framework.

COMPLETE STREETS IMPROVEMENT PROCESS



Development-Related Projects

Development-related projects are typically initiated by a property owner or developer. They include one of two types of projects, both of which are initiated through services provided by the Office of Sustainable Development and Construction. The Dallas Development Guide details the City's development process.

Development Projects with Thoroughfare Frontage

Private development projects on thoroughfares may include large, multiblock projects or incremental development. Development projects may be on streets that are built to thoroughfare plan standards or on streets that are not currently built to standard. For properties on thoroughfares that are not built to standard, or where the developer wishes to amend the thoroughfare plan, development projects may trigger the need to work through the City's platting process to ensure adequate right-of-way dedication. In addition to thoroughfare plan and platting requirements, zoning will also have important bearing on the street design in terms of sidewalk and streetscape standards. Besides planned development districts that may have special sidewalk and streetscape standards, other zoning districts default to the minimum standards in the development code.

Developments with Minor/Local Street Frontage

Private developments may front on residential or nonresidential local streets. Local street design is governed by zoning and platting requirements rather than through the Thoroughfare Plan for right-of-way and pavement widths, as well as sidewalk and streetscape standards. In addition, private streets contained entirely within private developments must also follow local street standards.

Interim Complete Street Projects

While large capital initiatives and development-related projects provide an opportunity to reimagine Dallas' streets on a grand scale, funding constraints and long delivery timelines are often barriers to implementing these types of complete street projects. Interim projects with short turnaround times can provide an opportunity to test roadway geometric changes, new public plazas, road diets, and other complete street components in a more cost efficient and timely manner.

Temporary Complete Street Trials

Construction of a temporary cross section with traffic signs, barricades, and pavement marking tape can provide valuable feedback on roadway geometry and lane configuration changes prior to full build out. Trials should last for at least 2 weeks to properly assess traffic conditions and neighborhood sentiment about the street changes. The Knox Street Demonstration Experience was Dallas' first

complete street trial and serves as a learning experience for future demonstration projects.

Better Block Trials

"Team Better Block temporarily re-engineers and re-programs auto dominated, blighted, and underused urban areas into complete ones by working with cities, developers, and stakeholders to create quick, inexpensive, high-impact changes. Team Better Block uses pop-up shops to test the local economic development potential of streets re-engineered for walkability. Additionally, Team Better Block bolsters civic pride by enlisting the community in the build-out of the temporary installation." [From Better Block's website, www.betterblock.org]

Better Block Trials have a community building and economic development focus while also serving as a trial for complete street ideas. Several trials have already been completed in east Dallas and Oak Cliff and involved a partnership between Better Block staff, city agencies, non-profits and local businesses. [URL: <http://betterblock.org/>]

Cyclovias, Open Street Events, and Other Temporary Street Closures

Temporary street closures to automobile traffic encourages walking, bicycling, recreation and community interaction. A long tradition in South American countries, Cyclovias have been adapted to the U.S. as popular "Open Street" events around the country. During the course of several hours to an entire weekend, a strategic set of streets are closed to automobiles so communities can use them for outdoor recreation. Nearby businesses are also encouraged to take advantage of increased foot traffic with outdoor "pop up" shops. [URL: <http://openstreetsproject.org/>]

Dallas Public Plaza Program

Based on NYC DOT's successful public plaza program where excess roadway space is converted to shaded outdoor seating areas, a similar program in Dallas will test the viability of more long term pedestrian plazas in a cost efficient manner. Typically constructed out of painted asphalt and concrete planters, public plazas can be installed for up to several years provided they are maintained. Neighborhoods, local non-profits or businesses will be responsible for maintaining new public plazas after they are installed by the city.

Low Cost Pedestrian Safety Improvements

Also based on NYC's successful pedestrian safety initiative, low cost improvements involve expanding medians, creating new pedestrian refuge islands, widening sidewalks, and daylighting intersections. Materials used include asphalt paint, concrete planters and bollards to delineate new pedestrian spaces for a trial period lasting up to one year while roadway changes are assessed.

Implementing Complete Streets through Private Development

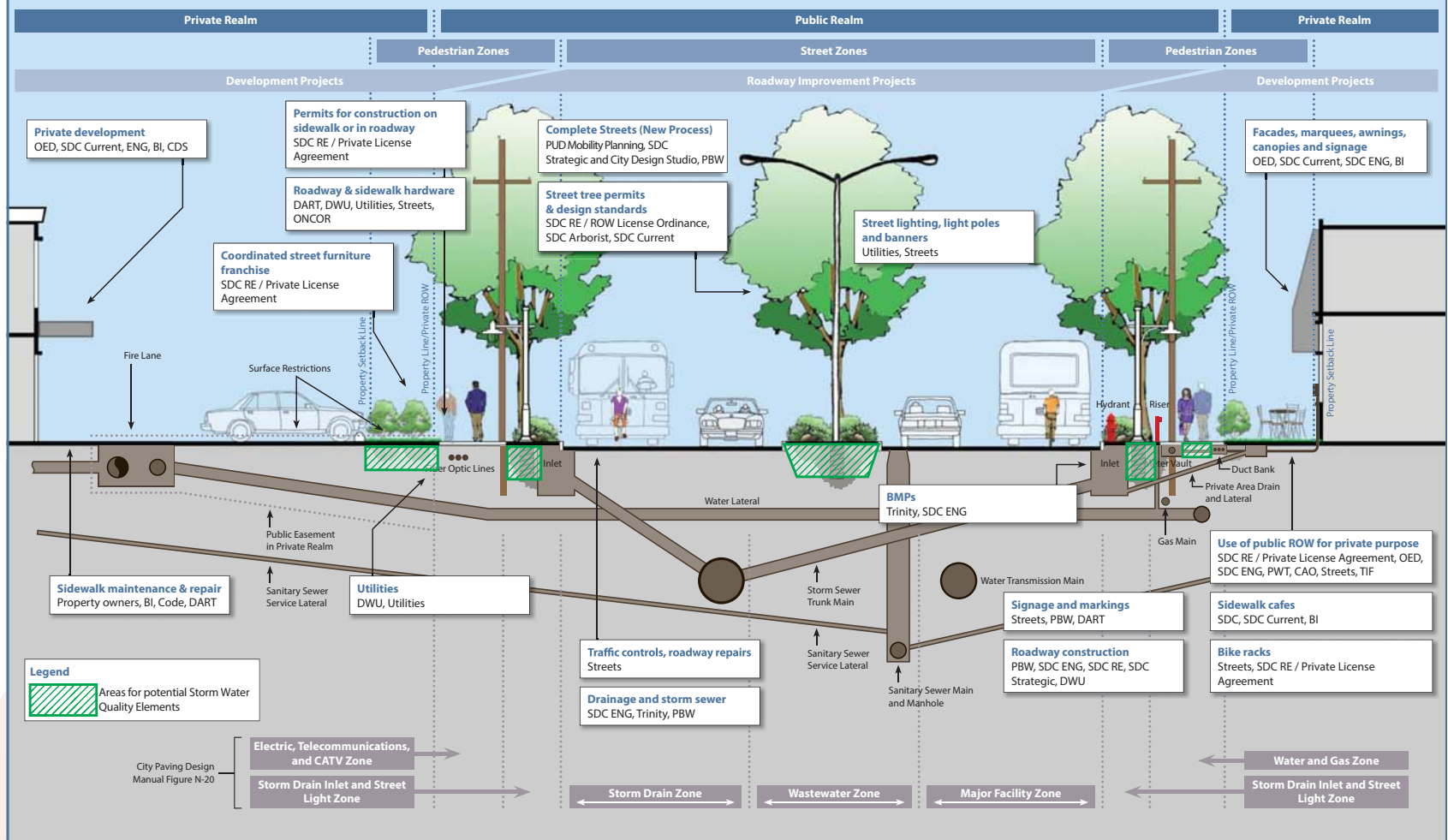
In initial interviews with members of the development community, developers indicated that Complete Streets benefited their developments economically, and the majority of those interviewed expressed desire to be able to integrate their developments into lively, safe, and walkable communities. These developers expressed an interest in the greater predictability that Complete Streets will bring to future development. The street design policies and standards contained in this manual are intended to influence private development projects in the following ways:

- The Complete Streets Vision Maps establish expectations for various street types in terms of sidewalk and streetscape design guidelines, as well as how developments should interact with the street to support alternative modes of transportation. These Vision Maps are intended to inform all planning and development processes that affect street design and make it easier for consistent developments to obtain approvals.
- The Design Elements Priorities Chart in Chapter 3 prioritizes elements by street type—providing guidance to developers on how to design and build their infrastructure improvements.
- The Complete Streets Design Manual should be used as guidelines in City decision-making for development proposals involving zoning before the Plan Commission and City Council.
- The Complete Streets Design Manual should also be used as a basis for requiring streetscape enhancements for developments seeking tax increment financing or other public incentives or support.
- Development projects with thoroughfare frontage may involve some improvements within the public right-of-way that may trigger new Complete Streets infrastructure standards related to sidewalks and streetscape.
- The Complete Streets Design Manual should also be used as a guide for considering future code amendments to introduce new sidewalk and streetscape standards for developments within existing zoning districts.
- As small area and special area plans are prepared, Complete Streets principles should be incorporated to guide future infrastructure plans associated with development.
- As development pressures occur in potentially changing areas, the City may initiate corridor studies to gain property owner input and support for a unified corridor vision that can be implemented incrementally as development occurs.
- Finally, the Complete Streets Design Manual supports a more flexible approach to handling improvements along the pedestrian zone that straddles the public and private realm. This guidance is intended to be used in future code amendments to make it easier for private developments to invest in street improvements that contribute to a more Complete Street.

Department/Agency Involved in Roadway Planning & Construction

CAO	City Attorney's Office
CDS	City Design Studio
Code	Code Compliance
DART	Dallas Area Rapid Transit
DWU	Dallas Water Utilities
Fire	Dallas Fire Rescue
DFRD	Fire Prevention & Investigations Bureau
Housing	Housing and Community Services
OED	Office of Economic Development
OED TIF	Tax Increment Financing District (as applicable)
Parks	Park and Recreation
PBW	Public Works
PBW Coord	Intergovernmental Coordination
PD	Dallas Police Department
PUD	Planning and Urban Design
SDC	-Sustainable Development & Construction
SDC Arborist	Sustainable Development & Construction – Arborist
SDC BI	Sustainable Development & Construction – Building Inspection
SDC Current	Sustainable Development & Construction – Current Planning
SDC ENG	Sustainable Development & Construction – Engineering
SDC RE	Sustainable Development & Construction – Real Estate Division
SDC RE / PLA	Sustainable Development & Construction – Real Estate Division (for Private License Agreement)
SDC Strategic	Sustainable Development & Construction – Strategic Planning
Streets	Street Services
TWM	Trinity Watershed Management
Utilities	Utilities: Atmos, Verizon, ATT, ONCOR

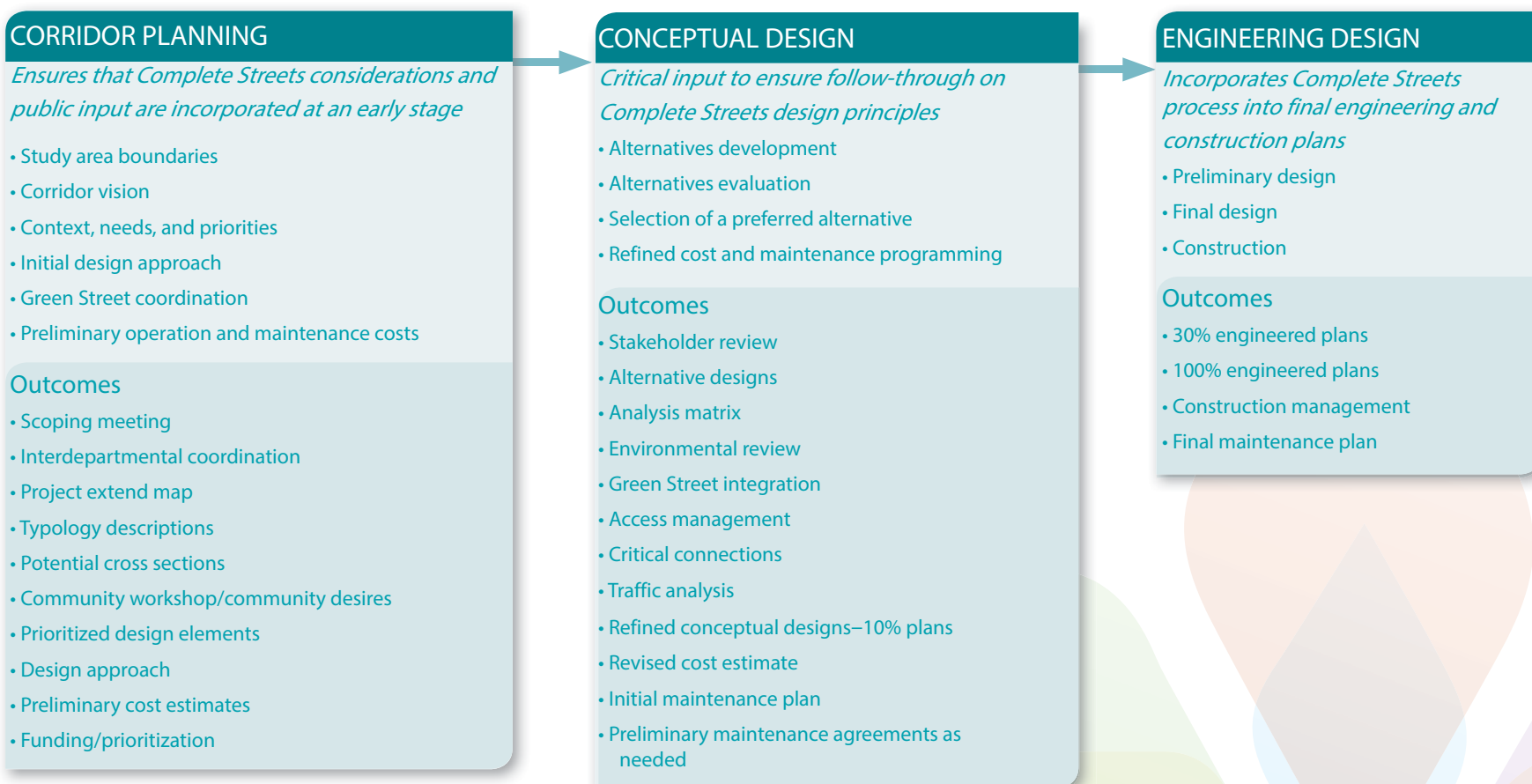
Agency Roles on the City's Streets



City Department Roles and Responsibilities

Street design and development involves many departments and agencies from several jurisdictions. The “Agency Roles on the City’s Streets” graphic on the next page shows a typical street cross-section, above and below grade, and illustrates the various agencies, departments and entities that have authority in the public right-of-way. This graphic should be used to communicate and coordinate on the complex and overlapping issues and the required reviews and approvals. The table on this page names the agencies, authorities and other organizations that are frequently involved in the design of streets in the City of Dallas. This list is provided as a reference tool for informational purposes only, and is not an exhaustive list.

Because so many departments and agencies are involved in road building, implementation of a Complete Streets program will require effective leadership and coordination of resources and responsibilities across departments. Strategies include assigning a leadership and ombudsman role to one group or department, establishing team responsibilities to include coordination of all aspects of Complete Streets policy implementation, and providing ongoing staff training to institutionalize Complete Streets design practices.





Right-of-way space is allocated in the corridor planning process.

DETAILED COMPLETE STREETS DESIGN REVIEW PROCESS

The Complete Streets process evaluates the many conditions and factors affecting the roadway design, and uses a three-phase process to develop projects: corridor planning, conceptual design, and engineering. This section describes the three phases and then reviews the steps in each of the three phases.

THREE-PHASE DEVELOPMENT PROCESS

Corridor Planning

Corridor planning is essential for resolving Complete Streets issues related to allocation of right-of-way space for various functions, prioritization of design elements, and preliminary definition of costs and approaches to maintenance. The corridor planning process builds on the tools provided in this manual, including the vision map, street type characteristics, priority charts, lane width charts and the best practices examples.

Kick-Off Meeting

The project kick-off meeting ensures that all agencies involved in the implementation are brought into the process at the onset. A

coordinated kick-off meeting to discuss the information presented in the Complete Streets Design Review Checklist (see Appendix A), and determine critical issues and steps forward must be scheduled. Projects initiated by the City must have a pre-application meeting with the following departments represented:

- Planning and Urban Design
- Public Works
- Sustainable Development and Construction
- Street Services
- Dallas Water Utilities
- Economic Development
- Park and Recreation
- Trinity Watershed Management
- Others as appropriate

The first step in initiating and planning for a Complete Street is to fill out as thoroughly as possible the Complete Streets Design Review Checklist developed for this manual.

The Checklist requests a range of information about existing and proposed conditions, project information, street operations, iSWM and environmental conditions, and the Paving Design Manual requirements. The purpose of this checklist is to help identify the issues associated with the proposed street construction and ensure that appropriate City and agency representatives are at the table to facilitate the design and construction

of the desired project.

Key process and decision points in the corridor planning and project initiation phase include:

- Develop project scope
- Identify agency lead and review departments
- Identify adjacent and overlapping projects
- Integrate surrounding land use, environmental, social, and historical context
- Review related plans (City or institutional)
- Engage community stakeholders in developing a corridor vision
- Estimate project costs
- Propose design and construction timeline
- Identify design and construction funding sources
- Amend the thoroughfare plan if necessary, based on preliminary design concepts

Conceptual Design

Concept designs are typically developed through extensive community and interagency review. Designs must adhere to the Dallas Complete Streets policies and design guidelines. The conceptual design phase should explore and present detailed design considerations to ensure that proposed elements are feasible. Key process and decision points in the conceptual design phase include:

- Complete traffic and operations analysis/impact/modeling as needed
- Develop design alternatives and select a final alternative based on community stakeholder input
- Propose a roadway and urban design plan with alignment and dimensions of sidewalks; lane functionality for motor-vehicles, bicycles, parking and transit; proactive accommodation for the disabled; street furniture; and intersection geometry
- Programming
- Integrate green features to promote sustainability, and smart features to maximize technological advances, into the design
- Flag potential right-of-way issues, easements, area-ways, and conflicts with major utility lines
- 30% design
- Develop preliminary cost estimate

Engineering Design

Engineering design involves extensive review by City agencies to ensure that all technical standards are being met. Applicable state and federal agency requirements may need to be followed, particularly if they are funding the project. Detailed requirements and review processes are customized project-by-project in contract documents.

Key steps in the design development and review process include:

- Street and sidewalk surveys and inventory
- Sub-surface investigation
- Design submission and approvals to progress from 25% design to 75% design with public process plan
- Right-of-way plans coordinated with abutters
- Utilities plans coordinated with utility companies
- 100% design and PS&E
- Finalization of maintenance agreements

Approvals and/or detailed review are required from the following agencies:

- Transportation Planning
- Dallas Public Works Department (including Lighting Division)
- Commission for Persons with Disabilities, Architectural Access Board
- Dallas Redevelopment Authority
- Dallas Water Utilities
- Dallas Park and Recreation Department
- Dallas Fire Department
- TxDOT district office (if necessary)

Based on location and design features additional review may be required by the Coordinated Street Furniture Program, the Dallas Groundwater Trust, Dallas Conservation Commission, Dallas Landmark Commission, and the Dallas Arts Commission.



Grand Avenue – existing condition



Photo simulation of Grand Avenue as a Complete Street

Construction Management

Construction will be managed by a resident engineer typically appointed by the Dallas Public Works Department, or by TxDOT for state and federal funded projects.

Key steps to steer the project to completion include:

- Construction bids, contractor selection and award
- Preparation of Construction Management Plan (CMP) (SDC and PWB approval)
- Development of a public notification plan and designation of a point of contact for the public
- Monitoring of construction to ensure quality standards are met, change-orders are reviewed, and community impact mitigation commitments are honored
- Punch list
- Accept equipment, installation, and materials

Developing a Maintenance Plan

An important consideration for a successful Complete Streets project is the determination of cost and responsible entities to maintain the various elements. A partial list includes the following:

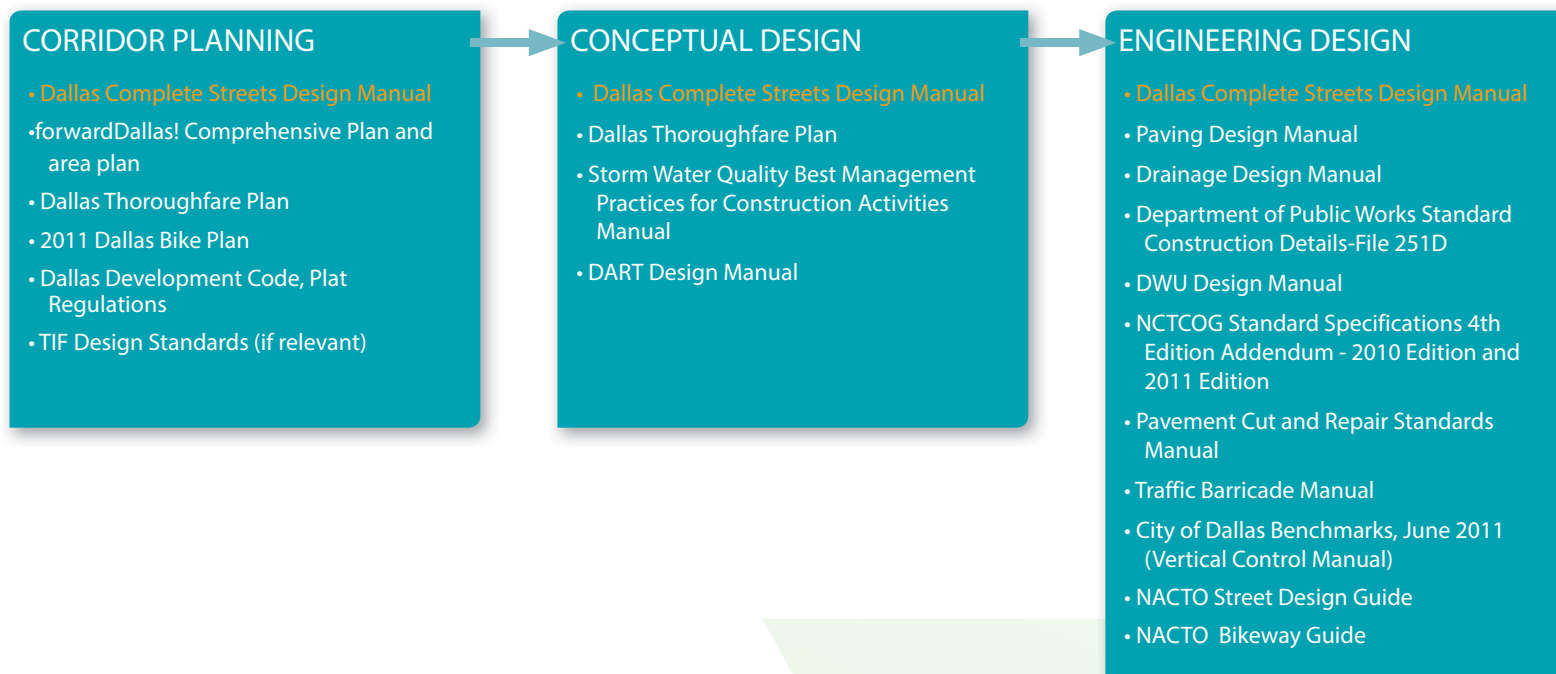
- Maintenance and Indemnification Agreements
- Tree inspection during warranty
- Update of databases/asset management
 - Tree database
 - Pavement markings plan
 - Signal timing plan
 - Street cleaning schedule
 - Trash collection routes
- Plan for pruning, seasonal plantings, lighting and decorating, and abutter responsibilities
- Plan for power washing, drain clearance, and recharge basin silt removal
- Source list for all materials

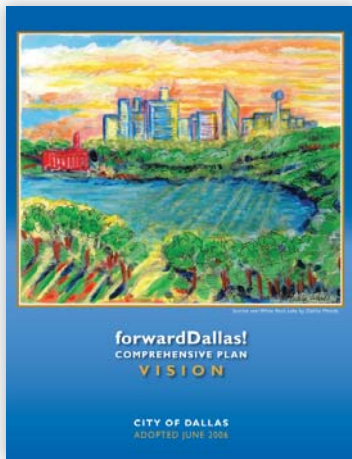
EXISTING MANUALS AND STANDARDS

Designing a Complete Street entails the use of new tools in this manual in combination with all of the traditional design tools and guidelines available to plan and design a safe transportation project that meets all of the current design standards. The Complete Streets Design Manual provides a road map for the corridor planning and conceptual design process. The diagram below lists many of the resources that must be considered in a Complete Streets project.

These documents range from planning documents to regulatory documents to required engineering standards. These resources are provided by phase of street development. Please reference the latest edition of each reference during the design process.

Relevant Manuals And Standards by Project Phase





Corridor Planning Documents and Standards forwardDallas! Comprehensive Plan

The forwardDallas! Comprehensive Plan, adopted in 2006, provides a consensus vision for Dallas that is based on the ideals and goals residents have for Dallas' future. The forwardDallas! Policy Plan provides the overall policy framework to guide decisions over time toward achieving the Vision. The Policy Plan guides decisions made in regard to land use, housing, transportation, neighborhoods, environment, economic development, and urban design. The Transportation and Urban Design Elements set policy for achieving more walkable and pedestrian-friendly development throughout Dallas.



Dallas Thoroughfare Plan

The Thoroughfare Plan provides a detailed discussion of the history, purpose, approach, and goals of the thoroughfare system within Dallas and details the current functional and dimensional classifications of the thoroughfares in the City. The plan provides descriptions of designated routes and minimum and standard pavement cross sections.

2011 Dallas Bike Plan

The primary purpose of and impetus for the 2011 Dallas Bike Plan is to provide an update to the 1985 Dallas Bike Plan. The 2011 Bike Plan update provides a master plan and an implementation strategy for a new bicycle network—the Dallas Bikeway System—which will be made from designated on-street and off-street facilities. This document also provides recommendations for supporting policies and the identification of bicycle-related programs to be recognized, sponsored, or supported under the Plan. The guiding principles for the 2011 Dallas Bike Plan provides an overarching mission and vision, in addition to goals and objectives. The guiding principles form the basis for the identified programs, specifications for the Dallas Bikeway System Master Plan, and the implementation strategy. Ultimately, the 2011 Bike Plan is structured around creating actions to implement the goals and objectives.

Dallas Development Code

The Dallas Development Code, Chapter 51A provides the regulations governing zoning and platting that define streetscape standards associated with private development as well as minor street standards. Paving infrastructure requirements for developments in the City of Dallas are provided in this code. Additional relevant Chapter 51A articles that should be consulted include Article V, Flood Plain and Escarpment Zone Regulations, and Article IX, Thoroughfares. The Dallas Development Guide details the development process.

iSWM Criteria Manual

The purpose of this manual is to provide design guidance and a framework for incorporating effective and environmentally sustainable stormwater management into the site development and construction processes and to encourage a greater regional uniformity in developing plans for stormwater management systems that meet the following goals:

- Control runoff within and from the site to minimize flood risk to people and properties
- Assess discharges from the site to minimize downstream bank and channel erosion
- Reduce pollutants in stormwater runoff to protect water quality

and assist communities in meeting regulatory requirements

Note: The City's Drainage Design Manual update will incorporate iSWM techniques.

Following the criteria provided in the manual will help projects meet sustainable development goals for public roadways as well as private development by integrating appropriate green infrastructure elements within the street cross-section. There are many ways that sustainable development may be achieved while following these criteria. For example, a development that reduces individual lot imperviousness and a development that has high lot density in one area and a large open space in another can both meet sustainability requirements.

Conceptual Design Documents and Standards

Thoroughfare Plan

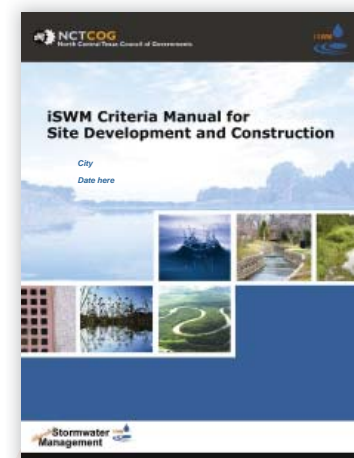
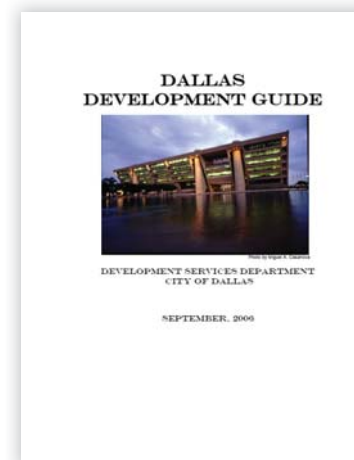
(Described above under Corridor Planning)

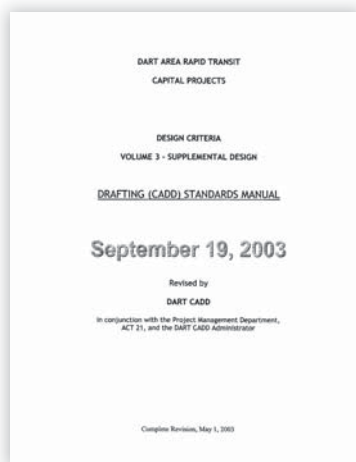
The Storm Water Quality Best Management Practices For Construction Activities Manual

The Storm Water Quality Best Management Practices for Construction Activities Manual provides the guidelines, criteria, and standard details for the design of storm water pollution prevention plans which may be required on City construction projects.

TIF Design Standards

The City's Tax Increment Finance (TIF) program is used to finance new public improvements in designated areas. The goal is to stimulate new private investment and thereby increase real estate values. Potential improvements include wider sidewalks, utilities, public landscaping, lighting, environmental remediation, demolition, and historic façades etc. The City's TIF Districts provide design standards and criteria for these public improvements to provide for a consistent, unified design within the district.





DART Design Manual

The Dallas Area Rapid Transit Light Rail Project Design Criteria Manual provides design standards and criteria for future development and expansion of the system. The purpose of this manual is to establish a standard to maintain the safety, image, and efficient function of the transit system. The design characteristics covered in this manual include standards for the vehicles, traction electrification, signal system, communications, the control center and supervisory system, fare collection, corrosion control, system grounding and raceways, operations, reliability and maintainability, and systems safety.

Engineering Design Documents and Standards Paving Design Manual

The purpose of the Paving Design Manual is to provide guidelines for designing streets and thoroughfares and preparing construction plans in the City of Dallas. These guidelines will be used by the Department of Public Works, other City departments, Consulting Engineers employed by the City for street and thoroughfare improvement projects, and engineers for private developments in the City of Dallas. The standards set forth in this document are the minimum criteria permitted by the City of Dallas to be used in paving design. Unusual circumstances or

conditions may arise which require variance from the standards. Any variances from the standards set forth in this manual must be accompanied by prior written approval from the Director of Public Works.

The scope of this Paving Design Manual includes the various design elements, criteria, standards, and instructions required to prepare paving plans for the Department of Public Works. Included in the manual is the classification of the various streets according to the City Thoroughfare Plan. Geometric design standards to be used on the various classifications and criteria for design of pavement structures are also presented. These guidelines should result in the construction of safe, economical, comfortable riding streets, and thoroughfares carrying acceptable traffic volumes while providing for pedestrian traffic as well.

In the Complete Streets Design Manual, design speed is replaced by target speed which is based on the functional classification, thoroughfare functional classification, thoroughfare type and context, whether the ground floor land uses fronting street are predominantly residential or commercial. Target speed then becomes the primary control for determining the following geometric design values:

- Minimum intersection sight distance
- Minimum sight distance on horizontal or vertical curves; and
- Horizontal and vertical curvature

Drainage Design Manual

The purpose of the Drainage Design Manual is to provide guidelines for designing facilities in the City of Dallas. This manual is for use by the Department of Public Works (DPW), other City departments, consulting engineers employed by the City, and engineers for private development in the City. It is not intended to limit the design capabilities or engineering judgment of the design professional, nor to limit the use of new technical developments in engineering. Special problems may require special methods. Unusual circumstances or special designs requiring variance from standards within this manual may be approved by the Director of Public Works. The City's Drainage Design Manual update will incorporate iSWM techniques.

The guidelines contained in this manual have been developed from a comprehensive review of basic design technology as contained in various engineering publications, and through the experience of individual engineers who have contributed to the content. This manual addresses storm drainage situations which are generally relative to the City of Dallas and its immediate geographical area. Accepted engineering principles are applied to these situations in detailed procedures. The documentation is not intended to limit initiative, but rather is included as a

standardized format to aid in design, and as a record source for the City. Additional information on the City of Dallas regarding drainage design can be found in the Development Code under the Floodplain, Escarpment, Platting Regulations, and in adopted Floodplain Management Plans.

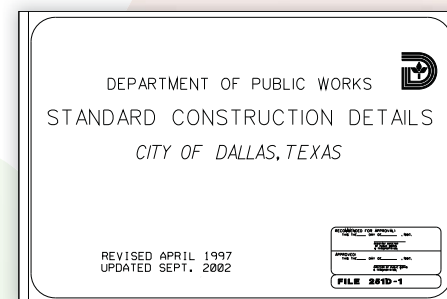
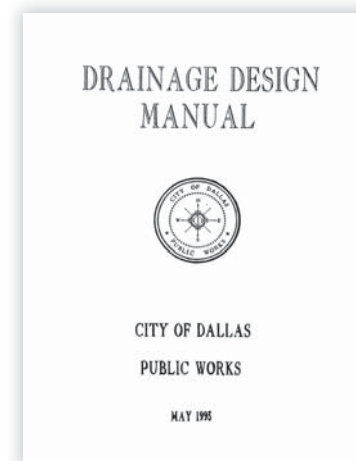
The Drainage Design manual will be incorporated into the iSWM Criteria Manual when it is adopted.

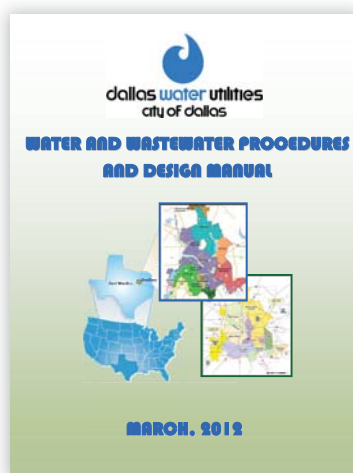
Standard Construction Details - File 251D

The Standard Construction Details provides standard detailed paving, drainage, traffic control, and related facility drawings showing construction items and features to be used with paving plans provided for the City of Dallas.

Paving plan designs prepared for the City shall be consistent with the Standard Construction Details, as currently amended. Specific details have specific functions and uses, and this set of standard details must not be considered a catalog from which to choose. Special situations will require the designer to develop special details for approval from the Director of Public Works.

This document is presently being updated to include revised cross-sections and details. Coordination of this update with the measure contained within this manual is recommended.



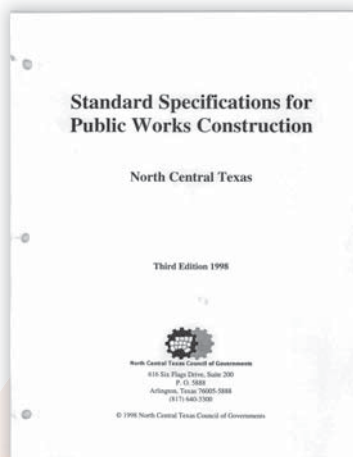


Water and Wastewater Procedures and Design Manual

This manual is provided for use in the design and construction of water, wastewater and reclaimed water mains owned and operated by Dallas Water Utilities (DWU). DWU provides water, wastewater and reclaimed water services to customers within the City of Dallas and other adjacent communities.

NCTCOG Standard Specifications for Public Works Construction

The North Central Texas Council of Government (NCTCOG) Standard Specifications provides a framework for public works construction. These standards strive to decrease construction costs while allowing the implementation of new technologies, materials and methods. The standards recorded in the document list the materials and construction methods supported by NCTCOG. The construction method standards include provisions for erosion and sediment control, pavement systems, water distribution, wastewater collection, and stormwater drainage.



These regional provisions are recommended by the Public Works Advisory Committee. Modifications to these standards must be developed under the supervision and seal of a registered professional engineer and the program manager of the Public Works Department.

NCTCOG Standard Specifications for Public Works Construction, 3rd & 4th Editions Addendum

(Described above under NCTCOG Standard Specifications)

The Addenda to the 3rd & 4th Editions set forth exceptions and requirements of the City of Dallas Public Works Department, and are consequently the most current standards to be followed. These specifications take precedence over existing requirements and conditions listed in previous standards.

Pavement Cut and Repair Standards Manual

The Pavement Cut and Repair Standards Manual provides a reference for the repairs, excavations, installations, restorations and other operations to streets within the Dallas area. The standards detailed in the manual are set as the current methods to be utilized, with the provision that new methods and technologies may be employed as well. Engineering, technical, and

other criteria and standards to be improved upon must be approved by the Director of the Public Works Department. However, it is the responsibility of the permit holder to make certain that current standards are being used. The purpose of this manual is to ensure the safe and durable construction of roadways.

Traffic Barricade Manual

The primary function of this manual is to promote the safe and efficient movement of people and goods by providing traffic safety guidelines for persons working in or near the public right-of-way. This manual provides guidance for implementing the most effective temporary traffic control for urban streets. This manual is based on Manual Uniform Traffic Control Devices (MUTCD), which defines national standards.

City of Dallas Benchmarks, June 2011 (Vertical Control Manual)

This list of City of Dallas survey benchmarks was compiled using modern Global Positioning equipment and techniques. It contains elevations in NAD27 for most benchmarks, as well as both State Plane Coordinates and Latitude and Longitude based on NAD83.

To inquire about these documents, please contact:

Public Works Department
320 E. Jefferson Blvd. Room 307
Dallas, TX 75203
214-948-4250
http://www.dallascityhall.com/public_works/index.html

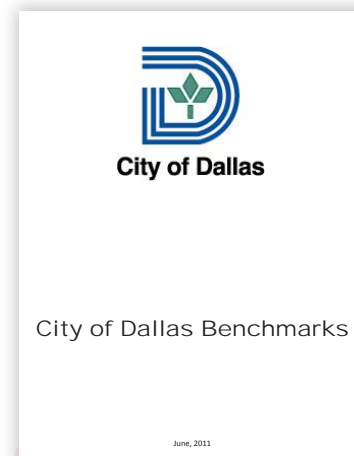
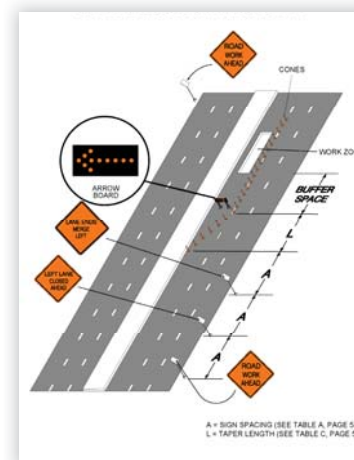
Federal and State Design Manuals

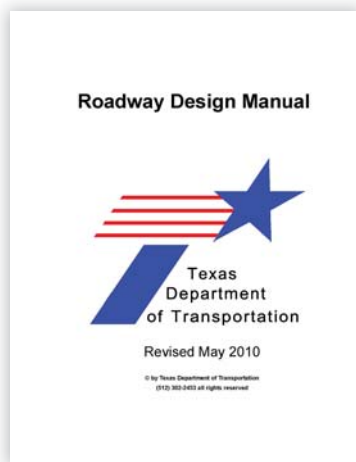
Local governments that wish to use certain federal funds must use a street classification system based on arterials, collectors, and local streets. These funds are for streets and roads that are on the federal aid system. Only arterials and certain collector streets are on this system. The federal aid system encourages cities to designate more of these larger streets, and to concentrate modifications along these larger streets.

AASHTO Green Book

A Policy on Geometric Design of Highways and Streets (The Green Book) provides guidance for designing geometric alignment, street width, lane width, shoulder width, medians, and other street features. The Green Book applies only to streets and roads that are part of the National Highway System (NHS). These are Interstate Freeways, principal routes connecting to them, and roads important to strategic defense. Although the Green Book's application is limited to these streets, some cities apply these recommendations to all streets.

Further, the Green Book provides guidance that cities often unnecessarily treat as standards. The Green Book encourages flexibility in design within certain parameters as evidenced by the AASHTO publication A Guide to Achieving Flexibility in Highway Design. For example, 10-foot lanes, which cities often avoid out of concern of deviating from standards, are within AASHTO guidelines.





TxDOT Roadway Design Manual

The TxDOT Roadway Design Manual (RDM) applies only to state highways and bikeways within local jurisdictions. If cities deviate from the minimum widths and geometric criteria for bikeways spelled out in Chapter 6, they are advised to follow the exemption process or experimental process as applicable. The RDM does not establish legal standards for designing local streets.

MUTCD and TMUTCD

The Manual on Uniform Traffic Control Devices (MUTCD) and the Texas version of the MUTCD (TMUTCD) provide standards and guidance for the application of all allowed traffic control devices including roadway markings, traffic signs, and signals. The Federal Highway Administration oversees application of the MUTCD.

The rules and requirements for the use of traffic control devices are different than for street design criteria. Local agencies have limited flexibility to deviate from the provisions of the TMUTCD in the use of traffic control devices due to the relationship between the TMUTCD and state law. The TMUTCD does provide flexibility within its general provisions for items such as application of standard traffic control devices, use of custom signs for unique situations, traffic sign sizes, and sign placement

specifics. In contrast, agencies do not generally have the flexibility to develop signs that are similar in purpose to signs within the manual while using different colors, shapes, or legends. Agencies are also not authorized to establish traffic regulations that are not specifically allowed by, or are in conflict with, state law. The provisions of the TMUTCD and related state laws thus make it difficult to deploy new traffic control devices in Texas. This can result in complications, especially in the areas of speed management, pedestrian crossings, and bikeway treatments.

The State of Texas and the Federal Highway Administration have procedures that allow local agencies to experiment with traffic control devices that are not included in the current TMUTCD.

Both the MUTCD and TMUTCD are amended through experimentation. After one or more experiments have shown benefit, the new devices are sometimes adopted into these manuals. In Texas, the Vehicle Code must be changed first if it prevents use of the new device.

The MUTCD and TMUTCD establish warrants for the use of some traffic control devices. For example, stop signs, traffic signals, and flashing beacons are expected to meet minimum thresholds before application. These thresholds include such criteria as number of vehicles, number of pedestrians or other uses, distance to other devices, crash history, and more. These warrants often prevent local engineers from applying devices that, in their opinion, may improve safety. For example, pedestrian crossings on a busy, high-speed, wide arterial street may need signals for user safety, but may not meet the warrants.

As with street design guidelines, cities may establish their own warrants or modify those suggested by the TMUTCD to suit their context in order to use some traffic control devices. In special circumstances that deviate from their own warrants, cities need to document their reasons for the exception. As an example, they may say the trail crossings or school crossings qualify for certain traffic control devices.

Texas Fire Code

The Texas Fire Code can impede street design in limited circumstances. The state legislature has adopted the National Fire Code. The National Fire Code is written by a private agency and has no official legal standing unless states or municipalities adopt it, as has been done in Texas. The primary barrier caused by this adoption is the requirement for a minimum of 20 feet of an unobstructed clear path on streets. In order to comply, streets with on-street parking on both sides must be at least 34-feet wide. This prevents municipalities from designing “skinny” and “yield” streets to slow cars and to make the streets safer, less land consumptive, and more hospitable to pedestrians and bicyclists.

There are ways around this requirement. If the local jurisdiction takes measures such as installing sprinklers and adding extra fire hydrants, or the adjacent buildings are built with fire retardant materials, it may be able to get the local fire department to agree to the exception.

Alternatively, the state legislature could repeal its adoption of the 20-foot clear path requirement due to

- The arbitrary and unresearched nature of the provision
- The safety problems associated with the resulting excessively wide streets
- The contradiction that this provision causes with properly researched guidelines and standards by ITE, CNU, AASHTO, and others for streets under 34 feet wide

It is likely that the state legislature was unaware of these issues when it adopted the code in its entirety.

Texas Streets and Highways Code and Texas Vehicle Code

The Texas Streets and Highways Code and the Texas Vehicle Code include laws that must be followed in street design. These are embodied in the TMUTCD. Changes to the Streets and Highways Code and the Vehicle Code may cause the TMUTCD to change.



2. THE DALLAS COMPLETE STREETS VISION



Complete Streets provide an opportunity to improve the return on infrastructure investments by integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of the project, which spares the expense of retrofits later.



2. THE DALLAS COMPLETE STREETS VISION



Bicycle and public transit

The Dallas Complete Streets Vision is to build streets that are safe and comfortable for everyone; young and old, motorists and bicyclists, walkers and wheelchair users, and bus and train riders alike. A key element of the Complete Streets Vision is to incorporate a green approach to the roadway design process in order to reduce the impacts on the stormwater system and create an environment where safe, comfortable, and healthy streets are the preferred design choice.

This chapter describes a new Complete Streets Typology that takes into account a range of street contexts and all modes of travel, and establishes Complete Streets Vision Maps that overlay these street types on the City's thoroughfare system.



Multimodal thoroughfare



Conventional, Separate-Use:

Large shopping centers represent a typical conventional, separate-use development. The forwardDallas! Plan calls for these areas to become more like town centers, with transit enhancements as well as landscape buffers from the street including sidewalks and street trees.



Walkable, Mixed-Use:

This West Village sidewalk cafe provides a great example of a walkable, mixed-use development—outdoor seating and retail on the lower floors, and residential living on the upper stories.

COMPLETE STREETS CONTEXT AND DEVELOPMENT POLICY

The Complete Streets approach designs streets for all modes of travel including pedestrians, cyclists, public transit, and freight and private vehicles. This approach also designs streets to suit the surrounding neighborhood character, as well as the capacity needs for all modes of transportation, with emphasis given to varying modes, depending on the type of street. The different types of Complete Streets take into account the context or types of uses within an area to then determine the most appropriate design for the use of the right-of-way.

Along with the more flexible functional classification design standards, the street context—or the character of the area adjacent to the roadway—plays an important role in the way a street looks. One type of street design will not satisfy all of the different needs within the City and, therefore, it is important that the design standards offer flexibility to allow for these distinctions. There is no “one size fits all” in the framework of street design.

The forwardDallas! plan identifies two overall categories of contextual building blocks: Conventional, Separate-Use and Walkable, Mixed-Use. These building blocks, shown in the forwardDallas! Vision Illustration, provides an important basis for identification of the Complete Street types in the Complete Streets Vision Map shown later in this chapter. Historically, Dallas has been developed in the Separate-Use context, but recent trends see widespread desire to implement more Walkable, Mixed-Use development in many parts of the City.

Conventional, Separate-Use Contexts are areas that follow a development pattern of defining distinct areas for housing, jobs, and shopping. These areas are typical of post-war, suburban cities and are inherently more dependent on the automobile. They are characterized by large parking lots and faster-moving arterial traffic. Opportunities for walking and bicycling tend to be limited and, for the most part, are confined to quieter residential areas.

Walkable, Mixed-Use Contexts are areas where a healthy balance of housing, jobs, and shopping exist in proximity with one another, allowing residents to live, work, shop, and play all in the same neighborhood. Public transit, bicycling, and walking are priorities in these areas, though cars continue to play an important role. These areas have slower moving traffic, and the average car trip is shorter. This Manual is a key tool in implementing the forwardDallas! mixed-use building blocks.

Complete Streets policies reflect the characteristics of a city's built environment. Approximately 15 percent of Dallas' total land area consists of roadway and rail infrastructure, creating a substantial opportunity to influence the economic, environmental and social health of the city through complete streets improvements.

There are a significant number of jobs and city residents located within ½ mile of DART light rail stations as well, increasing the likelihood of replacing automobile trips with transit, cycling, and walking modes through complete street designs. With recent mixed use development around many light rail stations, it's expected that employment and residential densities will continue to grow within Dallas' light rail station catchment areas. With about 30 percent of the city's population already living in census blocks with densities greater than 15 persons per acre, Dallas can leverage the benefits of higher density neighborhoods through sustainable transportation policies.

Stats		Percentage
Total city land area dedicated to roadway/ other infrastructure (acres)	38,112	15%
Number of jobs within 1/2 miles of DART stations	269,969	13%
2010 Population within 1/2 miles of DART stations	172,948	14%
2010 Population living in census blocks with population density of >15 persons per acre	359,094	30%

CLASSIFYING COMPLETE STREETS

The Complete Streets approach promotes designing streets for all—including pedestrians, bicyclists, transit users, and motorists—while taking into consideration both the context and the functional classification. This approach assumes that all streets present opportunities to be complete, although each will need different priority design elements depending on the context. Complete Streets typology considers context, location, and use to guide appropriate design of streets. The following section presents detailed descriptions and preferred elements for each of the Complete Streets types:

- Mixed-Use streets
- Commercial streets
- Residential streets
- Industrial streets
- Parkways

MIXED-USE STREETS

Downtown streets and other main streets are examples of mixed-use streets. These streets encompass a variety of types of streets and land use contexts, from downtown to small Main Street locations. Buildings are usually close to the street and offer a vibrant blend of opportunities to live, work, shop, and play. In their present form, these streets already have some pedestrian and bicycle activity. In some locations, they carry heavy traffic. On Mixed-Use Streets, the focus is on slower traffic speeds and a greater emphasis on pedestrian, bicycle, and transit choices. This street type is the most flexible and has a multipurpose use of street space.

On-street parking is common along Mixed-Use Streets. Often there are large parking areas and other auto-oriented land uses located at the edges of commercial areas. The design of Mixed-Use Streets is targeted for pedestrians, bicyclists, and transit users with the goal of reducing motor vehicle speeds and creating a more desirable environment for people.

A range of street types are identified in the Downtown Dallas 360 Plan that further define streets and uses in the City Center, and additional information can be found in the CBD Comprehensive Transportation Plan.



Mixed-Use street

Key Features

- Higher pedestrian activity
- Reduced motor vehicle speeds
- Bike lanes or shared use of travel way
- Pedestrian-oriented development, street furniture, and lighting
- Mix of commercial, residential, and civic uses oriented to the street
- Maximized on-street parking
- Higher transit accessibility

MIXED-USE STREET EXAMPLES



Main Street at Akard Street—two-lane thoroughfare with on-street parking



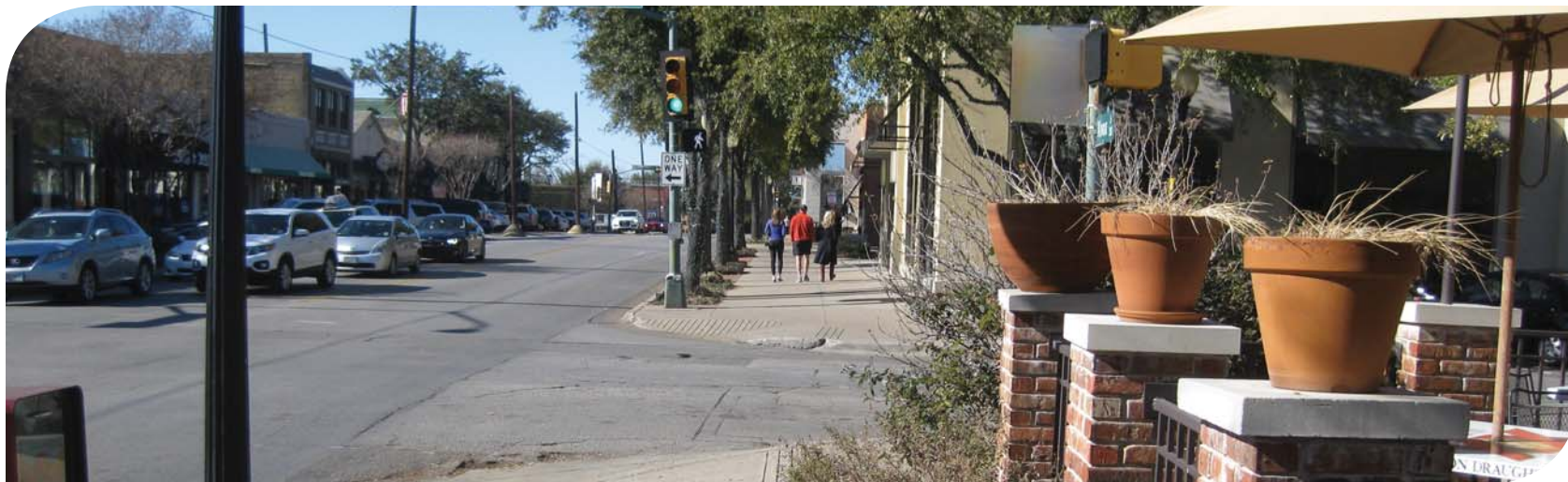
Greenville Avenue at Alta Avenue—two-lane local road (Main Street)



State Street at Allen Street—two-lane street with on-street parking, buffer, and sidewalk



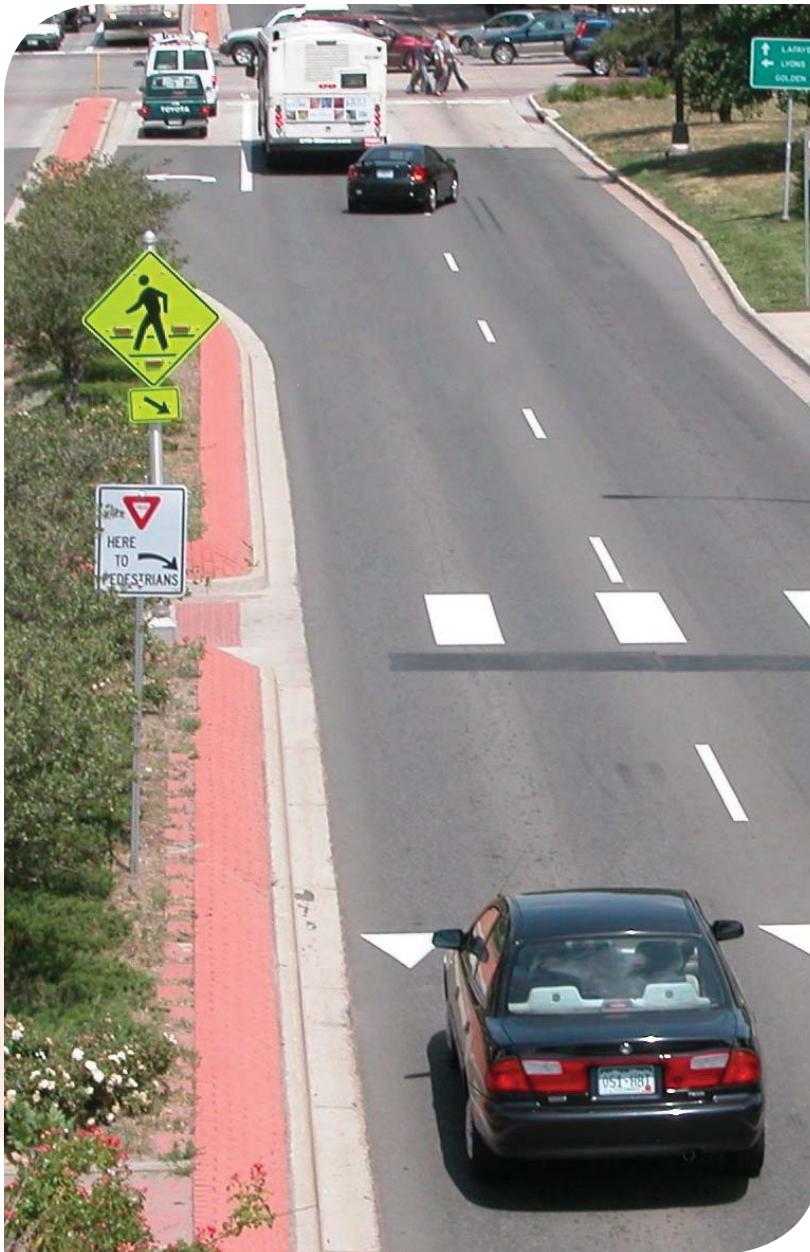
State Street at Ellis Street—two-lane street with on-street parking, buffer, and sidewalk



Knox Street at Cole Avenue—four-lane thoroughfare with head-in parking and combination of adaptive re-use and new buildings



Preston Road between Wentwood and Northwest Highway—four- and five-lane thoroughfare with head-in parking



Commercial street

COMMERCIAL STREETS

These streets serve mostly commercial or institutional areas with low densities. Buildings are likely set back from the road and do not feature on-street parking. These streets are often multi-lane and serve faster moving traffic and provide regional connections. However, there are many opportunities for improving walking, biking, and transit between destinations in this street type.

Adjacent land uses function as service and job destinations, with buildings located on separate parcels. Land uses include offices, restaurants, and a range of retail and commercial uses. Adjacent land uses may also include multifamily housing in low- to mid-rise apartment buildings.

Commercial Streets should be designed to accommodate pedestrians while still maintaining vehicle traffic flow. This objective may be inherently unpleasant for pedestrian, bicycle, and transit users, however, the safety of such users—particularly at intersections—is a paramount concern.

Key Features

- Emphasis on travel lanes and automobile capacity
- Access management with the use of landscaped median or two-way left turn lane
- Usually serve faster moving traffic
- Less use of on-street parking
- Lower pedestrian activity, but provide safe opportunities for use with wider landscaped buffers and sidewalks

COMMERCIAL STREET EXAMPLES



Lovers Lane at Devonshire Drive—four- and five-lane thoroughfare



Preston Road at Sherry Lane—four- and five-lane thoroughfare with off-street parking adjacent to roadway with sidewalk and buffer



Montfort Drive at Belt Line Road—four-lane divided with sidewalk and buffer



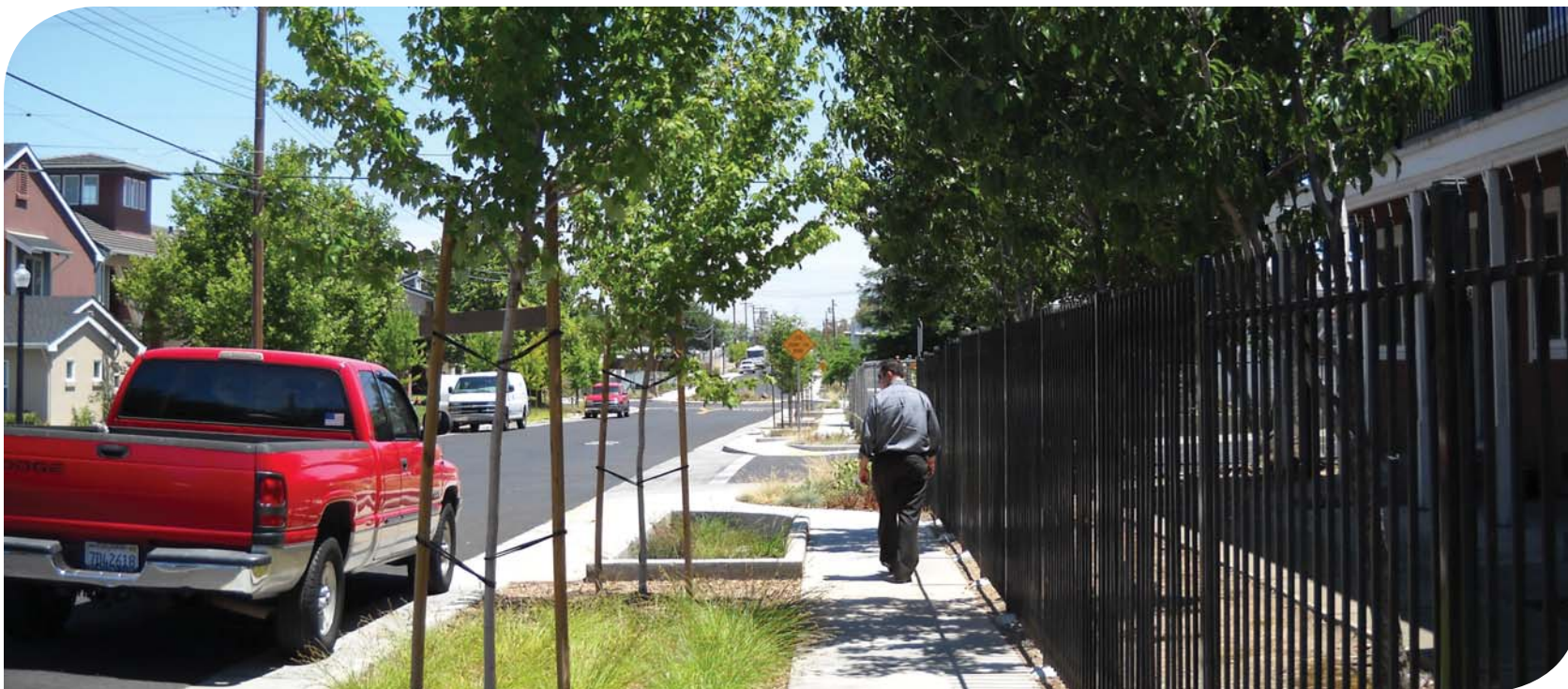
Garland Road at Buckner Boulevard—six-lane divided thoroughfare with bus stops, sidewalk, crosswalks, and planting zone



North Haskell Avenue at Capitol Avenue—six-lane divided thoroughfare with planting zone and sidewalks in a shopping strip environment



Preston Road at Weldon Howell Parkway—four- and five-lane thoroughfare with sidewalk, buffer, and off-street parking adjacent to commercial in a slip street format



Residential street

RESIDENTIAL STREETS

These streets serve residential land uses as well as schools, churches, and businesses within residential neighborhoods. Residential streets can vary from serving high volumes and fast moving traffic to serving moderate traffic volumes and lower speeds, depending on the surrounding neighborhood context. However, the dominant land use is single family. This type of street will likely also have pedestrian, bicycle, and transit activity to connect important neighborhood destinations.

Residential Streets are designed to reduce motor vehicle speeds and place an emphasis on intersection safety for pedestrians, bicyclists, and transit users.

Key Features

- Safety for pedestrians and bicyclists
- Medians on major facilities
- Increased sidewalk buffering from traffic through on-street parking, bicycle lanes, and landscaping

RESIDENTIAL STREET EXAMPLES



Winnetka Avenue at Temple Drive—two directional local road with historic single family homes



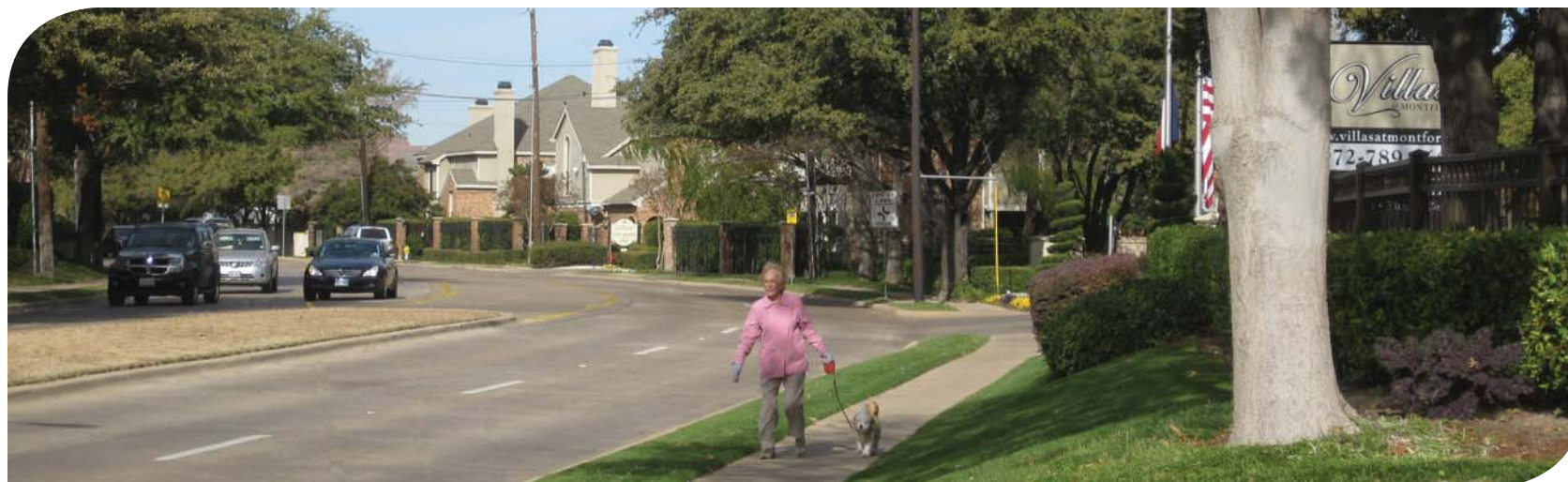
North Bishop Avenue at West Canty Street—four-lane thoroughfare with bike lane



West Colorado Boulevard at North Oak Cliff Boulevard—two-lane local road with single family homes, sidewalks, and wide buffer



Florence Street at Texas Street—two-directional local road



Montfort Drive at Preston Oaks Road—four- and five-lane thoroughfare with multi-family dwellings



Northwest Highway at Thackery Street—six-lane divided thoroughfare with single family homes on south side, multifamily with slip street on north side



Industrial street

INDUSTRIAL STREETS

Industrial Streets serve industrial corridors. They are built with wide lanes and intersections to accommodate trucks and other large vehicles. Industrial streets are located within large areas of land with a mix of low- and medium-density industrial buildings and industrial yards. They often have large surface parking lots for cars and trucks, and should have quality access. Due to the need for freight rail access, industrial streets are often linked to rail lines.

Transit, sidewalks, and pedestrian amenities are typically limited in these areas. However, industrial streets may serve as through-routes to adjacent land uses, and thus should provide for the safety of all modes. Pedestrian provisions should not be overlooked on industrial streets if residential or transit facilities are in the vicinity.

Key Features

- Emphasis on managing large truck traffic
- Ability to safely mix industrial traffic with vehicular and pedestrian traffic

INDUSTRIAL STREET EXAMPLES



Alberta Drive at Newberry Street—two-lane divided



North Cockrell Hill Road at Adler Drive—six-lane divided thoroughfare



Manor Way at Cedar Springs Road—two-lane street



Manor Way at Cedar Springs Road—two-lane street



Rock Quarry Road at North Cockrell Hill Road—four-lane local road



Royal Lane at Luna Road—six-lane divided



Parkway

PARKWAYS

Parkways extend through natural areas (such as lakes, rivers, floodplains, streams, and parks) where there is a desire to maintain or create a park-like feel to the roadway. Parkways serve sensitive areas—such as White Rock Lake, the Escarpment, and the Great Trinity Forest—and may be elevated over these areas. Design elements may include wide landscaped medians, structures with natural materials, and shared use paths alongside the roadway in lieu of sidewalks.

The primary objective of a parkway is to reduce motor vehicle speeds and provide safe intersections for pedestrians, bicycles, and transit users.

Key Features

- Emphasis on pedestrian, bike, and vehicle access to natural areas
- Shared use travelways
- Landscaped medians and edges

PARKWAY EXAMPLES



Turtle Creek Boulevard at Hall Street—six-lane divided thoroughfare



Garland Road at White Rock Lake spillway—six-lane divided thoroughfare



East Lawther Drive—two-directional local road with adjacent trail



Merriman Parkway at Fair Oaks Avenue—two-directional local road with adjacent trail



Mockingbird Lane at White Rock Lake Dog Park—six-lane divided thoroughfare



East Lake Highlands Drive at North Buckner Boulevard—six-lane divided thoroughfare

DALLAS COMPLETE STREETS VISION MAPS

The Complete Streets Vision Maps designate thoroughfares (excluding highways) throughout the entire City into contextual street types and bike or transit network street types. These designations are intended to serve as overlays on the Thoroughfare Plan functional and dimensional classification system. For example, a four-lane, divided arterial may be a Mixed-Use Street and also a Bike or Transit Network Street.

Contextual Street Types Overlay

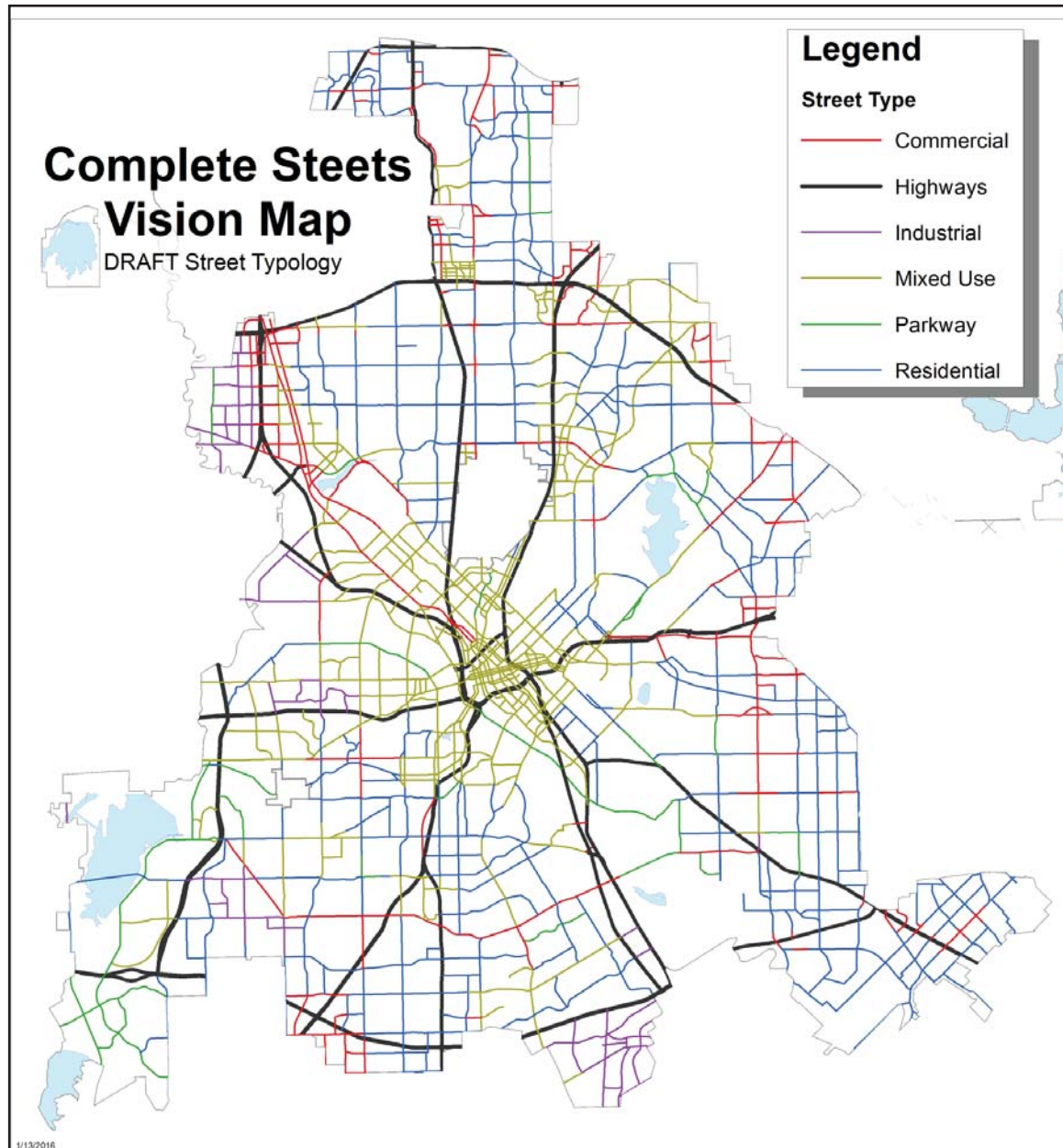
The Contextual Street Types Overlay Vision Maps identify five contextual street types: mixed-use streets, commercial streets, residential streets, industrial streets, and parkways. These designations, discussed in more detail in following sections, are shown on the Complete Streets Vision Map and provide the first step in determining how a particular street should be initially designed, as well as the enhancement elements potentially needed given the use, capacity, and context of the area.

Bike Network Streets

The Bike Network Streets Overlay identifies thoroughfares with designated bike facilities based on the 2011 Dallas Bike Plan. The 2011 Bike Plan provides proposed cross sections for retrofit projects and should be consulted for all minor projects and resurfacing. During new construction, the cross section will be considered to provide a high level of accommodation for bicycles, if needed.

Transit Network Streets

Transit Network Streets are streets that serve high levels of transit activity such as fixed rail, streetcars, bus rapid transit, or other enhanced transit service. This category is not intended to encompass all streets where transit exists, rather, the more transit-intensive streets.



This vision map is intended to be incorporated into the Thoroughfare Plan and periodically updated through the Thoroughfare Plan amendment process.

This map is intended to be overlaid on the Thoroughfare Plan functional and dimensional classification map. It is intended to serve as the point of reference for applying the relevant design guidance for each street based on the identified contextual street type.

This classification reflects predominant land use patterns along street frontages rather than site-specific delineation of uses.

This map is intended as the starting point in the planning and design process, and should be further informed by site-specific considerations and public input. Particular site specific consideration should be given to points of transition from one street type to another.

Highways are shown here for reference only. They do not fall under the City's purview for design standards.

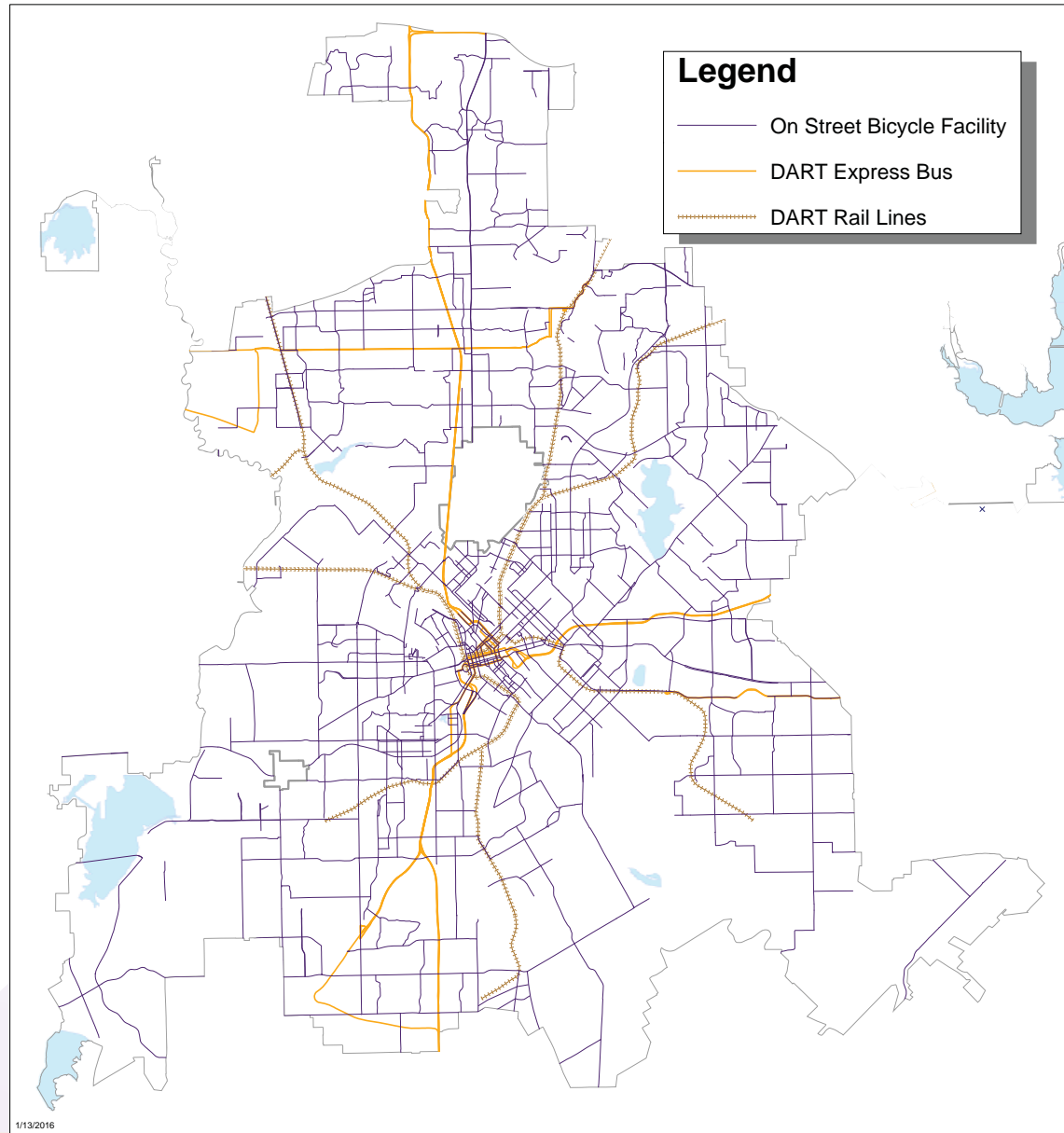
This map is intended to be overlaid on the Thoroughfare Plan functional and dimensional classification map in addition to the Contextual Street Types Overlay Map.

The bike network overlay includes thoroughfares designated as bike routes on the 2011 Dallas Bike Plan.

The transit network overlay includes thoroughfares with light rail lines, street car lines, express bus lines, and other enhanced transit service lines identified in the DART System Plan as amended and approved by the City. When designing these streets, extra consideration should be given to the design elements for transit facilities highlighted in Chapters 4, 5, and 6.

In addition, a 1/4 mile (5-minute walk) radius is shown around all DART light rail stations to highlight areas within which special consideration should be given to pedestrian, bike, and transit feeder linkages. All streets within these zones should follow the guidance for Mixed-Use Streets.

Vision Map: Bike Network and Transit Network Overlay



This vision map is intended to be incorporated into the Thoroughfare Plan and periodically updated through the Thoroughfare Plan amendment process.

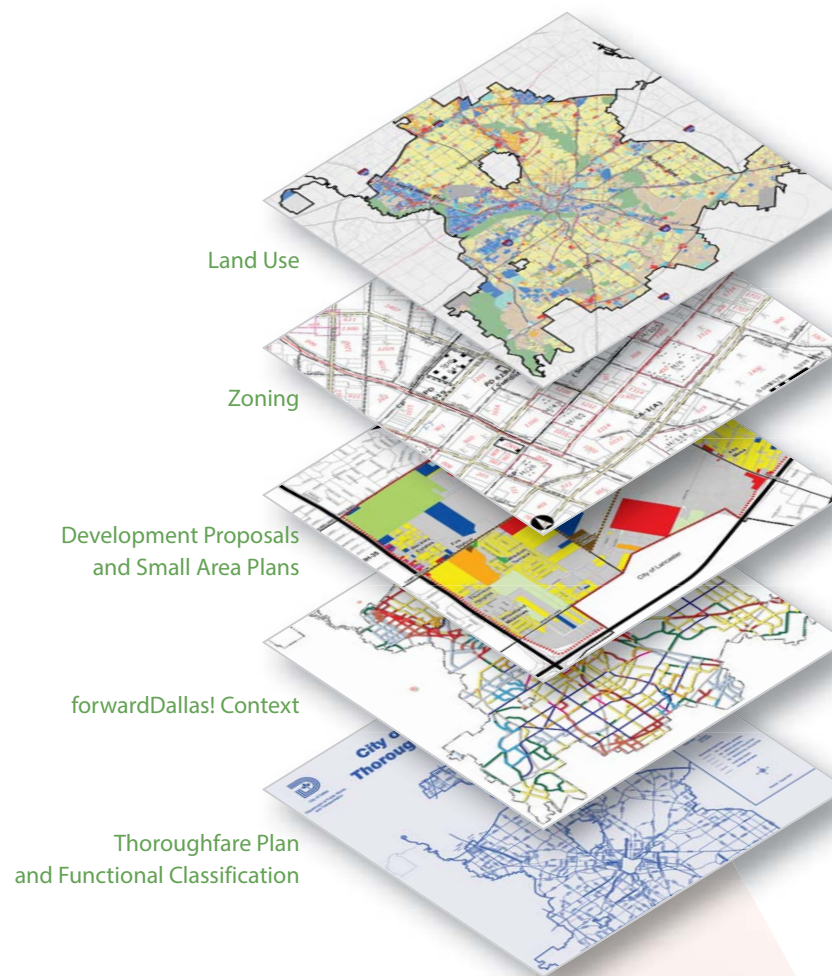
How the Vision Map was Developed

The approach to applying the typologies to particular thoroughfares reflects the future development vision based on the forwardDallas! Plan. It takes into account existing land use patterns where the transition to future development patterns are anticipated to be long-term, and reflects the prevailing patterns of use along a corridor, rather than site-specific uses.

The Vision Map was created by starting with the City's Thoroughfare Plan and functional classification system and looking at future street type needs based on both the current context and built form, and the future land use context, as proposed in the forwardDallas! Plan. In addition, development proposals, small area plan recommendations, and zoning and surrounding land use were considered. Using this information, along with thoroughfare operational characteristics, initial typologies were designated for each thoroughfare on the Thoroughfare Plan.

Applying the Complete Streets Typology to Minor Streets

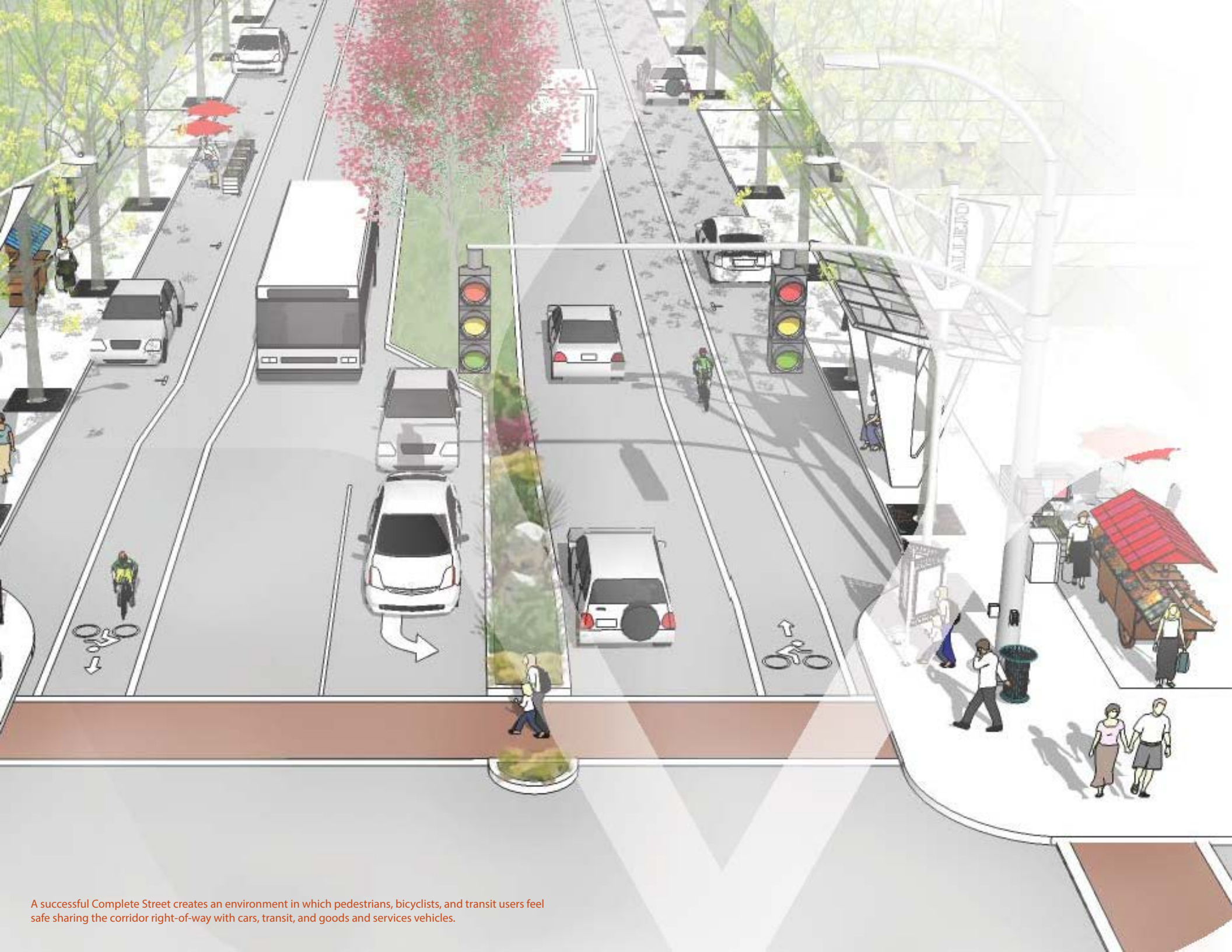
The Complete Streets Vision Maps do not identify local or minor streets due to the need for a greater degree of detailed site specific consideration. The following chart provides guidance for classifying local or minor streets according to the complete streets typology based primarily on forwardDallas! Building Blocks. In addition other existing site-specific land use and transportation considerations should be taken into account. This guidance is intended to enable the complete street types to be applied to individual local or minor streets on a case by case basis as needed, so that complete street design guidance can be brought to bear on future public or private improvements affecting these streets.



Complete Street Type	Complete Street Typology for Minor Streets
Mixed Use Streets	Streets located within Downtown, Urban Mixed Use, Transit Center, Campus District, or Urban Neighborhood Building Blocks Streets or street segments in any location that have a mix of existing and/or proposed land uses that may generate the potential for pedestrian trips between destinations on the street
Commercial Streets	Streets located within Business Corridor and Commercial Center or Corridor Building Blocks
Residential Streets	Streets located within Residential Neighborhood Building Blocks
Industrial Streets	Streets located within Industrial Area Building Blocks
Parkways	Streets or street segments located in or adjacent to natural features, parks or other open spaces
Bike Network Streets	Streets identified in the 2011 Bike Plan as updated



3. COMPLETE STREETS POLICY FRAMEWORK



A successful Complete Street creates an environment in which pedestrians, bicyclists, and transit users feel safe sharing the corridor right-of-way with cars, transit, and goods and services vehicles.

3. COMPLETE STREETS POLICY FRAMEWORK



Complete Streets public outreach



Greenville Avenue crosswalk

Many agencies and regulations provide guidance in evaluating and weighing the trade-offs of the physical, operational, and multimodal characteristics needed to design Complete Streets in a variety of contexts. Currently, City guidance in designating the dedication and use of right-of-way is provided by the Thoroughfare Plan and Chapter 51A of the Development Code. Complete Streets expands the factors influencing the use of the City's right-of-way, and provides guidance on designing roadways in constrained conditions.

This chapter establishes a policy framework for designing Complete Streets in order to guide decision making during the planning and design process on how to weigh the trade-offs encountered in an urban area. This chapter establishes the key principles that guide the Complete Streets design approach; defines various functional zones within the street, clarifying their roles and relationships to each other; and provides the over-arching design policy guidance associated with the development of walkable urban streets.



Bus/bike shared lane



Walnut Hill DART Station and local connector routes

GENERAL COMPLETE STREETS POLICY GUIDANCE

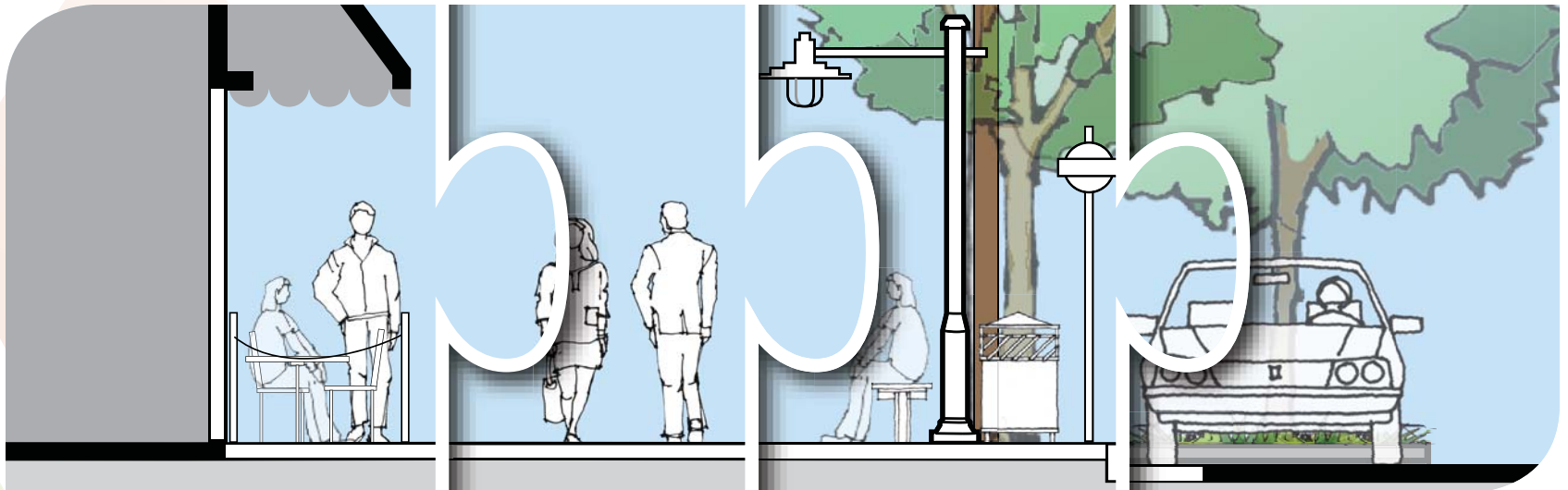
The following principles address the City's priority for the design of streets that are safe, multimodal, and green. These principles will guide the planning and construction of privately and publicly funded roadways. Five new street types implement these principles: mixed-use, commercial, residential, industrial, and parkway. These street types will supplement the existing Thoroughfare Plan street classification system. The new street types reflect the varying conditions of Dallas' streets and land uses and help ensure that streets are multimodal.

- Dallas' roadway design has traditionally been focused on auto speeds and vehicular movement through the City. The new Complete Streets approach expands this focus to better accommodate additional modes including walking, bicycling, and transit.
- Streets are designated and planned in context with the surrounding land uses and cultural and environmental considerations. Streets add to the vibrancy of the City and enhance Dallas' public spaces.
- Complete Streets optimize the street space and right-of-way to balance the needs of pedestrians, bicyclists, transit riders, and motorists, with emphasis placed on different modes, depending on the type of street and adjacent land uses.
- Complete Streets take advantage of opportunities to reallocate

roadway space once reserved for motor vehicle use to wider sidewalks, bikeways, on-street parking, and green spaces where possible.

- Roadway design integrates green street elements that support policies adopted by the City to reduce and filter stormwater runoff.
- Street design is based on an evaluation of network capacity along with multimodal level of service considerations.
- To ensure safety for all roadway users, streets are designed to limit excessive speeds, and reflect limits for the street type and context of surrounding land uses.

Policy guidance for the Intersection Zone, the Pedestrian Zone, and for the Street Zone is provided on the following pages.



Intersection Zone Policy Guidance

1. **Maximize Safety**—Safe and accessible designs for all modes of transportation are the driving principles for intersection design in Dallas. Intersections should be designed to minimize conflicts, reinforce the message that drivers and bicyclists should slow down, and respect the needs of pedestrians. National guidelines for accessible design in the public right-of-way should be followed with a commitment to achieving the best outcome for all users within the constraints of each site.
2. **Improve Access, Mobility, and Connectivity**—A dense network of intersections should distribute traffic amongst different routes and prioritize users based on the surrounding context and land uses connecting people to work, goods, and services. Intersections should efficiently move traffic, reducing delay and travel times. New developments should offer a mix of land uses and aim to minimize block lengths.
3. **Design for Predictable Movements**—Intersection designs should facilitate predictable movements by all modes, and encourage everyone to obey traffic laws.
4. **Reclaim Space**—Intersection and roadway design has been traditionally oriented toward automobile traffic. Undefined and underutilized areas of pavement not necessary for the efficient movement of motor vehicles should be used to reclaim street space for pedestrians, transit users, and bicyclists.
5. **Minimize Signal Cycle Lengths**—Signal cycle lengths should be kept to a minimum to reduce delay for all users. For coordinated signal corridors, consideration for pedestrian movements should be factored into the timing plans. As technology advances, traffic signalization should move towards a system that passively detects all modes in order to become more efficient, reducing delay and improving safety.
6. **Reduce Footprint and Improve Sustainability**—Intersection designs should strive to live within the current right-of-way and incorporate green, sustainable street elements wherever possible to reduce impervious surfaces, treat stormwater at the source, and reduce the heat island effect.
7. **Reduce or Eliminate**—Free right-turn movements and configurations should be reduced or eliminated where possible.

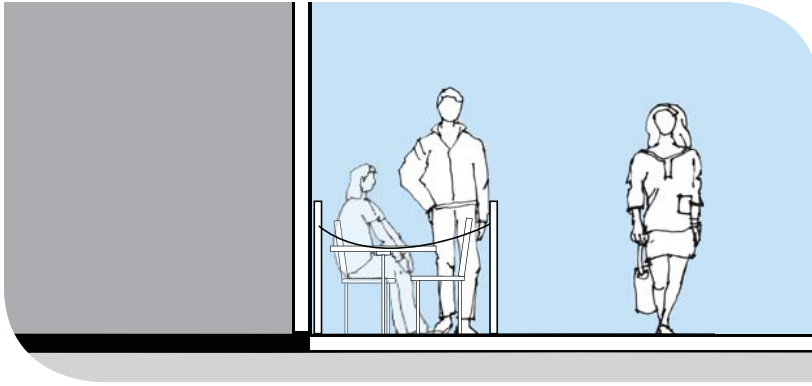


Under-utilized areas of pavement should be reclaimed for pedestrian use.



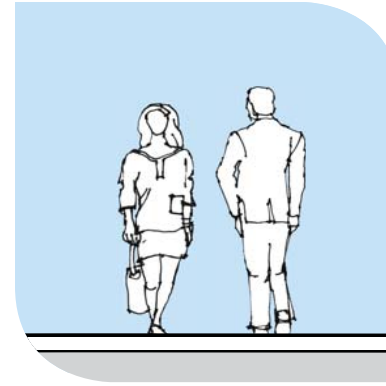
Safe and accessible designs for all modes of transportation must guide intersection design.

The Pedestrian Zone Defined



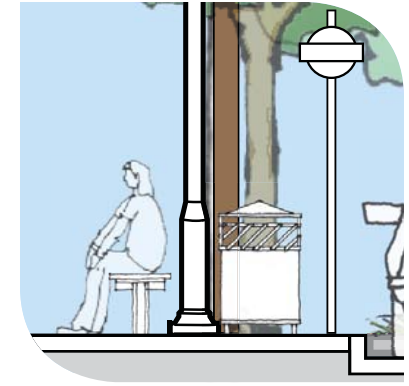
The Frontage Zone

The frontage zone is defined as the area between the face of the adjacent building and the sidewalk clear zone. This zone is ideally located on private property in the building setback area where design standards are controlled by zoning. In existing dense urban situations such as Downtown and Old Main Street corridors, frontage zone design elements may need to extend into the public right-of-way. Given the variety of development patterns in Dallas, the frontage zone will vary from buildings with narrow or no setbacks to buildings with large setbacks. For buildings with narrow setbacks, the frontage zone provides a place for sidewalk cafés, outdoor retail displays, and landscaping, among other things. It is important that these elements do not infringe on the sidewalk clear zone.



Sidewalk Clear Zone

The sidewalk clear zone is the portion of the pedestrian zone that is specifically reserved for pedestrian travel. It should be well-lit and meet ADA accessibility guidelines. This zone should be free of any physical obstructions to allow for continuous pedestrian movement. Materials used in the pedestrian zone should be consistent, and should not vary from block-to-block. Utility poles, signal boxes, street furniture, and vegetation should not encroach into the sidewalk clear zone.



The Buffer/Furnishing/Curb Zone

The buffer/furnishing/curb zone is the area between the curb and the sidewalk clear zone that provides separation and protection from moving vehicle traffic. The buffer zone also provides space for the placement and organization of street elements such as landscaping, street furniture, and above and below ground utilities. Where parking is allowed, the buffer zone creates space between the curb and vertical elements for proper clearance from moving vehicles or to allow car doors to open, and motor vehicle drivers to access the sidewalk. It also allows space for driveway aprons to ramp down from the grade of the sidewalk to the street in order to maintain a level sidewalk clear zone. In constrained areas, or where utilities are required, landscaping may be shifted to the frontage zone.

THE PEDESTRIAN ZONE

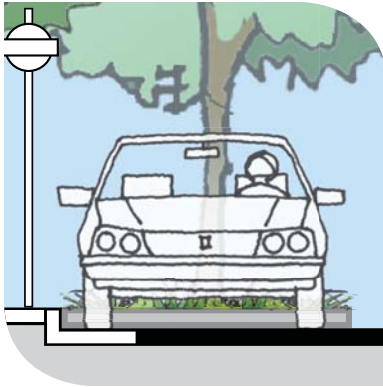
The Pedestrian Zones involve the portion of the street that accommodates non-vehicular activity—walking as well as the business and social activities—of the street. These zones extend from the face of the building or edge of the private property to the face of the curb. Streets are the most extensively used civic spaces in the community. The Pedestrian Zones are comprised of three functional elements:

- The Frontage Zone
- The Sidewalk Clear Zone
- The Buffer/Furnishing/Curb Zone

Pedestrian Zone Policy Guidance

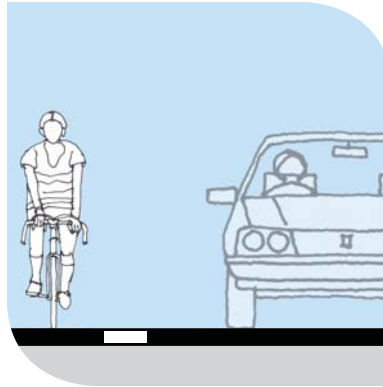
1. **Maximize Safety**—Sidewalks should be located on both sides of all streets to reduce the need for pedestrians to cross the street in order to access a safe walking area. Driveways and other elements should be minimized and designed to enhance pedestrian safety.
2. **Provide a Comfortable Walking Environment**—Roadway design should strive to maximize the buffer between pedestrians and adjacent motor vehicle traffic. On-street parking provides an excellent buffer and is encouraged. Street trees and planting strips or furnishing zones also make excellent buffers. Sidewalks should only be placed at the back of the curb in extremely constrained conditions.
3. **Promote Active and Inviting Building Frontages**—Ground-level land uses and building frontages should encourage foot traffic. Building entrances should be visible and easily accessible from the sidewalk. The transparency of building frontages should be maximized (e.g., fences and blank facades in mixed-use commercial areas should not be allowed) and elements such as sidewalk cafes, parklets, landscaping, and shading devices should be encouraged.
4. **Buffer Parking Areas**—Off-street parking should cover no more than 25 percent of the property frontage facing the street. Parking should generally be placed behind or underneath buildings or in structures. Where structures face the street, they should incorporate ground-floor uses that activate the pedestrian zone. In commercial areas, landscaping or small-scale commercial uses such as food vendors or kiosks should be provided to buffer parking from the pedestrian zone.
5. **Provide for Universal Access and Continuity**—The pedestrian zone should meet all applicable accessibility guidelines. The sidewalk should provide a smooth, stable, and slip-resistant surface. Surface materials and design should be consistent along street corridors. Users of all ages should be able to safely move within and across streets.
6. **Provide a Direct Route**—Sidewalks should align with crosswalks at intersections, and curves in the sidewalk should always serve a purpose. In most cases, sidewalks should be straight and follow the alignment of the roadway. It may be desirable in some locations for a sidewalk to curve to form a more direct route to an intersecting walkway, to preserve significant trees, or to provide a greater degree of separation between the sidewalk and the roadway for a distance.
7. **Provide Connectivity**—Convenient, safe, and comfortable pedestrian access linkages between adjacent land uses and the pedestrian zone should be addressed through the site planning process. Access should be provided to primary and secondary building access points, should maintain good sight lines, and should incorporate lighting where necessary. Pedestrian crossings at driveways and drive aisles should be minimized.
8. **Enhance Green Infrastructure**—The pedestrian zone should incorporate green infrastructure elements such as planting strips, street trees, and rain gardens. In physically constrained urban environments, there is a need for creative solutions such as pervious pavement and/or vegetated stormwater management features, stormwater planters, and tree box filters. The goal is to minimize impervious surfaces to reduce runoff and the heat island effect.
9. **Provide Pedestrian-Friendly Amenities**—The pedestrian zone should incorporate some pedestrian-friendly amenities such as including public art, street furniture, etc., that encourage pedestrians to linger and lends character to the street. This policy promotes the provision of these amenities through a combination of public and private investment.
10. **Maintenance**—Newly-constructed pedestrian facilities such as sidewalks, curb extensions, and plazas should be constructed with ease and efficiency of maintenance in mind. Maintenance of features within the pedestrian zone such as landscaping, street furniture, and pedestrian lighting should be achieved through public-private partnerships utilizing mechanisms such as Business Improvement Districts.
11. **Promote a Cohesive and Legible Walking Environment**—While street and land use character and conditions may change from block to block and require context-sensitive design solutions, unifying elements such as sidewalk material and joint pattern, street trees and landscaping, street furniture, and wayfinding signage help to create a cohesive and comfortable walking environment.
12. **Coordination with Land Use Decisions and Designations**—Provision of sidewalks should be made a priority, no matter the land use designation of adjacent properties.

The Street Zone Defined



The Parking Zone

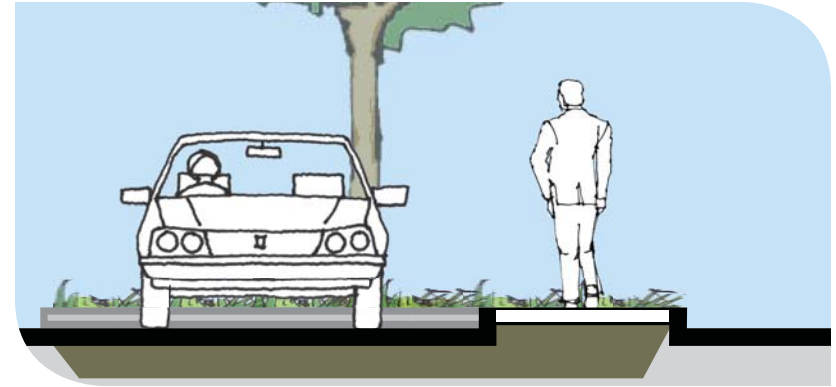
The parking zone is adjacent to the curb and may be parallel, perpendicular, angled, or back-in diagonal parking, which is preferred when adjacent to bike lanes. The presence of on-street parking increases street activity, meets the parking needs of adjacent uses, and protects pedestrians from moving traffic. On-street parking increases pedestrian comfort by providing a buffer between pedestrians and moving traffic; slows traffic, making pedestrian crossing safer; and increases the economic activity of a street. The parking zone may double as a travel lane at peak hours in high traffic areas or be dedicated for parking through the use of bulb outs. The parking zone is not an essential component of all streets as on-street parking may not be appropriate or necessary on some streets.



The Travelway Zone

The travelway zone is the portion of the street that is reserved for vehicular travel, including transit, bicycle, and other two wheelers. Speed and width are important in designing the travelway in walkable contexts. Because of the pedestrian-oriented nature of these areas, the target speed should be kept low (25–30 miles per hour). Lower speeds improve the user's perception of the street, creates a safer environment, accommodates parking maneuvers, and is consistent with restricted sight distances encountered in urban places.

The width of the travelway affects users' perceptions of the speed and volume of the street. Wide travelways may be perceived as a barrier to crossing where frequent crossings are desired and encouraged.



The Median Zone

Medians are the center portion of a street that separates opposing directions of travel. Medians vary in width and purpose and can be raised with curbs or painted and flush with the pavement. Medians on low-speed urban thoroughfares are used for access management, accommodation of turning traffic, safety, pedestrian refuge, and landscaping. Well-designed and landscaped medians can serve as a focal point of the street or an identifiable gateway into a community, neighborhood, or district. Medians can be used for landscaping, lighting, and urban design features. Sunken medians can be used to incorporate bioswales to improve water quality and reduce infrastructure costs.

Wider medians provide pedestrian refuge at long intersection crossings and midblock crossings. Medians are not an essential element for all streets. They are specified in the Thoroughfare

Plan if considered necessary for thoroughfares.

THE STREET ZONE

The Street Zones support adjacent land uses and should be designed to balance the efficiency of motor vehicle travel with considerations for pedestrians, bicyclists, and transit users. The Street Zones encompass the areas between the curbs and include the portion of the street that accommodates vehicular activity—transit, bicycle and motor vehicles. The Street Zones are comprised of three functional elements:

- The Parking Zone
- The Travelway Zone
- The Median Zone



The safety of vulnerable users is particularly important, as they are at greater risk when crashes occur.

Street Zone Policy Guidance

1. **Multimodal Streets**—The design of the traveled way should include considerations for every mode. Street space will be optimized to balance the needs of pedestrians, bicyclists, transit riders, and motorists, and will not be dominated by cars.
2. **Safety is a Paramount Concern**—Even if one mode is given priority within a street type, the design cannot compromise the safety of any mode for the benefit of another. The safety of vulnerable users is particularly important, as they are at greater risk when crashes occur.
3. **Design for Slower Speeds**—The safety and comfort of pedestrians and bicyclists is negatively impacted by fast motor vehicle traffic. For street types oriented to pedestrian and bicycle travel, motor vehicle speeds should be slower. A wide variety of roadway design strategies can help to reduce motor vehicle speeds without causing undue frustration for drivers. These are discussed later in this chapter.
4. **Street Design Should Reinforce Adjacent Land Uses**—The design of the traveled way should complement and reinforce adjacent uses. This approach can help to increase property values and foot traffic to local businesses.

TRADE-OFFS IN LIMITED RIGHT-OF-WAY

A primary goal of the corridor planning stage of the Complete Streets improvement process described in Chapter 1 is to define conceptual cross sections of the street to include the desired design elements within the available right-of-way. The majority of streets in Dallas serve already developed areas. Acquisition of additional right-of-way to accommodate a new design will typically be the option of last resort.

The Design Priorities Chart is provided to help clarify the relative importance of each zone and sub-zone in the right-of-way.

Designing streets in constrained rights-of-way necessarily involves balancing priorities for various design elements. Higher priority design elements are those that enable the street to meet the context sensitive community vision. Establishing design priorities requires re-cycling through the steps of the design process, potentially requiring a review of the community vision for the context and street relationship.

Often the width of the public right-of-way varies along existing streets, making the job of the designer more challenging. When identifying priorities for design elements in the context of street segments with varying right-of-way widths, it is advisable to explore conceptual cross-section options that address the priorities for the typical range of widths available, and to understand the issues that arise at the transition points from one cross-section to another.

If the vision for the corridor is long range, then the necessary right-of-way for a more desired cross section may be acquired over time as the adjacent property redevelops. Under these circumstances, the optimal roadway width may be phased in over time, beginning with a functional minimum design in the initial phase, where most of the higher priority elements can be accommodated within the predominant right-of-way width available. Consideration may also be given in these circumstances to the possibility of some design elements being incorporated into developments on private property as they occur. Examples of these include additional sidewalk width beyond the minimum, and elements within the frontage zone such as landscaping or sidewalk cafes.

The chart on the following page provides policy guidance for allocation of space between competing design elements within constrained right-of-way. The community should be engaged in setting street design priorities using the guidance in this chart as a starting point during the corridor planning phase of the Complete Streets design process. This chart should be used in conjunction with the Lane Width Chart to guide width choices for various design elements. It should also be used to guide decisions to relinquish lower priority design elements in some situations. It should also be used in conjunction with the Design Element Priorities Chart to guide appropriate use of design elements in various contexts.

In certain situations, design component tradeoffs will need to be made based on community input, funds available for ROW acquisition, existing building setbacks, and engineering/utility issues. These should be considered on a case-by-case basis.

Trade-Offs in Limited Right-of-Way Priorities Chart

Contextual Street Types and Functional Classifications	Pedestrian Zone			Street Zone		
	Frontage Zone (private)	Sidewalk Clear Zone	Buffer/Furnishing/Curb Zone	Parking Zone	Travelway Zone	Median Zone
Mixed Use Streets						
Principal Arterial		1	2	5	4	3
Minor Arterial		1	2	3	4	5
Collector		1	2	3	4	5
Minor/Local		1	2	3	4	5
Commercial Streets						
Principal Arterial		1	3	5	1	4
Minor Arterial		1	4	5	2	3
Collector		1	3	4	2	5
Minor/Local		1	4	3	2	5
Residential Streets						
Principal Arterial		1	2	5	3	4
Minor Arterial		1	4	2	5	3
Collector		1	4	2	3	5
Minor/Local		1	4	2	3	5
Industrial Streets						
Principal Arterial		2	3	4	1	5
Minor Arterial		2	3	4	1	5
Collector		2	3	4	1	5
Minor/Local		2	3	4	1	5
Parkways						
Principal Arterial		2	4	5	3	1
Minor Arterial		2	4	5	3	1
Collector		2	4	5	3	1
Minor/Local		5	1	4	3	2

High Priority
Medium Priority
Low Priority

General Notes:

1. The numbers rank various zones between 1 and 5, with one being the highest priority and 5 being the lowest. The priority level is intended to guide width choices (low priority means minimum width, high priority means desired width).
2. Refer to the On-Street Bike and Transit Facility Priorities Chart later in this chapter for additional guidance on the travelway zone.
3. The Parking and Median Zones are not essential on all streets. A low priority ranking for these zones implies that they may be eliminated. A high priority implies that it is desirable to include them even if minimum dimensions are used.
4. The Frontage Zone priorities shown in this chart reflect the importance of using the public right-of-way for this zone. A low priority implies that the Frontage Zone should be incorporated on private property. A high priority implies that allowing this zone to expand into the right-of-way is an important consideration.
5. For streets within a 1/4 mile radius of train stations as shown on the Vision Maps, the Sidewalk Clear Zone and the Buffer/Furnishing Zone should be given a High Priority.
6. This chart is intended to be used as a starting point for engaging the community in setting design priorities during the corridor planning stage of the Complete Streets process.

Street Elements Widths

The Recommended Width Chart for Dallas Complete Streets Elements presents proposed guidelines by street type in the City of Dallas. The widths shown in the chart should be considered minimums in new construction, major reconstruction, and retrofit street design. A design exception may be required for some values on federal or state-funded projects.

Engineering judgment is necessary to make final determinations regarding widths. In response to specific conditions on a given street, such as constrained right of way or specific types of uses, widths that are different from those in the chart may be required. The City of Dallas will make the final determination on appropriate widths on a project-by-project basis.

General Notes:

- Consult the DART 2030 Transit System Plan for locations of existing and future transit lanes and facilities.
- Consult the Dallas Bike Plan for proposed locations and types of bicycle facilities.

Specific Notes:

1. Local streets are also covered by the subdivision code.
2. Where ranges are shown for preferred width, the width should be based on anticipated volumes.
3. Widths for the Frontage Zone are not included in this chart. Frontage Zone widths are controlled by local zoning.
4. For all streets within a 1/4-mile of transit stations, regardless of street type, a minimum six foot clear pedestrian through zone must be provided.
5. Street trees are preferred on thoroughfare streets. In locations where there is insufficient width for street trees, these minimum values apply.
6. Additional width in the curb zone beyond six inches should be calculated as a part of the Buffer/Furnishing Zone.
7. Decisions regarding parking lane width when adjacent to bicycle lanes should consider parking turnover rates. In areas with higher turnover rates, such as on Mixed-Use street types, consider providing a door zone buffer or wider bicycle lanes.
8. Back-in angled parking is preferred to front-in angled parking due to safety benefits for pedestrians and bicyclists.
9. Flex lanes are parking lanes that convert to travel lanes during peak hour commuting times. Twelve-feet is the minimum width of a flex lane to accommodate bicycles and parked vehicles during off-peak times.
10. Minimum width is suitable only in locations with low truck traffic,

typically less than eight percent.

11. In locations where insufficient widths do not provide room for separate bicycle facilities, roadways must be shared by bicyclists, motor vehicle drivers, and transit vehicles. Shared lane markings can be installed on lanes of any width, in locations with and without parking. However markings should not be provided on roadways with speeds greater than 35 mph. Specific details on placement are provided in the latest edition of the TMUTCD.
12. Shared travelways on local streets generally do not use centerlines. Local streets are also covered by the subdivision code.
13. This may include street trees.

RECOMMENDED WIDTH CHART FOR DALLAS COMPLETE STREETS ELEMENTS											
	Mixed-Use Streets		Commercial Streets		Residential Streets ¹		Industrial Streets		Parkways		
Recommended Designation	Min	Pref	Min	Pref	Min	Pref	Min	Pref	Min	Pref	
Pedestrian Zone²											
Frontage zone: ³											
Frontage zone ³	-	-	-	-	-	-	-	-	-	-	-
Sidewalk clear zone: ⁴											
Sidewalk clear zone	6'	8'-15'	5'	6'-10'	5'	5'-10'	5'	5'-7'	5'	8'-10'	
Buffer/furnishing zone:											
Buffer with street tree	6'	8'	6'	10'	6'	10'	6'	10'	6'	20'	
Buffer (adjacent to on-street parking) ⁵	2'	6'	2'	6'	2'	7'	2'	7'	-	-	
Buffer (adjacent to travel lane, on-street parking not permitted) ⁵	5'	8'	5'	10'	5'	10'	5'	7'	5'	20'	
Curb zone: ⁶											
Curb zone width ⁶	6"	1'-2'	6"	1'-2'	6"	1'-2'	6"	1'-2'	6"	1'-2'	
Street Zone											
Parking zone: ⁷											
Parallel parking	7'	8'	-	-	7'	8'	7'	8'	-	-	
Back-in angled parking ⁸	15'	22'	-	-	15'	22'	15'	22'	-	-	
Flex lane ⁹	12'	15'	-	-	-	-	-	-	-	-	
Travelway zone—lanes on thoroughfares:											
General purpose inside travel lane ¹⁰	10'	11'	10'	11'	9'	10'	11'	12'	10'	11'	
Inside travel lane (adjacent to bicycle lane and parking lane)	10'	10'	-	-	10'	10'	11'	12'	-	-	
Inside travel lane (adjacent to bicycle lane and curb, parking not permitted)	10'	10'	10'	11'	10'	10'	11'	12'	10'	12'	
Left-turn lane	9'	10'	10'	10'	9'	10'	10'	11'	10'	10'	
Two-way left turn lane	10'	12'	10'	12'	9'	12'	11'	12'	-	-	
Shared use lane (adjacent to on-street parking, includes streetcars) ^{10, 11}	10'	12'	-	-	10'	10'	11'	12'	-	-	
Shared use lane (adjacent to curb, parking not permitted,) ^{10, 11}	10'	12'	10'	12'	10'	10'	11'	12'	10'	12'	
Travelway zone—lanes on local (non-thoroughfare plan) streets: ¹²											
Two-directional two-lane roadway (curb to curb width, parking permitted)	26'	26'-36'	-	-	26'	26'	36'	36'	-	-	
Two-directional two-lane roadway (curb to curb width, parking not permitted)	18'	18'	20'	20'	18'	18'	22'	22'	18'	18'	
Travelway zone—bicycle facilities: ²											
Paved shoulder (with curb)	-	-	-	-	5'	8'	5'	8'	5'	8'	
Paved shoulder (without curb)	-	-	-	-	4'	8'	4'	8'	4'	8'	
Bicycle lane (parking permitted)	5'	6'	5'		5'	6'	5'	6'	-	-	
Bicycle lane (parking not permitted, curb and gutter present)	5'	5'	5'	6'	5'	5'	5'	5'	5'	6'	
Bicycle lane/paved shoulder (parking not permitted, no curb and gutter)	-	-	4'	8'	4'	5'	4'	5'	4'	8'	
Buffered bicycle lane (includes buffer)	7'	9'	7'	12'	7'	12'	7'	12'	7'	12'	
Cycle track (one-way, includes buffer)	8'	10'	8'	10'	8'	10'	8'	10'	8'	13'	
Cycle track (two-way, includes buffer)	11'	13'-18'	13'	13'-18'	13'	13'-18'	13'	13'-18'	13'	13'-18'	
Travelway zone—transit lanes:											
Bus-only lanes	11'	12'	11'	12'	11'	12'	11'	12'	11'	12'	
Bus and bicycle-only lanes	11'	14'-16'	11'	14'-16'	11'	14'-16'	11'	14'-16'	11'	14'-16'	
Median zone:											
Pedestrian refuge	6'	-	6'	-	6'	-	6'	-	6'	-	
Continuous with landscaping ¹³	6'	15'	6'	15'	6'	15'	10'	15'	6'	20'	
Continuous without landscaping	4'	15'	4'	15'	4'	15'	4'	15'	4'	20'	



Shared lane markings and wayfinding signage along Martin Luther King, Jr. Boulevard

Complete Streets Design Elements

A variety of design elements may be considered for inclusion on specific streets in order to achieve the design character and target speed that is appropriate to the street context. The following Design Element Priorities Chart provides a quick reference for prioritization of key design elements to be considered in the Pedestrian, Street, and Intersection Zones according to Complete Streets type. The design elements are organized in this chart according to sections in Chapters 4, 5, and 6 where more detailed design guidance is provided for each design element.



Design Element Priorities Chart

	Pedestrian Zone														Street Zone										Intersection Zone										
	Wide Sidewalks	Shared Use Paths	Trees and Greenscape (Buffer Zone)	Street Furniture				Transit Stops	Limited Curb Cuts and Driveways	Plazas/PocketParks/Parklets	Sidewalk Cafes	Pedestrian Lighting	Wayfinding			Slip Streets	Couplets	Shared Streets (Woonerfs) ⁴	Trees and Greenscape (Median)	On-street Parking	Road/Lane Diet ⁵	Chicanes	Midblock Pedestrian Crossings	Special Pavement Treatment/Speed Tables ⁴	Street Lighting	Multimodal Intersection Design	Curb Extensions/Bulbouts	Modern Roundabouts	Traffic Circles	Crossing Islands ²	Special Pavement Treatment/Speed Tables	Special Pedestrian Signals	Special Bicycle Treatments ³	Special Transit Treatments	
				Seating	Bicycle Parking Facilities	Bollards	Newspaper Racks						Recycling/Garbage Cans	Information Kiosks	Pedestrian Signage																				Bicycle Signage
Contextual Street Type Overlays																																			
Mixed-Use Streets	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary		
Commercial Streets	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary		
Residential Streets	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary		
Industrial Streets	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary		
Parkways	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary		
Bike and Transit Network Overlays																																			
Bike Network Overlay	Primary	Primary	Primary	Primary	Primary	Primary	Primary	N/R	Primary	Primary	Primary	Primary	Primary	Primary	N/R	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	N/R		
Transit Network Overlay ¹	Primary	N/R	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	N/R	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	Primary	N/R	Primary		

General Notes:

- Guidance for choice of bicycle facilities and transit facilities within the travelway based on Complete Street type is provided in the Bike and Transit Network Facility Priorities Chart.
- The guidance in this chart for Bike Network and Transit Network Streets should be considered in conjunction with the underlying contextual street types on which they are overlaid.
- More detailed design guidance on each design element is provided in Chapters 4, 5, and 6.
- Incorporation of Green Street and iSWM techniques to reduce stormwater run-off and improve energy efficiency are a primary consideration for all street types in all zones.

Specific Notes:

- The guidance in this chart for transit network streets also applies to all street types that are located within a 1/4-mile radius of DART rail stations.
- The guidance in this chart assumes that crossing islands are only relevant on divided roadways.
- Note that bicycle boxes as a special bike treatment at intersections are a primary consideration on streets with dedicated bike facilities and more than two lanes of traffic. See Chapter 6 for more detailed guidance.
- Shared Streets and Special Pavement Treatments may be appropriate on streetcar corridors.
- Road diets require a traffic impact analysis to assess the impacts on the roadway network.

Primary Consideration
Secondary Consideration
Optional Consideration
Not Desirable
N/R Not Relevant



Bicycle and transit facilities—when integrated—typically exist in a symbiotic relationship. That is, each mode generates additional usage for the other mode.

Incorporating A Bicycle And Transit Facility Network Within Streets

The Vision Map titled “Bike Network and Transit Network Overlay” identifies streets where bike facilities and transit facilities are envisioned to be priorities based on the Dallas Bike Plan and long range transit system planning efforts by DART and the City of Dallas. These networks are generally intended to provide continuous bike and transit connectivity across the city. They typically extend along long street stretches that may have a variety of Complete Streets types. It is anticipated that there will be opportunities during the corridor planning stage for proposed Complete Streets improvement projects to consider and refine the choice of bike and transit facility types to be included within the travelway zone of these streets.

The following chart provides general policy guidance for selection of appropriate bike and transit facility types for Bike and Transit Network streets based on the contextual street type and functional classification. It is intended to be used in conjunction with the other charts in this chapter to engage the community in defining the key conceptual design components and priorities for specific street improvement projects, and to help make trade-offs for use of limited right-of-way space in constrained situations. It should be noted that besides the Bike and Transit Network streets identified in the Vision Map, all city streets should be designed with consideration given to bikes and transit, albeit without dedicated facilities.

BIKE AND TRANSIT NETWORK FACILITY PRIORITIES CHART									
Contextual Street Types and Functional Classification	Bike Network Facilities						Transit Network Facilities		
	Signed Route Only (No Lane Markings)	Paved Shoulders (Signed Route)	Bike Boulevards	Shared Bicycle Lane Markings ²	Bicycle Lanes	Buffered Bicycle Lanes / Offset Bicycle Lanes ⁴	Cycle Track (One-Way or Two-Way) ⁵	Shared Lanes ³	Priority Transit Lanes ⁶
Mixed Use Streets									
Principal Arterial									
Minor Arterial									
Collector									
Minor/Local									
Commercial Streets									
Principal Arterial									
Minor Arterial									
Collector									
Minor/Local									
Residential Streets									
Principal Arterial									
Minor Arterial									
Collector									
Minor/Local									
Industrial Streets									
Principal Arterial									
Minor Arterial									
Collector									
Minor/Local									
Parkways									
Principal Arterial									
Minor Arterial									
Collector									
Minor/Local									

Preferred
Acceptable
Need Site Specific Review
Not Desired

General Notes:

- This chart is intended as a quick reference during the corridor planning stage to guide choices of bike and transit facility types within the travelway zone for Complete Streets projects on streets included in the Bike Network and Transit Network Overlay Vision Map.
- This chart should be used in conjunction with the Priorities Chart for Trade-Offs in Limited Right of Way.
- The guidance in this chart is based on the recommended target speed ranges for each contextual street type and functional class identified in the Target Speed Chart. Note that the Operational Speed must also be considered in situations where the desired target speed is not achievable through design measures within the scope of a street improvement project.
- Refer to the Dallas Bike Plan for facility type recommendations for specific bike network streets.
- Refer to Chapter 5 and the Dallas Bike Plan for definitions and design guidance for the various bicycle and transit facility types included in this chart.

Specific Notes:

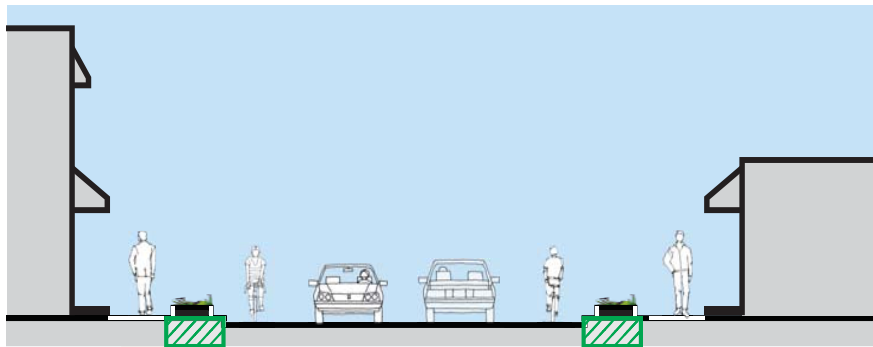
- Bike Network Facilities may also include shared use paths that would typically be located within the pedestrian zone. Refer to the Design Element Priorities Chart for guidance on shared use paths based on Complete Streets type.
- Shared Bicycle Lane Markings are not desirable on streets with target speeds exceeding 30 mph or on Shared Lanes with rail transit. Shared Lane Markings on two-lane roadways require special site-specific review relative to traffic volume and speeds.
- Shared Lanes for buses may also include Shared Bicycle Lane Markings if the target speed does not exceed 30 mph.
- Buffered Bicycle Lanes or Offset Bicycle Lanes are preferred in situations where bicycle lanes are provided adjacent to designated on-street parking.
- Cycle Tracks are preferred for bike routes on heavily-travelled roadways with target speeds exceeding 40 mph and where space is available to provide a physical separation.
- Priority Transit lanes are not desirable on two-lane roadways.

EXAMPLES OF STREET CROSS SECTIONS FOR VARIOUS STREET TYPES AND RIGHTS-OF-WAY

The following cross section examples illustrate how the Priorities Chart for Trade-Offs in the Public Right-of-Way on page 81 can be used to configure Complete Streets in different situations. The cross sections do not represent specific streets for specific situations.

Mixed-Use Street Examples

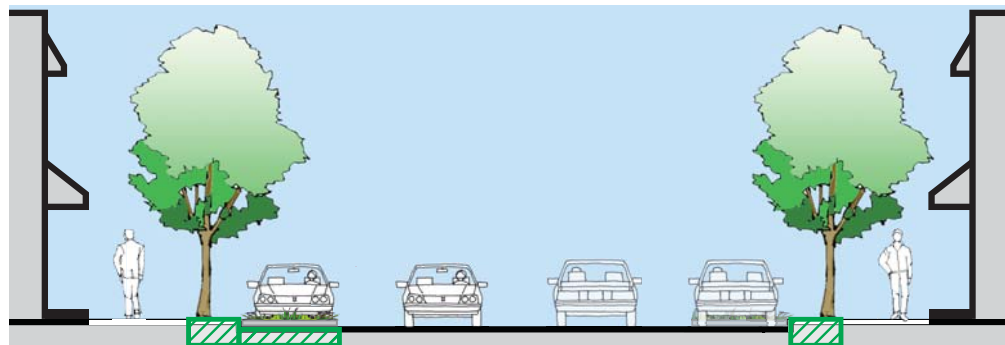
EXAMPLE MIXED-USE TYPOLOGY CROSS SECTION: 50' ROW



Private Property	Sidewalk	Buffer	Bike Lane	Travel Lane	Travel Lane	Bike Lane	Buffer	Sidewalk	Private Property
	5'	5'	5'	10'	10'	5'	5'	5'	
Total Width: 50'									
	Min.	Min.	Pref.	Min.	Min.	Pref.	Min.	Min.	

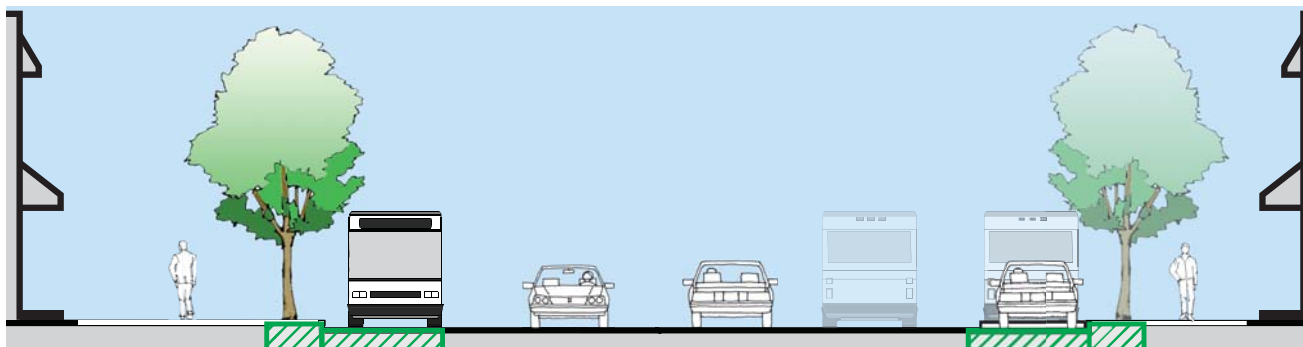


EXAMPLE MIXED-USE TYPOLOGY CROSS SECTION: 60' ROW



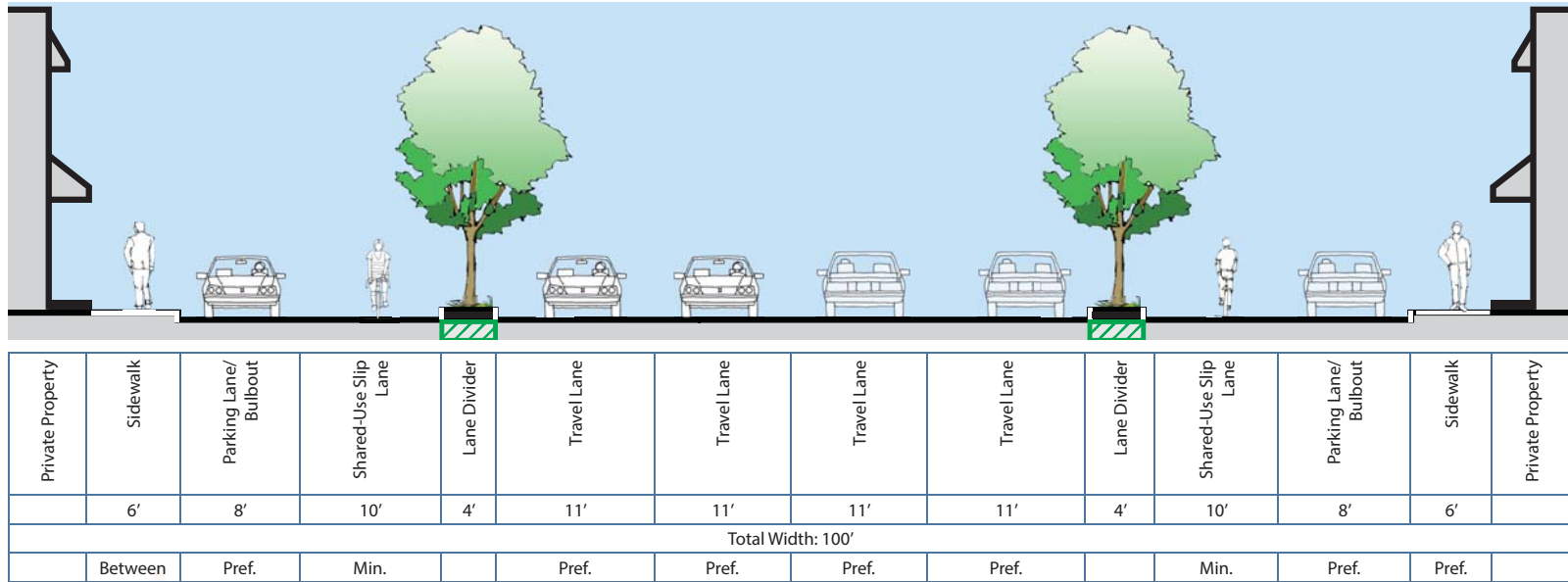
Private Property	Sidewalk	Street Trees and Streetscape Furniture	Parallel Parking with Planted Bulbouts	Travel Lane	Travel Lane	Parallel Parking	Buffer	Sidewalk	Private Property
	5'	6'	8'	11'	11'	8'	6'	5'	
Total Width: 60'									
	Min.	Pref.	Pref.	Pref.	Pref.	Pref.	Pref.	Min.	

EXAMPLE MIXED-USE TYPOLOGY CROSS SECTION: 80' ROW

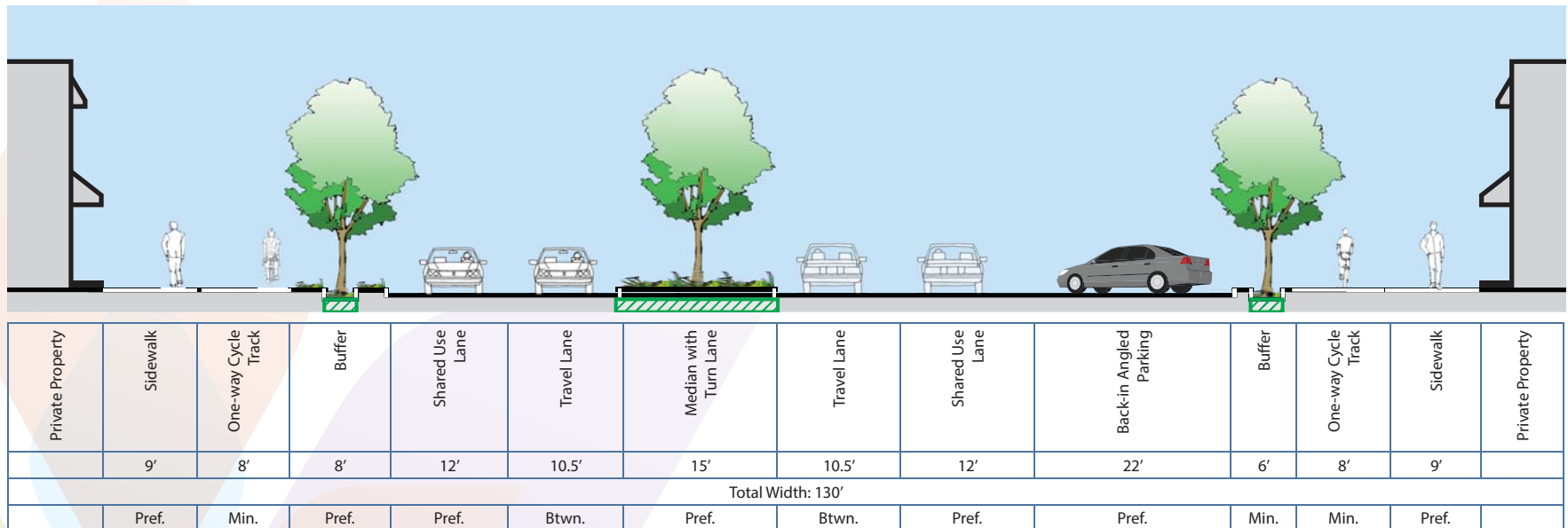


Private Property	Sidewalk	Landscaping and Furniture	Transit-only Lane	Travel Lane	Travel Lane	Transit-only Lane	Parallel Parking/Transit Stop	Landscaping and Furniture	Sidewalk	Private Property
	8'	8'	12'	11'	11'	12'	8'	4'	6'	
Total Width: 80'										
	Pref.	Pref.		Pref.	Pref.		Pref.	Min.	Pref.	

EXAMPLE MIXED-USE TYPOLOGY CROSS SECTION: 100' ROW



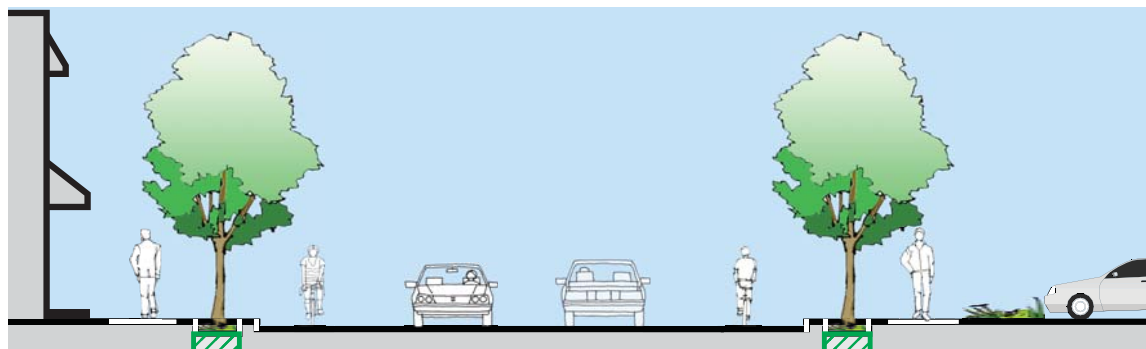
EXAMPLE MIXED-USE TYPOLOGY CROSS SECTION: 130' ROW





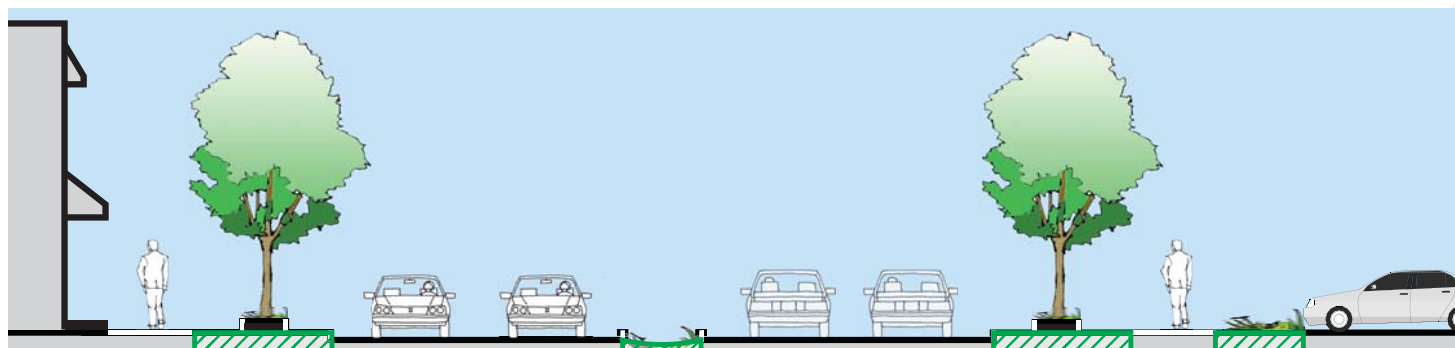
COMMERCIAL Street Examples

EXAMPLE COMMERCIAL TYPOLOGY CROSS SECTION: 60' ROW



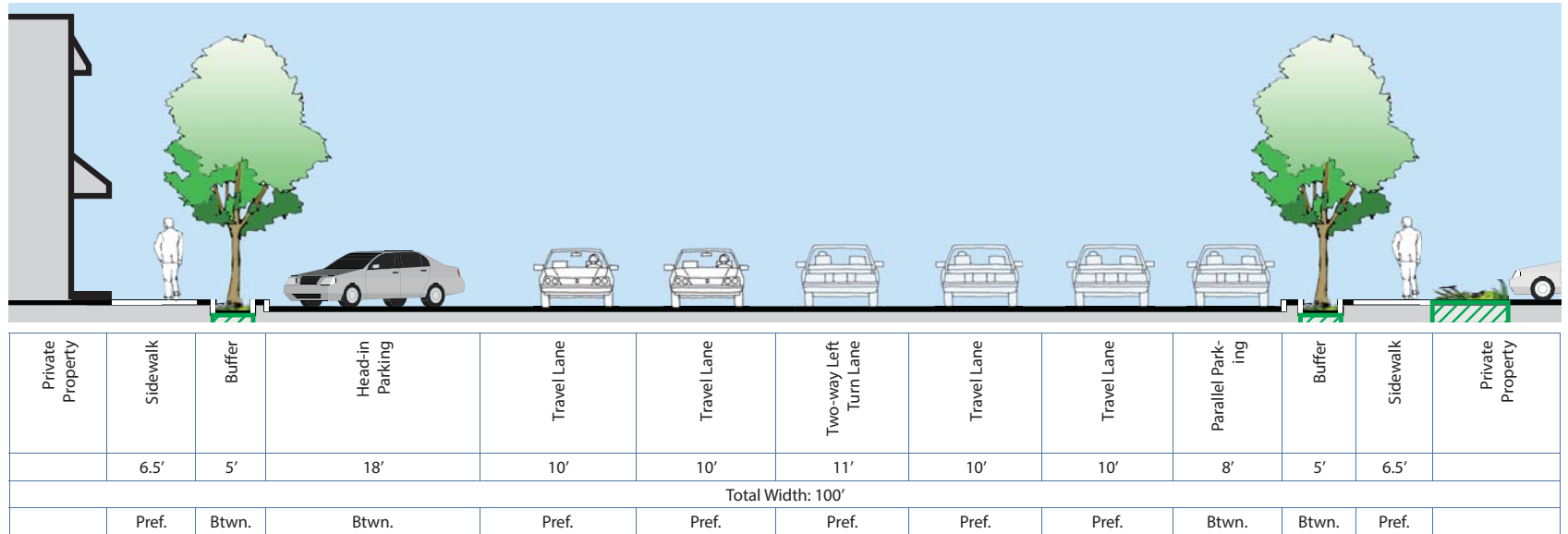
Private Property	Sidewalk	Buffer	Buffered Bike Lane	Travel Lane	Travel Lane	Buffered Bike Lane	Buffer	Sidewalk	Private Property
	5'	6'	8'	11'	11'	8'	6'	5'	
Total Width: 60'									
	Min.	Btwn.	Btwn.	Pref.	Pref.	Btwn.	Btwn.	Min.	

EXAMPLE COMMERCIAL TYPOLOGY CROSS SECTION: 80' ROW

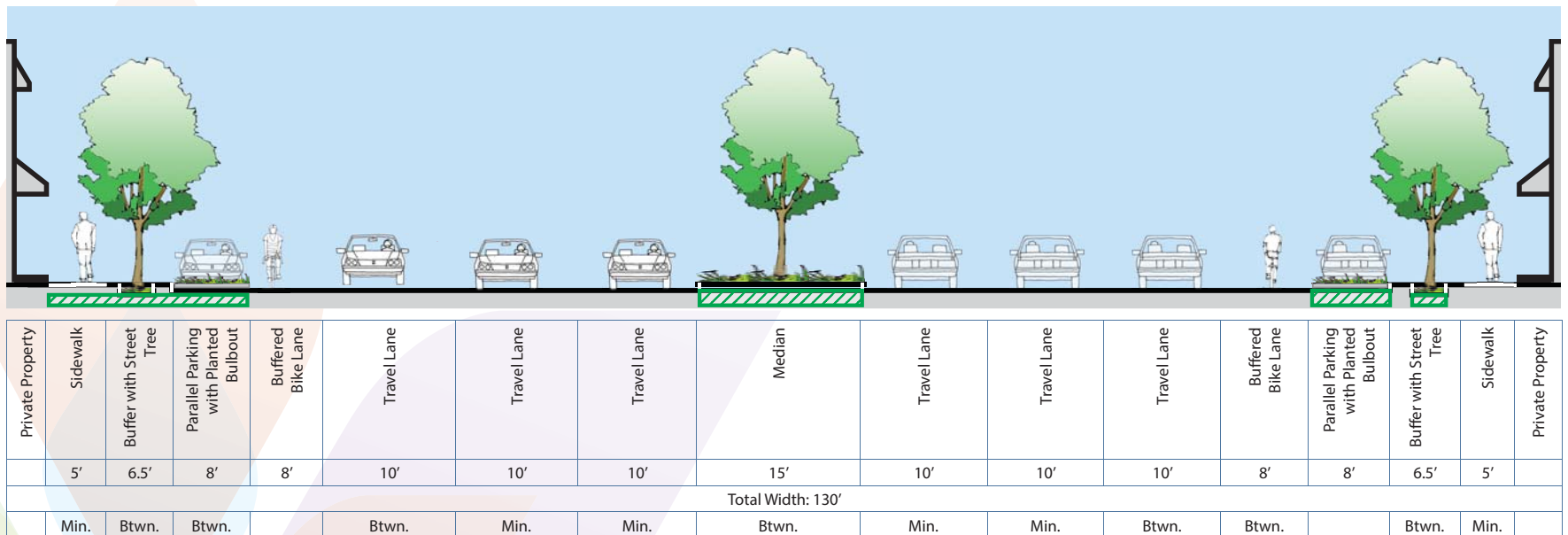


Private Property	Sidewalk	Buffer with Street Tree	Travel Lane	Travel Lane	Pedestrian Refuge Median	Travel Lane	Travel Lane	Buffer with Street Tree	Sidewalk	Private Property
	6'	10'	11'	10'	6'	10'	11'	10'	6'	
Total Width: 80'										
	Pref.	Pref.	Pref.	Min.	Min.	Min.	Pref.	Pref.	Pref.	

EXAMPLE COMMERCIAL TYPOLOGY CROSS SECTION: 100' ROW



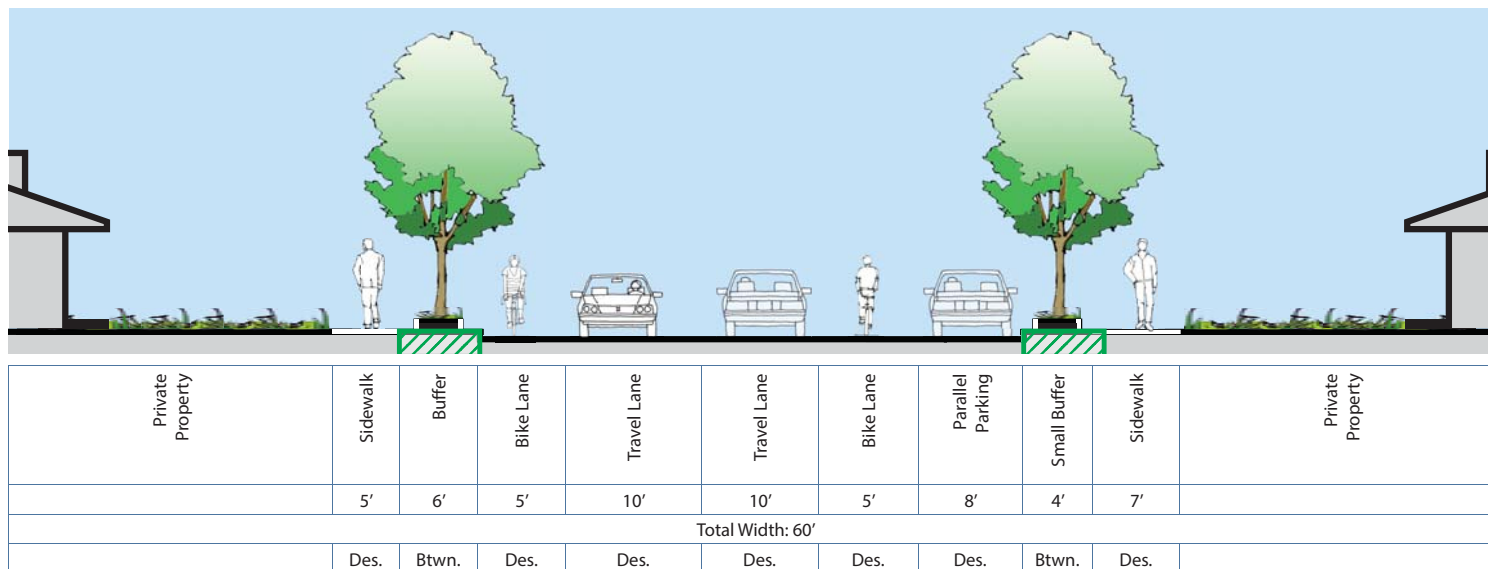
EXAMPLE COMMERCIAL TYPOLOGY CROSS SECTION: 130' ROW



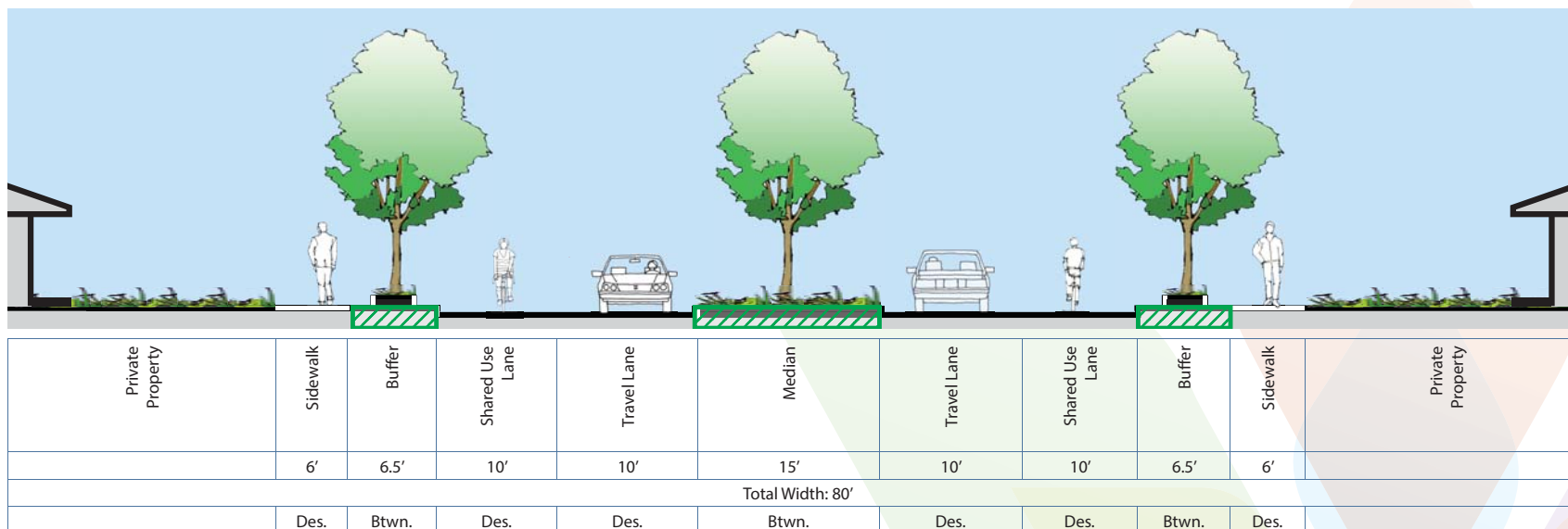


Residential Street Examples

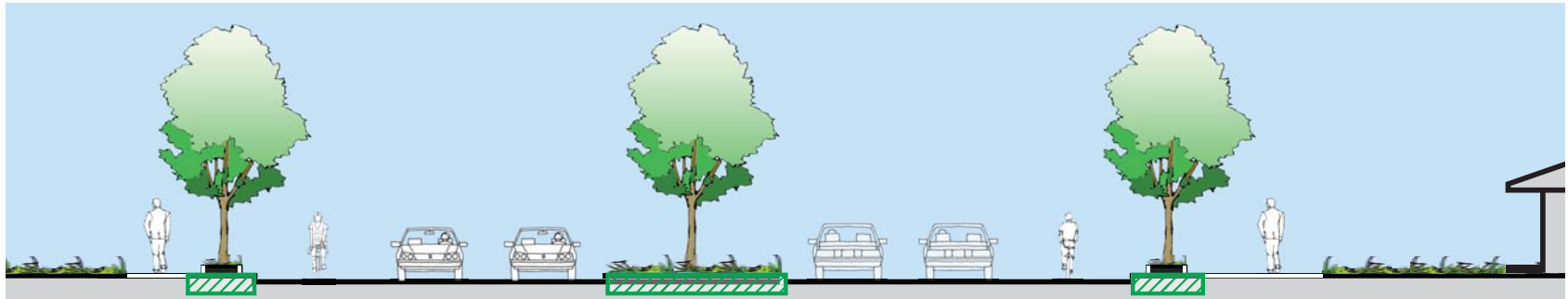
EXAMPLE RESIDENTIAL TYPOLOGY CROSS SECTION: 60' ROW



EXAMPLE RESIDENTIAL TYPOLOGY CROSS SECTION: 80' ROW



EXAMPLE RESIDENTIAL TYPOLOGY CROSS SECTION: 100' ROW



Private Property	Sidewalk	Buffer	Buffered Bike Lane	Travel Lane	Travel Lane	Median	Travel Lane	Travel Lane	Buffered Bike Lane	Buffer	Sidewalk	Private Property
	5'	7'	8'	10'	10'	15'	10'	10'	8'	7'	10'	
Total Width: 100'												
	Des.	Btwn.	Btwn.	Des.	Des.	Des.	Des.	Des.	Btwn.	Des.	Des.	

EXAMPLE RESIDENTIAL TYPOLOGY CROSS SECTION: 130' ROW



Private Property	Sidewalk	Buffer	Back-in Angled Parking	Buffered Bike Lane	Travel Lane	Travel Lane	Median	Travel Lane	Travel Lane	Buffered Bike Lane	Back-in Angled Parking	Buffer	Sidewalk	Private Property
	6.5'	8'	18'	8'	10'	10'	15'	10'	10'	8'	18'	7'	6.5'	
Total Width: 130'														
	Des.	Des.	Des.	Des.	Des.	Des.	Des.	Des.	Des.	Des.	Des.	Des.	Des.	



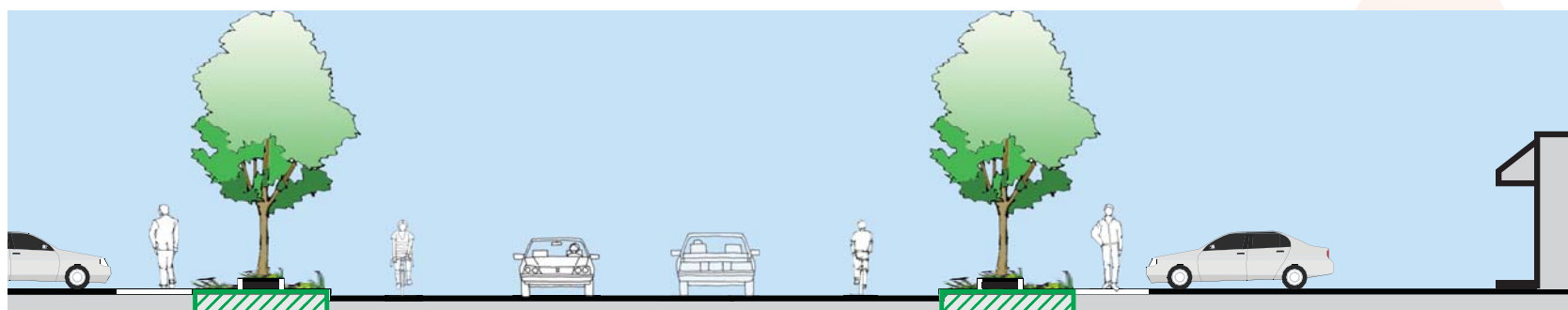
Industrial Street Examples

EXAMPLE INDUSTRIAL TYPOLOGY CROSS SECTION: 60' ROW



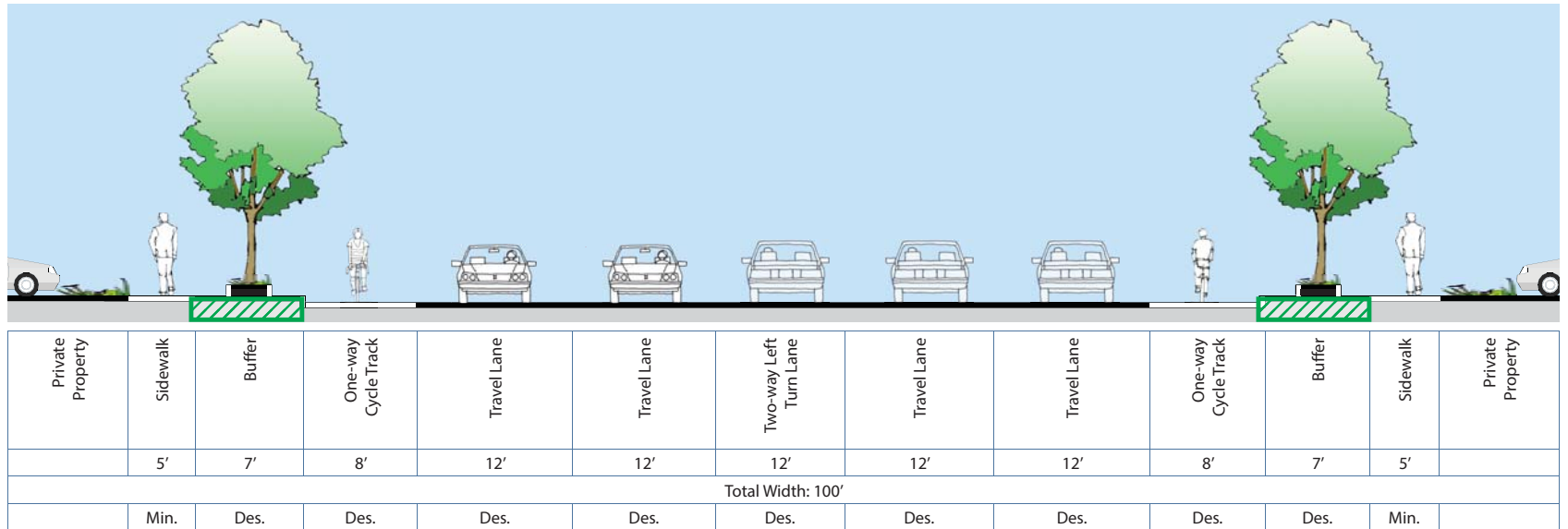
Private Property	Sidewalk	Buffer	Buffered Bike Lane	Travel Lane	Travel Lane	Buffered Bike Lane	Buffer	Sidewalk	Private Property
	5'	6'	7'	12'	12'	7'	6'	5'	
Total Width: 60'									
	Des.	Btwn.	Min.	Des.	Des.	Min.	Btwn.	Des.	

EXAMPLE INDUSTRIAL TYPOLOGY CROSS SECTION: 80' ROW

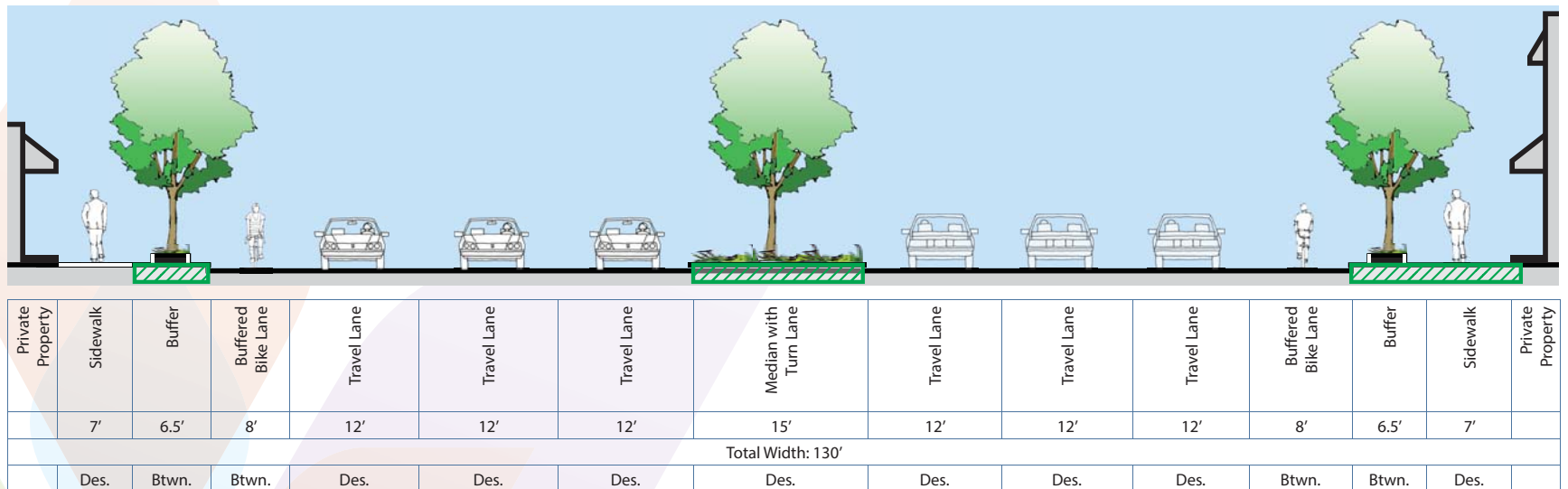


Private Property	Sidewalk	Buffer with Street Tree	Shared Use Lane	Travel Lane	Travel Lane	Shared Use Lane	Buffer with Street Tree	Sidewalk	Private Property
	6'	10'	12'	12'	12'	12'	10'	6'	
Total Width: 80'									
	Des.	Des.	Des.	Des.	Des.	Des.	Des.	Des.	

EXAMPLE INDUSTRIAL TYPOLOGY CROSS SECTION: 100' ROW

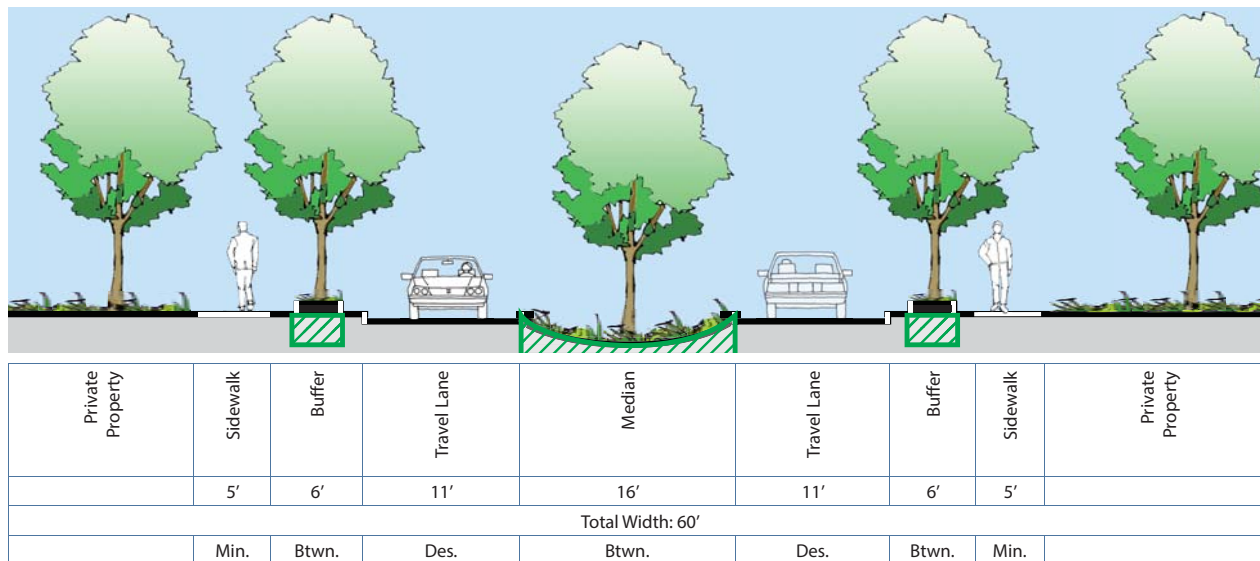


EXAMPLE INDUSTRIAL TYPOLOGY CROSS SECTION: 130' ROW

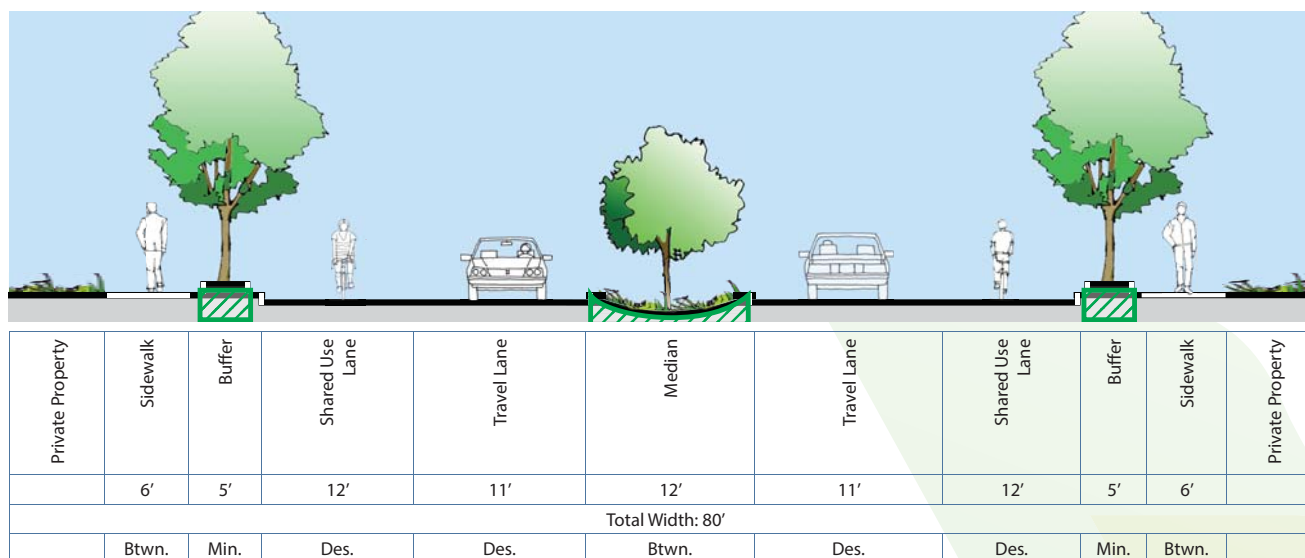


Parkway Examples

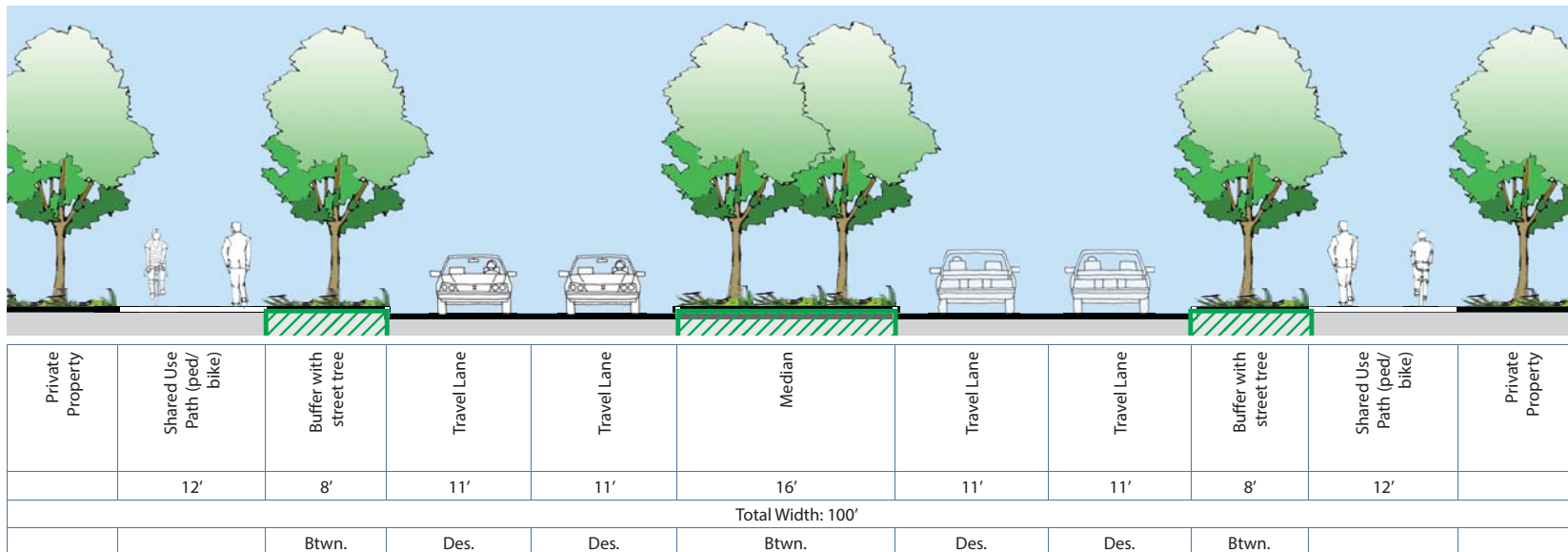
EXAMPLE PARKWAY TYPOLOGY CROSS SECTION: 60' ROW



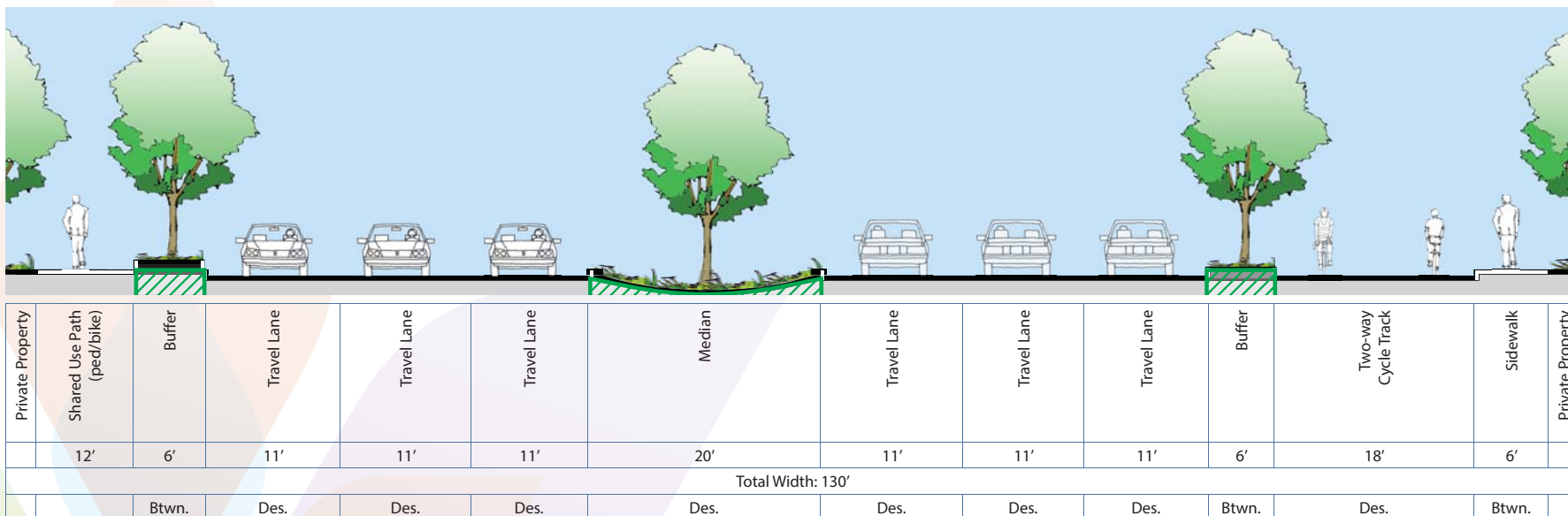
EXAMPLE PARKWAY TYPOLOGY CROSS SECTION: 80' ROW



EXAMPLE PARKWAY TYPOLOGY CROSS SECTION: 100' ROW



EXAMPLE PARKWAY TYPOLOGY CROSS SECTION: 130' ROW



DESIGN CONTROLS

An important resource in developing a successful Complete Street is the Institute of Transportation Engineers' *Designing Walkable Urban Thoroughfares: A Context Sensitive Approach*. It describes the physical, operational, policy, and multimodal characteristics that should be considered when selecting the design criteria that will affect the speed of vehicles.

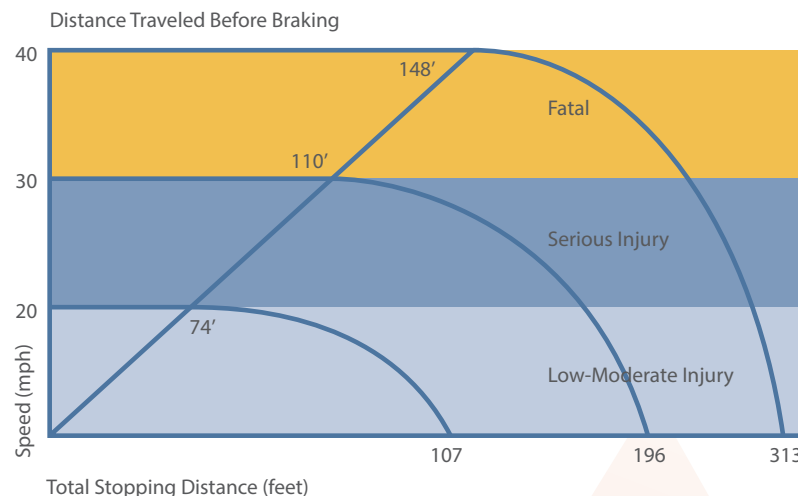
Design controls are the primary criteria used to guide the design of roadways. When designing walkable urban thoroughfares, it is important to carefully consider a broad range of design controls. Some design controls are fixed—such as terrain, climate and certain driver performance characteristics—but most controls can be influenced in some way through design and are determined by the roadway designer.

The American Association of State Highway and Transportation Officials' (AASHTO) *A Policy on Geometric Design of Highways and Streets* (Green Book) and *A Guide for Achieving Flexibility in Highway Design* identify design controls and establish design criteria. AASHTO identifies a number of design controls that have varying degrees of influence on thoroughfare design:

- Design vehicle
- Vehicle performance (acceleration and deceleration)
- Driver performance (age, reaction time, driving task, guidance and so forth)
- Functional classification
- Traffic characteristics (volume and composition)
- Speed
- Capacity and level of service
- Access control and management
- Pedestrian and bicycle facilities
- Safety
- Environment

AASHTO recognizes the influence that context has on driver characteristics

and performance, and how design controls respond to these influences. The Green Book defines the environment, or context, as “the totality of humankind’s surroundings: social, physical, natural, and synthetic,” and states that full consideration to environmental factors should be used in the selection of design controls.



The relationship between speed and pedestrian crash severity illustrates the benefit of reduced speeds in walkable urban places.

Source: Derived from Anderson, McLean, Farmer, Lee and Brooks, *Accident Analysis & Prevention* (1997).



Thoroughfare design should influence actual operating speeds.

TARGET SPEED DEFINED

AASHTO identifies functional classification and design speed as primary factors in determining highway design criteria. Although design speed used to be defined as the “maximum safe speed,” FHWA’s Flexibility in Highway Design recommends that design speed take into account topography, anticipated operating speed, adjacent land use, and functional design. Currently AASHTO defines operational speed as the speed at which drivers are observed operating their vehicles during free-flow conditions.

Design controls in the application of Complete Streets principles that may be used differently than in the conventional design process include speed, location, design vehicle, and functional classification. The Institute of Transportation Engineers (ITE) recommends replacing design speed with target speed. Target speed is the highest speed at which vehicles should operate on a thoroughfare in a specific context, consistent with the level of multimodal activity generated by adjacent land uses, to provide both mobility for motor vehicles and a safe environment for pedestrians, bicyclists, and public transit users. The target speed is intended to be designed as the posted speed limit. Traditionally, the speed limit is established based on the operational speed of the roadway defined as the 85th percentile speed. The posted speed limit is generally 5-10mph less than the design speed or equal to or less than the operational speed. Therefore, it is important for the design of the thoroughfare to encourage actual operating speeds that are equal to the target speed.

In this manual, design speed and operational speed are replaced with target speed, which becomes the primary control for determining the following geometric design values:

- minimum intersection sight distance
- minimum sight distance on horizontal and vertical curves
- horizontal and vertical curvature

Target speed ranges from 25 to 40 mph for the primary thoroughfare types described in this manual. A lower target speed is an essential characteristic of thoroughfares in walkable, mixed-use urban areas.

Target Speed By Street Type and Functional Classification

Contextual Street Types and Functional Classification	Thoroughfare Plan			Complete Streets Policy
	ROW	Number of Lanes	Volumes	Target Speeds
Mixed Use Streets				
Principal Arterial	60'-130'	4-8 Lanes	>3,500 v/l/d	25-35 mph
Minor Arterial	50'-107'	2-6 Lanes	2,500-5,000 v/l/d	25-30 mph
Collector	50'-80'	2-4 Lanes	1,250-3,500 v/l/d	25-30 mph
Minor/Local	50'-56'	2-4 Lanes	<1,250 v/l/d	20-30 mph
Commercial Streets				
Principal Arterial	60'-130'	4-8 Lanes	>3,500 v/l/d	25-40 mph
Minor Arterial	50'-107'	2-6 Lanes	2,500-5,000 v/l/d	25-40 mph
Collector	50'-80'	2-4 Lanes	1,250-3,500 v/l/d	25-30 mph
Minor/Local	50'-56'	2-4 Lanes	<1,250 v/l/d	25-30 mph
Residential Streets				
Principal Arterial	60'-130'	4-8 Lanes	>3,500 v/l/d	25-35 mph
Minor Arterial	50'-107'	2-6 Lanes	2,500-5,000 v/l/d	25-30 mph
Collector	50'-80'	2-4 Lanes	1,250-3,500 v/l/d	20-25 mph
Minor/Local	50'-56'	2-4 Lanes	<1,250 v/l/d	20-25 mph
Industrial Streets				
Principal Arterial	60'-130'	4-8 Lanes	>3,500 v/l/d	25-45 mph
Minor Arterial	50'-107'	2-6 Lanes	2,500-5,000 v/l/d	25-40 mph
Collector	50'-80'	2-4 Lanes	1,250-3,500 v/l/d	25-35 mph
Minor/Local	50'-56'	2-4 Lanes	<1,250 v/l/d	25-30 mph
Parkways				
Principal Arterial	60'-130'	4-8 Lanes	>3,500 v/l/d	25-45 mph
Minor Arterial	50'-107'	2-6 Lanes	2,500-5,000 v/l/d	25-30 mph
Collector	50'-80'	2-4 Lanes	1,250-3,500 v/l/d	25-30 mph
Minor/Local	50'-56'	2-4 Lanes	<1,250 v/l/d	25-30 mph

NOTE: With limited exceptions the goal is to achieve a 25 mph target speed on street segments within 1/4-mile of rail transit stations.



Textured paving materials notify drivers of the possible presence of pedestrians.

Design Factors That Influence Target Speed

Establishing a target speed that is artificially low relative to the design of the roadway will only result in operating speeds that are higher than desirable and difficult to enforce. Consistent with AASHTO, this manual urges sound judgment in the selection of an appropriate target speed based on a number of factors and reasonable driver expectations. Factors in urban areas include transition from higher- to lower-speed roadways, terrain, intersection spacing, frequency of access to adjacent land, type of roadway median, presence of curb parking, and level of pedestrian activity. AASHTO's *A Guide for Achieving Flexibility in Highway Design* (2004) summarizes the selection of speed in urban areas:

- Context-sensitive solutions for the urban environment often involve creating a safe roadway environment in which the driver is encouraged by the roadway's features and the surrounding area to operate at lower speeds.

Urban thoroughfare design for walkable communities should start with the selection of a target speed.

The target speed should be applied to those geometric design elements where speed is critical to safety, such as horizontal and vertical curvature and intersection sight distance. The target speed is not set arbitrarily, but rather is achieved through a combination of measures that include the following:

- Setting signal timing for moderate progressive speeds from intersection to intersection
- Using narrower travel lanes that cause motorists to naturally slow their speeds
- Using physical measures such as curb extensions and medians to narrow the traveled way
- Using design elements such as on-street parking to create side friction
- Minimal or no horizontal offset between the inside travel lane and median curbs
- Eliminating super elevation
- Eliminating shoulders in urban applications, except for bicycle lanes
- Smaller curb-return radii at intersections and elimination or reconfiguration of high-speed channelized right turns
- Paving materials with textures (e.g., crosswalks, intersection operating areas) detectable by drivers as a notification of the possible presence of pedestrians

- Proper use of speed limit, warning, advisory signs, and other appropriate devices to gradually transition speeds when approaching and traveling through a walkable area

Other factors widely believed to influence speed include a canopy of street trees, the enclosure of a thoroughfare formed by the proximity of a wall of buildings, and the striping of edge lines of bicycle lanes or parking lanes. These are all elements of walkable, mixed-use urban areas but should not be relied upon as speed-reduction measures until further research provides a definitive answer.

The practitioner should be careful not to relate speed to capacity in urban areas, avoiding the perception that a high-capacity street requires a higher target speed. Under interrupted flow conditions, such as on thoroughfares in urban areas, intersection operations and delay have a greater influence on capacity than speed. The Highway Capacity Manual (TRB 2000) classifies urban streets (Class I through IV) based on a range of free-flow speeds. The thoroughfares upon which this report focuses have desired operating speeds in the range of 25 to 35 mph (Class III and IV based on the Highway Capacity Manual). Level of Service C or better is designated by average travel speeds ranging from 10 to 30 mph. Therefore, adequate service levels can be maintained in urban areas with lower operating speeds.

Capacity issues should be addressed with highly connected networks and sound traffic operations management such as coordinated signal timing, improved access management, removal of unwarranted signals, and the accommodation of turning traffic at intersections.



On-street parking creates side friction and reduces vehicle speeds.



The design vehicle influences many roadway characteristics.

Design Vehicle

The design vehicle—the vehicle that the road is designed for—influences the selection of design criteria such as lane width and curb-return radii.

Some practitioners will conservatively select the largest design vehicle (WB 50 to WB 67) that could use a thoroughfare, regardless of the frequency. Consistent with AASHTO, this manual emphasizes an analytical approach in the selection of a design vehicle, including evaluation of the trade-offs involved in selecting one design vehicle over another.

In urban areas, it is not always practical or desirable to choose the largest design vehicle that might occasionally use the facility; the impacts to pedestrian crossing distances, speed of turning vehicles, and other design criteria that may be inconsistent with the community vision, goals, and objectives for the thoroughfare. In contrast, selection of a smaller design vehicle in the design of a facility regularly used by large vehicles can invite frequent operational problems. The practitioner should select the design vehicle that will use the facility with considerable frequency (for example, bus on bus routes and semi-tractor trailer on primary freight routes or accessing loading docks). Two types of vehicles are recommended:

Design vehicle—This is the vehicle that must be regularly accommodated without encroachment into the opposing traffic lanes. A condition that uses the design vehicle concept arises when large vehicles regularly turn at an intersection with high volumes of opposing traffic (such as a bus route).

Control vehicle—This vehicle's infrequent use of a facility must be accommodated, but encroachment into the opposing traffic lanes, multiple-point turns, or minor encroachment into the street side is acceptable. A condition that uses the control vehicle concept arises when occasional large vehicles turn at an intersection with low opposing traffic volumes (such as a moving van in a residential neighborhood or once-per-week delivery at a business) or when large vehicles rarely turn at an intersection with moderate to high opposing traffic volumes (such as emergency vehicles).

In general, the practitioner should obtain classification counts to determine the mix of traffic and frequency of large vehicles, should estimate how this mix will change as context changes, and should keep consistent with the community's long-range vision. If there are no specific expectations, the practitioner may consider the use of a single-unit truck as an appropriate design vehicle.

Speed Management

Under the conventional design process, many arterial thoroughfares have been designed for high speeds and traffic volumes. As the context of these thoroughfares change over time—which would include walkable, compact mixed-use areas—the speed encouraged by the design becomes a matter of concern. Further, municipalities establishing speed limits based on the measured 85th percentile speed are finding they are required to establish higher speed limits than the community desires for the area. In these cases, traffic engineers are tasked with identifying methods to reduce arterial speeds. This section identifies research and the practical experience of agencies in managing arterial speeds.

It is popularly held that higher operating speeds result in higher crash rates and higher severity of crashes. Research on the effect of actual operating speed on crash rate is inconclusive (TRB 1998). However, research shows that higher operating speeds do result in higher crash severity, higher percentages of injury, fatality crashes, and more serious property damage. Therefore, lower vehicular traffic speeds are beneficial when collisions occur with other vehicles or pedestrians.

Speed management is an approach to controlling speeds using enforcement, design, and technology applications. While “traffic calming” is a type of

speed management usually used on local residential streets, speed management can be used on all types of thoroughfares.

Speed management methods can use technologies that provide feedback to the motorist about their speed, or designs in which the motorist perceives the need for a lower speed. These techniques include signage, signalization, enforcement, street designs, and built environments that encourage slower speeds. Other methods include physical devices that force drivers to slow down, such as roundabouts, raised intersections, narrowed sections created by curb extensions, and raised medians. Physical devices are generally more effective at changing driver behavior, but may be more costly to implement and may not be appropriate on all thoroughfares. Speed management is often a multidisciplinary decision because it requires input from emergency services, engineering, street maintenance departments, law enforcement, and transit service providers. The process of implementing a speed management program benefits from public involvement to understand how the community uses thoroughfares and how it perceives various speed management methods. Bicycle and pedestrian advocacy groups should also be involved in the process. Effective speed management requires knowledge of the existing traffic patterns, both quantitative and

qualitative. Quantitative measures of traffic counts, intersection turn movements, and speeds help to determine the existing condition and the need. Qualitative information, often gathered from the public or through observation, can explain behavioral issues. Implementation of speed management should be examined along corridors and across jurisdictions. It is important for a corridor to have a consistent speed through different jurisdictions if the character and context also remain constant.

The following is a list of speed management measures commonly used in the United States on thoroughfares designated as arterials or collectors.

“There is a strong statistical relationship between speed and road safety. When the mean speed of traffic is reduced, the number of accidents and the severity of injuries will almost always go down. When the mean speed of traffic increases, the number of accidents and the severity of injuries will usually increase. A 10% reduction in speed can be estimated to reduce the number of road accident fatalities by 37.8%”: - Elvik, Institute of Transport Economics research paper, Elvik, R. (2004, December 1). Speed and road accidents. Retrieved December 26, 2015, from https://www.toi.no/getfile.php/Publikasjoner/TØI_rapporter/2004/740-2004/740-2004.pdf



Speed management methods can use displays that provide feedback to motorists.



Roundabouts are a way to maintain traffic flow and control speeds.

Active Measures

- Roundabouts, particularly when used within a “roundabout corridor”
- Road diets (reducing the number of lanes by adding medians, converting travel lanes to parking, or adding bike lanes)
- Lateral shifts or narrowing (curb extensions with a center island or other techniques that require vehicles to move out of a straight path or create neck downs)
- Smaller curb-return radii to slow turning vehicles, and the elimination of free-flow channelized right-turn lanes
- Provision of on-street parking where adjacent land uses and activities will generate demand
- Speed humps and speed tables (not widely used on arterials and lack support of emergency service providers)
- Speed cushions or speed platforms (less impact on emergency vehicles than hump and tables)
- Narrowed travel lanes
- Raised crosswalks combined with curb extensions to narrow street
- Speed actuated traffic signals where a vehicle traveling at excessive speeds will trigger the signal to change to red

Passive Measures

- Synchronized signals to create progression at an appropriate speed
- Radar trailers/speed feedback signs flashing
- “SLOW DOWN” message when speed exceeds a preset limit (most effective when coupled with enforcement)
- Visually narrowing road using pavement markings
- Visually enclosing street with buildings, landscaping, and street trees
- Variable speed limits (using changeable message signs based on conditions)
- Speed enforcement corridors combined with public education
- Flashing beacons on intersection approaches to slow traffic through the intersection
- Speed limit markings on pavement
- Mountable cobblestone medians or flush concrete bands delineating travel lanes for visual narrowing
- Shared streets using signs and pavement markings (such as bicycle boulevards)
- Automated speed enforcement (including red-light enforcement)

Existing Speed

Management Tools

Dallas has six traffic management

techniques available that are designed to address speed and cut through traffic in residential areas. These implementation items are for alleys, local roadways, and in two instances, collectors. They must be initiated by petition with a consensus from area residents. The available tools include:

- All-way stops
- Road Humps
- Alley Rumble Strips
- Residential Parking Districts
- Residential Parking Only
- Street Closures

Given the limitations of these elements to residential locals and collectors, it is important to explore additional, broader traffic management techniques to calm roadways and create the desired Complete Streets environment.

Rethinking Speed

U.S. traffic engineering practices have historically been dominated by passive safety measures. Some of these measures include effective automobile safety devices such as air bags and crumple zones, but as currently applied to roadway design, passive measures have created high speed, auto-centric streets throughout the country. Clear zones, wide streets and traffic lanes, removal of street trees and generally increasing a driver's room for error were considered the primary way to improve roadway safety when

national street design guidelines were drafted 50 years ago.

In the mid 20th century, passive safety was pushed at the expense of more holistic, design-oriented solutions which protected vulnerable road users and slowed traffic. An engineering principle still used to support passive safety ideas is the 85th percentile rule, a guideline used by almost all local and state Departments of Transportation to set roadway speeds. It states that traffic laws should reflect the behavior of the majority of motorists and that reducing speed limits will not decrease the number of crashes nor increase safety. (source: http://www.ite.org/standards/speed_zoning.pdf). The 85th percentile rule almost exclusively focuses on reactively setting speed limits based on existing driver behavior. Because most complete streets policies desire to proactively reduce traffic speeds through changing driver behavior, there is often a disconnect between the goals of 85th percentile guidelines and complete streets policies.

Even though the U.S. has about 32,000 traffic deaths a year, roadway design is typically outside of our national public discourse. Though total traffic deaths have decreased since 2005, pedestrian and cyclist deaths and injuries have increased in recent years (source: <http://www.nrd.nhtsa.dot.gov/Pubs/811701.pdf>). According to NHTSA, three out of every four pedestrian fatalities occur in urban

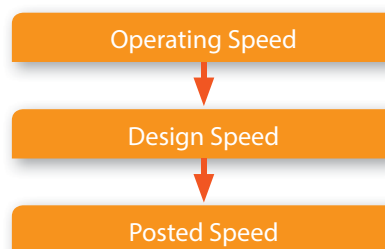
areas, and Texas is one of the top four states for pedestrian fatalities. Creating safer streets through roadway design in Dallas is an opportunity to substantially improve traffic safety and save lives in our city.

While passive safety measures have made automobiles safer, they have often made conditions for vulnerable roadway users more dangerous. Research shows that simply reducing automobile speeds from 30 mph to 20 mph can increase survival rates for pedestrians who are struck by vehicles by more than 50 percent. A more balanced approach is now needed. When street improvements are contemplated, a complete street design approach should aim to effectively influencing driver behavior to travel at a “target speed” that is safer for all street users in that context. This target speed should be the dominant factor in shaping the design of the street, while the 85th percentile speed of the existing street should only be a consideration to ensure design of a safe transition to the desired target speed. While the 85th percentile rule is appropriate for changing speed limits when no geometric changes to a street are proposed, a passive “majority rules” approach may not be applicable when planning for substantial design changes to a corridor. Actively designing our streets to reduce speeds to create safer environments for vulnerable roadway users is a more

appropriated new paradigm for the 21st century.

Highway design often set speed limits based on the observed speed of vehicles using the 85th percentile rule. Using this approach, called the “operating speed” of a roadway, speed limits were set reactively based on driver behavior, regardless if recorded speeds were appropriate for a particular street context. The NACTO Urban Street Design Guide mentions the following sequence for conventional highway design:

Conventional Highway Design:



As recommended by NACTO, a more proactive approach for setting speeds is encouraged for complete streets projects. Target speed, or the speed you intend for drivers to go, should be the primary consideration for setting posted speed limits. The target speed of a corridor should be set at the beginning of the design process based on:

- Complete streets typology
- Neighborhood classification as per the Forward Dallas plan



Pedestrian volumes are one method used to gauge the effectiveness of Complete Streets policies.

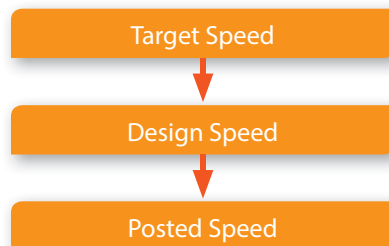


Knox Street demonstration project installed and monitored the operation of a two-way cycle track.

- Projects specific design elements proposed for the corridor

Given this new methodology, the preferred sequence for setting posted speeds is:

Proactive Urban Street Design:



Other project specific criteria, such as TxDOT speed guidelines for state roads and reduction of posted limits in areas where no design changes are proposed will need to be addressed on a case-by-case basis. determining preferred target speeds at the beginning of the design process for new complete streets projects, however, helps clarify project goals and informs major design components of a project.

Multimodal Level of Service

Traditional Level of Service (LOS) measurements focus on vehicular delays at intersections. A more robust measurement of street quality is needed to account for a variety of modes. The 2010 Highway Capacity Manual (HCM) provides methods for measuring Multimodal Level of Service. The following factors should be taken into account when assessing

Multimodal Level of Service based on HCM guidelines:

- Pedestrians: Crash and injury rates, traffic volumes, speeds, buffers between roadways and sidewalks
- Bicyclists: Traffic volumes, traffic speeds, lane widths, separated bike lanes vs. sharrows
- Transit: Service reliability, speeds, headways, transit stop amenities

Multimodal Level of Service assessments should be used during the corridor planning development process to determine current roadway conditions and plan for future complete streets improvements.

KNOX STREET DEMONSTRATION EXPERIENCE

Under special circumstances it can be desirable to test Complete Streets concepts prior to implementing them permanently. These tests, or demonstration projects, can be extensive, held over 30 to 90 days, and at a significant expense; or they can be short-term, lasting a day, several days, or weeks with a minimal cost. With creativity, any number of Complete Streets features may be tested on a temporary basis, including road diets which explore adding bike lanes, on-street parking, enhanced cross walks, bulb-outs, medians, turn lanes, or other roadway features.

In September 2012 the City of Dallas authorized a four-day demonstration project on Knox Street from North Central Expressway to the Katy Trail. The project was intended to reduce Knox street from four to three vehicle lanes, install a two-way cycle track on the north side, and restripe all head-in parking on the south side of the street to 60-degree angle parking. This demonstration would allow testing the feasibility of combining a road diet, exclusive bicycle facility, and new parking in a narrow and congested retail corridor.

The project was monitored and evaluated to confirm whether a reduced number of lanes could function adequately to warrant implementing the demonstrated cross-section on a permanent basis. In order to implement the demonstration project, the following objectives were anticipated:

- Construct a temporary cross-section with traffic signs, barricades and pavement marking tape and test it for a total of four days (two weekdays and two weekend days).
- Create a Traffic Control Plan (TCP) approved by the City of Dallas that utilizes temporary construction barricades and devices and meets budget constraints.
- Develop and execute a TCP that encourages an appropriate vehicle speed, minimizes delays and



congestion, accommodates high turnover of on-street parking, and is robust enough to provide a safe separation between vehicular and bicycle traffic.

- Create a more walkable and bikeable street than currently exists.

The project was a success in terms of creating a safe and durable roadway environment for pedestrians and bicyclists. Traffic volumes through the corridor and at major intersections were approximately the same before, during, and after the demonstration. However, congestion increased during each peak period which was confirmed by the Level of Service analysis. Adjacent retailers and restaurateurs indicated that their patrons were disappointed by the level of congestion. The following Lessons Learned are catalogued by category.

Design Concept

- For high turnover angle parking, extra maneuvering room is desirable.
- All signs and pavement markings should conform to MUTCD.
- Provide adequate advance warning for a lane drop.
- Bulbouts were useful to direct drivers and minimize

street crossing distances.

- Set stop bars back from the intersection to accommodate turning vehicles.
- Converting from 90-degree to angle parking loses a few parking spaces.
- A yellow delineator placed at the ends of the cycle track provided a visible and positive delineation.
- Existing utility poles and concrete bases are a barrier.
- Lower, water-filled traffic barriers would have been more than adequate to protect the bicyclists and pedestrians and not obstruct sight distance.

Installation

- Black-out tape or paint to cover up existing pavement markings is a necessity.
- Tape stands up to rain and normal traffic, but not to turning and parking maneuvers.
- Be vigilant and flexible—we created a temporary bulbout to direct drivers safely.
- Restriping of parking requires additional surface cleaning and care.
- Installation during off-peak vs. peak traffic periods is preferable.



The Knox Street demonstration used blackout tape to temporarily cover up existing pavement markings.



The Knox Street demonstration tested the installation of an eight-foot cycle track.

- The weather forecast is critical and determines what materials you use.
- Four-foot bike lane (eight-foot cycle track) next to a gutter is tight.
- The east terminus of the cycle track had confusing signs and markings for bicyclists.
- Start the design as early as possible and work with the contractor to minimize costs.

Conclusions

- Demonstrations can model permanent Complete Streets installations for a reasonable cost.
- Demonstrations can ensure input from all stakeholders.
- Knox Street can technically be converted to a three-lane section and accommodate the existing vehicle demand.
- The allocation of space in the existing ROW would still need to be perfected during the detailed design phase.

Demonstration projects are valuable in giving residents, property owners, and City staff an opportunity to understand the impact on operational changes that may occur.

Monitoring and Evaluation

In order to determine the success or failure of new Complete Streets policies, the City of Dallas should refer back to the initial livability goals and principles. Well-documented public outreach and continued engagement through the implementation stages will make for a smoother experience. A monitoring program should:

- measure the impacts and effectiveness of the Complete Streets Initiative over time
- establish benchmarks based on multimodal levels of service. (For instance, transit quality of service, pedestrian safety, and vehicle delay)



The Knox Street Demonstration project provided a two-cycle track from the Katy Trail crossing on Knox Street to McKinney Avenue.

- develop a multimodal travel counts process to monitor the actual corridor performance versus the pre-established benchmarks
- establish operations policies to better link street maintenance, utilities, and rehabilitation with bicycle lanes and pedestrian improvements.

Making Adjustments

Through public surveys, the City can continue to monitor customer satisfaction, ease of use, and behavior adjustments. A business activity and satisfaction survey should be conducted after three months to gauge effectiveness of policies. These results can be compared with results from public surveys. In the end, there may still be unsatisfied customers. However, through documentation of the publicly-established goals for the community, the strategies selected to achieve those goals, as well as the indicators for success, can be used to help explain why the policy changes were necessary and what they have accomplished. Maintaining an open communication platform is important; take time to respond to emails and answer phone calls about the policy changes.

Some examples of evaluation measures include local business activity, pedestrian and vehicle volumes, average time spent searching for a parking space (determined through public surveys), number of people biking or walking to work, adoption of commuter benefits to promote alternatives to solo driving, and customer satisfaction. If these indicators are showing little-to-no improvements, the Complete Streets strategies need to be re-evaluated with new strategies selected to reach target goals.

Driving will continue to be the primary mode of transportation for many people in the region, but it must be evaluated as one part of a comprehensive multimodal transportation system that includes walking, biking, and transit.



Portions of Chapter 4 were derived from the Boston Complete Streets Guidelines, prepared by the City of Boston Transportation Department, with permission.

4. PEDESTRIAN ZONE DESIGN ELEMENTS



Streets are the most extensively used civic spaces in the community.

4. PEDESTRIAN ZONE DESIGN ELEMENTS



Good sidewalks provide a clear, unobstructed path for pedestrians.

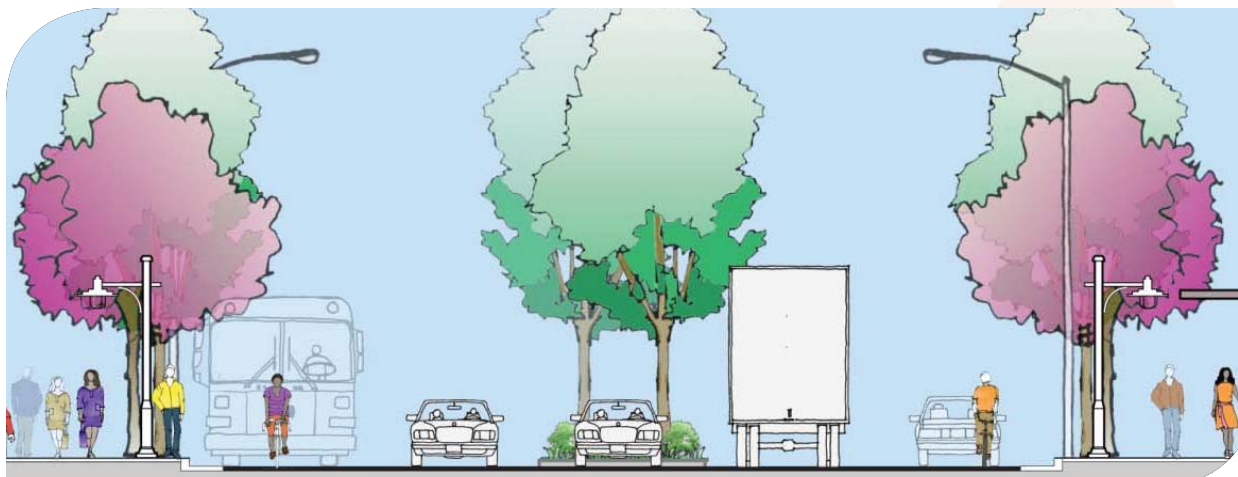
A safe pedestrian zone is an essential component of a well-designed street. The pedestrian zone is composed of several elements, including the sidewalk, the spaces between the sidewalk, the roadway on one side, and the building front on the other side. In addition to the provision of a basic sidewalk, the walking experience is affected by numerous elements that are contained within this pedestrian zone such as driveways, utilities, transit stops, furnishings, and public art.

Though Dallas currently has many streets with sidewalks, many of these sidewalks are not continuous, or do not feel comfortable due to specific design issues. There are also locations in Dallas where the pedestrian zone provides for excellent walking conditions such as Flora Street, McKinney Avenue, and Bishop Avenue.

This chapter covers the essential design elements of the pedestrian zone that ensure that people can safely and comfortably walk along streets throughout Dallas. It provides a menu of components and specific guidance to make the pedestrian experience more welcoming and safe.



Senior citizens depend on sidewalks for mobility and exercise.



Pedestrian zone



A comfortable walking environment encourages car-free short trips.



Universal access enhances quality of life for all citizens.

WIDER SIDEWALKS

The minimum recommended sidewalk widths are shown in the Lane Width Chart, however, there are many locations where a wider sidewalk is necessary to accommodate anticipated pedestrian volumes. Where these volumes are known or can be reasonably estimated, the Highway Capacity Manual should be used to determine the appropriate sidewalk width.

Wider sidewalks should be provided as a matter of course along school and university properties and key school walking routes, along streets with frequent transit service, all streets within 1/4-mile radius of DART rail stations, along all mixed-use streets, and in any location where the sidewalk is likely to be shared between pedestrians and bicyclists. Other factors to consider when determining sidewalk widths include materials, placement of trees and landscaping, ensuring continuous walking surfaces, and transitioning between different street types and between the sidewalk and building entrances. In addition, the sidewalks should be designed in coordination with the placement of utilities to minimize potential obstructions.

TREES AND GREENSCAPE

Trees and other greenscape plantings have a variety of functions. They can provide shade, buffer pedestrians from passing vehicles, and provide aesthetic enhancements. Trees and other plantings must conform to Article X of the Dallas Development Code, The Landscape and Tree Preservation Ordinance. When placing trees, consideration should be given to the placement and interaction of pedestrian lighting, utilities, and street furniture. Tree and plant selection is very important to ensure selection of climate-appropriate trees and plants (see Article X and the NCTCOG's iSWM guidelines). Another consideration is the tree's anticipated mature canopy height, which will affect the clearance for pedestrians, buses and utilities. With proper considerations all of these elements can function together efficiently.

STREET FURNITURE

Well-designed street furniture makes the sidewalk realm more comfortable. Benches provide places to rest, catch-up with neighbors, or have lunch. Properly distributed trash receptacles help to keep the street clean and presentable. Appropriately located bicycle racks and shelters are easier to use than improvising with meters and fences. In addition to providing amenities, street furniture can also provide a buffer from the noise and commotion of vehicles in the street.

Street furniture that is not thoughtfully laid out can result in obstructions and clutter in the sidewalk environment. This section provides design guidelines for street furniture frequently located in the pedestrian zone, including bicycle parking, seating, and waste receptacles. Street furniture is normally installed in the buffer/furnishing zone, although it can also be installed in the frontage zone, on curb extensions, and on medians.

A key goal of these guidelines is to organize the City's street furniture in a way that maximizes safety, comfort, and function for all users. In addition to location considerations, the design of street furniture should be simple and compatible with the existing environment. Street furniture should be durable, maintenance-free, and should utilize green material (recycled plastics and metals) whenever possible. Ultimately, City staff will review and approve all proposals for the placement of street furniture in the public right-of-way, and may request the addition of street furniture for some projects.

The following are typical requirements of the City of Dallas:

- Benches must not exceed three feet in length between dividers to discourage people from sleeping on benches
- Bicycle racks and benches must be made out of steel, concrete or other water proof material
- Bicycle racks and benches must be placed on a safe area without being an obstruction to pedestrians, bicyclists and vehicles.

Seating

Description

Providing a place to sit is a basic necessity, particularly for mixed-use streets in Dallas. Seating gives pedestrians a place to rest, wait, or simply to relax and enjoy street life. Providing comfortable, inviting places to sit can transform a sidewalk into a gathering area and enhance its role as a public space. Providing a shaded seating area is particularly important during hot weather. Seating is also important to provide for seniors who may be walking between transit and their destination.

Application

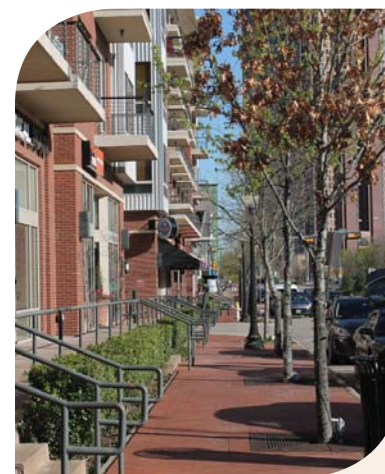
Seating comes in a variety of temporary and permanent forms, such as chairs, benches, seating walls, steps, monuments, planters, and raised tree beds. People enjoy watching others move about, and the design and location of seating should respond to how the surrounding space is used. Where possible, seating should be arranged to define social spaces.

The following considerations apply to seating areas in the public right-of-way.

- Seating should be affixed in such a way that it is not easily damaged or removed. Care should be exercised to



Space for sidewalk cafés should not come at the expense of pedestrian zone width.



On-street parking and trees create a pedestrian buffer that enhances comfort.



Street furniture should be illuminated at night and shaded during the day.



A place to sit is a basic necessity, especially for the very young and old.

ensure that seating does not interfere with entrances to buildings, heavily used loading zones, parked vehicles, access to fire hydrants, and other potential conflicts.

- Seating should accommodate a minimum of two people. Seating can be integrated into buildings and building frontages.
- Seating should be situated to enable pedestrians to view street activity while being outside of the immediate flow of pedestrian traffic, and should be buffered from noise and vehicle exhaust whenever available. Where possible, seating should provide a sense of protection to the person seated.
- Benches at bus stops with no shelter should be located at the back of the sidewalk and should face the street.
- The following clear widths must be maintained when installing benches:
 - 3' minimum on either side of the bench
 - 5' minimum from fire hydrants
 - 2' recommended clearance from all utilities and utility appurtenances
 - 5' minimum, ideally 6' clear path in front of the bench when located at the back of the sidewalk, facing the curb
 - Where the back of the bench abuts a building, wall, or other obstruction, a one-foot minimum clear width should be provided for maintenance and debris removal.

Considerations

Seating should be provided with and without armrests if possible. Armrests provide stability for those who require assistance sitting and standing. Armrests in the middle prevent sleeping while still allowing access from the side. Seating without armrests allows a person in a wheelchair to maneuver adjacent to seating or to slide on easily.

Climatic conditions should be taken into consideration when seating materials are determined. Bare metal and other heat absorbing materials should not be used.

Movable seating allows the flexibility for an individual to control the amount of sun exposure or an allowance for groups to determine their desired seating arrangement. Movable seating may be most appropriate for plazas, street parks, or in association with certain retailer groups where activities may spill out into streets. Movable seating, however, requires a commitment to continually maintain and replace elements that become damaged or stolen.

Bicycle Racks

Description

Providing ample, well-designed bicycle parking is a key component of the City's strategy to increase bicycling. When bike parking is provided, bicyclists are less likely to lock their bikes to sign posts, trees, or railings, which can do damage or create obstructions. Bicycle parking has been installed as part of the DART Station Access Program, and bicycle parking is identified as an implementation project in the Dallas Bike Plan. The City of Dallas issues licenses for private installation of bicycle racks in the right-of-way. Other relevant requirements related to bicycle parking (in addition to these guidelines) are provided in the Dallas City Code, Chapter 43.

Application

The following guidelines cover the design of bicycle racks in the public right-of-way. They can be sculptural or utilitarian, and hold one or multiple bicycles. Good bicycle parking designs maximize capacity while maintaining an orderly appearance. Bicycle rack designs should meet the following criteria:

- The rack should be affixed to a paved surface.
- The rack should support the frame of the bicycle at two points (in consideration of different frame sizes and styles).
- The rack should be simple and easy to use.
- The rack should allow easy locking of the frame and, preferably, both wheels.
- The rack should be placed so that bicycles park parallel to the curb or building frontage, or angled if there is additional space available while still meeting the minimum clearances.
- The rack should meet ADA guidelines to be detected with a cane.

Some bicycle rack designs that are available commercially do not meet these criteria, and therefore should not be used. The dimensions that follow represent the recommended minimum clearance between the nearest element of an unoccupied bicycle rack and the adjacent object. Racks should be installed so that parked bicycles do not obstruct the pedestrian through zone or access to fire hydrants.

Supplement "Location of a Bicycle Parking Device" (Dallas City Code Sec. 43-125)

- 3' from back of curb
- 3' from building or building frontage while maintaining a five-foot minimum pedestrian clear path
- 10' from a building doorway
- 3' from all street furniture or fixed objects including trees and vegetation, light poles, benches and other bike racks
- 5' from a marked crosswalk
- 10' from a fire hydrant, fire call box, police call box or other emergency facility
- 5' from a driveway
- 3' from the front and 15' from behind a designated bus stop sign post
- Place bicycle racks within 50' of the target building entrance so that bicyclists are not tempted to use other objects that are closer to the entrance



Seating design and location should relate to how the surrounding space is used.



The design of bike racks and street furniture can complement each other.



Street furniture can turn the pedestrian realm into an urban “living room.”



Bicycle shelters should be located close to building entrances.

Considerations

In-street bicycle parking should be considered where there are space constraints on the sidewalk and high bike parking demand. 8-10 bicycles may be parked in the space of one motor vehicle. When placed close to an intersection, in-street bicycle parking can work as a curb extension to lessen the distance pedestrians must cross the street.

Bicycle Shelters

Description

Bicycle shelters are structures that provide secure, covered areas for bicycle parking. DART has installed a number of shelters at transit stations. These guidelines encourage the use of additional covered shelters on City streets that accommodate numerous bikes for short- and long-term parking needs. Street types such as mixed-use with wide sidewalks are appropriate to consider for bicycle shelters. Ideally, they are within sight distance or close proximity to significant building entrances or transit stops. Where possible, bicycle parking shelters should provide weather protection for as many parked bicycles as possible. Installation footings must meet all structural and loading requirements.

Application

Bicycle shelters should be:

- Located within 50' of the main entrance to the building or transit station to encourage use of the shelter
- A minimum of 8' wide and a minimum of 7' from floor to ceiling (if a bicyclist is expected to enter the shelter to lock the bike to a rack); the length of the shelter depends upon the number of bicycle racks the shelter is designed to accommodate
- Placed so that, when occupied, bicycles do not intrude into the pedestrian through zone

Considerations

- Bicycle shelter installation on pavers requires approved footing.
- Bicycle shelters should be located in well-lit areas. Passive detection lighting should be provided in areas of low ambient light.
- Signs should be used to help direct bicyclists to shelters.

Bollards

Description

Bollards are permanent or temporary posts or objects used to create an unobtrusive boundary between different modes of transportation and realms of the street. Their main functions are to protect pedestrians, bicyclists, buildings, and specified areas from vehicular access and to highlight traffic calming measures. On streets without curbs (shared streets), bollards can delineate the edge of the roadway. Bollards can also provide a location for installation of low-level lighting.

Bollards can be fixed, flexible, or removable; they can be designed to withstand heavy impacts, or give way on impact. Breakaway bollards are intended to deter vehicle access, but allow emergency vehicles access. Bollards come in all forms, from metal posts to concrete blocks and planters.

Application

The most important design feature when using bollards is visibility. Bollards must be clearly visible in all lighting conditions for all users, particularly pedestrians and motor vehicles. Reflective material, lighting, and colors that provide contrast to the surrounding environment should be used. Proper size and spacing should balance restricting vehicular access and providing a clear pedestrian path free from obstructions.

Bollards can be used to

- Restrict vehicular access to car-free zones, pedestrian malls, plazas, etc.
- Prevent delivery trucks from using sidewalks in downtown commercial and mixed-use areas
- Provide security measures for buildings and infrastructure such as government and financial institutions
- Narrow turning radii to reduce vehicular speeds around corners
- Create protected space for street furniture
- Protect stormwater management features such as rain gardens, stormwater planters, and green curb extensions
- Direct traffic flow and highlight traffic calming measures such as chicanes on shared streets
- Separate the roadway from the pedestrian realm on streets without grade separation or curbs

Considerations

- Bollards require proper maintenance when damaged due to accidents or deterioration from environmental wear. When not maintained they can create hazards for pedestrians.
- Removable bollards should be considered if restricting access is only needed during part of the day or during special events.



Bicycle shelters have been installed at DART rail stations.



Bollards define and protect a pedestrian space by restricting vehicular access.



Parking meters and pay stations



Recycling bins along mixed-use trails make it easy to recycle plastic water bottles.

Parking Meters and Pay Stations

Description

In many parts of the city where on-street parking is present, motorists are required to pay at meters or pay stations. Dallas, like many other cities, is moving towards smart parking technologies, which enhance parking information, achieve desired turnover levels, optimize parking space occupancy, consolidate single parking space meters to multispace pay stations, improve operational efficiency, and reduce maintenance costs. Dallas is working to consolidate single space parking meters to multispace pay stations in the Downtown core to reduce streetscape clutter and to achieve the objectives above.

Application

- All meters and pay stations should be located in the greenscape/furnishing zone at a minimum of 18" from the curb; meters may not be placed in the pedestrian zone. A clear path should provide access to and from parked cars to the pay station.
- Typically, one pay station should be provided for every 8 to 10 parking spaces, centered in the middle of these spaces. This spacing typically puts pay stations no greater than 80 to 100' from the farthest parking space. In some cases, such as where there are fewer than 16 spaces on a block face, it is possible to only provide one pay station in the middle or two pay stations placed equidistant from each corner of the block. Street trees, utilities, and other street furniture elements may also dictate where pay stations can be installed.
- Sensors can be programmed to convey occupancy rates to a centralized system that can collect, store, and analyze the data over time, making it possible to identify patterns in how parking spaces are utilized and develop new management programs. Parking spaces that are underutilized should be identified. Usage rates can be used to develop or adjust pricing schemes. Sensors can report time violations to parking enforcement officers and help increase efficient use of parking spaces. Sensors can also be useful for documenting and publishing actual real-time usage and availability rates.

Considerations

- On-street parking rates should be on par with rates of private and public lots and garages
- Pay stations should be solar powered where possible. Solar panels should be strategically placed to reduce visual clutter on the street.
- When removing individual parking meters to install multispace meter kiosks, installing bicycle racks, street trees, benches, and other sidewalk amenities should be considered.
- The use of smartphone apps should be considered for remote payment at meters. Smartphone apps can also convey parking availability in real-time to drivers, however this can also lead to distracted driving. The benefits of such applications should be weighed against safety concerns
- If individual spaces are demarcated, information on usage can be collected per space. However, striping parking spaces can limit the number of spaces utilized, particularly with the growing popularity of smart cars, scooters, and motorcycles.

Recycling Bins and Garbage Cans

Description

Providing receptacles for trash and recycling is important to prevent the spread of litter while demonstrating the City's commitment to waste reduction through recycling. In order to ensure a minimum five-foot pedestrian through zone is maintained, receptacles for trash and recycling should be placed in the buffer/furnishing zone. Where a furniture zone is not present, placement of receptacles should adhere to the clearance requirements listed below. Otherwise, other street types can accommodate trash receptacles where space is available such as at high-use transit stops.

Many on-street garbage cans are owned and maintained by DART. DART's guidelines state that a bench or shelter must be present for placement of a trash can. Alternatively, a trash can may be placed as part of DART's Adopt-A-Can program. Under this program, the entity that requested the trash can is required to maintain it.

The following are typical requirements of the City of Dallas:

- Must meet preselected style requirements
- Must have a pop open cover
- Must use 30,40, or 55 gallon liners and be keyless

Application

The following clearance requirements apply to the placement of trash receptacles:

- 5' minimum from fire hydrants
- 1' minimum from any in ground obstruction, i.e., manhole, tree pit etc.
- 3' minimum from other street furniture
- 5' minimum, ideally 6', pedestrian through zone

Newspaper Racks

Description

Streetscape projects should provide the option of consolidated distribution newspaper racks. In Dallas, news racks are licensed and their number and placement per block is determined on a case-by-case basis.

Application

In districts with special design standards, news racks must conform to the district standards. Distributors are determined by lottery for each block face. Proposed sidewalk extensions are ideal locations for newspaper boxes.

Considerations

- See Chapter 51A of the Dallas City Code for additional detailed guidance
- Place racks near transit stops and other street furniture where people may be waiting or sitting
- Annual licenses for both free-standing and attached news racks are available through the City Office of Sustainable Development and Construction



Garbage cans can be designed to coordinate with other pedestrian zone elements.



Free standing newspaper racks are typically clustered together.



Amenities may include shelter, schedule information, and wayfinding signage.



Signage can help define a bus stop and give passengers a good first impression.

TRANSIT STOPS

Sidewalks provide access to transit and locations for transit stops. Transit stops are typically located in the buffer/furnishing zone. They should provide adequate space for people who are waiting without crowding the pedestrian through zone. This area should be paved to provide an accessible pathway to the door of the transit vehicle. Where space permits, shelters should be added to transit stops to make them more comfortable and inviting. Transit stops may also be located on curb extensions and floating islands.

Information for travelers should also be provided at transit stops. This should include, at a minimum, schedule information and real-time arrival information where possible. Bus stops can also be locations for local area maps and wayfinding information. All transit stops should be fully ADA accessible for passengers.

The DART 2030 plan recommends adding amenities to the vast majority of bus stops in Dallas by 2030. During the complete streets planning process, an assessment of all bus stops in the project area should be taken to determine which stops are eligible for upgraded amenities. All bus stop upgrades should be coordinated with DART. The following items should be considered:

- Signed bus stops with no amenities should be assessed to determine if a bench or shelter is warranted.
- Bus stops with only a bench should be assessed to determine if a shelter is warranted
- Corridors with high bus ridership should be assessed for next generation bus shelters which include real time arrival LEDs
- New bus stop opportunities or upgraded amenities for existing stops should be assessed for private development projects in coordination with DART
- For corridors with dedicated bus lanes or high volumes of regular bus service, 11' travel lanes are preferred to reduce bus conflicts with automobiles

Bus Stops

Description

Bus stops are the most basic transit stop and should be comfortable, safe, and accessible. Bus stop accommodations improve operations, ridership, and the value of transit to the community. They can include benches, trash and recycling receptacles, shelters, lighting, bicycle racks, bus schedules, maps, real-time next bus arrival information, newspaper boxes, and public art.

Stops should be visible, providing a clear sight line between bus operators and users of the system. Simple stops without shelters are appropriate for lower volume routes. Installation of amenities should be done in consultation with DART and the City of Dallas, as most amenities will require maintenance agreements.

Application

The length of the stop depends on the length of the vehicle as well as the placement of the stop, (i.e., nearside, farside, or midblock) and should be determined in consultation with DART. DART buses are up to 40' in length. In general, bus stops should be a minimum of 60' in length (80' long if midblock).

The pedestrian through zone of the sidewalk should extend to the curb at stops so that passengers may access the sidewalk

directly from the bus doors.

The area on the sidewalk where passengers load and unload at bus doors is called the landing pad. The landing pad at the front of the bus stop must provide a clear zone 5' long, parallel to the curb, and a minimum of 8' deep. The landing pad should consist of ADA accessible surface materials such as concrete or asphalt.

Trees should not be planted within landing pad and door zones of a bus stop. When street trees are desired near or within bus stops, DART should be consulted.

Bus stops should be set back a minimum of 5' from crosswalks. Where feasible, a 10' setback is preferred.

Where possible, trash and recycling receptacles should be placed to the front of the bus stop, at a minimum of 18" from the landing pad, a minimum of 3' away from benches, and in the shade. They should also be anchored to the pavement to deter theft.

Considerations

Curb extensions can provide additional pedestrian space and improve bus travel time by reducing the time needed for loading and unloading. The width of the curb extension is determined by the width of the adjacent parking lane, and the length should be long enough to allow passengers to board and exit at all doors of the bus. Be aware that curb extensions can delay through traffic (since the bus essentially stops in the travel lane and does not pull over).

Bus Shelters

Description

Well-designed transit stops can help make transit use more comfortable and convenient. Transit shelters in Dallas are currently provided and installed by DART. Transit shelters should be provided on all key bus routes if sidewalk space allows. The I-STOP program installs solar-powered bus shelters with lighting at all new bus shelter locations. When providing a bus shelter, the bus stop must be ADA compliant with a 5' long (parallel to the curb) by 8' deep landing pad and a 4' minimum clear path.

Shelter placement must allow for unobstructed loading, unloading and unimpeded pedestrian through movements on the sidewalk.

Application

The following minimum clear widths for shelter placement must be maintained:

- 1' from the building face
- 4' from the back of curb
- 15' from crosswalks at nearside bus stops for visibility.



Stops should provide a clear sight line between operators and passengers.



Attractive, well-shaded bus stops encourage transit use.



Bus Rapid Transit (BRT) stops typically feature an array of passenger amenities.



Bus bays allow a bus to pull out of travel lanes; typically at park-and-ride facilities.

- 1' from any ground obstruction (i.e., manhole, tree pit, sign)
- 10' from fire hydrants
- 3' from the landing pad (maximum 25' to the right of the landing pad)

Considerations

Bus shelters should be prioritized and installed based on ridership, with the goal of benefitting the largest number of riders. Special consideration should be given to areas where high numbers of transfers are expected, where waiting times for riders may be longer, or where stops are close to facilities such as schools, medical centers, rehab centers, or high density housing and senior centers. Other considerations include the physical constraints of bus stop sites, preferences of adjacent property owners, bus stop requests by riders, and construction costs.

DRIVEWAYS AND CURB CUTS

Description

Driveways cross through the pedestrian zone and put vehicles in direct conflict with people who are walking. Therefore, driveway design and the number of driveways has a considerable influence on pedestrian safety and comfort. Generally, the frequency of driveways should be minimized and access should be provided via alleys, where possible. Driveway consolidation should be evaluated where driveway spacing is less than 50 feet.

Vehicles entering the right-of-way are required to yield to all cross traffic, including pedestrians. It is important to convey this requirement through design of the driveway/sidewalk interface. Driveways should be designed to look like driveways, rather than like roadway intersections.

Application

Different roadway types require different driveway treatments depending on the adjacent property use, the relationship between the property and the street, and the type of vehicles using the driveway. The following guidelines should be applied:

- The sidewalk should be clearly delineated across the driveway and maintain the grade, slope, and material of the adjacent sidewalk on either side of the driveway.
- Driveway design should meet current ADA guidelines.
- Maintain a 5' minimum sidewalk across driveways with no more than a 2% cross slope.
- The driveway apron should be contained within the buffer/furnishings zone to avoid a cross slope on the sidewalk. Where no buffer/furniture zone is present, the sidewalk approaches and crossing of driveway should be pulled back to ensure no more than a 2% cross slope.

Considerations

- Place driveways a minimum of 20' from crosswalks to provide good sight lines between vehicles and pedestrians and so that vehicles do not block the visibility of pedestrians.
- Consolidate driveways whenever possible to minimize the number of conflict points along the sidewalk. Planners, designers and engineers should follow the guidance for driveway spacing provided in the City of Dallas Off-Street



Parking and Driveways Handbook (2004) prepared by the Department of Development Services.

URBAN OPEN SPACES

Urban Open Spaces are places within a city where people gather to partake in a wide variety of activities: to celebrate, to demonstrate, to shop, to meet friends, and to relax. They are important to civic life, and serve as public living rooms for City events. Urban open spaces can be a variety of sizes and dimensions, from pocket parks like the Bexar Street Clock Tower Plaza to public plazas like Main Street Garden. Urban open spaces also include linear pedestrian malls such as Stone Place. Urban open space should be seamlessly integrated with the sidewalk and adjacent buildings.

Plazas, Pocket Parks, and Parklets

Description

Plazas are typically hardscaped open spaces that adjoin the sidewalk in the frontage zone. Plazas may be of various sizes and their design should take into account the human scale or “social field of vision.” As a general rule of thumb, plazas should be no wider than 200 feet to encourage interaction between people. Plazas may be designed to accommodate active uses such as temporary markets or street performances, or passive activities such as sitting.

Pocket parks are small areas that may adjoin the sidewalk (typically in the frontage zone), or be visually and/or physically connected to the sidewalk. Pocket parks may provide additional green space, gardens, play areas for children, or other public amenities. Pocket parks can be included in building developments or within the right-of-way where underutilized space is available.

Parklets are small extensions of the pedestrian zone that occupy former parking spots and include amenities such as plantings, seating and sidewalk cafés. They are a low-cost solution to expanding the pedestrian zone in areas where existing sidewalk widths cannot accommodate pedestrian amenities. They can be temporary or long-term.

The following guidelines suggest ways to design urban open spaces that encourage pedestrian activity.



Urban open spaces break up the concrete monotony of a city.



Urban open spaces should encourage pedestrian activity and interaction.



Landscape materials should be as sustainable as possible to reduce maintenance costs.



A mix of sun and shade is preferred in open space settings.

Application

- Locate urban open space adjacent to areas with high pedestrian activity throughout the day such as near transit stations or other pedestrian generators to encourage use throughout the day.
- Consider environmental elements that will make the plaza comfortable throughout the year. A mix of sun and shade is preferred. During the summer months, shade, water elements, and reduced hardscape help to reduce heat. In the winter, sun exposure and wind protection makes a plaza more inviting.
- Reclaim odd-shaped parklets, especially in high density areas, to provide interesting open spaces.
- Look for opportunities to reclaim odd space. By creating a pleasant and inviting area out of small areas of transition, utilization of these odd spaces can be increased.
- Make the transition between the sidewalk and plaza as broad and seamless as possible in order to make it inviting, visible, and easy to enter.
- Landscaping designs should be as sustainable as possible and take into consideration the level of maintenance that can practically be achieved in the space over the long run. The proportion of landscaping to paving should take long-term maintenance needs into account.
- Look for opportunities to make stormwater management a visible amenity. Trees and planters should be designed to provide shade, manage stormwater, and create visual interest. Porous materials may be used to reduce runoff. Rain gardens and bioswales can be incorporated in the design to promote water quality and reduce runoff. The use of cisterns is an optional method of capturing water for landscape irrigation during the summer months.
- Within plazas, provide a variety of seating options, some of which should be movable. Seating can be incorporated into building edges, walls, and landscaping containers; seating along plaza edges is encouraged because people tend to gather and feel comfortable at edges. A good rule of thumb is to dedicate at least 10% of a plaza's open-space to seating. Movable chairs provide ultimate flexibility for a public space and allow for endless variation in arrangements to suit personal preference, to capture sun or shade, or to sit in a group or alone.
- Open spaces are important to develop and maintain, especially in high density, urban areas, where they provide social gathering places and support the lifestyles of urban dwellers.
- NYC's Public Plaza Program: <http://www.nyc.gov/html/dot/html/pedestrians/publicplaza-sites.shtml>
- NACTO's low-cost public plaza design guidelines: <http://nacto.org/usdg/moving-the-curb>

Considerations

- Install temporary or permanent public art to energize the space, and to highlight local artists.
- Consider roadway surface treatments to delineate slow zones adjacent to public open spaces.
- Adjacent businesses can share responsibility as caretakers of the space.
- Designate locations for movable vending carts or stalls as part of the design. Vending configurations should take into consideration maintenance of a clear pedestrian path and the potential for supplying power or water to stalls or carts.
- Program the space with performances or activities at times when pedestrian activity is greatest.
- Offer public Wi-Fi, if possible.

Dallas Public Plaza Program

Several large U.S. cities have successfully installed new public plazas located in the street zone using low cost street furniture, movable concrete planters, and colored street paint. These types of projects are a cost effective, efficient way to provide new pedestrian-only public space while better utilizing results while providing traffic calming for excessively wide streets in mixed use neighborhoods and downtown. Preferred criteria for installation of new plazas are:

- Mixed streets
- Areas where public, pedestrian-only space is lacking
- Excess roadway capacity (if plaza is proposed in street zone)
- Demonstrated community support for a plaza
- Minimum size of 2,000 square feet

Maintenance of new pedestrian-only plazas should be the responsibility of participating community groups that commit to operate, maintain, and manage these spaces so they are vibrant pedestrian plazas.

Sidewalk Cafés

Description

Sidewalk cafés are outdoor areas in the public right-of-way with seating and tables at which patrons of adjacent restaurants can dine. Sidewalk cafés should be encouraged because they add interest, and enliven the sidewalk area.

Sidewalk cafés are encouraged along Mixed-Use Street where commercial activities occur. Careful attention must be given to the design and layout of sidewalk cafés to maintain sidewalk functionality and the quality of the public environment.

Where sidewalk cafés are proposed within the public right-of-way, proposals must be approved by the City Council, which must grant a license by ordinance before any sidewalk café can be installed. The submission requirements and regulations for permitting are available in Dallas City Code Section 43-115. The following guidelines focus specifically on the impact of sidewalk cafés on the pedestrian environment.

Application

- All sidewalk cafés must comply with American with Disabilities Act (ADA) guidelines. This applies to all aspects of the design, including maintaining of access on the sidewalk adjacent to the café, access into the café from the restaurant or the street, compliance of barriers around the dining area for detectability and railing height, overhead clearance, service aisle design, and wheel chair access to tables.
- A clear pedestrian path of at least 5' must be maintained on the sidewalk. This is measured from the outside edge of the sidewalk café to the first obstacle such as a bicycle rack, light pole, tree pit, etc. In areas with high pedestrian traffic, a clear path of 6' or more should be maintained. This clear path can include a portion of the furnishing zone but cannot direct pedestrians onto a non-ADA compliant surface. The clear path should be a straight line of travel. Pedestrians in the through zone should not be required to walk around any part of the sidewalk café or be required to navigate around obstructions in the pedestrian through zone.



Sidewalk furniture should be durable, free-standing, and matching.



A clear pedestrian path must be maintained on the sidewalk.

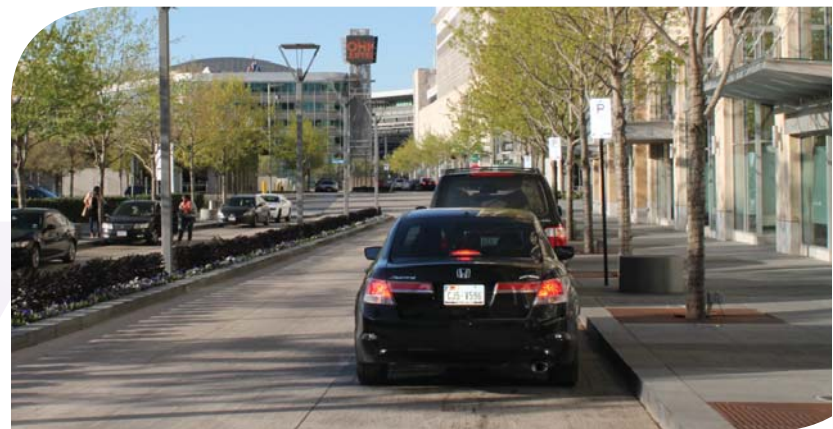


Minimum vertical clearances must be adhered to in cases of planter installations.

- Required size, location, and set-backs for sidewalk cafés include the following:
 - 5' from alleys entrances
 - 5' from parking meters, kiosks, traffic signs, and utility poles, fire hydrants, bike racks, and other street furniture except planter boxes.
 - 10' from pedestrian street crossing
 - Cafés can be as little as 6' deep (e.g., a single row of movable tables and chairs)
 - Sidewalk cafés can be placed in the buffer/furnishing zone instead of the frontage zone, as long as the pedestrian through zone is maintained and a 3' buffer between the curb and seating is maintained
 - Whether placed within the frontage zone or adjacent to a curb, sidewalk cafes should not interfere with the loading and unloading of transit vehicles, handicap parking, or commercial vehicles (where there is designated curb side space provided)

If alcohol is served, the State of Texas requires that there be an enclosure. Barriers must be attached to the ground, but can be made of fence, rope, chains, or live plants and be no higher than 3'. Permanent anchors may be installed as long as the barrier is removable, and the anchor is flush with the ground and ADA compliant. Fully enclosed sidewalk cafes are discouraged because they are less effective at activating the sidewalk.

- Furniture should be durable, free-standing, and matching.
- Awnings and/or umbrellas are desirable but may not extend into the pedestrian clear zone unless they are 7' or higher above the sidewalk but no more than 10' high. Heat lamps must meet fire codes.



New pedestrian lighting is required to be dark-sky compliant.

Considerations

In locations where the sidewalk is not wide enough for sidewalk cafés, or where additional seating is desired, consider the use of motor vehicle parking spaces for movable decking to extend the pedestrian environment.

The City will consider providing permits for “tables and chairs” only. This permit allows businesses to apply for seating to be used by patrons, but the seating is also available for the public to use. Table service is not permitted, nor is service of alcoholic beverages.

Pedestrian Lighting

Description

Appropriate pedestrian lighting facilitates safe movement and provides a sense of safety and security for pedestrians. Adequate street lighting lends character to a street and, by highlighting salient features, can reveal a unique identity. Pedestrian lighting is particularly important in business districts along mixed-use street types where it can enhance the environment and highlight businesses.

Pedestrian scale lighting is installed by the City of Dallas and by private developers. The City contracts with Oncor Electric Delivery to perform installation, operations, and maintenance of street lights within the City. Oncor installs a limited number of pedestrian lighting fixture styles. Within developments, the developer pays for installation, operations, and maintenance, and lighting costs for 20 years. Currently, pedestrian-scale lighting specifications are determined on a case-by-case basis, the process by which the City reviews and approves new street lighting installations.

The following are typical requirements of the City of Dallas:

- Pedestrian lighting fixture styles will be selected from several available designs, including historic and modern styles
- City will install pedestrian lighting only in areas with pedestrian counts exceeding 100 people per hour
- Lighting levels may not exceed 0.75 footcandles for residential and 1.5 footcandles for commercial areas

Application

- Lighting is critical to ensure the safety of intersections and midblock pedestrian crossings. Lamps are needed at both sides of crosswalks.
- Pedestrian-scale lighting (lampposts lower than 20' tall) should be used alone or in combination with roadway-scale lighting in high activity areas.
- New pedestrian lighting shall be dark-sky compliant with cutoff fixtures to ensure that 2.5% or less of the lamp lumens are emitted above a horizontal plane through the luminaire's lowest part, and 10% or less of the lamp lumens are emitted at a vertical angle 80 degrees above the luminaire's lowest point, per the Texas Health and Safety Code Chapter 425, Regulation of Certain Outdoor Lighting.
- Light poles should typically be located in the furnishing zone and should not impede the pedestrian zone. The location of light poles must coordinate with landscape, civil, utility, and traffic control plans to ensure that appropriate clearances are maintained and that lighting is not obscured by tree canopies.
- Light poles should be placed a minimum of 3' from the curb face and 5' from fixed objects such as fire hydrants.



Proper lighting enhances pedestrian safety at crosswalks.



Light spacing should eliminate dark spots between light poles.



Wayfinding kiosks are an attractive and useful street feature.

- Lighting should coordinate with structures.
- Coordinate the position of light poles with current and future planned street trees.
- Overhead pedestrian lighting should be 12-15' above the sidewalk.
- Light spacing should be determined by the type of light fixture and amount of light emitted to maintain continuous illumination along the sidewalk and to avoid dark spots between light poles.
- Banners and plants must be installed parallel to the roadway.
- Minimum vertical clearance for attachments are as follows:
 - 15' banner brackets
 - 9' bottom of banner
 - 13' hanging plant brackets
 - 9' bottom of hanging plant

Considerations

- Paired alignment of light poles across a street provides a more formal look, while staggered arrangement of light poles provides a less formal look that may allow for fewer lights
- Lighting designs on neighborhood residential streets are often affected by existing utilities. Staggered spacing is preferred to provide more uniform lighting
- As LED technology develops, future consideration should be given to providing network control devices to allow for dimming and/or color control as a way to highlight locations during emergencies or to reduce energy consumption and dark sky impacts during periods of lowest activity (12 a.m. to 5 a.m.)

SIGNAGE

Informational Kiosks

Description

Kiosks in public areas provide valuable information, such as maps, bulletin boards, and community announcements. Kiosks can often be combined with gateway signs and are an attractive and useful street feature.

The following are typical requirements of the City of Dallas:

- Beautification banners and banners for special events are allowed on utility poles
- A rendering of the proposed banner and its location must be presented to the city for approval

Application

Kiosks may be located in any of the following areas:

- The sidewalk, furniture, or frontage zones
- Curb extensions
 - Near to transit stops: At a minimum, kiosks should be placed at least 7' from the stop, typically downstream. Their placement should not interfere with loading or unloading. They should be placed in a manner to allow pedestrians to easily access and read the panels.

- Placement of kiosks should:
 - Ensure appropriate sidewalk clearances (minimum 5')
 - Preserve sight visibility for motorists
 - Maintain 18" clearance from back of curb
 - Be no more than 18" from sidewalk zone if buffer/furnishing zone is wider than 7' 6"
 - Not obstruct more than 50% of a retail display window if this is the only display window of the affected business
 - Not block scenic views

Considerations

- Kiosks should include bulletin boards or an enclosed case for display of information.
- As a gateway element, the kiosk should include the neighborhood, commercial district, street, or park name; a map; and other pertinent information.
- Kiosks should have details and features coordinated with other street elements and should have a similar architectural character.

Wayfinding

Downtown, as defined by the Downtown Dallas 360 plan, is divided by highways. Additional wayfinding linking all downtown neighborhoods and their amenities is imperative for increasing foot and bike traffic in downtown. Quality wayfinding can have an influential effect by psychologically linking neighborhoods separated by barriers such as elevated highways while reducing the perceived distance of amenities throughout Downtown Dallas.

Additionally, quality wayfinding at large institutions such as medical campuses will be increasingly important as many Dallas hospital campuses and colleges expand. The city should work with large institutions on improving wayfind connecting institutions to adjacent neighborhoods with a focus on commonly used pedestrian and cycling routes. Other employment centers such as the Southwestern Medical District, the Baylor Hospital area, and the UNT-Dallas Campus area should also be a focus for enhanced wayfinding systems, as these areas constitute major trip generators and are often in need of more visible signage for pedestrians, cyclists, and drivers.

Digital tags and wayfinding systems can also provide dynamic information and services for residents and tourists. Systems can include real time transit information, pedestrian and cycling routes, interactive maps of nearby destinations, and bike and car share availability. Modeled after the electronic kiosks at Klyde Warren Park, digital wayfinding systems in Dallas can also provide information about upcoming community activities and other important information during special events.

Description

Vehicle and pedestrian wayfinding signs direct people to destinations within a city and influence the safe travel of all street users. Messages typically include guidance toward important destinations, landmarks, and parking areas.

Application

- Signs intended for vehicles should be placed in the curb zone or the median. A limited number of messages should be included on these signs for ease of reading while driving.



Wayfinding sign



Utilities



Utility construction should avoid disruption of sidewalks.



Above ground and overhead utilities should be relocated or placed underground whenever possible.

- Pedestrian signs are intended to be read while walking and may be placed in either the edge, curb, or furnishing zones.
- Bikeway wayfinding signs are intended for bike users and may include route options, direction of travel, time/distance to destinations, and bicycle safety information.
- Informational signs are intended to give more detail about the City surroundings. They may include parking information, location maps, area business directories, and other public information.

Considerations

- Overuse of wayfinding should be avoided as to not create a cluttered streetscape.
- Design of wayfinding signs can enhance a distinctive corridor or district identity by use of a standard design format, color scheme, and logo.

UTILITIES

Description

Utilities are a necessary and ubiquitous element of streetscape environments. Though essential, utilities often constrain the ability to locate other streetscape elements and can create a cluttered visual environment. Conversely, other streetscape elements may conflict with the ability to access and maintain utilities. Utilities in the streetscape consist of utility poles and overhead wires, surface-mounted utility boxes, utility mains, laterals, vaults, and valves. They include sewer, water, gas, and telecommunications, as well as traffic signals, street lights, and electrical poles and wires.

Well-organized utility design and placement can lead to:

- Minimization of streetscape clutter to achieve a cohesive streetscape design
- Maximization of space for plantings
- Improved efficiency of utilities and integrated alignment with stormwater facilities, street furnishings, and street lighting
- Reduced cutting and trenching
- Possible reduction of long-term street and sidewalk closures
- Reduced long-term maintenance conflicts and potential costs
- Improved pedestrian safety, quality of life, and right-of-way aesthetics

Considerations

Utilities should be placed to minimize disruption to pedestrian through travel and potential planting and site furnishing locations, while maintaining necessary access for maintenance and emergencies. The following guidelines apply to utilities in the public right-of-way:

- Utilities should be located underground wherever possible, as opposed to overhead or surface-mounted.
- Large utility vaults such as network or transformer vaults, and conduits running the length of a City block, should be located in the roadway or parking lane where access requirements allow.
- Small utility vaults, water meters, gas valves, and gas vaults should be located in the curb zone.

- Utilities should be consolidated to the extent feasible for efficiencies and to minimize disruption to the streetscape.
- Utility vaults and boxes should be located outside of the furnishing zone whenever possible to maximize the number and size of tree wells and the ability to connect tree wells into continuous strips.
- Major utilities (sewers, fire hydrants, gas and water meters and mains, manholes and utility vaults, and utility poles) should be installed at least five feet from the edge of existing or proposed tree basins.
- Minor utilities (laterals, vaults, valves, etc) should be installed at least three feet from the edge of new or existing tree basins.
- Utility laterals should run adjacent to, not directly under, potential site furnishing and tree planting locations wherever possible (such as through driveways or between tree basins).
- Subsurface utility conduits and irrigation lines should avoid running under the length of the planting area to minimize root interference.
- If several shallow utility laterals are unavoidable, planting areas may still be created and should utilize ground cover or low shrub plantings without the incorporation of deep-rooted trees.
- Utility vaults and valves should be minimized in curb extensions where plantings or site furnishings are desired, or where they may obstruct sight lines to pedestrians entering a street crossing.
- If existing vaults conflict with curb ramp areas, vaults should be moved or modified to meet accessibility requirements as feasible as part of utility upgrades.
- Other design, operations, and maintenance considerations include the following:
 - Utility vault covers should be made of slip resistant materials
 - Any utility-related roadway or sidewalk work should replace paving material in-kind (e.g., brick for brick) where removed during emergency or construction
 - Utility construction should use “trenchless” technologies, such as sealants, pulling cables through tunnels, etc., wherever possible, to avoid excavation and disruption of streetscape elements



Sometimes existing overhead power lines can remain to serve adjacent property owners and be successfully incorporated into the complete streets design.



Burying or relocating overhead utilities creates extra room for pedestrian amenities.



Portions of Chapter 5 were derived from the Boston Complete Streets Guidelines, prepared by the City of Boston Transportation Department, with permission.

5. STREET ZONE DESIGN ELEMENTS



Complete Streets provide the opportunity to avoid traffic congestion and increase the overall capacity of the transportation network.

5. STREET ZONE DESIGN ELEMENTS



Street Zone

The organization and distribution of right-of-way has a profound effect on safety, roadway capacity, and how comfortable and convenient transportation modes are relative to each other. In the past, the Thoroughfare Plan was the primary driver of roadway design in Dallas. The focus was on moving motor vehicles safely and efficiently. A Complete Streets approach takes a more comprehensive view of the street and all users. In a Complete Streets zone, the space typically between curbs supports adjacent land uses and balances the efficiency of motor vehicle travel with considerations for pedestrians, bicyclists, and transit users.

The example cross sections in Chapter 2 identify primary and secondary priorities within the cross section for each street type. Since there is significant variation in how each street cross section can be configured, these priorities are intended to help direct decisions with respect to roadway design, particularly in constrained rights-of-way where ideal widths cannot be met. This chapter provides further design guidance on specific elements within the traveled way.



Street Zone



Street Zone



A speed table will encourage drivers to slow down and be more alert to pedestrians and bicyclists.



Mini-roundabouts provide traffic calming benefits in residential areas.

TRAFFIC CALMING ELEMENTS

Safe Speeds

The streets in Dallas will be designed to limit the excessive speeds of vehicles. Managing vehicular speed is particularly important on streets where pedestrian and bicycle use is desired. In crashes involving these more vulnerable users, vehicular speed at the point of impact is directly related to pedestrian or bicyclist survival. For example, a pedestrian who is hit by a motor vehicle traveling at 20 mph has a 95% chance of survival, whereas a pedestrian hit by a motor vehicle traveling at 40 mph has a 15% chance of survival. Studies have also shown that motor vehicle crashes decline where roadway speed is reduced. In addition, drivers are far more likely to yield to pedestrians at crosswalks when speeds are lower.

Mixed use and residential streets in Dallas should be designed for a target design speed of 25 mph. The context of an individual street should factor into whether or not adjustments to this base design speed are appropriate. Target design speed will be lower at intersections and crossings. Transportation Planning and Street Operations should take the lead on determining target design speeds during the corridor planning stage of the Complete Streets design process in the context of the community vision.

For major roadway construction and reconstruction projects, the geometric design of the roadway should be such that excessive speeds feel uncomfortable. This can be accomplished through a creative approach to roadway design. Curves (chicanes) should be incorporated, long vistas should be broken with vertical elements such as street trees, and traffic calming features should be introduced.

The following speed-reduction strategies will be considered for traveled way design on Dallas roadways and are discussed in more detail later in this chapter:

- Lane widths
- Road diets
- Center medians/islands
- Midblock curb extensions (neckdowns)
- Bikeways
- Transit lanes
- On-street parking
- Paving treatments
- Shared streets
- Chicanes
- Speed tables
- Street lighting

Road Diets

Description

There are many streets in Dallas that are wider than necessary given the volume of traffic they carry during peak hours. Road diets are therefore a solution that can be useful on a wide variety of roads throughout Dallas. A road diet reduces the number of travel lanes on a roadway, typically removing one lane of traffic in each direction. The reduction of travel lanes provides additional space for expanded sidewalks, bike lanes, or plantings.

Road diets not only provide additional space necessary to build a Complete Street, but they also provide measurable safety benefits to all users. Research has shown that road diets reduce the total crashes from 81% to 53%. Road diets are officially recognized by the Federal Highway Administration (FHWA) as a proven safety countermeasure. In a January 2012 memorandum, FHWA division offices were advised to advance the use of road diets with their State DOT counterparts.

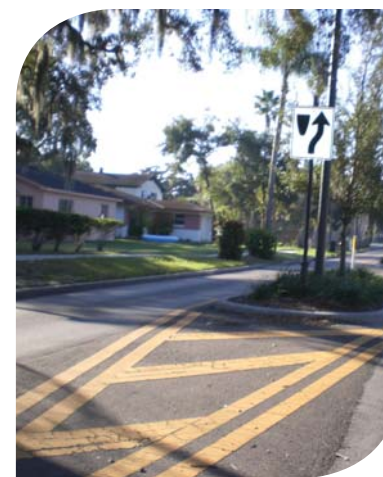
Application

Road diets are an important measure in the implementation of Complete Streets principles in Dallas. The following issues should be considered when reducing travel lanes on streets:

- Four-lane roads with average daily traffic volumes up to 20,000, and six-lane roads with up to 30,000 vehicles per day are candidates for road diet treatments. A capacity analysis may be necessary to ensure the reduction of travel lanes does not create significant delays for motor vehicles.
- On four-lane undivided roadways, road diets typically remove two travel lanes and convert the road to a two-lane road with a center-turn lane and bike lanes. In Dallas, however, many of the roads that are eligible for road diets already have left turn lanes, thus the additional space can be used for buffered bike lanes, transit lanes, and expanded streetscape improvements.
- Some road diets will be implemented as a part of the roadway repaving/reconstruction process, as this offers an opportunity to reconfigure the roadway with new pavement markings.

Considerations

- Particular to the individual project, a thoroughfare plan amendment might be necessary.
- Road diets require special attention to public involvement of surrounding communities. Gaining public support is a key aspect in the success of a road diet.
- A low-cost road diet reconfigures existing roadway space and does not involve curb reconstruction. While sidewalk width remains the same, these types of road diets still benefit pedestrians due to the increased buffer between the sidewalk and the nearest motor vehicle travel lane.
- Where road diets are implemented through the repaving/reconstruction process, consideration should be given to the long-term maintenance needs of the resulting bike lanes. They will need periodic maintenance to remove debris and ensure they are usable facilities.
- Road diet projects require careful attention to motor vehicle capacity issues at intersections.



Road diets and medians are used to reduce excess travel lane capacity and improve conditions for other travel modes.



Clearly marked crosswalks and bulbouts increase pedestrian safety.



Road diets encourage drivers to slow down



Road diet with median

One-way to Two-way Street Conversions

Description

Converting a one-way street to a two-way street is an effective strategy for managing traffic patterns, improving vehicle circulation, reducing motor vehicle speeds, improving access and visibility to businesses, and changing the character of a neighborhood from being a pass-through to a destination for motorists. Conversely, conversion of a two-way street to a couplet may provide space for other street zone elements within the same right-of-way as the two-way option, such as a pedestrian plaza, bicycle facilities, or stormwater management features.

Application

One-way streets are often designed as part of a couplets system—a pair of one-way streets, typically separated by one city block—which often results in a higher vehicle capacity than an equivalent two-way street. In some cases, developing a couplet system could be considered as an alternative to widening a two-way thoroughfare. This may be a beneficial option when trying to preserve pedestrian space, trees, and other aesthetic features.

In terms of pedestrian safety, there are benefits of both one-way and two-way streets, so the decision to convert a one-way street to two-way (or vice versa) is context sensitive.

Studies have shown that converting two-way to one-way streets generally results in fewer crashes involving pedestrians because there are fewer turning movements. However, one-way streets tend to encourage higher motor vehicle speeds, and may increase vehicular traffic if motorists are required to circle around to access destinations in a dense, urban environment. Two-way streets may reduce vehicle speeds due to increased turning movements and increased perceived friction along the roadway. In addition, many one-way streets have multiple lanes, which may create a multiple threat crash condition for pedestrians crossing the road. Converting a multi-lane, one-way street to a two-lane, two-way street can eliminate this safety issue if crossing islands are installed.

If a street is converted to one-way, it should be evaluated to see if additional changes should be made. Potential changes include lane diets, road diets, curb extensions, turning radius reductions, and signal timing that discourages higher vehicle speeds. Also, traffic circulation in the surrounding area must be carefully considered before converting streets to one-way travel.

Considerations

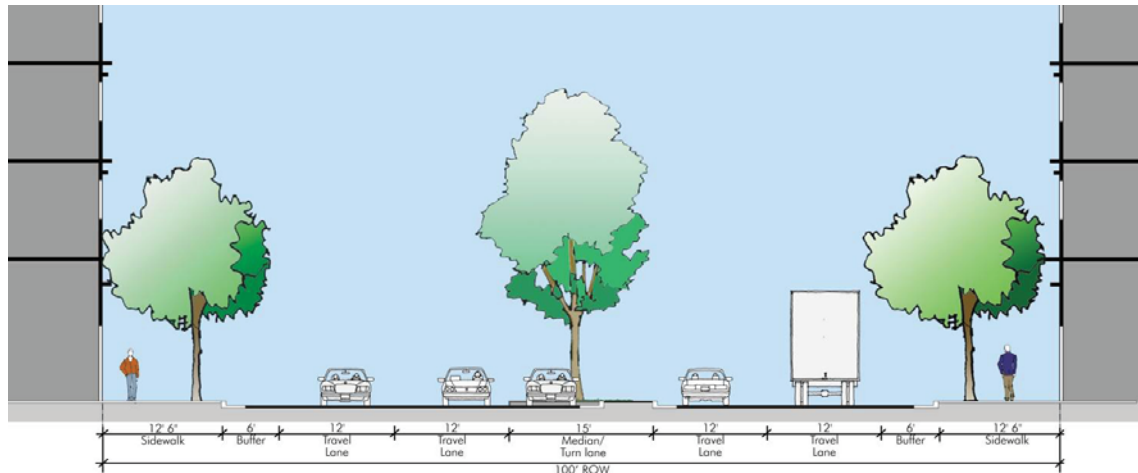
- Protected left-turn signal phasing and center turn lanes are not required on one-way couplets. Conversion of a two-way street to a couplet may provide space for other street zone elements within the same right-of-way as the two-way option.
- Narrowing the travelway of a one-way couplet street provides opportunities for shorter and safer pedestrian crossings.
- Streets with lower traffic volumes may not demand the use of one-way couplets.
- Many communities have found that local businesses benefit from one-way to two-way conversions because access is improved and motorists are more likely to stop and patronize businesses.

Slip Streets

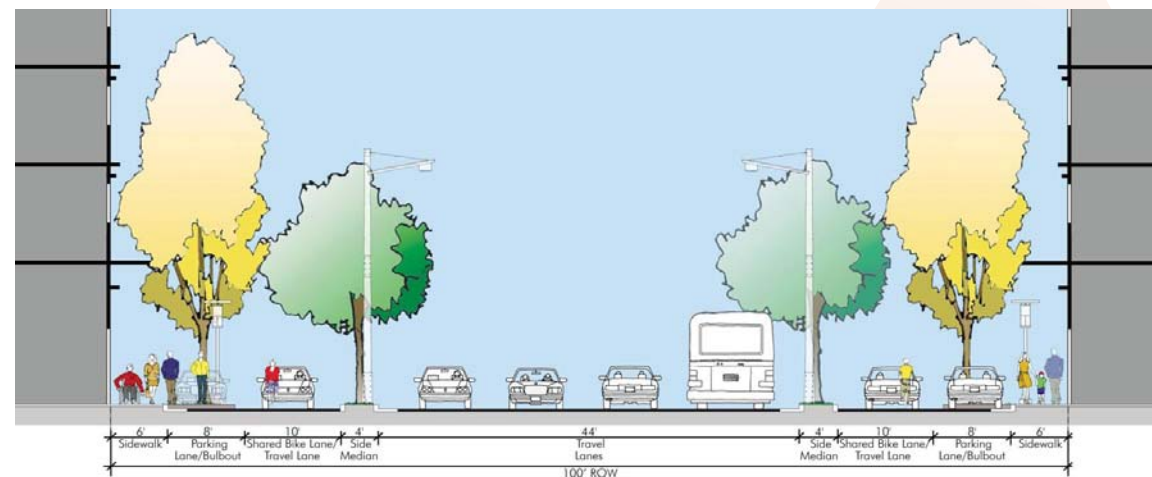
Description

Slip streets, also known as frontage roads or local roads, run parallel to higher speed limited access roads. Slip streets are located between an arterial and developed land and are often used where a major road passes through an urban area. They may provide access to private driveways, shops, or houses. Slip streets provide a more pedestrian-friendly street edge with slower moving local traffic, parked cars, and landscaping essentially acting as a buffer between land uses and the higher volume, higher-speed arterial street. A good example is Northwest Highway east of Preston Road.

PEARL STREET- TYPICAL SECTION - EXISTING CONDITIONS



PEARL STREET- TYPICAL SECTION- FUTURE





Slip streets, such as this one along Blackburn, slow traffic adjacent to the pedestrian zone.



Bike lane with parallel parking

Slip streets reduce conflict points between through traffic and turning traffic associated with direct property access to the arterial. These streets also reduce conflict points on the arterial, increasing roadway safety, and operations, and provide a more pedestrian friendly street edge with slower moving traffic on the outside lanes. This roadway configuration also improves compatibility between high capacity arterials and lower intensity contexts.

Application

Slip streets separate local traffic from through traffic, and are most effective on higher volume, higher speed arterials. Opportunities to construct slip streets are generally restricted to locations with substantially longer block lengths, little if any existing development, and a development plan. Retrofits may be possible where developed properties have large setbacks or redevelopment of existing land uses is occurring.

Considerations

- Space between the slip street and main roadway should be of sufficient width to accommodate plantings and a comfortable waiting area (refuge) for pedestrians waiting to cross the main roadway. This space may also provide opportunities for natural drainage elements
- Slip streets provide improved access to individual properties, which may in turn increase value and potential of adjacent properties
- Separation between arterials and slip streets should be carefully planned to reduce conflict areas of vehicles entering and exiting the roadways. Slip streets require a larger area of space dedicated to roadways, and increase the distance pedestrians have to cross roadways. See Chapter 6 for guidance on slip street intersections

Center Medians/Islands

Description

Medians are raised barriers in the center portion of the roadway. Median width can vary greatly, from a minimum of 6' to 20' or more along parkways and light rail transit lines. Medians with street trees or other landscaping can be used to add prominence to a segment of road, extend a park-like environment along a corridor, and to reduce the heat island effect. Medians can also provide a location for transit and a refuge for pedestrians crossing multi-lane roadways. Studies show that intermittent (midblock) islands can result in up to a 7% reduction in motor vehicle speeds.

Dallas has many streets with concrete medians, however, these were mainly constructed to channelize turning movements and to control access to adjacent land uses. Through a Complete Streets approach, medians on Dallas roadways should be pedestrian-friendly, reduce travel speeds, and should provide landscaping whenever possible.

Sunken medians can be used to incorporate water quality and reduce infrastructure costs.

Application

- Medians are particularly helpful as pedestrian refuges at controlled and uncontrolled crossings. When designed properly, medians offer protection to pedestrians crossing the road.
- The minimum width for a center median is six feet. This width is necessary to ensure the median serves as an adequate pedestrian refuge. A wider median is necessary if it will serve a dual purpose as a left turn lane, to accommodate both

the width of a turn lane as well as adequate space for the pedestrian refuge

- Signalized intersections with medians should be designed to allow pedestrians to cross the entire roadway during a single signal cycle.
- Pedestrian cut through medians should be of at least equal width to the approaching sidewalks. At midblock locations, consider angling the pedestrian cut to direct pedestrian sight lines to on-coming traffic.
- Care should be taken to ensure median plantings do not limit the sight lines for pedestrians and motorists at intersections.

Considerations

- Center medians should be carefully designed to ensure proper drainage and maximize potential for on-site stormwater retention and filtration. Drought-resistant and low-maintenance plant species should be used.
- Trees and landscaping should be maintained for sight lines and vehicle operation per the Department of Street Services MOWmentum Program Landscaping Guidelines.
- Sidewalks should not be reduced in width, and bike lanes should not be eliminated, to provide space or additional width for medians.

Midblock Curb Extensions (Neckdowns)

Description

At midblock locations with on-street parking, curb extensions, also called neckdowns, can be installed on both sides of the road to create a visual pinch-point, helping to calm motor vehicle traffic. They are particularly useful on streets with longer block lengths where motorists tend to gain speed at midblock locations. They can be combined with midblock pedestrian crossings to further enhance pedestrian safety by lowering motor vehicle speeds, reducing crossing distances, and increasing visibility.

Application

- Midblock curb extensions should only be used on streets with on-street parking.
- They can be used on two-way streets with one lane in each direction, and one-way roads. Where used on streets with multiple lanes in one direction, other crossing enhancements such as crossing islands, which allow pedestrians to cross the street in two stages, and rapid flashing beacons should be considered.
- Midblock curb extensions are sometimes combined with intermittent medians to reduce speeds along the length of a roadway and provide a crossing refuge in the center of the street, allowing pedestrians to cross the street in two stages.



Midblock curb extension



Midblock curb extension



Cycle track



Shared-use path

- Where curb extensions provide pedestrian crossings, ADA compliant curb ramps, tactile warning strips, and cross slopes must be provided.
- Street trees are encouraged within midblock curb extensions. However sight distance is a primary issue at midblock pedestrian crossings. Therefore, shrubs and other types of vegetation that would block drivers' view of approaching pedestrians should be avoided.
- Stormwater quality elements such as rain gardens can be incorporated in curb extensions.

Considerations

- Midblock curb extensions can be combined with speed tables to provide raised crossings for pedestrians. For more information, see Chapter 6: Intersections Design Guidelines, Raised Crossings, and Intersections
- Bicycle lanes should not be eliminated at midblock curb extensions. In constrained spaces, care should be taken to avoid suddenly squeezing bicyclists into the traffic flow on streets with higher volumes of traffic, particularly in locations with steep uphill grades where bicyclists may be travelling considerably slower than motor vehicle traffic
- On low-volume residential streets, midblock curb extensions can reduce the street to one lane, requiring on-coming drivers to alternate passage through the midblock curb extensions, while keeping enough space for fire trucks and other large vehicles

Access Management

Description

Access management reduces the potential for conflicts between vehicles, bicyclists, and pedestrians. Managing access also improves the traffic flow of a street and provides more capacity. Improved traffic flow may also reduce the need for road widening, allowing part of the right-of-way to be recaptured for other users. A major challenge in street design is balancing the number of access points to a street, as most conflicts between users occur at intersections and driveways. The presence of multiple driveways, in addition to the necessary intersections, has the potential to create several conflicts between vehicles entering or exiting, bicyclists, and pedestrians. When possible, new driveways should be minimized, old driveways should be eliminated or consolidated, and raised medians should be placed to limit left turns in and out of driveways.

Application

Access management through limiting driveways, providing for shared access, and providing raised medians should be implemented when conceptually planning a

corridor. Property owners should be involved from the outset to ensure that solutions do not have adverse impacts on access to their properties, and to facilitate a consensus solution that works for all owners.

Considerations

- Access management reduces the number of conflict points, especially by replacing center-turn lanes with raised medians, as left turns by motorists account for a higher number of crashes with bicyclists and pedestrians.
- Pedestrian crossing opportunities are enhanced with a raised median.
- Universal access for pedestrians is easier, as the sidewalk is less frequently interrupted by driveway slopes.

The following possible negative effects of management should also be considered:

- Streamlining a street may increase motor vehicle speeds and volumes, which can be detrimental to other users.
- Reduced access to businesses may require out-of-direction travel for all users, including walkers and bicyclists.
- Concrete barriers and overly-landscaped medians act as barriers to pedestrian crossings. Medians should be designed with no more than normal curb height, and with landscaping that allows pedestrians to see to the other side.

Careful, site-specific access management solutions developed with property owners will minimize potential negative impacts.

BIKEWAYS

Bicyclists should be considered and anticipated on all streets in Dallas where they are legally allowed to operate. The bicycle is an ideal vehicle for trips that are too far to comfortably walk, but are still fairly short. Bicycling is an excellent option for trips that are less than three miles in length, which are half of all trips made on a daily basis. The 2011 Dallas Bicycle Plan sets a vision for bicycle facilities in Dallas in the future, and identifies specific locations where it may be feasible to retrofit roadways to accommodate bicycles.

Like pedestrians, bicycles are vulnerable road users who can be seriously injured in a simple collision. For many people, bicycling in close proximity to faster moving traffic can be an uncomfortable experience. Lack of bicycle accommodations on the street can increase the number of bicyclists riding on the sidewalk, which conflicts with pedestrian traffic. Well-designed bikeways reduce these conflicts and create a more predictable traffic environment for everyone.

Bikeways can be divided into two general categories: exclusive facilities, where roadway space is designated for bicycle use, and shared facilities, where bicycles and other vehicles share roadway space.

In general, shared facilities are more appropriate in low speed (25 mph or less) environments where motorists are going slow enough to be able to see and react to the presence of bicyclists. As vehicular speeds increase, so does the need for greater separation between the bicyclists and motor vehicles.

General Design Considerations for Bicyclists

- In order to provide adequate space for bicycle facilities, road diets and lane diets should be considered. More guidance on optimizing street capacity is provided earlier in this chapter, including minimum lane widths for Dallas streets.



Buffered bike lane



Bike lane with parallel parking



Bike lane with right-turn lane



Left side bike lane

- Bicyclists provide their own energy and as such are sensitive to distance and frequent stops. They typically seek the most direct, continuous route that does not require a lot of stops and starts. Bikeway design should always keep this in mind.
- Bicyclists are more sensitive to broken or uneven pavement which can cause them to lose balance or swerve suddenly. This includes potholes, uneven or sunken drainage structures, and utility access covers. Where possible, the installation of bicycle facilities should be coupled with an evaluation of pavement conditions and improvements as necessary to ensure a smooth riding surface.
- Drainage inlets should be safe for bicycle wheels
- Angled parking, if provided adjacent to an on-street bikeway, should be back-in to increase visibility between bicyclists and drivers exiting spaces.
- Refer to the AASHTO Guide for the Development of Bicycle Facilities for additional guidance.

More detailed information on several common bikeway types is provided on the pages that follow. Guidance on bicycle facility design at intersections is provided in Chapter 6.

Bike Lanes

Description

Bike lanes provide an exclusive space for bicyclists through the use of lines and symbols on the roadway surface. Bike lanes are for one-way travel and are normally provided on both sides of two-way streets, or on one side of one-way streets. Bicyclists are not required to remain in a bike lane when traveling on a street, and may leave the bike lane as necessary to make turns, pass other bicyclists, or to properly position themselves for other necessary movements. Bike lanes may only be used temporarily by vehicles accessing parking spaces and entering and exiting driveways and alleys.

Application

- Bike lanes are normally placed on the right-hand side of the road to reflect the general traffic principle of slower traffic keeping to the right.
- The minimum width of a bike lane next to a parked car or a vertical curb is five feet. Bike lanes on open shoulders (without curbs) may be a minimum of four feet wide (refer to the lane width chart for desired widths).
- Bike lanes are typically installed by reallocating existing street space—narrowing other travel lanes, removing travel lanes, and/or reconfiguring parking lanes.
- Bike lanes require on-going maintenance to ensure debris does not collect in the lane
- Refer to the MUTCD and the AASHTO Guide for the Development of Bicycle Facilities for more information on bike lane design

Considerations

- On one-way streets and streets with wide medians, a left-side bike lane can be advantageous, particularly in locations with heavy bus traffic or frequent right-turns.
- Where additional space is available, consider providing a buffered bike lane (three-foot minimum buffer recommended). The buffer can either be placed between the bike lane and the travel lane (in locations with

higher speeds and volumes), or between the bike lane and the parking lane (in locations with a high rate of parking turnover).

- Where there is insufficient space to provide a buffered bike lane on a street with designated on-street parking, offsetting the bicycle symbol to encourage bicyclists to ride in the left side of the bike lane, away from the door zone of parked vehicles, should be taken into consideration.
- Contra-flow bike lanes may be used on one-way streets to provide more convenient connections for bicyclists where other alternative routes are less desirable or inconvenient.
- Wider bike lanes enable bicyclists to pass one another on heavily traveled corridors and increase separation from faster traffic.

Cycle Tracks

Description

Cycle tracks are a portion of the right-of-way contiguous with the traveled way for the exclusive use of bicyclists. Cycle tracks provide added separation that enhances the experience of bicycling adjacent to streets. Separation is achieved through a variety of methods. Some cycle tracks are placed at a higher elevation than the adjacent street (i.e. curb height, or at an intermediate height between the curb and the street level). Other cycle tracks are placed at street level, but are physically separated from the adjacent travel lane by a raised median, a row of parked cars, flexible bollards, or some combination of these.

Application

- Refer to the Lane Width Chart on page 83 for minimum cycle track widths.
- Cycle tracks may be placed between the parking zone and the pedestrian zone. Other configurations are acceptable as well, such as a cycle track that is separated from the motor vehicle lane by a concrete curb or other buffer.
- When adjacent to on-street parking, a minimum 3' buffer should be provided between parking and the cycle track. The buffer serves as a pedestrian loading and unloading zone.
- Cycle tracks can either be one-directional (one-way on each side of a street), or two-directional (two-way on one side of a street).
- Intersection design for cycle tracks is very complex and requires careful attention to conflicts with turning vehicles. For example, turning movements across cycle tracks should be carefully assessed to reduce or eliminate conflicts. If intersection conflicts cannot be adequately addressed, it is likely that a cycle track will not be a feasible solution due to safety concerns.
- Cycle tracks can be useful on streets that provide connections to off-street trails, since bicyclists on these streets may be more accustomed to riding in an area separated from traffic.
- Consult the NACTO Urban Bikeway Design Guide and the Dallas Bike Plan for more information on cycle track design.



Cycle track



Cycle track



Shared lane marking



Shared lane marking

Considerations

- Cycle tracks require increased parking restrictions as compared to bike lanes to provide for visibility at intersection transitions.
- Frequency of driveway crossings is a factor in determining if a cycle track is feasible. Frequent driveway crossings are incompatible with cycle track design
- Colored pavement can be beneficial to highlight the presence of a cycle track, particularly at intersections and other locations where motor vehicle traffic crosses the cycle track.
- When a cycle track is provided on the same side of the road as transit operations, transit stops and waiting areas should be provided between the cycle track and the roadway to reduce conflicts with pedestrians loading and unloading.
- The presence of drainage and utility structures along the curb may reduce the effective width of the cycle track.

Shared Lane Markings

Description

Shared lane markings, or sharrows, are pavement markings that are placed within the vehicular travel lane of the roadway. Unlike bike lanes, they do not designate a particular part of the roadway for the exclusive use of bicyclists. The symbols alert motorists to locations where bicyclists should be expected to ride, and encourage safer passing behaviors.

Application

- Shared lane markings are typically used on streets where space constraints make it impractical to provide bike lanes. They should not be used on streets with speed limits higher than 30 mph.
- On streets with narrow lanes, the shared lane marking is typically placed in the center of the lane to indicate that motorists must change lanes to pass bicyclists.
- On narrow travel lanes adjacent to on-street parking, shared lane markings should be placed in a location that is outside of the door zone of parked vehicles
- Refer to the MUTCD and the AASHTO Guide for the Development of Bicycle Facilities for more information on the application of shared lane markings.

Considerations

- Marked, shared lanes should be provided after considering narrowing or removing travel lanes, parking lanes, and medians as necessary to provide a bike lane or cycle track.
- Shared lane markings are less effective on streets with an on-street parking lane that is frequently unoccupied, because bicyclists often feel more comfortable riding in the parking lane.
- Shared lane markings can be used in lieu of a bike lane where bicyclists may be operating at higher than normal speeds due to downhill grades, particularly if bicyclists are operating at or near the speed of adjacent motor vehicle traffic.
- Shared lane markings can be used in constrained corridors as a temporary solution to complete connections between bike lanes and other facilities.

BICYCLE BOULEVARDS

Description

Bicycle boulevards, also called neighborhood greenways, are streets with low motor vehicle speeds that are designed to allow bicyclists to travel comfortably in a low-stress environment. Bicycle boulevards often give priority to bicycle use, and discourage through-traffic by motor vehicles. Ideally, they are designed to minimize the number of stops that a bicyclist must make along the route. Separated bicycle facilities (i.e., bike lanes) are not necessary on bicycle boulevards because motor vehicle speeds and traffic volumes are low. Bicycle boulevards are typically designated by special wayfinding signs and pavement symbols.

Application

- Bicycle boulevards are usually more feasible in neighborhoods with a gridded street network (one street is chosen as the bicycle boulevard), but can also be accomplished by combining a series of road and trail segments to form one continuous route.
- At major street crossings, bicycle boulevards may need additional crossing measures for bicyclists, such as quick-response traffic signals with bicycle-sensitive loop detectors, crossing beacons, median refuge islands, and/or curb extensions.
- Traffic calming measures can be used to maintain low speeds (20 mph or less) on bicycle boulevards.

Considerations

- Ideally, bicycle boulevards should not carry more than 1,000 motor vehicles per day to be compatible with bicycling. Diverters and other traffic management devices are typically used to discourage motor vehicle through-traffic, while still enabling local traffic access to the street.
- Bicycle boulevards should be long enough to provide connectivity between neighborhoods and common destinations.

PAVED SHOULDERS

Description

Paved shoulders provide space on the outside of travel lanes for bicycle and pedestrian use. Paved shoulders should be a minimum of 4' without the curb and 5' minimum with a curb. Travel lanes can be narrowed to achieve adequate space to provide a paved shoulder within the lane width. On some roadways without curbs, paved shoulders can provide important bicycle connections. Paved shoulders also improve safety for motor vehicles and prevent pavement damage at the edge of the travel lanes.

Application

Continuous paved shoulders are typically provided in rural areas where bike lane markings are not necessary. They can be supplemented with bike route signage (see section below) where necessary to provide wayfinding for bicyclists.



Bicycle boulevard



Bicycle boulevard



Back-in angled parking



Parallel parking

Considerations

- A bicycle level of service analysis (per the Highway Capacity Manual) is useful to determine the amount of width needed to achieve the desired level of comfort for a bicyclist.
- Rumble strips located on paved shoulders create hazardous conditions for bicyclists and should be avoided. If they are needed due to a history of run-off-the-road crashes, they should be designed according to the AASHTO Guide for the Development of Bicycle Facilities
- Paved shoulders should be continuous at turn lanes and shoulder bypass lanes.

BICYCLE ROUTE SIGNAGE

Description

Bicycle route signs are wayfinding signs that guide bicyclists along preferred, designated routes to destinations within the City of Dallas and throughout the region. The intent is to create a single, integrated signing system that is instantly recognizable by bicyclists.

Application

It is expected that as the Dallas bike network is built out over time that many bicycle facilities will have bicycle route signage. Some routes within the planned bike network may only have signs and no other treatments.

ON-STREET PARKING

Description

On-street parking is clearly a key to the success of small business districts, and can add energy and excitement to the street. This encourages the concept of park once. On-street parking has a very positive impact on the pedestrian realm—research shows that pedestrians feel far more comfortable and safe on streets with occupied on-street parking. Parked cars provide a traffic calming effect by visually narrowing the roadway and increasing friction along the edge of the roadway. Permeable pavement can be considered in these areas as a means to improve stormwater quality.

It is important to get the ingredients right to achieve the maximum benefit from on-street parking. When on-street parking is underutilized, the result is a wider street with faster speeds.

The following are typical requirements of the City of Dallas:

- Parking is not allowed within 50 feet from an intersection to allow for visibility between drivers and pedestrians in crosswalks
- Head in spaces must be a minimum of 18 feet long and 9 feet wide
- Parallel spaces must be a minimum of 22 feet long and 9 feet wide
- Indented parking spaces must be a minimum of 18 feet long and 9 feet wide
- Compact spaces may be a minimum of 16 feet long and 7.5 feet wide
- Reverse in spaces must be a minimum of 18 feet long and 9 feet wide

Application

- On-street parking is most appropriate for mixed-use and residential streets. In these types of streets it can provide a traffic-calming effect and convenience to local shops and residences
- On-street parking is ideally created by these parking types: parallel, angle, reverse angle, and unmarked parallel spaces.
- Parking lanes should be a minimum of 7' wide, with 8' being the desired width. The potential hazard of opening car doors should be considered when developing an appropriate design. Crashes can occur in locations with high parking turnover; such as main streets and commercial streets with restaurants and businesses. Adjacent to a narrow parking lane (7 feet) with high turnover, a 6-foot bicycle lane is recommended.

Considerations

- In mixed-use, a parking lane can be designated for different purposes throughout the day, such as commercial loading during the morning, public parking during the day, and valet at night.
- On-street parking should be prohibited approaching intersections or driveways since it can obscure sight lines for all users of the road. See Chapter 6 for more detail.
- Angled parking maximizes the parking supply and is appropriate when sufficient curb-to-curb widths are available. Where angled parking is used, the preferred orientation is back-in angled parking. This configuration has been shown to provide numerous safety benefits for pedestrians, bicyclists, and motorists. It provides more visibility when pulling back into traffic, and more visibility between bicyclists and motorists. Back-in angled parking requires the use of wheel stops to ensure parked vehicles do not encroach upon the sidewalk. Consideration should be given to outdoor cafés and seating areas adjacent to back-in parking.
- Parallel parking is appropriate on streets with narrower curb-to-curb widths, and when trying to accommodate other elements such as bicycle lanes and wider sidewalks. Higher volume arterial streets should primarily use the parallel configuration.

TRANSIT LANES

Description

A transit-way is an exclusive space separated from motor vehicle travel lanes. Transit-ways can be located in a median or on the outside lane of a multi-lane road. In Dallas, transit-ways may accommodate buses, street cars, or light rail.

Providing dedicated space for transit within the public right-of-way can improve transit service, reduce traffic volumes, and reduce conflicts between modes of travel. Transit lanes should be considered for streets shown on the Transit Overlay Map on page 68 of Chapter 2. There are two types of transit lanes:

- **Priority transit lanes** are not separated from travel lanes, however they may include treatments such as signal prioritization, queue jumps, bulbouts, signage, turning restrictions for other vehicles, and off-vehicle fare collection. Priority transit lanes can increase the frequency and reliability of bus and streetcar service along a corridor and reduce congestion in other travel lanes. In Dallas, priority transit lanes can accommodate bus and streetcar service.



Exclusive transit-way



Priority transit lanes



Shared street



Shared street

- **Shared transit lanes** are travel lanes in which transit is given priority using some or all of the treatments mentioned above, however, transit vehicles may share the travel lane with other vehicles such as bicycles and right-turning motor vehicles.

For transit route locations and recommendations, refer to the DART System Plan as amended.

Application

- Refer to the Lane Width Chart on page 83 for the minimum widths of transit lanes for different street types in Dallas.
- Median transit-ways are in the center of multi-lane streets with station stops located on traffic islands. Pedestrian and bicyclist safety, connectivity, and comfort are critical to the success of median transit-ways. Intersections should be carefully designed and well-lit to encourage pedestrian and bicycle access to the stop. Distances and out-of-direction travel for pedestrians should be kept to a minimum.
- With regard to bus lanes, curb side bus lanes are less expensive and easier to install than median bus transit-ways, and are easier for pedestrians to access. However, curb side bus lanes can be compromised by turning vehicles, double parked vehicles, and vehicles accessing parking lanes. Design and enforcement are critical to deter vehicles from blocking the lane.

Considerations

- Shared bus/bike lanes (SBBLs) can be considered where there are street right-of-way constraints, and in situations where there is a need to accommodate both bicyclists and buses on a particular street. Sharing a lane between buses and bicyclists is generally not conducive if bus headways are heavy and there are frequent bus stops.
- A SBBL may vary in width, but generally should not be narrower than 12'. When SBBLs are developed, it is desirable for the lanes to be wide enough (15' min.) for buses and bicyclists to pass each other comfortably in the lane.
- Pavement markings within a SBBL may include a bicycle symbol or "BUS/BIKE ONLY" symbol. A shared lane marking may be used, but only on roadways with a posted speed limit of 35 mph or less. More guidance on shared bus/bike lanes can be found in the AASHTO Guide for the Development of Bicycle Facilities.
- Shared use with bicycles should not be considered in exclusive transit ways where buses travel at high speeds and curbs limit access to the travel way
- Colored or different pavement materials can be used to differentiate transit lanes from other parts of the roadway

SHARED STREETS

Description

Streets where the curb and gutter are eliminated create roadways with no designation between the traveled way and the side of the road. These are often called shared streets since all roadway users share the same space.

Shared streets work well when the total right-of-way is relatively narrow, and motor vehicle speeds and volumes are low. They create zones of extreme traffic calming, ensuring that the difference in speed between different modes of travel is minimal. Shared streets maintain vehicular access for loading and emergency vehicles, but otherwise

function as extensions of the sidewalk to accommodate free-flowing pedestrian movements, bicyclists, street vendors and cafes, and occasionally on-street parking.

Shared streets can be paved with special materials to help indicate the special type of low-traffic zone. Chicanes, described later in this chapter, can be used to ensure speeds are low on shared streets.

Application

Shared streets are appropriate for some mixed-use streets in Dallas. Shared streets, or secondary networks—such as rail beds in the Design District and commercial alleys in high density areas—can be permanent installations, or can be shared for events or on certain days of the week. Examples include streets in State Thomas and the MEWS in Addison Circle.

Planters, bollards, or other vertical markers can be used to designate zones within a shared street. Paint and roadway materials can also delineate zones of the street. Since the goal of a shared street is to mix roadway uses and users as much as possible, treatments to delineate space should be limited. In addition, care should be taken when using bollards to delineate space, since they can become tripping hazards for pedestrians during crowded events.

Shared streets are also appropriate for some residential streets, as well as in subdivisions, campuses, and parks where there is a desire to limit motor vehicle traffic while placing a high priority on non-motorized traffic. A system of linked shared streets or alternating shared streets with standard streets can create a pedestrian-oriented district that maintains access for local traffic.

Considerations

- Parking is sometimes allowed on shared streets. Paint or special paving can be used to demarcate a parking zone or individual spaces.
- Because there are no curbs, shared streets require special drainage treatments and grading to prevent ponding of water.

CHICANES

Description

Chicanes are curb extensions that alternate from one side of the street to the other, creating an “S” curve that drivers must weave through. Chicanes provide opportunities to increase sidewalk space and introduce green street elements in the right-of-way. Chicanes can be created with curb extensions, tree pits or planters, or by alternating parking from one side of the roadway to the other.

In addition to slowing vehicular traffic, chicanes can provide opportunities to increase sidewalk space and introduce landscaping in the right-of-way. They can be used in combination with other traffic calming devices such as speed tables, discussed below, and with midblock neckdowns or center islands (discussed earlier in this chapter).



Chicane



Chicane



Speed table



Bike lane

Application

- Chicanes are appropriate for mixed use and residential streets.
- On residential streets, chicanes can serve as an alternative to speed tables, and provide additional areas for landscaping or neighborhood amenities.
- Chicanes and neckdowns can be used on two-way streets with one lane in each direction, and one-way roads with no more than two lanes.
- The amount of horizontal deflection in a chicane should be based on the target design speed of the roadway.

Considerations

- Vegetation used in chicanes should generally be low-growing (less than two-feet tall) and low-maintenance. In locations with midblock pedestrian crossings, sight lines should be maintained.
- Bikeways should be continuous through chicanes so that bicycles are not squeezed into the traffic flow, particularly in locations with steep uphill grades where bicyclists may be travelling considerably slower than motor vehicle traffic. Shared bikeways are appropriate on streets with chicanes that result in low-speed environments.
- Chicanes can serve in conjunction with stormwater quality principles as bioswales and rain gardens.

SPEED TABLES

Description

Speed tables are raised pavement areas that are placed at midblock locations to reduce vehicle speeds. They are gentler than speed bumps (which are not recommended for public streets) but have been shown to effectively reduce 85th percentile speeds by 13 to 15 mph. Well-designed speed tables enable vehicles to proceed comfortably over the device at the intended speed, but cause discomfort when traversed at inappropriately high speeds.

Speed tables are a good tool for retrofitting streets with traffic calming devices. If full reconstruction is planned, consider achieving traffic calming with horizontal devices such as roadway width and chicanes, which are more subtle and require less signage. Speed tables should be used in combination with other traffic calming devices such as curb extensions, chicanes, and crossing islands.

Application

- Residential streets and mixed-use are appropriate locations for speed tables.
- Speed tables are usually 3" higher than the roadway surface. They are typically 10' to 14' in length and extend the full width of the roadway, although sometimes they are tapered at the edges to accommodate drainage patterns.
- Speed tables should be designed with a smooth leading edge and a parabolic profile, which provides a smoother transition for bicyclists.
- Speed tables should be clearly marked with reflective pavement markings (per the MUTCD) and signage so that motorists and bicyclists are aware of their presence and can adjust their speed accordingly.
- Speed tables are generally not appropriate for streets with bus routes. They can, however, be installed on streets with school bus service.

Considerations

- Longer speed tables (up to 22' in length) have a design speed of 25 to 30 mph and are easier for large vehicles to negotiate.
- Avoid placing speed tables at the bottom of steep inclines where bicyclists travel at higher speeds and may be surprised by their presence.
- When used alone without complimentary traffic calming devices, speed tables may result in speed spiking where motorists may travel at higher speeds between tables.
- Speed tables should be located in consultation with the Dallas emergency service departments
- Raised crosswalks are discussed in Chapter 6.

STREET LIGHTING

Description

Street lighting is an important consideration in the design of the traveled way. A well-lit street contributes to the safety and comfort of vulnerable users, but is also a factor in economic development. Basic standards for street lighting are established in the Street and Pedestrian Lighting Criteria for the City of Dallas.

It is important to provide increased illumination where modes merge or cross paths, such as at intersections, bus stops, and midblock crossings.

The City of Dallas primarily maintains high-pressure sodium and metal halide lamps. The light produced by metal halide lamps is closer in quality to daylight, which improves the visibility of pedestrians to motorists, and is often perceived as providing greater personal security. Dallas is currently testing LED lighting, which has lower energy demands, longer lamp life, and light color similar to metal halide lamps.

The following are typical requirements of the City of Dallas:

- Street lighting styles will be selected from several available designs, including historic and modern styles
- Street lights are to be installed every 200 ft. along thoroughfares, 300 ft. along residential streets
- Lighting levels may not exceed 0.5 footcandles for residential and 1.0 footcandles for commercial locations

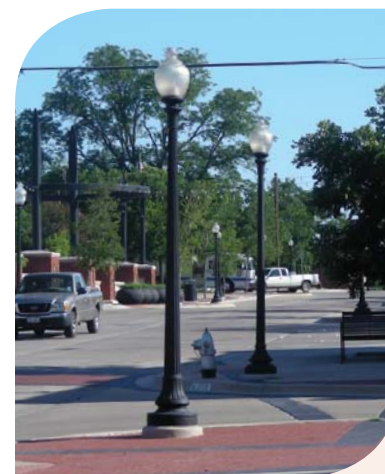
Application

The street type, hours of activity, and adjacent uses are all important factors in setting street lighting levels.

- Mixed-use streets require the highest level of illumination. These streets are designed to encourage all modes of travel, especially those along the edges of the traveled way. Street activity is encouraged to extend into the evening
- Parkways may require lower overall levels of illumination. However, trail crossings and intersections should meet the recommended safety standards for light levels. It may also be appropriate to operate street lights on parkways for longer periods than on other street types, since side-paths have increased recreational activity at dawn and dusk.



Street lighting



Street lighting



Street lighting



Street lighting

- Residential streets should have lower levels of illumination except in the vicinity of transit stops, schools, other public buildings, and parks. Meeting illumination minimums is essential in order to encourage pedestrian travel, particularly for trips to and from school and transit, which may occur around dawn and dusk. Dimming the lights during the middle of the night, when there is very low activity on residential streets, can cut down on light pollution and energy costs. Street lighting should illuminate the public right-of-way, but be shielded from private property.

There are three basic poles for street lighting in Dallas: utility poles, fiber glass poles, and steel poles.

- Utility Poles are permissible on parkways, industrial, and residential streets. However, these will be phased out as more utility lines are buried.
- Fiberglass poles are acceptable on residential streets, in addition to steel and utility poles. Light fixtures in residential districts are mounted lower than on other street types since the travel way is generally narrower
- Steel poles are the preferred pole and are required on commercial and mixed use.
- Transit ways may have special fixtures that are combined with overhead-power structures

The spacing of light fixtures is mainly a function of the output of the type of lamp and the mounting height. Fixtures should be spaced to provide sufficient light to meet illumination standards. Regardless of the overall spacing, it is important to illuminate.

• Considerations

- Over-illumination should be avoided to diminish light pollution and conserve energy.
- Requests for lighting above the guidelines in the Street and Pedestrian Lighting Criteria require cost participation for the portion in excess of the City's standard expenditure.
- Street lighting and pedestrian lighting fixtures may be combined in some locations. Refer to Chapter 3 for pedestrian lighting guidelines.
- Special fixtures are allowed in historic districts and plazas. Where possible, they should meet the same energy standards as other types of fixtures.
- Tree growth can reduce the amount of light that reaches the roadway or side of the road. Regular tree maintenance is recommended so that crossings and critical points along the traveled way, such as neckdowns or chicanes, are sufficiently illuminated. Pedestrian-scaled lighting along the side of the road closer to pedestrians and bicycles can also mitigate heavy foliage along the traveled way.

LOW COST PEDESTRIAN SAFETY IMPROVEMENTS

While full roadway reconstruction projects offer an opportunity for complete street improvements, low cost pedestrian safety projects using colored paint and traffic barriers are an interim option for certain high risk locations. New York City has had success with similar pedestrian safety projects and serves as a model for progressive, yet low cost safety upgrades in Dallas. Roadway paint, bollards (or concrete planters), pedestrian areas can be extended, traffic lanes narrowed, medians widened, and turning radii tightened in a low cost, efficient manner. While created from less permanent materials, these roadway changes can remain for months and even years given proper maintenance.

Because low cost construction components are less permanent than full reconstruction, these types of projects can also serve as trials or as an initial phase for future, long term improvements. If, for example, a road diet and pedestrian safety project were planned for a mixed use street in downtown Dallas, large planters and paint could be used to delineate a wider median, additional sidewalk space, and pedestrian refuge areas near crosswalks to ascertain traffic and pedestrian conditions well before a more permanent full build out commenced.

The added benefit of low cost safety improvements is expedited implementation time and better responsiveness to current multi-modal roadway issues. Procurement and installation time for traffic barriers and roadway paint is substantially less than traditional reconstruction projects.



Street lighting and tree space

TREES AND GREENSCAPE SPACE

Trees and other greenscape plantings have a variety of functions – they can provide shade, buffer traffic in opposing travel lanes in medians, and provide aesthetic enhancements. Trees and other plantings must conform to Article X, of the Dallas Development Code, The Landscape and Tree Preservation Ordinance. When placing trees, consideration should be given to the placement and interaction of street and pedestrian lighting, utilities, and signage. Tree and plant selection is very important to ensure selection of climate-appropriate trees and plants (see Article X and the NCTCOG's iSWM guidelines.) Another consideration is the tree's anticipated mature canopy height, to ensure that there is adequate clearance for vehicles, buses, pedestrians and utilities so that all of these elements can function together efficiently.



An abundantly landscaped traffic diverter creates a new greenscape opportunity

6. INTERSECTION DESIGN ELEMENTS





A Complete Streets approach creates intersections that balance the needs of all users.

6. INTERSECTION DESIGN ELEMENTS



Intersection

Intersections are where streets converge, modes come together, and most conflicts occur on the roadway. Traditionally, Dallas' intersection design has been focused on maximizing the efficient movement of vehicles through the City. The Complete Streets approach expands this focus so that safety is the primary driver of intersection design. All intersections must safely accommodate people, whether they are walking, bicycling, driving, or riding transit.

Intersections should be designated and planned in context with the surrounding land uses as well as cultural and environmental considerations. Intersections should highlight the unique spaces where streets converge, making seamless connections from one street type to another. These Complete Streets guidelines emphasize the need to create multimodal intersections that are vibrant public spaces, balance the needs of all users, and enhance the quality of street life.



Intersection



Roundabout intersection



Underutilized areas of pavement should be reclaimed for pedestrian use.



Safe and accessible designs for all modes of transportation must guide intersection design.

MULTIMODAL INTERSECTIONS

Multimodal safety, with an emphasis on safety for vulnerable users, is the driving factor for intersection design in Dallas. It is important to recognize that non-motorized users are more vulnerable, and suffer far greater injuries in the event of a crash. Regardless of whether a trip is made on foot, by bicycle, via transit, or in an automobile people should feel safe, comfortable, and experience a minimal amount of delay during all trips.

Extensive guidance exists to design streets for motor vehicles. Specific engineering factors such as horizontal and vertical alignments, sight distance calculations, capacity, and coordinated signal timing are covered by a range of design manuals.

Traditionally in the City of Dallas, the manuals listed below have been used by engineers to design intersections and roadways:

- U.S. Access Board's Public Right-of-Way Accessibility Guidelines (PROWAG)
- AASHTO's Policy on Geometric Design of Highways and Streets
- Texas Department of Transportation (TxDOT) Roadway Design Manual
- Highway Capacity Manual (HCM)
- Texas Manual on Uniform Traffic Control Devices (TMUTCD)
- Institute of Traffic Engineers (ITE) Traffic Signal Timing Manual
- Dallas Paving Design Manual

The Dallas Complete Streets Design Manual is geared to supplement these manuals and guide the City's design process to require incorporation of multimodal design. Multimodal intersections should be functional and easy to navigate through clear regulatory and wayfinding signage, pavement markings, and signals. Designs should reflect users' desired travel paths as seamlessly as possible.

There are trade-offs with all intersection designs, and different design elements that can improve conditions for one mode may negatively impact conditions for other modes. The goal of Complete Streets is to consider the needs of all users, ensuring safe and efficient movements for all. The following guidelines discuss types of intersection controls, and key geometric design.

INTERSECTIONS AT SLIP STREETS

Description

Slip streets—also called frontage roads—are local roads, designed for lower volume local traffic, that run parallel to limited access highways or major arterials. Although slip streets reduce conflict points along the arterial by separating through traffic from vehicles accessing local businesses, parking, and residences, slip streets can create complicated intersection geometries and need to be carefully laid out in order to minimize conflicts.

Application

- Slip streets should control access points to and from the main arterial.
- An important objective for intersection design with slip streets is to ensure that slip streets do not become another route for through traffic.
- Through traffic on local roads may be discouraged by implementing low speed limits, narrow lane widths, on-street parking, bicycle facilities, and other traffic calming measures such as diverters.

Considerations

- Slip streets should prioritize safety and facilitate pedestrian and bicycle access.
- Space between the slip street and main roadway should be sufficient to accommodate plantings and a comfortable waiting area for pedestrians waiting to cross the main roadway. This space may also provide opportunities for natural drainage elements.
- Intersections with slip streets and major arterials may pose challenges to pedestrians due to the higher number of turning movements and associated conflict points.
- One strategy to reduce conflicts is to terminate a slip lane prior to a major intersection, forcing local vehicles into the main arterial or an adjacent street. Such a strategy may be implemented where there is a high demand for pedestrian crossings, or where there is the potential for cut-through traffic.
- Turning restrictions, including right-turn-on-red restrictions and left turn restrictions, should also be considered to minimize conflicts where slip streets intersect with other streets.
- Bicycle and pedestrian connectivity should be maintained on corridors with slip streets, and may require special treatments at major intersections such as exclusive phasing, signage, and pavement markings. Where a slip lane is terminated (e.g., vehicle traffic is diverted into the main arterial), bicycles and pedestrians should be allowed to continue along the path of the slip street and not diverted to the main roadway.



People should feel safe and comfortable, and should experience minimal delay during trips.



The goal of Complete Streets intersection design is to consider the needs of all users.



Mid-block crossing design should emphasize clear sight lines for all users.



Stop-controlled intersection in the West End

INTERSECTION CONTROLS

Uncontrolled and midblock crossings can be the most challenging places to provide safe pedestrian crossings.

Uncontrolled Intersections

Uncontrolled intersections are those where no traffic control devices facilitate the movement of traffic, and users yield the right-of-way to those who have already been established in the intersection, or those approaching from the right.

Midblock Crossings

A midblock crossing is a pedestrian crossing that is not located at a roadway intersection. If a midblock crossing is not designated by a marked crosswalk, then pedestrians must yield the right-of-way to motorists.

A discussion of when to mark crosswalks and provide additional safety treatments at uncontrolled intersections and midblock crossings is provided in this chapter. Specific warrants provided in the TMUTCD must be met in order to create signalized midblock crossings.

Uncontrolled intersections and midblock crossings should aim to maximize safety for all users by providing the following:

- Clear sight lines
- Appropriate lighting levels
- Regulatory and warning signage
- Marked crosswalks as determined by an engineering study (see Crosswalk Markings at Uncontrolled Locations)
- Traffic calming strategies

Stop-Controlled Intersections

Stop-controlled intersections are easiest for pedestrians to cross because motorists and cyclists must stop, encouraging them to yield to pedestrians and reducing pedestrian wait time. However, the use of STOP signs must balance safety with efficient traffic flow for all modes, including bicycles and transit vehicles. STOP sign installation on a major street requires that specific certifications be met, as determined by the TMUTCD.

In general, STOP signs may be appropriate if one or more of the following conditions exist:

- Where the application of the normal right-of-way rule (yield to those already in the intersection or to those approaching from the right) would not provide reasonable compliance with the law
- A street entering a highway or through street
- An unsignalized intersection in a signalized area
- High speeds, restricted view, or crash records indicate a need for control by a STOP sign

STOP signs should be installed in a manner that minimizes the number of vehicles having to stop. At intersections where a full stop is not necessary at all times, consideration should be given to using less restrictive measures, such as YIELD signs. Where feasible, the use of STOP signs should also be limited on streets with bikeways, especially on bicycle boulevards, as it requires significant energy to stop and start for bicyclists, resulting in lower levels of compliance.

Signalized Intersections

The Dallas Traffic Management Center remotely controls over 1,275 traffic signals in Dallas. The system can scan all traffic signals within 12 seconds for the status of equipment failure, as well as monitor, coordinate, and adjust the signals to improve traffic flow and pedestrian safety. Any recommended changes to signalized intersections should be based on an engineering study.

All signalized intersections should contain signals for motor vehicles and pedestrians. Additionally, bicycle signals and transit signals should be considered where appropriate. Signal phasing and timing should be designed to meet the unique needs of all users at the intersection. By optimizing signal phasing and timings, multiple modes are able to move safely and comfortably through the intersection with limited conflicts and delay.

Signalized intersections should conform to the latest version of the TMUTCD, HCM, and the Institute of Transportation Engineer's Traffic Signal Timing Manual. The TMUTCD contains specific warrants for the installation of a traffic signal at an intersection. The Dallas Public Works Department reviews and approves all proposed signal designs.

Signal Timing

Description

The overall goal of signal timing is to minimize cycle lengths to reduce delay for all users. Long cycle lengths make walking less convenient and may encourage unsafe behavior such as pedestrians jaywalking and bicyclists running red lights. Signals should be optimized to balance the needs of all users and to minimize delay for pedestrians, bicyclists, motor vehicles, and transit vehicles.

Signal timing is a tool used to optimize safety and efficiency for all modes of travel through an intersection. Over time, traffic volumes and patterns change; retiming signals requires evaluating changes in traffic patterns to minimize signal cycle lengths, reduce delay, improve safety, and reduce fuel consumption and emissions.

Application

- Signal retiming should be conducted to optimize intersection operations, and to globally coordinate the function of signals in relation to one another. This will allow groups or platoons of vehicles to efficiently travel through a series of intersections along a corridor. Vehicles can progress along a corridor at a set speed in order to obtain green lights at signalized intersections. Signal progression at slower speeds can help calm traffic, but should be used in conjunction with other methods to deter speed spiking in between signals.



Signalized intersection



Signalized intersection



Residential street roundabout



Residential street roundabout

- Proper optimization of a traffic signal system is performed by a traffic engineer. The process includes taking an inventory of the system, collecting traffic and pedestrian volume data, reviewing intersection safety, and updating signal timing software.
- Traffic changes, which can occur due to new development along a street, may require the adjustment of traffic signal timing.
- Signal retiming should be evaluated regularly to better optimize the performance of signalized intersections due to changing development and traffic flow patterns.

Considerations

- Technology improvements in signal timing hardware and software should be considered during system upgrades.
- Changes in the number of travel lanes, switching direction of traffic, and other travelway enhancements can be considered with the signal retiming process.
- Factors of effective signal timing include a lack of travelway capacity, a high use of midblock access points, irregular signal spacing, transit/rail influence, and pedestrian signal demands. These should be assessed during the regular reviews of the system performance.

Modern Roundabouts and Traffic Circles

Description

Modern roundabouts are circular intersections designed for yield-controlled entry and typically channelized approaches. Modern roundabouts are different from traditional traffic circles, also called rotaries, are designed with larger diameters of about 300' or more and operate at higher speeds (30 mph or greater). Some rotaries use signals, stop signs, or yield signs at one or more entries. Rotaries tend to be difficult for pedestrians and bicyclists to navigate because of higher speeds.

- Modern roundabouts have different design specifications than rotaries. The important difference between roundabouts and rotaries is the reduction in speeds and diameters, as well as yield-controlled entry in roundabouts. Pedestrian access is allowed, and median islands are installed where necessary to make crossing safer. Modern roundabouts should be designed to encourage slow entry speeds. In accordance with the Draft PROWAG, multi-lane roundabouts require accessible pedestrian signals at all crosswalks, which are expensive but necessary for safety.
- Another type of circular intersection is a neighborhood traffic circle, which is a smaller type of roundabout and generally used for low-speed residential street types.
- Roundabouts and traffic circles provide an opportunity to incorporate Green Street techniques through bioretention or other techniques described in Chapter 7.

Application

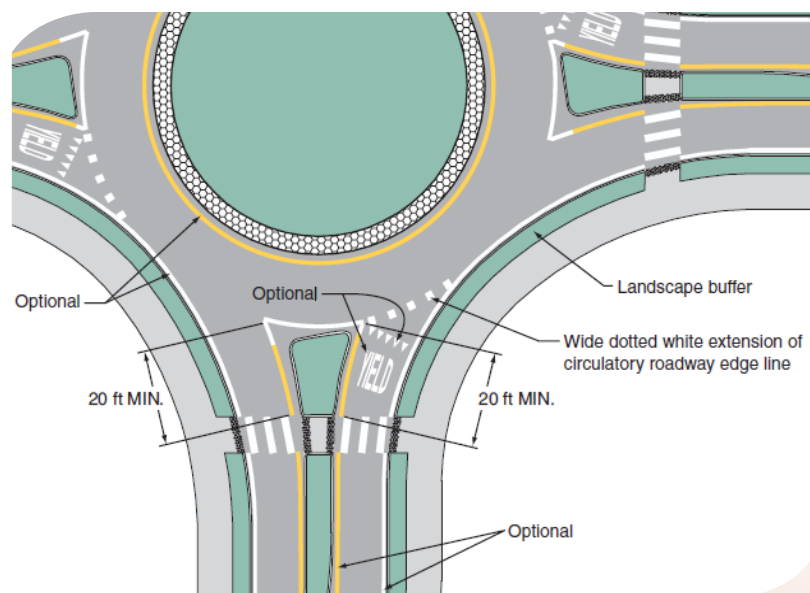
When determining whether to install modern roundabouts, general considerations would include the design vehicle, pedestrian volumes, amount of pedestrians with visual impairments, and effects on pedestrian route directness. Roundabouts are not recommended if they would create greater vehicle delay or increased difficulty for pedestrians navigating the intersection. Intersections with more than four legs can be good candidates for conversion to modern roundabouts. However, an engineering study must be conducted in order to determine whether a modern roundabout would be appropriate.

Modern roundabout designs should reduce relative speeds and improve traffic flow. ADA compliant pedestrian crosswalks with detectable warning strips and ramps at least 20' from the entry of the roundabout should be provided. Sight distance for drivers entering the roundabout must be maintained to the left so that drivers are aware of vehicles and bicycles in the circle (visibility across the center of the circle is not critical). Proper signing and pavement markings must conform to the latest version of the TMUTCD.

Considerations

- Yield lines should be provided at the entry of the roundabout.
- High pedestrian volumes may require larger crosswalk widths.
- Multi-lane roundabouts may not be recommended in areas where high levels of pedestrian and bicycle activity are anticipated.
- If multi-lane roundabouts are installed, in addition to pedestrian signals, splitter island medians should be provided on the approaches in order to reduce crossing distances, and allow pedestrian to cross one direction of travel at a time. At-grade pedestrians cut-throughs should be provided at splitter island medians with ADA compliant detectable warning strips
- Intersections near active railroad at-grade crossings are typically poor candidates for roundabouts since traffic would be blocked in all directions when trains are present
- Where there are high pedestrian volumes, signal controls should be considered

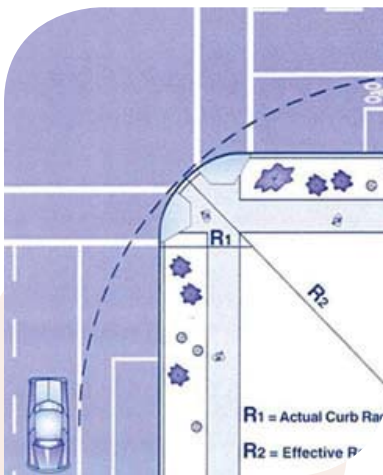
- Permitting bicyclists to use the sidewalk at roundabouts should be considered for comfort and safety of all types of bicyclists, such as young children. Ramps from the street to the sidewalk, as well as appropriate signage to inform pedestrians of a mixing zone, should be installed if sidewalk riding is permitted.
- Modern roundabouts also provide excellent opportunities to implement stormwater management techniques. Visibility and sight distances must not be obstructed due to plant growth.



Example of markings for approach and circulatory roadways at a roundabout



DART buses are up to 40 feet in length, and must be able to navigate intersections.



Adding parking and/or bicycle lanes increases the effective radius of a corner.

KEY GEOMETRIC DESIGN GUIDANCE

Well-designed intersection geometry is crucial for creating safe, efficient, and multimodal intersections. Changes in geometry can help to reduce vehicle turning speeds, increase pedestrian comfort and safety, and create space for dedicated bicycle facilities. Dallas' intersections must combine well-designed geometry with efficient traffic control measures to maximize safety for all users.

Curb Radii

Description

Corner design has a significant impact on how well an intersection serves the diversity of roadway users. Larger curb radii typically result in higher-speed turning movements by motorists, while smaller curb radii require sharper turns that reduce speeds, shorten crossing distances for pedestrians, and improve sight distances.

Two of the most important corner design elements are the effective radius and the actual curb radius. Actual curb radius refers to the curvature along the curb line. Effective radius refers to the curvature that vehicles follow when turning, which may be affected by on-street parking, bicycle lanes, medians, and other roadway features.

Many arterial intersections with right turn channels have high design speeds with wide intersection radii. Even with pedestrian islands, these intersections discourage pedestrian access due to wide crossing distances and high speed right turning vehicles. Because of these issues, right turn channels are discouraged on Mixed Use and Residential Streets and used judiciously on other streets with particular consideration for pedestrian safety and convenience. Geometric changes to these types of intersections should be considered as part of larger corridor improvement projects for areas with high pedestrian volumes and relatively low truck traffic. To improve pedestrian safety and convenience, arterial intersections change, particularly on Mixed Use and Residential Streets should include:

- Removal of right turn traffic channels to create a contiguous landscaped area between adjacent parcels and intersection
- Extension of sidewalks (if not already present) through new landscaped area
- Installation of continental crosswalks
- Possible bumpouts if full time on-street parking exists. If not, determine possibility of lane configuration changes.

Application

The smallest practical actual curb radii shall be chosen to accommodate the design vehicle while balancing the needs of pedestrians. When designing the actual curb radii to accommodate the chosen design vehicle, assessments should be based on how the effective radius interacts with the design vehicle's turning radius. See Chapter 3 for more information on design vehicles.

An actual curb radius of 5'-10' should be used wherever possible, including where

- there are higher pedestrian volumes,
- there are low volumes of large vehicles,
- bicycle and parking lanes create a larger effective radius.

The desired maximum effective curb radius is 35' for large vehicles. There are several factors that may affect the curb radii and must be taken into consideration. These include

- the street types,
- the angle of the intersection,
- curb extensions, and
- the receiving lane width.

Where there are high volumes of large vehicles making turns, inadequate curb radii could cause large vehicles to regularly travel across the curb and into the pedestrian waiting area.

Considerations

A variety of strategies can be used to accommodate large vehicles while preserving benefits for pedestrians:

- Adding parking and/or bicycle lanes increases the effective radius of the corner.
- Striping advance stop lines on the destination street of multi-lane roadways (at least two lanes in each direction) enables large vehicles to make the turn by encroaching into the opposing lane.
- Installing a textured, at-grade paving treatment discourages high-speed turns while permitting turns by larger vehicles.
- Varying the actual curb radius over the length of the turn, also known as a compound curve, creates a radius that is smaller as vehicles approach a crosswalk and larger as they make the turn.
- Restricting access and operational changes prohibit certain turning movements.

Curb Ramps

Description

A curb ramp is a ramp that provides a smooth transition from the sidewalk to the street. Appropriately designed curb ramps are critical for providing access across intersections for people with mobility and visibility disabilities. One of the key considerations of intersection geometry is the location of curb ramps and crossings relative to desired lines and vehicle paths.

Application

Title II of the ADA requires that all pedestrian crossings be accessible to people with disabilities by providing curb ramps. Curb ramps must comply with standards established by the Dallas Public Works Department.

Curb ramps, not including flares, must be a minimum of 4' wide and contained within the marked crosswalk. Curb ramps shall have a slope of no more than 8.33 percent, a minimum 2' detectable warning strip, and level landing pads at the top and bottom of the ramp. Detectable warning strips include a series of truncated domes and are colored to contrast with the surrounding pavement.



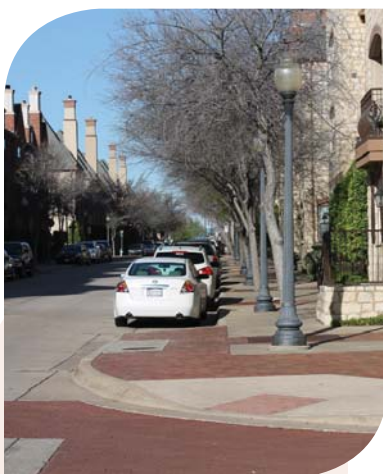
Curb ramps in the State-Thomas neighborhood



Major thoroughfare curb ramps



Dallas Main Street curb extensions



State Street curb extensions

Intersection geometry should be influenced by the following curb ramp design principles:

- Wherever feasible, curb ramps should be located to reflect pedestrians' desired path of travel through an intersection, while also considering sight lines of approaching motor vehicles.
- If possible, two separate curb ramps should be provided at corners instead of a single ramp that opens diagonally at the intersection.
- Curb ramps should be designed to avoid accumulation of water or debris to the maximum extent feasible. Drainage inlets should be considered with the design of curb ramps.

Considerations

There are a variety of standard curb ramp designs, including perpendicular ramps and parallel ramps. The appropriate design for a particular location is determined on a site-by-site basis. Key factors to consider include pedestrian desire lines, sidewalk widths, buffer widths, curb heights, street slopes, and drainage patterns.

Raised crossings extend the sidewalk environment across a roadway and do not require people to navigate curb ramps. Consider installing raised crossings at locations with high pedestrian volumes, and where low speeds are desired. Detectable warning strips are also required at crossings where there is no grade separation between the sidewalk and the roadway, such as at raised crossings and intersections. For more information, see Special Paving Treatments on page 179 of this chapter.

Curb Extensions

Description

Curb extensions, also known as neckdowns or bulbouts, reduce the effective width of the street by extending the curb line across a parking lane to the beginning of the adjacent travel lane.

Curb extensions have a variety of potential benefits:

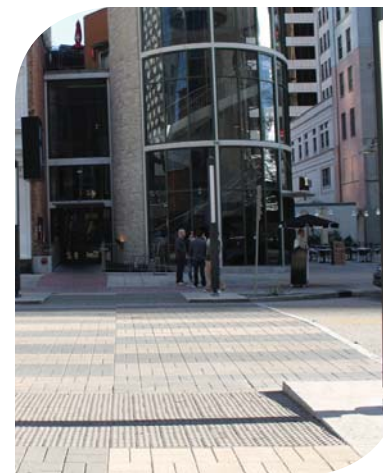
- Additional space for pedestrians to queue before crossing
- Improved safety by slowing motor vehicle traffic and emphasizing pedestrian crossing locations
- Less exposure for pedestrians by reducing crossing distances
- Space for ADA compliant curb ramps where sidewalks are narrow
- Enhanced visibility between pedestrians and other roadway users
- Restricting cars from parking too close to the crosswalk area
- Space for utilities, signs, and amenities such as bus shelters or waiting areas, bicycle parking, public seating, street vendors, newspaper stands, trash and recycling receptacles, and stormwater management elements or street parks

Application

- Curb extensions should be considered only where on-street parking is present, including at corners and midblock
- A typical curb extension extends 6' from the curb (the approximate width of a parked car).
- The minimum length of a curb extension shall be the width of the crosswalk, allowing the curvature of the curb extension to start after the crosswalk. Note that the angle of curvature should deter parking, supplemented by NO STOPPING signs. The length of a curb extension can vary depending on the intended use (i.e., stormwater management, bus bulb, restrict parking).
- Curb extensions should not reduce a travel lane or a bicycle lane to an unsafe width.
- Curb extensions at intersections may extend into either one or two legs of the intersection, depending on the configuration of parking.
- Street furniture, trees, plantings, and other amenities must not interfere with pedestrian flow, emergency access, or visibility between pedestrians and other roadway users.

Considerations

- Curb extensions are particularly valuable in locations with high volumes of pedestrian traffic, near schools, or where there are demonstrated pedestrian safety issues.
- The turning needs of larger vehicles should be considered in curb extension design. When curb extensions conflict with turning movements, they should be reduced in size rather than eliminated.
- Emergency access is often improved through the use of curb extensions if intersections are kept clear of parked cars.
- Curb extension installation may require the relocation of existing storm drainage inlets.
- Curb extensions may also impact underground utilities, curbside parking, delivery access, garbage collection, and street sweepers. These impacts should be evaluated when considering whether to install a curb extension.
- Curb extensions are not desirable on arterials that have peak hour parking restrictions to move traffic more efficiently.



Downtown Dallas Main Street curb ramps



Residential street curb ramps



New crossing median island



Crossing island on commercial thoroughfare

Crossing Islands

Description

Crossing islands are raised, protected areas within a crosswalk that divide a roadway into segments so pedestrians only have to cross one direction of traffic at a time. Crossing islands reduce pedestrian exposure, and are particularly valuable when used along multi-lane roadways. Crossing islands can be used at signalized intersections, but signal timing should always be designed to allow pedestrians to cross the entire roadway in one stage.

Application

Crossing islands design should:

- include at-grade pedestrian cut-throughs as wide as the connecting crosswalks and detectable warning strips, and be gently sloped to prevent ponding and ensure proper drainage;
- direct pedestrians at an angle to face on-coming traffic;
- be at least 6' wide, but preferably 8' wide;
- accommodate turning vehicles if applicable;
- extend beyond the crosswalk at intersections;
- incorporate diverging longitudinal lines on approaches to crossing islands, per TMUTCD standards.

Considerations

- Crossing islands should be considered where crossing distances are greater than 50'.
- Where possible, stormwater management techniques should be utilized on crossing islands with adequate space, but not in the pedestrian clear path to and from crosswalks. Plantings should not obstruct sight lines.

Diverter

Description

Diverter are curb extensions or traffic islands at intersections used specifically to restrict motor vehicle access and deter heavy volumes of through vehicle traffic on residential street types. All diverters should maintain pedestrian and bicycle access. There are many types of diverters:

- full-closures—restricts travel in both directions
- half-closures—restricts travel in one direction on an otherwise two-way street
- diagonal diverters—placed diagonally across an intersection, preventing through traffic by forcing turns in one direction
- forced turns—forces travel in a specific direction

Application

- Diverters should be installed on streets where eliminating cut-through traffic is desired.
- Diverters should only be considered as part of an overall traffic calming strategy. Include street direction changes for an area when less restrictive measures, such as signs, are not effective.
- Diverters should be designed to impact motor vehicle movement, but should facilitate bicycle and pedestrian access.
- The design of diverters must consider impacts to emergency vehicle response times. Designs that allow emergency vehicle access are preferred and should be coordinated with a local emergency response program.
- Diverter designs should be carefully thought out to ensure proper drainage and maximize the potential for on-site stormwater retention and infiltration.
- Vegetation used in diverters should be low growing to maintain sight lines and also be drought-resistant.

Considerations

- Diverters directly affect people living in the neighborhood and so require strong local support. A highly interactive public input process is essential.
- Different elements can be used as a diverter, including concrete medians, stormwater planters, public art sculptures, etc. Diverters provide excellent opportunities to introduce green elements at intersections, and can be used to absorb stormwater and reduce the heat island effect.
- Temporary diverters can be installed to test how permanent diverters might affect traffic flow.
- Diverters are an important component of bicycle boulevards, which allow through bicycle traffic but discourage through motor vehicle traffic.

A diverter's impact on speeding is generally limited to the intersection. Additional countermeasures are usually necessary to address speeding at midblock locations.



Diverter detail showing drainage design



Bike lane diverter



Marked pedestrian crosswalk



Pedestrian crosswalk with stained pavement treatment

KEY PEDESTRIAN TREATMENTS

Pedestrians are the most vulnerable users of the transportation system. As a pedestrian, motor vehicle speeds greatly affect the severity of crashes and impact fatality rates. Streets with high pedestrian activity should maintain slow motor vehicle speeds, which can be achieved through roadway design and traffic calming strategies. In addition, areas close to parks, schools, and similar pedestrian destinations require special pedestrian consideration. Pedestrian-oriented designs should also aim to minimize conflicts with other modes and exposure to motor vehicle traffic.

Intersections must be designed for pedestrians of all ages and abilities. ADA compliant curb ramps, crosswalks, and accessible pedestrian signals should be provided to the maximum extent feasible, following the minimum guidelines set by the U.S. Access Board PROWAG.

Crosswalk Design

Description

Well-designed crosswalks are crucial to creating pedestrian-friendly walking environments. Crosswalks may be marked or unmarked. While most intersections have marked crosswalks at each approach, other locations can be marked specifically to emphasize unique pedestrian desire lines and to ensure safe access to local institutions, parks, and housing for the elderly. Due to their high visibility and well known applications in other major cities, continental crosswalks are preferred at all mid-block crossings, near schools, downtown, major transit stops and at major aerial pedestrian crossings.

Safety for all pedestrians, especially for those with disabilities, is the single most important criteria informing crosswalk design. Crosswalks serve a dual function of guiding pedestrians to locations where they should cross the street, and alerting drivers of pedestrian movements.

Application

- All crosswalk designs must conform to the latest edition of the TMUTCD.
- The Dallas Public Works Department shall approve the location of all proposed crosswalks.
- Different types of crosswalk markings can be used in the City of Dallas. Typically, two parallel transverse lines (or continental style) crosswalk markings are installed.
- Enhanced crosswalks should be considered in transit areas.
- Nonstandard materials and designs must be approved by the Director of Public Works and Transportation, and typically require a special maintenance agreement.
- Crosswalks should be at least 10' wide or the width of the approaching sidewalk if it is greater. In areas of heavy pedestrian volumes, crosswalks can be up to 25' wide.
- ADA-compliant curb ramps should direct pedestrians into the crosswalk and the bottom of the ramp should lie within the area of the crosswalk. Flares do not need to fall within the crosswalk.
- The TMUTCD provides guidance on crosswalk markings for an intersection with an exclusive pedestrian phase that permits diagonal crossings.

Considerations

The location of crosswalk markings should be designed at right angles where practical, and must be balanced with pedestrian desire lines, accessibility requirements, and the constraints of the site. Particularly at complex intersections, crosswalks should be placed at locations that reflect pedestrian desire lines while also considering the safest location to cross—that is, where there is the least amount of exposure to conflicts with other modes. Crosswalk placement should also maximize the visibility of pedestrians to turning vehicle movements.

Crosswalk markings should consist of non-skid, thermoplastic, retro-reflective material. Durability and ease of maintenance must be a consideration in material selection.

Crosswalk Markings At Uncontrolled Locations

Description

This section presents guidance as to when and where it is appropriate to provide marked crosswalks at uncontrolled locations, as well as when additional safety enhancements are required to increase visibility, awareness, and yielding to pedestrians.

The NCHRP Report 562, Improving Pedestrian Safety at Unsignalized Intersections, found that “the safest and most effective pedestrian crossings use several traffic control devices or design elements to meet the information and control needs of both motorists and pedestrians.”

Additional safety improvements which are discussed on the following pages include the following:

- raised crossings and intersections
- advance yield markings and signs
- in-street YIELD TO PEDESTRIAN signs
- rectangular rapid-flash pedestrian beacons
- pedestrian signal leads
- accessible pedestrian signals
- signal phases for pedestrians

Application

An engineering study should be performed to determine the feasibility of a marked crosswalk at uncontrolled locations. Components of such a study include the following:

- traffic speeds and volumes
- crossing distances
- need or demand for crossing
- distance from adjacent signalized intersections and other crosswalks, and the possibility to consolidate multiple crossing points



Pedestrian crosswalk with special pavers



Pedestrian crosswalk with special pavers



Textured crosswalk pavement

- sight distance and geometry of the location
- availability of street lighting
- locations of drainage structures

Locations where crosswalk markings alone are insufficient to address pedestrian safety include any street where any of the following conditions exist:

- The roadway has four or more lanes of travel without a raised median or pedestrian crossing island and an ADT of 12,000 vehicles per day or greater
- The roadway has four or more lanes of travel with a raised median or pedestrian refuge island and an ADT of 15,000 vehicles per day or greater
- The speed limit exceeds 35 mph

Considerations

At uncontrolled intersections on major arterials, marked crosswalks may not be appropriate on each leg of the intersection. It is more appropriate to mark only a single side of the intersection, particularly in cases where pedestrians can easily be directed to one location. In selecting the most appropriate side of an uncontrolled intersection for the marked crosswalk, the following should be considered:

- pedestrian demand (such as location of bus stops or metro stations)
- vehicle turning movements; multi-leg intersections (three or more roadways) require a careful consideration of vehicular turning movements balanced against the pedestrian crossing
- sight distance
- proximity to other marked crosswalks or crossing locations

There are a number of measures that can be used at uncontrolled locations, in addition to marked crosswalks, to improve the safety of pedestrians crossing the street:

- Reduce the effective crossing distance for pedestrians by:
 - providing curb extensions
 - providing raised pedestrian crossing islands
 - performing road diets or lane diets
- Install traffic calming measures to slow vehicle speeds
- Provide adequate nighttime lighting for pedestrians
- Using various pedestrian warning signs, advance stop lines, rapid-flashing beacons, and other traffic control devices to supplement marked crosswalks (see the following sections for more details)
- Install traffic signals with pedestrian signals where warranted



Katy Trail crosswalk at Knox Street

SPECIAL PAVING TREATMENTS

Description

Special paving treatments can be used on roadway surfaces to reduce speeds, increase durability, manage stormwater, or to demarcate a special zone like a bike lane, bus stop, or speed table. A change of color or material can produce a traffic calming effect. Examples of special roadway materials include colored asphalt or concrete, textured asphalt or concrete, pervious pavement, stamped patterns, and pavers. The location and extent of special paving materials depends on the design of the roadway and the expected vehicle types and volumes. See Chapter 7 for additional information on the applicability of pervious surfaces for Green Streets implementation.

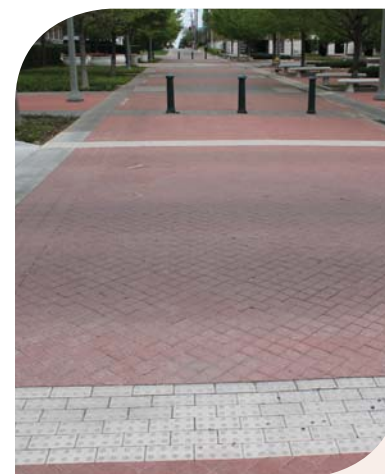
The choice of a contrasting paving material affects the safety and maintenance of the road. While decorative crosswalks with special paving treatments add character to neighborhood streets, many decorative designs are less visible to drivers and pedestrians compared to traditional transverse and continental crosswalks. Where special crosswalk paving treatments are used, two twelve inch solid white lines should be installed on their edges to increase visibility for roadway users. Different materials have different qualities with respect to road noise, porosity, heat absorption, surface friction, bicyclist comfort, and maintenance. Stamped concrete is preferred for special crosswalk paving treatments, as pavers tend to be costly to maintain and can be a tripping Hazard for pedestrians.

Application

- Light colored asphalt and concrete should be utilized wherever possible to reduce heat.
- Colored pavement can be used to delineate special lanes for transit, bicycles, or parking on mixed use streets. Limits to durability make this treatment less appropriate for lanes on parkways, industrial, and commercial streets. On these street types, colored pavement may be more appropriate for conflict zones, such as merge areas and intersections, or for special districts, shared paths, or streets meant for slower speeds.
- Special paving treatments may be used on crosswalks, in special districts to delineate the distinct streets designed for slower speeds, and streets intended to be shared with pedestrians.
- The use of concrete bus pads should be considered on high-frequency bus routes where heat and the heavy weight of vehicles can create channels in asphalt.
- Porous pavement, in theory, allows water to move through the pavement and become absorbed into the soil below, rather than running off the pavement into drains and ditches. Because many parts of Dallas are situated on clay soils which do not readily absorb rainwater, porous pavements will likely be designed with a liner to separate the infiltrated runoff from the subbase and in situ soil. A perforated pipe can be used to drain the pavement. Design guidance for porous pavement will be included in the paving and drainage manuals.



Pedestrian crosswalk with special pavers



Pedestrian crosswalk with special pavers



Raised intersection



Katy Trail crosswalk at Knox Street

Considerations

- Consideration should be given to long-term maintenance of porous pavement materials.
- Care must be taken to ensure textured pavements are structurally sound and able to support the type and volume of vehicles that are likely to use the street.
- Particular care should be taken with placing different materials adjacent to each other (for example, concrete pavers adjacent to an asphalt roadway). Over time, the edges between the two pavement materials can become uneven.
- Noise can be a concern with textured pavements.

Advance Yield Markings and Signs

Description

Advance yield markings are yield markings that are striped further back from the crosswalk and used in conjunction with YIELD HERE TO PEDESTRIAN signs. Advance yield markings make it easier for pedestrians and motorists to see one another. On multi-lane roadways, they help reduce multiple-threat collisions. Multiple-threat collisions occur when there are multiple lanes of travel in the same direction and the vehicle in the near lane yields to the pedestrian, blocking the view of the motorist in the far lane.

Application

- Advance yield markings and signs can be used on two-lane, three-lane, and four-lane roadways. They are less effective on four-lane roadways unless vehicle operating speeds are 25 mph or less. On four-lane roads with higher speeds, the rapid flashing beacon may be a better solution. See the Rectangular Rapid-Flash Pedestrian Beacon section later in the chapter.
- Yield markings at unsignalized crossings should be accompanied by YIELD HERE TO PEDESTRIAN signs.
- Advance yield markings and signs should be placed 20'-50' in advance of crosswalks on unsignalized multi-lane approaches. Parking should be prohibited in the area between the yield line and the crosswalk. Pavement markings can be used to reinforce NO PARKING signage.
- Yield lines should not be used at locations where drivers are required to stop in compliance with a STOP sign, a traffic control signal, or other traffic control device.

Considerations

- When determining where to place advance yield markings and signs within the 20' - 50' range, consideration should be given to the number of lanes pedestrians must cross, motor vehicle speeds, sight lines, on street parking, and turning movements.
- Advance yield markings may be staggered so that yield markings in one lane are closer to the crosswalk than the yield markings in an adjacent lane. Staggered yield lines can improve drivers' view of pedestrians, provide better sight distance for turning vehicles, and increase the turning radius for left-turning vehicles

In-street YIELD TO PEDESTRIAN Signs

Description

A variety of signs may be used to indicate locations where drivers must yield to pedestrians. In-street signs are placed in the roadway to alert drivers to be aware of the crossing and to effectively yield to pedestrians. In-street signs can be permanently installed in the roadway or mounted on a portable base. In-street YIELD TO PEDESTRIAN signs are a cost effective treatment to increase motorists' compliance to pedestrian laws.

Application

- In-street YIELD TO PEDESTRIAN signs must only be used at unsignalized intersections. They are prohibited from use at signalized or stop-controlled intersections.
- In-street YIELD TO PEDESTRIAN signs should be placed in the roadway prior to the crosswalk location on the center line, on a lane line, or on a median island. They should not obstruct the crosswalk, and should be designed to bend over and bounce back when struck by a vehicle.
- In-street YIELD TO PEDESTRIAN signs work best on low speed, two lane streets. They are not recommended on roads with high motor vehicle speeds or volumes, where drivers are less likely to see them.

Considerations

- In-roadway YIELD TO PEDESTRIAN signs require regular monitoring and should be replaced when damaged. Damaged signs send the message to pedestrians that a crossing is not safe.



In-street YIELD TO PEDESTRIAN sign



Rectangular rapid-flash beacon



Rectangular rapid-flash beacon

SPECIAL PEDESTRIAN SIGNALS

Rectangular Rapid-Flash Pedestrian Beacons

Description

At some unsignalized crossings, particularly those with four or more lanes, it can be very challenging to get drivers to yield to pedestrians. Vehicle speeds and poor pedestrian visibility combine to create conditions in which very few drivers stop.

One type of device proven to be successful in improving yielding compliance at these locations is the rectangular rapid-flash beacon. The rectangular rapid-flash beacon's effectiveness has been confirmed by multiple studies, including an FHWA study the Effects of Yellow Rectangular Rapid-Flashing Beacons on Yielding at Multilane Uncontrolled Crosswalks.

Rectangular rapid-flash beacons are placed curb side below the pedestrian crossing sign and above the arrow indication pointing at the crossing. They should not be used without the presence of a pedestrian crossing sign. The LED flash is an irregular flash pattern. The beacons are activated by a pedestrian call button.

Another LED panel should be placed facing the pedestrian to indicate that the beacon has been activated. The push-button and other components of the crosswalk must meet all other TMUTCD accessibility requirements.

Application

- Design of rapid-flash beacons should be in accordance with FHWA's Interim Approval for Optional Use of Rectangular Rapid-Flash Beacons issued July 16, 2008.
- Rectangular rapid-flash beacons can be used when a signal is not warranted at an unsignalized crossing. They are not appropriate at intersections with signals or STOP signs.
- Rectangular rapid-flash beacons are installed on both sides of the roadway at the edge of the crosswalk. If there is a pedestrian refuge or other type of median, a beacon should be installed in the median rather than the far-side of the roadway.

Considerations

- Rectangular rapid flash beacons are considerably less expensive to install than mast-arm mounted signals, but they are more expensive than signs. They can also be installed with solar-power panels to eliminate the need for a power source.
- Rectangular rapid-flash beacons should be limited to locations with critical safety concerns, and should not be installed in locations with sight distance constraints that limit the driver's ability to view pedestrians on the approach to the crosswalk.
- Rapid-flash beacons should be used in conjunction with advance yield pavement markings and signs, as discussed on the previous page.

Pedestrian Signal Heads

Description

Well-designed signalized intersections help reduce delay for all modes, minimize conflicts between modes, and help reduce risk-taking behavior. Pedestrian signal heads display each part of the pedestrian phase as listed below:

- The WALK indication, represented by a walking person symbol, signifies the WALK interval.
- The Flashing DON'T WALK indication, represented by a flashing upraised hand, signifies the pedestrian change interval. Typically, the flashing DON'T WALK indication is accompanied by a countdown display depicting how much time is left to cross the street. Countdown displays are required on new installations to encourage pedestrians to finish crossing before the DON'T WALK indication, and better serves pedestrians with faster walking speeds.
- The DON'T WALK indication, represented by a steady upraised hand, signifies that pedestrians are not permitted to cross. The DON'T WALK indication should be displayed for a three-second buffer interval prior to the release of any conflicting motor vehicle movements.
- Accessible pedestrian signals are discussed later in this chapter and on the next page.



Pedestrian signal head

Application

Pedestrian signal heads should be provided at all signalized intersections for all marked crosswalks. Additionally, it is highly recommended to install crosswalks on all legs of a signalized intersection unless determined otherwise by an engineering study. The design of pedestrian signal heads must conform to the 2011 edition of the TMUTCD.

Pedestrian countdown timers have become standard in many major cities due to increased pedestrian comfort. When pedestrians know the remaining length of a walk signal, they can make more informed decisions about when to cross a street. When existing pedestrian signal heads need to be replaced due to age or as part of a larger reconstruction effort, they should be replaced with pedestrian signal heads with countdown timers.

The timing for each phase must account for the walking speeds of people of all ages and abilities, especially children, the elderly, and disabled. The timing of each interval must conform to calculations according to the 2011 edition of the TMUTCD.



Accessible pedestrian signal



Accessible pedestrian signal

Considerations

One of the primary challenges for designers is to balance the goal of minimizing conflicts between turning vehicles with the goal of minimizing pedestrian and motorist delay. Requiring pedestrians to wait for extended periods can encourage crossing against the signal. The 2010 Highway Capacity Manual states that pedestrians have an increased likelihood of risk-taking behavior (i.e., jaywalking) after waiting longer than 30 seconds at signalized intersections. Strategies to achieve this balance include minimizing signal cycle lengths, restricting right-turn-on-red, introducing leading pedestrian intervals, and reducing turning speeds to increase yielding times.

Opportunities to provide a WALK indication should be maximized whenever possible. Vehicular movements should be analyzed at every intersection in order to utilize nonconflicting movements to implement WALK intervals. For example, at a four-leg intersection with the major road intersecting a one-way street, when the major road has the green indication pedestrians can always cross the approach where vehicles cannot turn.

Intersection geometry and traffic controls should facilitate turning vehicles yielding to pedestrians unless providing an exclusive turning interval or protected/exclusive pedestrian phase. At unsignalized intersections, turning vehicles must yield to pedestrians. This expectation should be continued to signalized intersections to the greatest extent possible.

Accessible Pedestrian Signals (APS)

Description

Accessible pedestrian signals (APS) and accessible detectors are devices that communicate the WALK and DON'T WALK intervals with nonvisual indications at signalized intersections to people with visual and/or hearing disabilities. Accessible pedestrian signals and detectors may include features such as audible tones, speech messages, detectable arrow indications, and/or vibrating surfaces.

The major functions of accessible pedestrian signals are to provide information for:

- location of push buttons, if used
- beginning of WALK indication
- direction of crossing
- location of destination sidewalk
- intersection street name in Braille or raised print
- intersection signalization with speech messages
- intersection geometry through detectable maps or diagrams or through speech messages

Push-button locator tones are used for locating the pedestrian push-button needed to actuate the WALK indication. Vibrotactile devices vibrate to communicate when the WALK indication is in effect. Detectable arrows indicate the direction of travel on the crosswalk.

Application

- All pedestrian signal designs must conform to the latest edition of the TMUTCD.
- Accessible pedestrian signals and detectors must be used in combination with pedestrian signal timing.
- The proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way require accessible pedestrian signals and push buttons when pedestrian signals are newly installed, when the signal controller and software are altered, or when the signal head is replaced. The TMUTCD currently states that accessible pedestrian signals shall be provided based on engineering judgment.
- Information provided by an accessible pedestrian signal must clearly indicate which pedestrian crossing is served by each device.
- At corners where two push buttons are present, to the maximum extent feasible they should be separated by at least 10'.

Considerations

- Accessible pedestrian signal detectors may be push buttons or passive detection devices. At locations with pretimed traffic control signals or nonactuated approaches, pedestrian push-buttons may be used to activate the accessible pedestrian signals.
- APS are typically integrated into the pedestrian push-button, and the audible tones and/or messages come from the push-button housing. APS also have a push-button locator tone and detectable arrow, and can include audible beaconing and other special features.
- Detectable arrows should be aligned toward the destination across the street; they should not point toward the beginning of the crosswalk or the curb ramp location. Misalignment of the arrow may direct pedestrians with disabilities into the center of the intersection.
- Audible WALK indications should coincide with the pedestrian WALK. If the pedestrian signal rests in the WALK phase, the audible indication should be provided in the first seven seconds of the WALK phase.
- Detailed information on accessible pedestrian signals is also provided through the United States Access Board.



Pedestrian signal with countdown indication



Pedestrian push button



Pedestrian signal



Pedestrian signage

Signal Phases for Pedestrians

Description

There are three ways to time a pedestrian phase:

- A **concurrent pedestrian phase** occurs when pedestrians have the WALK indication while parallel and conflicting (turning) vehicular traffic is permitted.
- A **protected pedestrian phase** occurs when pedestrians have the WALK indication while conflicting movements are prohibited by a signal or NO TURN ON RED sign.
- An **exclusive pedestrian phase** occurs when pedestrians have the WALK indication while all other movements are prohibited by a signal or NO TURN ON RED sign.

Application

Concurrent pedestrian phases are the most common application at signalized intersections where pedestrian accommodations exist.

Protected pedestrian phases can be used when there are high volumes of vehicle turning movements conflicting with pedestrian traffic. This phasing will provide a pedestrian WALK indication at the same time as the through movement in the same direction, while prohibiting the conflicting turning movements that could cross an active crosswalk.

Exclusive pedestrian phases can be used when there is a very high volume of pedestrians. This phase allows all pedestrian movements at once and may increase motorist delay. It may not be ideal at intersections with high volumes of motor vehicle traffic.

Exclusive pedestrian phases and protected pedestrian phases should generally be considered at intersections where:

- sight distance is restricted
- intersection geometry is complex
- the intersection is near elderly housing, schools, recreational areas, medical facilities, or other facilities within a safety zone
- the intersection is near special event locations with high pedestrian volumes

Considerations

- Exclusive pedestrian phases increase pedestrian safety, but also increase delay for vehicular intersection users.
- Leading pedestrian intervals may be considered for concurrent phasing where appropriate, and are discussed in Signalization Strategies to Reduce Conflicts.
- NO TURN ON RED signs should be considered at intersections with exclusive pedestrian phases.
- TURNING VEHICLES YIELD TO PEDESTRIANS and WATCH FOR TURNING VEHICLES signs may be used to provide additional awareness at intersections with concurrent pedestrian phases where conflicting vehicle/pedestrian movements are present.

Signalization Strategies to Reduce Conflicts

Description

There are several signalization strategies to reduce conflicts between pedestrians and other modes of transportation. These typically involve separating movements, including the following:

- exclusive and protected pedestrian signal phases
- leading pedestrian intervals
- lagging vehicle turn arrow
- restricting turns on red

Exclusive and protected signal phasing separates pedestrian traffic and reduces conflicts between pedestrians and motorists; however, there are significant impacts to signal cycle lengths that need to be considered.

Another strategy is called the Leading Pedestrian Interval (LPI), which initiates the pedestrian WALK indication three to seven seconds before motor vehicles traveling in the same direction are given the green indication. This technique allows pedestrians to establish themselves in the intersection in front of turning vehicles, increasing visibility between all modes. A 1998 traffic engineering study found that LPIs reduce collisions between turning vehicles and pedestrians by 28 percent (Reference: http://onlinepubs.trb.org/Onlinepubs/circulars/ec019/Ec019_i3.pdf). LPI's are a cost effective way to improve traffic safety, and should be considered at all intersections with high pedestrian crash rates as well as high traffic volume intersections where there is substantial pedestrian traffic.

Left-turn arrow indications can be provided before the opposite direction through movements (leading left-turn) or after the opposite direction through movements (lagging left-turn).

NO TURN ON RED signs can also be used to restrict vehicles from turning right—or from turning left on intersecting one-way streets during the red indication. Restricting this movement eliminates conflicts with pedestrians crossing in front of vehicles making turns.

Application

The LPI should be used at intersections with high volumes of pedestrians and conflicting turning vehicles, and at locations with a large population of elderly or school children who tend to walk slower. The LPI should be at least three seconds to allow pedestrians to cross at least one lane of traffic to establish their position ahead of turning traffic. The FHWA has determined that the LPI currently provides a crash reduction factor of 5%. Newly-installed LPIs should provide accessible pedestrian signals to notify visually-impaired pedestrians of the LPI.



Protected pedestrian signals



Pedestrian signals



NO TURN ON RED sign



NO TURN ON RED sign

Additionally, without an accessible pedestrian signal, visually-impaired pedestrians may begin to cross with the vehicular movement when motorists are not expecting them. Accessible pedestrian signals are discussed further on pages 184.

NO TURN ON RED signs should be considered when one or more of the following conditions apply:

- an exclusive pedestrian phase is provided
- LPI is provided
- poor sight distances reduce visibility
- geometry of the intersection may result in unexpected conflicts
- more than three accidents are reported in a 12-month period between pedestrians and vehicles where turns-on-red are permitted that could be prevented with this action

Considerations

- NO TURN ON RED signs can be provided at all times or by a dynamic sign that changes when pedestrians are present, by time of day, by a call made by an emergency vehicle, and/or at rail or light transit crossings.
- If concurrent phasing is provided in conjunction with NO TURN ON RED signs, there may be an increase of conflicts with pedestrians by forcing motorists to turn only when the green indication and pedestrian WALK indication overlap. At locations with high volumes of pedestrians crossing during a concurrent pedestrian phase, permitting turns on red or implementing exclusive pedestrian phasing should be considered.
- In general, concurrent pedestrian phasing should appropriately match the motor vehicle signal phasing. At intersections with high pedestrian volumes where drivers have difficulty finding gaps to turn, the green time can be intentionally extended past the DON'T WALK indication in order to allow the turning movement.
- Intersections with LPIs should be accompanied by appropriate signage, such as TURNING VEHICLES YIELD TO PEDESTRIANS.
- In addition to LPIs and NO TURN ON RED signs, bicyclists traveling in the same direction as pedestrians may be provided a leading bicycle interval using a bicycle signal head.

KEY BICYCLE TREATMENTS

The majority of motor vehicle crashes involving bicycles occur at intersections. In Texas, on-street bicycles are operating vehicles and are required to follow the same rules of the road as motorists. Yet intersection designs traditionally do not take into account the needs of bicyclists. Well-designed intersections that make bicycling more convenient and attractive, minimize delay, reduce conflicts with motor vehicles and pedestrians, and contribute to reduced crashes and injuries are of critical importance in order to increase bicycling in the City of Dallas.

The following principles are applied to intersection design in order to accommodate bicyclists:

- Provide a direct, continuous facility to the intersection
- Provide a clear route for bicyclists through the intersection.
- Reduce and manage conflicts with turning vehicles
- Provide signal design and timing to accommodate bicyclists, based on an engineering study.
- Provide access to off-street destinations.

Guidance on different types of bicycle facilities such as bicycle lanes and cycle tracks is covered in Chapter 5.

Intersection improvements for bicycles should be considered during all roadway improvement projects, street redesign, and safety improvements or upgrades. All bicycle-related improvements should be coordinated with the 2011 Dallas Bike Plan.

Design of bicycle facilities should be based on the following manuals:

- NACTO Street Design Guide
- NACTO Bikeway Guide
- TMUTCD
- AASHTO

Bicycle Lanes at Intersections

Description

Bicycle lanes provide a dedicated space for bicyclists to predictably ride along roadways and at intersections. When designing intersections for bicyclists, the approaches should be analyzed and designs should maintain continuity of bicycle facilities to the maximum extent possible. Streets with dedicated bicycle lanes may continue striping through unsignalized and complicated intersections to provide additional guidance and safety measures for bicyclists. This design principle is especially important at intersections where there are conflicting vehicular movements, unsignalized crossings, and/or crossings of more than four moving traffic lanes. Signalized intersections may not require striping through each intersection, and should be evaluated on a case-by-case basis.



Bike lane at signalized intersection



Well-designed intersections make bicycling more convenient and attractive.



Bike lane at intersection



Bike lane at intersection

Application

- Standard details for bicycle lane markings at intersections are provided in the TMUTCD and AASHTO Guide for the Development of Bicycle Facilities.
- Dedicated bicycle lanes should be provided on all major intersection approaches on street types that support on-street bicycle lanes and are recommended in the Dallas Bike Plan. For higher speed roadways, dedicated bicycle lanes may not be well-suited for the context and land-use of the street type, and grade separated cycle tracks or off-street facilities may be more appropriate. Also, shared lane markings may be appropriate on residential, lower volume roadways.
- At intersections with a dedicated right-turn lane, bicycle lanes should be provided to the left of the right-turn-only lane, unless bicycle signals and dedicated phasing is provided.

Considerations

- Bicycle lane markings—including green colored pavement, shared lane markings, dashed bicycle lane lines, and signage—may be provided through intersections, per engineering judgment.
- Selective removal of parking spaces may be needed to provide adequate visibility and to establish sufficient bicycle lane width at approaches to intersections.
- Shared lane markings may be used where space is not available for bicycle lanes at intersections.
- Although the minimum recommended width of a bicycle lane is 5', four-foot bicycle lanes may be considered at constrained intersections in order to provide a dedicated space for bicyclists, per engineering judgment.
- Bicycle lanes at the entrance and exit of a roundabout should allow direct access to a shared use bicycle/pedestrian path around the perimeter of the roundabout through properly designed ramps. They should also enable bicyclists to mix with traffic and proceed through the roundabout as a vehicle.

Bicycles at Signalized Intersections

Description

Bicycles have different operating characteristics than motor vehicles, and special considerations are necessary to design traffic signals that serve both motorists and bicyclists. In general, bicyclists have slower acceleration and velocity than motorists. To offset this disadvantage, traffic signal design should include considerations of minimum green intervals, clearance time, and extension time to ensure that bicyclists can safely traverse Dallas' intersections. Signal progression should balance the needs of all users with appropriate design speeds and traffic signal coordination settings. Appropriate signal timing also can minimize cyclist delay, discourage red-light running, and reduce potential crashes.

Application

Where actuated signals (loop or video detectors) are present, the signal system should detect bicycles as well as motor vehicles. In order for bicyclists to prompt the green indication at these intersections, loop or video detectors should be adjusted to detect bicycles, or separate bicycle-detectors should be installed.

- Detection devices should be located within bicycle lanes or bicycle boxes, marked with a bicycle detector symbol, and supplemented by appropriate signage according to the TMUTCD.
- When it is not feasible for the detection device to be located within the bicycle lane or bicycle box, detection devices should be located prior to the stop bar and span an appropriate distance to provide for left-, through-, and right-turning bicyclists.
- Bicycle signals can be used to separate conflicting movements, provide leading bicycle intervals, provide controls at shared-use paths, or to accommodate an exclusive left-turn phase.

Considerations

Please reference the latest edition of the AASHTO Guide for the Development of Bicycle Facilities for more details on the signal timing needs of bicyclists at intersections.

Special attention should be given to signal timing at locations with higher vehicular speeds and longer crossing distances. At these locations bicyclists are more likely to have different signal timing needs than motorists.

Bicycle signal heads can be used to provide dedicated signal indications to bicyclists and should be positioned to maximize visibility to bicycle traffic. They should be coordinated with pedestrian and non-conflicting vehicular movements to increase safety and minimize overall delay. Bicycle signal heads should be installed on a case-by-case basis determined by an engineering study.

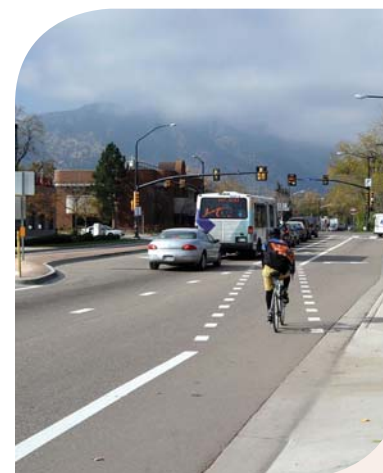
Bicycle Boxes

Description

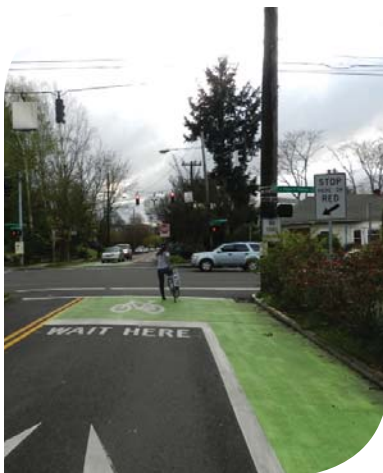
A bicycle box is dedicated space located between the crosswalk and the motor vehicle stop line used to provide bicyclists a dedicated space to wait during a red light at signalized intersections. Placing bicyclists ahead of stopped vehicular traffic at a red light improves visibility and reduces conflicts among all users. Bicycle boxes also provide bicyclists a head start to get through an intersection, which aids in making difficult turning movements and improves safety and comfort due to the difference in acceleration rates between bicycles and motor vehicles. In all cases, the bicycle box places bicyclists in front of motor vehicles, allowing them to “claim the lane” if desired. Bicycle boxes also provide more space for multiple bicyclists to wait at a red light, as opposed to being constrained to a 5’ wide bicycle lane.



Bike lane and buffer transition at intersections



Bike lane with right-turn lane



Bicycle box at an intersection



Bicycle box at an intersection

Application

In locations with high volumes of turning movements by bicyclists, a bicycle box should be used to allow bicyclists to shift towards the desired side of the travel way. Depending on the context of the bicycle lane—left or right side of the road—bicyclists can shift sides of the street to align themselves with vehicles making the same movement through the intersection.

In locations where motor vehicles can continue straight or turn right and cross a right side bicycle lane, the bicycle box allows bicyclists to move to the front of the traffic queue and make their movement first. This minimizes conflicts between the right turning motorist and the bicyclist. In order to successfully minimize this conflict, right-turn-on-red movements should be prohibited.

Considerations

- Bicycle boxes are currently an experimental treatment that requires TxDOT and FHWA approval.
- Bicycle box design should be supplemented with appropriate signage according the latest version of the TMUTCD.
- Where right-turn-only lanes for motor vehicles exist, bicycle lanes should be designed to the left of the turn lane. If right-turn-on-red is desired, consider ending the bicycle box at the edge of the bicycle lane to allow motor vehicles to make this turning movement.

Cycle Tracks at Intersections

Description

Cycle tracks provide an exclusive travel way for bicyclists alongside roadways separate from motor vehicle travel lanes, parking lanes, and sidewalks. This separation may increase comfort for bicyclists, but at intersections this can create a false sense of security and decrease visibility between all modes. Cycle track designs at intersections must manage conflicts with turning vehicles, and increase all users' visibility.

Application

Increasing visibility and awareness are two key design goals for cycle tracks at intersections. Parking restrictions between 20' and 40' minimum should be provided at the near- and far-side of intersections. Additional space may be needed based on sight distance calculations.

If possible, cycle tracks should be routed behind transit stops (i.e., the transit stop should be between the cycle track and motor vehicle travel lanes). If this is not feasible, the cycle track should be designed to include pavement markings, rumble strips, and signage to alert the bicyclist to stop for buses and pedestrians accessing transit stops. Cycle track design often involves relocating transit stops to the far-side of the intersection to reduce conflicts.

Cycle tracks should be given priority at low-volume intersections, which can be provided by markings and signage.

Considerations

- Cycle track designs at intersections must give consideration to signal operation and phasing in order to manage conflicts with turning vehicular movements and bicyclists. Bicycle signal heads should be considered in order to separate conflicts, especially for two-way cycle tracks.
- Shared lane markings and/or colored pavement can supplement short dashed lines through intersections, where engineering judgment deems appropriate.

At non-signalized intersections, design options to increase visibility and safety include the following:

- warning signs
- raised intersections
- special pavement markings (including green surface treatment)
- removing parking prior to the intersection

Consider narrowing or designing a chicane for cycle tracks at intersections to slow bicycle traffic. Another option is to remove the separation prior to the intersection and provide standard bicycle lanes with bicycle boxes to raise awareness and increase visibility.



Bike lane at intersection

KEY TRANSIT TREATMENTS

The following transit treatments should be considered for streets on the Transit Overlay Map, shown in Chapter 2. When designing intersections to accommodate transit vehicles, key goals are to improve the reliability and efficiency of transit service. Waiting at traffic signals accounts for at least 10% of overall bus trip time and up to 50% or more of bus delay.

Dallas Area Rapid Transit (DART) operated 135 bus routes and over 655 buses which provided about 38 million passenger trips in fiscal year 2010. A majority of DART's transit stops are located near intersections on Dallas' streets. This section covers design strategies to improve transit operations and reduce delay for transit vehicles at intersections. Design guidance on the individual bus stops and connections to the sidewalk are discussed in Chapter 4.

While individual strategies can be implemented independently, a combination of strategies including the appropriate location of the bus stop and traffic signal prioritization will be the most effective. Implementation of these strategies should also be complemented by operational improvements being carried out by DART, including smart fare payment systems and real-time tracking.



Raised intersection on the Katy Trail



Downtown transfer station



Transit service can increase bicycle usage.

Bus Stop Location

Description

All bus stop locations must be ADA compliant, as well as being safe, convenient, well-lit, and clearly visible. Proper spacing and siting of bus stops involves many considerations, such as the bus route, population density, popular destinations, transfer locations, intersection operations and geometry, parking restrictions, and sight lines.

Application

Where buses are required to pull out of traffic, bus stops should be located at the near- or far-side of intersections wherever possible, and not at midblock locations. Mid-block bus stops require the most amount of curb side space. Intersections are also convenient for passengers because they can easily intercept other transit connections, crosswalks, pedestrian routes, and building entrances.

Where bus bulbs are provided, the length of the bus stop can be less than the prescribed minimums listed below because buses will not be required to pull out of traffic. The minimum bus stop length at bus bulbs should provide a clear and level landing pad at each door of the bus. For more information on bus bulbs, please see page 197 of this chapter.

The frequency of stops should be a balance between passenger convenience and minimizing bus travel times. Spacing is typically determined by population density, with the minimum spacing between bus stops generally about 750'.

Considerations

Selecting a location for a bus stop at an intersection depends on a variety of factors:

- available curb side space
- conditions of sidewalks
- width of sidewalks
- traffic and pedestrian volumes
- number and width of travel lanes
- turning movements
- sight distances
- presence of parking, bicycle facilities, and crosswalks

At signalized intersections, far-side placement is generally recommended. Location selection should be done on a site-by-site basis in consultation with DART and the Dallas Public Works Department.

Additional advantages of locating stops on the far-side of an intersection include the following:

- Pedestrians are encouraged to cross behind the bus, reducing conflict and bus delay.
- Buses are allowed to take advantage of gaps in traffic flow, especially with signal prioritization, rather than needing to be at the front of the queue at an intersection for a near-side stop.
- Conflicts between buses and right turning vehicles are minimized and additional right-turn capacity is provided on the near-side of the intersection.

Transit Prioritization at Intersections

Description

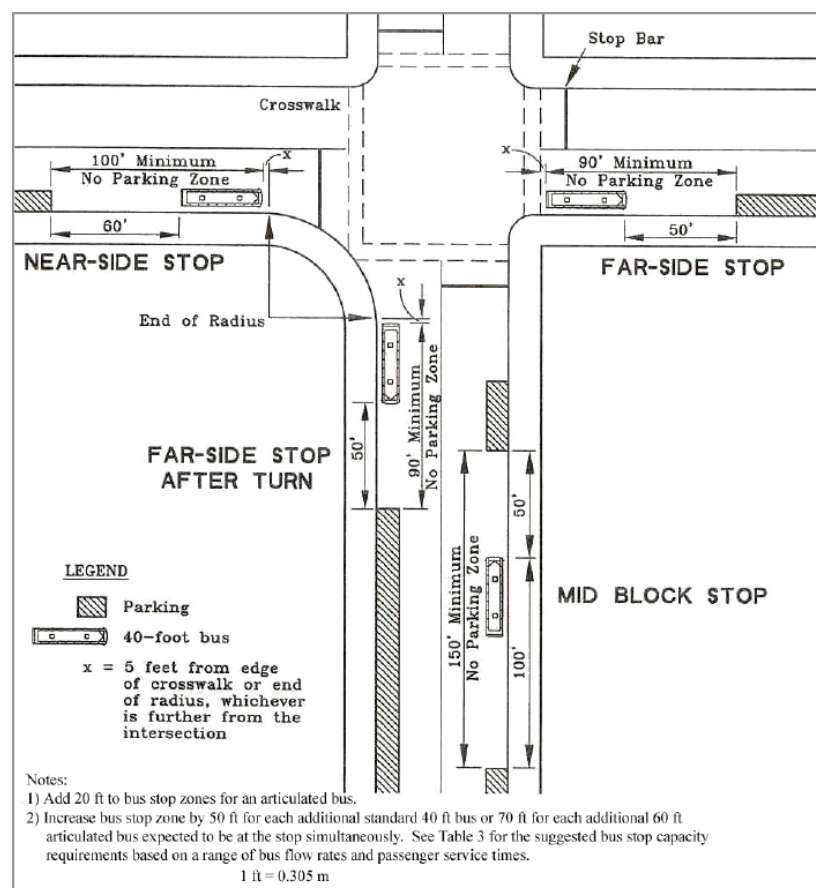
By prioritizing transit at intersections, service can become more reliable, efficient, and environmentally friendly due to less queuing and stopping and starting. This makes transit a more attractive mode of transportation. Transit prioritization strategies include signal coordination, signal priority, transit only lanes, and queue jump or bypass lanes.

The first strategy for improved traffic flow is coordinated signal timing. In addition to signal coordination, transit signal priority enables transit vehicles to shorten or extend a traffic signal phase without disrupting the phase sequence or overall signal timing.

Transit-only lanes at intersections provide transit vehicles a dedicated space to bypass traffic, and can typically be shared with bicyclists. Queue jump or bypass lanes are specially designated transit lanes at intersections that share a similar idea to the leading pedestrian interval discussed on page 183. Queue jump lanes provide an early green signal, or hold a green signal, for transit vehicles while other vehicles traveling in the same direction are given a red light.

Application

Signal coordination can reduce delay for transit as well as motor vehicles. In addition to coordination, signal priority for transit vehicles allows transit to stay on schedule during peak hours when there is congestion. Signal priority allows delay to be reduced by extending the green time for an approaching bus or shortening time for the opposing movements for a waiting bus. The difference in the time can be made up in the next cycle of the signal, but all other signal operations can remain intact. All transit signal prioritization must be coordinated with DART and the Dallas Traffic Management Center.



Typical types of, and dimensions for, on-street bus stops

Signal coordination and signal priority can be used with or without the presence of dedicated transit only lanes along a corridor or queue jump. Queue jump lanes can be used at intersections without a bus stop as well as with one at either the near- or far-side, so long as there is enough space on the roadway.



Near-side bus stop on Jefferson Street



Near-side bus bulb on Hillcrest

Placement	Definition	Advantages	Disadvantages
Far-side Bus Stop	The bus stops immediately after passing through an intersection	This type of stop minimizes conflicts between buses and vehicles turning right. It also encourages pedestrians to cross behind the bus.	An increase in the number of rear-end crashes may occur, since drivers do not expect buses to stop again after stopping at a red signal indication. Also, the traffic stopped behind the bus could queue into the intersection.
Near-side Bus Stop	The bus stops immediately prior to an intersection	Patrons can board and alight while the bus is stopped at a red signal intersection, and the bus driver has the width of the intersection available for pulling away from the curb.	Stopping at the near-side of an intersection can increase conflicts with right-turning vehicles and could limit sight distance to curb side traffic control devices and crossing pedestrians.
Midblock Bus Stop	The bus stops within the block	It can minimize intersection sight distance restrictions for vehicles and pedestrians.	It encourages patrons to cross the street at midblock, and it could increase walking distance.

Considerations

- Providing a queue jump lane with a leading signal phase must take into consideration the overall signal cycle lengths and impacts to delay for other users.
- If space is not available for a queue jump lane or bypass lane, consider using a right-hand turn lane to double as a bus advantage lane by allowing buses to move up in the queue at a signal where right- turn-on-red is permitted. If right-turn lanes are used, appropriate signage such as RIGHT LANE MUST TURN RIGHT must be accompanied by EXCEPT BUSES placards.
- Transit signal priority should be considered on all priority transit routes, as shown on the Transit Overlay map.
- Transit signal priority studies should be conducted to understand the impact to traffic on cross streets of the transit route.
- Signal coordination should not increase delay for other modes, and should take into consideration the acceleration rates and speeds of bicyclists.
- Transit agencies must train employees on how to handle bus and bicycle interactions in transit- and bus-only lanes.
- Transit priority may be considered for late buses only in order to keep on schedule.

Bus Bulbs

Description

Bus bulbs are curb extensions along the length of a bus stop that eliminate the need for buses to pull in and out of traffic. Similar to normal curb extensions found at intersections, bus bulbs have the same advantages of reducing crossing distances for pedestrians and providing additional space for street furniture such as bus shelters, landscaping, and pedestrian queuing.

Application

Bus bulbs are only appropriate on streets where on-street parking is present. Bus bulbs are most appropriate at stops with higher passenger volumes or where it is desired to eliminate buses pulling out of traffic.

Bus bulbs are effective in enforcing parking restrictions within bus stops and do not require as much space as curb side stops because the bus does not need space to pull in and out of the stop. They may, however, cause occasional traffic delay behind them. Bus bulbs will be installed on a case-by-case basis determined by an engineering study.

Considerations

- Since the bus remains in the travel lane while stopped, bus bulbs can result in traffic delays or unsafe maneuvers by drivers and bicyclists to steer around buses. Designs must consider the street type, number of travel lanes, and headways of buses.
- Bus bulbs can interfere with right-turning vehicle movements at near-side intersections.
- Bus bulbs are most effective at reducing travel time if they are utilized throughout a corridor by eliminating the need for buses to pull in and out of traffic all together.

Landscape areas within bus bulbs also offer opportunities for rain gardens.



Transit only lanes on Harry Hines Blvd



Midblock bus stop





7. GREEN STREETS



Complete Streets can reduce demand on existing infrastructure by incorporating stormwater management into street designs



7. GREEN STREETS



Integration of green street features

Green Streets are defined as urban transportation rights-of-way that provide source control of stormwater, limit its transport and pollutant conveyance to the collection system, and provide environmentally enhanced roads. Green streets improve water quality through the integration of stormwater treatment techniques, which use natural processes and landscaping. All of this works to reduce the heat island effect.

Currently, Dallas and other North Texas communities manage stormwater through a large system of drainage infrastructure that directs runoff back into the water system. This system requires a large capital outlay to build and maintain over time, with needs increasing exponentially as development increases impervious surfaces.



Integration of green street features



Integration of green street features



Bioswale

Cities across the country are looking for more sustainable solutions to handle stormwater. The most efficient and cost effective way to manage stormwater is to collect it where the water falls. Many cities are shifting their thinking on how to manage stormwater and are replacing conventional stormwater infrastructure with green infrastructure within the street right-of-way, as well as on development sites. These types of stormwater filtering and holding systems allow water to infiltrate into the soil instead of rushing into storm sewers and streams, carrying a toxic mixture of pollutants and chemicals. This type of treatment potentially reduces infrastructure costs, as more water is treated and filtered at the source. By using bioretention areas, permeable surfaces, bioswales, and other green techniques, roadways can be built to help reduce runoff into the stormwater system.

This chapter summarizes some of the techniques that can be used when developing buffers, sidewalks, paths, parking areas, medians, and other street facilities as described in Chapters 4, 5, and 6.

The North Central Texas Council of Governments' Green Streets program was used as a source for some of the materials and photos in this chapter.

GENERAL POLICY GUIDANCE

Green Streets

This chapter:

- introduces the application of Integrated Stormwater Management design practices within the roadway right-of-way to achieve Complete Streets objectives
- creates a link between the Complete Streets Manual and the City of Dallas' Drainage Design Manual
- provides an introduction and overview to Stormwater Management design practices that apply to roadways.

Development and redevelopment increase the amount of imperviousness in our surrounding environment. This increased imperviousness translates into loss of natural areas, more sources for pollution in runoff, rapid water flows, and heightened flooding risks. To help mitigate these impacts, more than 60 North Texas governments are cooperating to proactively create sound stormwater management guidance for the region through the Green Streets Program. It will help to implement low impact development concepts throughout the region. The City of Dallas will be incorporating Integrated Stormwater Management practices in the Paving and Drainage Design Manuals.

BENEFITS OF GREEN STREETS ELEMENTS

Green street elements reduce the need for stormwater infrastructure. Greenscape practices provide trees, shrubs, grasses, and other landscape plantings that play an important role in making streets inviting, comfortable, and sustainable. Used appropriately, they can help define the character of a street or plaza, provide shade and cooling in strategic locations, reduce energy consumption in buildings, and absorb and clean stormwater. They also absorb greenhouse gases and help filter airborne pollutants.

In addition to providing environmental benefits, a healthy greenscape provides psychological and social benefits. Plants help reduce stress and restore a sense of calm and focus.

Studies have shown that people are attracted to places that have well-maintained plantings. Healthy greenscapes are good for City life and business.



Tree boxes



Tree boxes



Integrating these practices into the design of a neighborhood can greatly increase the appeal of the street, while improving drainage.



Green streets

INTEGRATED SITE DESIGN PRACTICES

Review of Green Streets projects is incorporated into the street development process discussed in Chapter 1. The process follows the planning, conceptual design, and engineering phases outlined for roadway projects. The process is detailed in the Paving and Drainage Manuals.

Implementing stormwater management practices begins with the site planning and design process. Development projects can be designed to reduce their impact on watersheds when careful efforts are made to conserve natural areas, reduce impervious cover, and better integrate stormwater treatment. By implementing a combination of these non-structural approaches, it is possible to reduce the amount of runoff and pollutants that are generated from a site and provide for some nonstructural on-site treatment. The integration of site design includes the following:

- Managing stormwater (quantity and quality) as close to the point of origin as possible and minimizing collection and conveyance
- Preventing stormwater impacts rather than mitigating them
- Utilizing simple, non-structural methods for stormwater management that are lower cost and lower maintenance than structural controls
- Creating a multifunctional landscape
- Using hydrology as a framework for site design
- Reducing the peak runoff rates and volumes, and thereby, reducing the size and cost of drainage infrastructure

Integrated site design for stormwater management includes a number of site design techniques such as preserving natural features and resources, effectively laying out the site elements to reduce impact, reducing the amount of impervious surfaces, and utilizing natural features on the site. The aim is to reduce the environmental impact by filtering water through vegetation and soil while retaining and enhancing the owner or developer's purpose and vision for the site. Many of the integrated site design practices can reduce the cost of infrastructure while maintaining or even increasing the value of the property.

Operationally, economically, and aesthetically use of integrated site design practices offers significant benefits over treating and controlling runoff downstream. Therefore, all opportunities for using these methods should be explored before considering traditional stormwater controls.

Integrated site design can reduce the volumes of runoff and pollutants that need to be conveyed and controlled on a site. In some cases, the use of integrated site design practices may eliminate the need for structural controls entirely.

The level of impervious cover—i.e., rooftops, parking lots, roadways, and sidewalks—is an essential factor to consider in integrated site design for stormwater management. Increased impervious cover means increased stormwater generation and increased pollutant loadings.

Thus, by reducing the area of total impervious surface on a site, a site designer can directly reduce the volume of

stormwater runoff and associated pollutants that are generated. It can also reduce the size and cost of necessary infrastructure for stormwater drainage, conveyance, and control and treatment. In some cases the use of integrated site design practices may eliminate the need for stormwater controls entirely.

Stormwater integrated site design also has a number of other ancillary benefits:

- reduced construction costs
- increased property values
- more open space for recreation
- more pedestrian friendly neighborhoods
- protection of sensitive forests, wetlands, and habitats
- more aesthetically pleasing and naturally attractive landscape
- easier compliance with wetland and other resource protection regulations

The integrated site design practices and techniques covered in this manual are grouped into four categories:

Integrated Site Design Practices and Techniques

- preserve natural features and resources
- preserve undisturbed natural areas
- preserve riparian buffers
- avoid floodplains
- avoid steep slopes

Lower Impact Site Design Techniques

- fit design to the terrain
- locate development in less sensitive areas
- reduce limits of clearing and grading
- utilize open space development
- consider creative designs



Bioretention area



Bioswale



Reduced Parking Footprint

Reduction of Impervious Cover

- reduce roadway lengths and widths
- reduce building footprints
- reduce the parking footprint
- reduce impervious setbacks and frontages
- use fewer culs-de-sac
- create parking lot stormwater islands

Utilization of Natural Features for Stormwater Management

- use buffers and undisturbed areas
- use natural drainageways instead of storm sewers
- use vegetated swales instead of curb and gutter
- drain rooftop runoff to pervious areas

More detail on each site design practice is provided in the Paving and Drainage Design Manuals.

The integrated site design practices may be subject to other ordinances within a municipality and could require approval before implementation. All relevant materials should be reviewed before developing a site plan.



Bioswale

PAVEMENT

Porous/Permeable Pavement

Description

Permeable paving materials allow stormwater runoff to infiltrate through the material, unlike traditional paving materials that divert runoff to the storm sewer system. Water permeates through the material into the ground and recharge the water table or local waterway. Permeable materials filter pollutants, reduce flow rate, improve water quality, and reduce the volume of infrastructure necessary to direct and convey stormwater offsite. Parts of Dallas have high-plasticity soil and, therefore, the use of permeable pavements require special consideration.

Permeable pavements are typically underlaid with an infiltration bed and subgrade soil. Permeable materials come in five basic varieties:

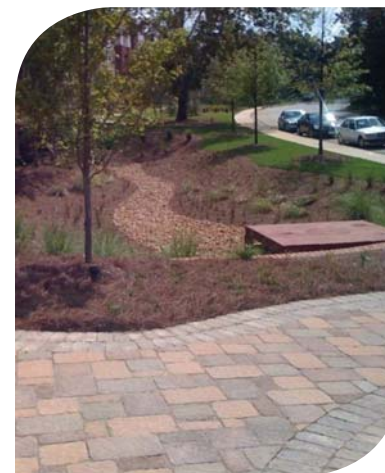
- soft paving such as grass, bark, mulch, crushed shells, and loose aggregate (gravel)
- permeable concrete paving, created by mixing concrete with fewer fine particles, creating void spaces that allow air and water to navigate throughout the material or porous asphalt
- open joined and open cell unit pavers filled with porous aggregate or turf
- plastic grid systems covered with pavers, soil and grass, or gravel
- bound resin with aggregates or bound recycled material such as glass, rubber, and plastic

Application

Permeable paving can be utilized in a broad variety of settings. All designs must consider the drainage characteristics of the underlying soils, the depth of the water table, and the slope of adjacent land.

Permeable pavements can be used in sidewalks, plazas, cafes, overflow parking areas, emergency access roads, and other low-traffic areas.

Soft paving materials and loose aggregate are only appropriate for the greenscape/furnishing zone or frontage zone typically around trees, planters, and enclosed greenscape elements.



Use of natural drainageways



Reduction of impervious cover in a parking lot



Street trees



Tree wells

Permeable concrete pavement can be used in the pedestrian zone, as long as the resulting surface is smooth, stable, slip resistant, and meets all other accessibility guidelines.

Porous unit pavers that utilize gaps are only appropriate in the greenscape/furnishing zone or the frontage zone (except where there is active pedestrian use).

In specific locations where infiltration is not desired, such as adjacent to building foundations, a geo-textile liner can prohibit infiltration and redirect discharge to an appropriate location while still providing the other benefits of permeable paving. Porous pavements will likely be designed with a liner to separate the infiltrated runoff from the subbase and in situ soil. A perforated pipe can be used to drain the pavement. Design guidance for porous pavement will be included in the paving and drainage manuals.

Considerations

Permeable pavements provide increased traction when wet because water does not pool, and the need for salt and sand is reduced during winter due to low/no black ice development. Nevertheless, permeable paving requires regular maintenance including the following:

- annual inspection of paver blocks for deterioration
- periodic replacement of sand, gravel, and vegetation
- annual vacuuming of pavements to unclog sand and debris (Note: The use of sand in ice prevention should be avoided because it will clog pavement pores.)

PERMEABLE ASPHALT/CONCRETE

Description

Permeable concrete is a concrete mixture using minimal cementitious materials to coat the aggregate, using little or no sand, leaving substantial void content through which water can drain. Porous asphalt is mixed at conventional asphalt plants, but fine aggregate is omitted from the mixture. The remaining large aggregate particles leave open voids that lend the material its porosity.

Application

Permeable asphalt and concrete should be used on a level street above the high water table with low pedestrian traffic and no vehicular encroachment. There must be adequate subsurface conditions to detain stormwater.

Considerations

This is not appropriate for use where there is water-sensitive subsurface infrastructure, or where there is the potential for soil contamination since porosity can convey harmful materials to the soil. Pervious concrete is not intended for use at greater than 5 percent slope. Routine vacuuming of the surface may be necessary to maintain porosity.

Special features, such as the underlying stone bed, are more expensive than conventional pavements, but these costs are often offset by the elimination of many elements of a conventional storm drain system.

Permeable Brick Pavers

Description

Permeable brick pavers enable stormwater to filter into the soil instead of draining into storms and rivers. They differ from conventional pavers in that they create more spacing between the pavers – a higher void area – which allows water to infiltrate through the pavement surface. The support system should consist of coarser aggregates than found in conventional construction.

Application

Commercial and residential applications are available and both can meet ADA requirements.

Considerations

A system that uses permeable pavers can help developers obtain LEED credits.



Permeable brick pavers

LANDSCAPING

Bioretention

Description

Bioretention areas are shallow stormwater basins or landscaped areas that utilize engineered soils and vegetation to capture and treat runoff. Bioretention areas function like stormwater planters, but generally have fewer structural elements. They may appear more like conventional landscaped areas, but are depressed rather than elevated from the surrounding area. They can be used in areas where a more natural, garden aesthetic is desired. Bioretention areas feature high pollutant removal and good absorption of wind, noise, and sunlight.

The following are typical requirements of the City of Dallas:

- Shrubs may not exceed 30 inches in height
- Tree canopies must be greater than 8 feet above sidewalks and 17 feet above streets
- Tree trunks must be at least 5 feet from the curb
- Bermuda is the recommended grass for medians and parkways

Application

Bioretention areas are commonly used in residential areas and urban settings with planting room, such as bulbouts, medians, and landscape areas. They are often larger and more diverse in plant community than planters. A maximum contributing drainage area of less than two acres is recommended.



Bioswale



Infiltration Trench



Infiltration Trench

Considerations

- Bioretention areas have very small drainage areas.
- They provide flexible siting and are good for highly impervious areas.
- Bioretention areas are good options for retrofits.
- They require relatively low levels of maintenance.
- They do, however, require extensive landscaping if in public areas.
- A maximum contributing drainage area of less than two hours is recommended.

Infiltration Trenches

Description

An infiltration trench is an area of soil that is covered with mulch, ground cover, grass, trees, or other plantings. Trenches are generally located in the furnishing zone, though they can also be located in the frontage zone. For stormwater benefits, the sidewalk should be pitched toward the open trench. Nonlinear open areas can also be used for planting trees in clusters. Trees planted in open trenches and areas with a sufficient amount of soil that is not compacted have the greatest chance of surviving and thriving in an urban environment.

Application

Curbside open trenches are commonly used on neighborhood residential street types. Wide trenches provide sufficient rooting volume while maintaining appropriate sidewalk clearances. The trench should be filled to sidewalk level to avoid creating a tripping hazard.

Considerations

Areas with heavily-used, high-turnover curbside parking are not ideal for open trenches, as the soils become compacted over time and will need to be replaced. Consideration should be given to planting bare-root trees where permissible.

Planter Boxes

Description

Planter boxes are precast concrete boxes, filled with bioretention type soil media, installed below grade at the curb line. A standard street tree or shrub is planted in the box, which resembles a curbside planter. Planter boxes are located upstream of a standard curb inlet. For low to moderate flows, stormwater enters through the planter box's inlet, filters through the soil, and exits through an underdrain into the storm drain. For high flows, stormwater will bypass the planter box filter if it is full and flow directly to the downstream curb inlet.

There are several variations of this basic design. The contained planter box receives only rainfall, which filters through the soil and is then either taken up by its vegetation or allowed to seep out the bottom of the planter to the pavement or sidewalk. The infiltration planter box can receive both rainfall and runoff, which eventually filters through the bottomless planter and enters the underlying soil. The flow-through planter box collects flow in a perforated pipe along the bottom of the box and discharges out the side of the planter into a storm sewer.

Application

Planter boxes are used on impervious surfaces in highly urbanized areas to collect and detain/infiltrate rainfall and runoff. The boxes may be prefabricated or constructed in place and contain growing medium, plants, and a reservoir. Planter boxes should not be used for stormwater containing high sediment loads, to minimize clogging potential. These are intended for space-limited applications, or for areas requiring additional pollutant removal capability.

Considerations

- The contained planter is not tied into underlying soil or pipes and can therefore be placed almost anywhere and moved when needed. However, it does not have a reservoir to provide additional storage for flow control. Care should also be used in placing it next to building foundations and heavy pedestrian traffic areas.
- The infiltration planter should not be used next to foundations and underlying soils must drain rapidly enough to avoid ponding.
- The flow-through planter can be used next to building foundations since it directs flow off to the side and away from the building. It must be located next to a suitable discharge point into the stormwater conveyance system.
- Vegetation will require frequent maintenance.
- Filter may require more frequent maintenance than most of the other stormwater controls.



Planters contained with bulb-outs provide a road diet



Tree depression



Tree box filter

Enhanced Swales

Description

Enhanced vegetated swales are linear bioretention areas that convey runoff that can be used to augment traditional pipe and gutter systems. They do this by slowing runoff velocity, filtering stormwater pollutants, reducing runoff temperatures, and—in low volume conditions—recharging groundwater.

Application

Grasses are the most common plants in vegetated swales. Check dams, placed periodically along the length of the swale, slow runoff and promote infiltration. The bottom width of the swale should be 2 to 8 feet with side slopes 4:1 recommended.

Considerations

Plant selection should reflect maintenance capacity, stormwater sources, and context. Low flow conditions may require a 100-year overflow path and engineered section. Combine stormwater treatment with runoff conveyance system. Swales are less expensive than curb and gutters, however, maintenance costs may be higher.



Landscaped median

Landscaping in Medians

Description

Landscaped medians are an effective way to improve the safety and accessibility of arterial streets. For pedestrians, landscaped median decreases the total crossing width of the street. Additionally, it gives the street a more natural, shaded appearance. Bioretention areas can be located in depressed landscaped medians.

Application

Landscaped medians are most useful on high volume, high speed roads.

Considerations

Landscaping in medians should not obstruct the visibility between pedestrians and approaching motorists.

Landscaped medians should be at least 6 feet wide to allow enough room for a pedestrian and a wheelchair to meet within a pedestrian refuge while crossing the street.

Desired turning movements need to be carefully provided so that motorists are not forced to travel on inappropriate routes such as residential streets, or an unsafe U-turn condition is not created.

Underground Detention

Description

In relatively dense urban areas where a large percentage of the landscape may already be developed, underground facilities may be the most practical way to achieve substantial flow volume and rate reductions. Although costs for constructing underground storage practices may be high, it may be the most economical way to detain stormwater in urban settings where land values are high. There are a number of types of underground storage available. In the simplest system, oversized pipes replace standard pipes in a storm drain, providing temporary storage of water. More storage can be achieved by using a series of interconnected pipes or a single large storage vault.

Application

The use of underground stormwater storage would be driven by project economics. Land cost, cost to remedy an inadequate receiving stormwater system, or some other unique condition or opportunity would warrant the typically substantial additional cost to construct such a system.

Considerations

Underground storage is effective for reducing stormwater runoff however little reduction of sediments or pollutants occurs without supplemental means to filter stormwater. The size of the system will largely be defined by the amount of stormwater to detain, the size of the site, and the elevation of tie-in points. Large continuous areas are more suited to large vault-type systems, while more linear, angular sites are better suited for pipe-based system. Construction materials are influenced by the usable depth and size of the site. Sites requiring more shallow construction should use pipes, because corrugated steel and plastic must be surrounded by more fill.

Pavement Design Considerations

Many Green Street applications and Green Streets Best Management Practices (BMPs) will introduce water to the subgrade adjacent to and below the roadway and sidewalk pavements. Our North Texas soils are very sensitive to changing moisture conditions, and those soils can create forces that will destroy the strongest pavements far short of their expected design life. Use of Green Streets BMPs and other penetrations into a traditionally designed watertight 'cap' over a moisture conditioned subgrade will require that innovative pavement design be performed where these methods are applied. The use of liners, geomembranes, underdrains, and other methods may be required to mitigate the introduction of additional sources of water into the pavement subgrade. Design guidance for porous pavement will be included in the Paving and Drainage Manuals.



Detention storage located in underground pipe and tank systems.



Stormwater can be retained and filtered in the medians of boulevards

APPENDIX

1. Complete Streets Design Review Checklist
2. Implementation Plan
3. Knox Street Demonstration Evaluation
4. Proposed Major Thoroughfare Plan Change Priorities
5. Potential Revisions to Manuals

Photo Credits

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COMPLETE STREETS DESIGN REVIEW CHECKLIST

The Complete Streets Design Review Checklist is intended for project sponsors to provide information about how bicyclists and pedestrians will be accommodated in the planning and design of transportation projects. Download the Dallas Complete Streets Design Review Checklist and fill it out as thoroughly as possible as the first step in initiating a Complete Streets project in the City of Dallas. This checklist will be used in initiating both public and private Complete Streets projects.

Step 1: Identify the street typology on the Dallas Complete Streets Vision Map.

- Mixed Use
- Commercial
- Residential
- Industrial
- Parkway

Step 2: Fill out the following questions to the extent possible.

Project Information

- a. Project Name:
- b. Project Budget/CIP Funding (if applicable):
- c. Design Completion (%):
- d. Lead Agency/Entity:
- e. Contact(s):
- f. Partner Agencies/Entities:
- g. Project Location
 - County (Dallas, Collin, Denton):
 - City Council District:
 - Special District(s) (Planned Development, PID, TIF, etc.) list all that apply:
 - TxDOT Highway Designation (if any)

- h. Project Area (precise street limits):
- i. Project Goals:
- j. Project Scope:
- k. Project History & Impetus:
- l. Project Budget:
- m. Funding Sources:
- n. Dates started/Anticipated to Start:

- Conceptual Planning:
- Preliminary Design:
- Final Design:
- Construction:

- o. Context Zone:

Describe the type of context zone that best describes corridor. Is it: Natural, Rural, Suburban, General Urban, Urban Center, Urban Core or a Special District? Definitions can be found on page 49 of Designing Walkable Urban Thoroughfares: A Context Sensitive Approach, An ITE Recommended Practice.

- p. History & Character:

Describe any special history or background of the area along the corridor that will assist in understanding the context of the project.

q. Land Use:

Describe the predominant land uses and densities within the project area, including any historic districts or special zoning districts, and the compatibility of the proposed design with these.

r. Major Sites:

Describe any major sites, destinations, and trip generators within or proximate to the project area, including prominent landmarks, commercial, cultural and civic institutions, educational facilities and public spaces, and how the proposed design can support these sites.

2. Operations

a. Walking:

Describe existing walking conditions within the project area, desired future conditions, and how the proposed design addresses walking conditions, including pedestrian safety, volumes, comfort and convenience of movement, important walking connections, and quality of the walking environment.

b. Bicycling:

Describe existing bicycling conditions within the project area, desired future conditions, and how the proposed design addresses bicycling conditions, including bicyclist safety, volumes, comfort and convenience of movement, existing or proposed bike routes and other important bicycling connections, and bicycle parking.

c. Motor Vehicles:

Describe existing motor vehicle conditions within the project area, desired future conditions, and how the proposed design addresses motor vehicle conditions, including motor vehicle safety, volumes, access, important motor vehicle connections, appropriateness of motor vehicle traffic to the particular street (e.g., local versus through traffic) and reducing any negative impacts of motor vehicle traffic. Provide specific traffic information including average daily traffic volumes, number of injuries and fatalities on corridor for all modes, posted speed, average speed (if available), and list of all corridor “hot spots” (where

crashes are the highest)

d. Transit:

Describe existing transit conditions within the project area, desired anticipated future conditions, and how the proposed design addresses transit conditions, including bus routes and operations, light-rail, commuter rail or other transit station access, and supportiveness of transit usage and users.

e. Trucks/Freight/Emergency Vehicles:

Describe existing truck conditions within the project area, desired future conditions, and how the proposed design addresses truck conditions, including truck routes, safety, volumes, access, and mobility and reducing the negative impacts of truck traffic.

f. Access:

Describe how the proposed design addresses the needs of those with increased access or mobility requirements such as the disabled, elderly, and children, including ADA compliance and any school or senior safety zones within the project area, if applicable.

g. Curbside Conditions:

Describe existing curbside demand and usage patterns within the project area, desired future conditions, and how the proposed design addresses curbside conditions, including allocation of space for parking, loading, and drop-off, and pedestrian space.

h. Public Space:

Describe existing public space conditions within the project area and how the proposed design affects public space, including any proposed new public space and any new pedestrian seating or other enhancements to the public realm.

i. Drainage:

Describe potential disturbance to existing stormwater flow patterns or existing catch basins, and /or the need for new sewer

connections.

j. Street Cuts:

Describe observed frequency of utility “cuts” into the roadway within the project area and how the proposed design addresses street cut conditions, including improvement or consolidation of utility infrastructure.

3. **Green Streets**

a. Street Trees:

Describe existing street tree coverage within the project area and how street trees are included in the proposed design.

b. Greenstreets & Plantings:

Describe any existing Greenstreets and Greenstreet opportunities within the project area and any Greenstreets or other planted areas that are included in the proposed design.

c. Stormwater Control:

Describe stormwater runoff conditions including the infiltration ability of underlying soil within the project area and what, if any, stormwater source controls area included in the proposed design.

d. Flooding:

Describe any flooding conditions within the project area and how the proposed design addresses flooding.

e. Maintenance Partner(s):

Describe any potential and/or committed public and/or private maintenance partners and level of commitment (e.g., watering, weeding, pruning, litter removal, replacements).

f. Permits:

Describe whether any wetlands or protected areas are within 100 feet of the project area and whether permits from the State of Texas or Army Corps of Engineers approvals are necessary.

4. **Paving Design Manual**

a. Materials, Lighting & Furniture:

Describe existing and proposed street materials, lighting and furniture, including paving materials; lighting poles, fixtures and levels; and street furniture.

b. Application:

Describe how the proposed design follows the guidelines of the City of Dallas Paving Design Manual in regards to overall policies and principles, street geometry, materials, lighting, and street furniture.

c. Major Deviations from Guidelines:

Where the design deviates from the guidelines or policies and principles of the manual, provide explanation.

d. Pilot Treatments:

Describe any pilot treatments being proposed, whether geometric or material treatments.

5. **Additional Information**

Provide any additional information or considerations regarding this Complete Streets project that will help the design and review teams make informed decisions.

IMPLEMENTATION PLAN

DALLAS COMPLETE STREETS INITIATIVE Implementation Strategies

Implementation Strategies Summary	
Action 1	✓ Adopt the Complete Streets Design Manual through Council Resolution
Action 2	✓ Task a Team with Effective Leadership, Resources and Responsibility for Complete Street Policy Implementation Across Departments
Action 3	✓ Enhance Inter-departmental Coordination on Street Improvement Projects
Action 4	✓ Implement Phased Thoroughfare Plan amendments
Action 5	✓ Amend Other Related Policies and Regulations for Consistency and Linkage to Complete Streets Manual
Action 6	✓ Explore Alternative Funding Mechanisms for Complete Streets Capital and Operation / Maintenance Costs
Action 7	✓ Expand Dallas' Current Offering of Neighborhood-Initiated Street Improvement Programs to Incorporate Complete Streets Design Options and Considerations
Action 8	✓ Initiate a Monitoring & Evaluation Program

Action 1	Adopt the Complete Streets Design Manual through Council Resolution
TBD	<ul style="list-style-type: none"> ✓ Acknowledge changing trends towards a more sustainable transportation system through proactive implementation of a complete streets program ✓ Establish the Dallas Complete Street Manual as the comprehensive street design policy guide and the basis for future Thoroughfare Plan and code amendments

Action 2	Task a Team with Effective Leadership, Resources and Responsibility for Complete Street Policy Implementation Across Departments
TBD	<ul style="list-style-type: none"> ✓ Assign leadership and ombudsman role to one group/department ✓ Include city departments involved in planning, design, construction, operation and maintenance of street-related improvements: <ul style="list-style-type: none"> ➤ Public Works & Transportation ➤ City Design Studio ➤ Sustainable Development and Construction ➤ Economic Development ➤ Streets ✓ Establish team responsibility to include coordination of all aspects of complete streets policy implementation: <ul style="list-style-type: none"> ➤ Street improvement projects ➤ Regulatory changes ➤ Private development project review ➤ Ongoing staff training to institutionalize complete streets design practices

Action 3	Enhance Inter-departmental Coordination on Street Improvement Projects
TBD	<ul style="list-style-type: none"> ✓ Maintain an ongoing priority list of capital improvement and resurfacing/restriping projects that are budgeted to include complete street design elements ✓ Ensure early and continual inter-departmental coordination to facilitate proper execution of complete streets design principles from corridor planning through conceptual design, engineering and construction ✓ Focus on reaching early consensus on conceptual street cross-sections and project scopes through community involvement during the corridor planning stage ✓ Conduct temporary complete street demonstrations as needed to test new street design solutions involving significant change from current conditions ✓ Ensure coordination with utility repair/replacement projects where relevant

Action 4	Implement Phased Thoroughfare Plan amendments
Initiated; Ongoing	✓ Implement a strategic work program of targeted, case-by-case thoroughfare plan amendments to incorporate complete street design changes for funded street improvement projects
TBD	✓ Amend the Thoroughfare Plan to clarify the administrative relationship with the Complete Streets Manual and to incorporate complete street typology and policy guidance
TBD	✓ Undertake a comprehensive, citywide update of the Thoroughfare Plan functional and dimensional classifications

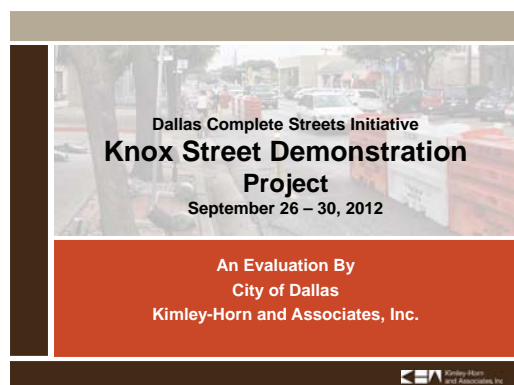
Action 5	Amend Other Related Policies and Regulations for Consistency and Linkage to Complete Streets Manual
TBD	✓ Update forwardDallas! Comprehensive Plan Transportation Element
TBD	✓ Incorporate complete street design practices into engineering manuals in coordination with iSWM (Public Works, Streets, Dallas Water Utilities, Trinity Watershed Management)
TBD	✓ Evaluate ROW use regulations and licensing requirements relative to best practices for encouraging private investment in public ROW: <ul style="list-style-type: none"> ➤ Landscaping, street furniture, street lighting, awnings and other non-revenue producing public amenities ➤ Sidewalk cafes and other private revenue producing uses ➤ On-street parking
TBD	✓ Amend Chapter 28 and 43 of the Dallas Code <ul style="list-style-type: none"> ➤ Consistency in traffic operation and speed standards ➤ Encouraging on-street parking where appropriate
TBD	✓ Amend the Dallas Development Code (Chapter 51-A) <ul style="list-style-type: none"> ➤ Sidewalk and planting zone width requirements and design standards ➤ Complete street design guidance for minor streets and subdivisions ➤ Sidewalk policy waivers ➤ Standards for access management, drive cuts and drive closures ➤ Ownership, responsibility and maintenance of rights-of-way

Action 6	Explore Alternative Funding Mechanisms for Complete Streets Capital and Operation / Maintenance Costs
TBD	<ul style="list-style-type: none"> ✓ Review TIF and PID requirements to proactively encourage Complete Streets implementation and maintenance through public/private partnerships ✓ Explore new opportunities for funding capital and operation/maintenance costs through public-private partnerships and grant opportunities ✓ Bring recommendations forward for Council consideration to address enhanced public funding options for complete street project implementation

Action 7	Expand Dallas' Current Offering of Neighborhood-Initiated Street Improvement Programs to Incorporate Complete Streets Design Options and Considerations
TBD	<ul style="list-style-type: none"> ✓ Expand on current traffic calming, pavement and sidewalk petition and safe routes to school programs to incorporate a broader list of design options based on the Complete Streets Design Manual
TBD	<ul style="list-style-type: none"> ✓ Initiate a competitive neighborhood matching grant program focused on modest complete street improvements similar to the Loving My Community program


Action 8	Initiate a Monitoring & Evaluation Program
TBD	<ul style="list-style-type: none"> ✓ Incorporate a Monitoring & Evaluation Plan into the Complete Street corridor planning and development process ✓ Identify measures of effectiveness consistent with the Complete Street project context using FHWA and AASHTO as a guide ✓ Measure return on Complete Streets investment in quantitative and qualitative terms ✓ Establish time horizons and frequency of data collection based on realistic expectations ✓ Identify and conduct public surveys that respond to stakeholder goals, objectives and expectations

KNOX STREET DEMONSTRATION EVALUATION



Dallas Complete Streets Initiative
Knox Street Demonstration Project
September 26 – 30, 2012

An Evaluation By
City of Dallas
Kimley-Horn and Associates, Inc.



Knox Street




Knox Street




Knox Street




Why Complete Streets ?




Why Complete Streets ?





Why Complete Streets ?





Description

- Reduce Knox Street from North Central Expressway to the Katy Trail from 4 to 3 vehicle lanes, install a 2-way cycle track on the north side and restripe all head in parking on the south side of the street to 60 degree angle parking



Goal

- To confirm whether a reduced number of lanes could function adequately to warrant implementing the “demonstrated” cross-section on a permanent basis



Objectives

- Construct a temporary cross-section with traffic signs, barricades and pavement marking tape and test it for a total of four days (2 weekdays and 2 weekend days);
- Create a Traffic Control Plan (TCP) approved by the City of Dallas that utilizes temporary construction barricades and devices and meets budget constraints;

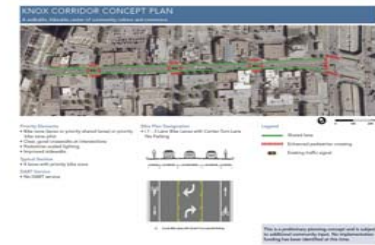
CSA Group, Inc. and Associates, Inc.

Objectives

- Develop and execute a TCP that encourages an appropriate vehicle speed; minimizes delays and congestion; accommodates high turnover on-street parking; and is robust enough to provide a safe separation between vehicular and bicycle traffic.
- Create a more walkable and bikeable street than currently exists.

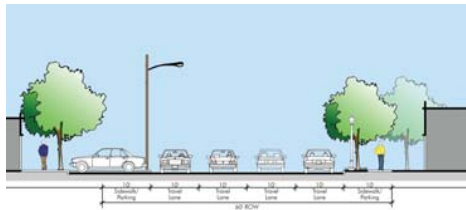
CSA Group, Inc. and Associates, Inc.

Concept Plan



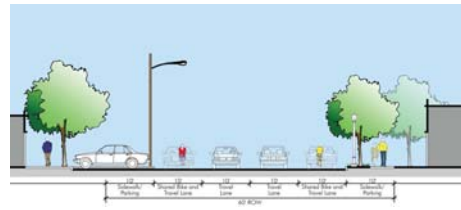
CSA Group, Inc. and Associates, Inc.

Existing Conditions



CSA Group, Inc. and Associates, Inc.

Option A



CSA Group, Inc. and Associates, Inc.

Option A



CSA Group, Inc. and Associates, Inc.

Option B



CSA Group, Inc. and Associates, Inc.

Option B



CSA Group, Inc. and Associates, Inc.

Option C



CSA Group, Inc. and Associates, Inc.

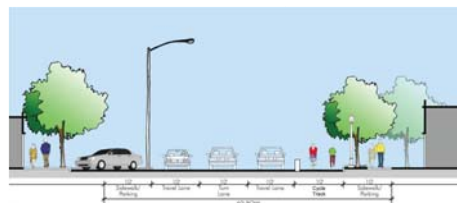
Option C

Knox - Plan - Future (Option C)



CSA Group, Inc. and Associates, Inc.

Proposed Demonstration Option



CSA Group, Inc. and Associates, Inc.

Proposed Demonstration Option



CSA Group, Inc. and Associates, Inc.

Proposed Demonstration Option



CSA Group, Inc. and Associates, Inc.

Traffic Control Plan



CSA Group, Inc. and Associates, Inc.

Demonstration



CSA Group, Inc. and Associates, Inc.

Demonstration



CSA Group, Inc. and Associates, Inc.

Demonstration



CSA Group, Inc. and Associates, Inc.

Demonstration



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Demonstration



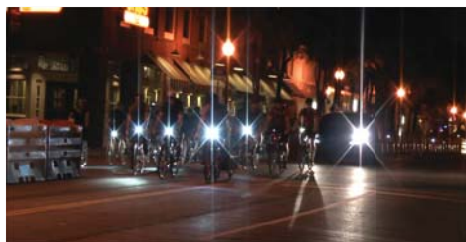
CSA Group, Inc. and Associates, Inc.

Demonstration



CSA Group, Inc. and Associates, Inc.

Demonstration



CEA Group, Inc. and Associates, Inc.

Knox 24-Hour Traffic Volumes

Day	Before	During
Thursday	-	19,755
Friday	18,254	16,970
Saturday	16,794	15,005*
Sunday	12,560	12,609
*Rain		
Source: City of Dallas		

CEA Group, Inc. and Associates, Inc.

Knox Intersection Counts

	Travis Before	Travis During	McKinney Before	McKinney During	N. Central Before	N. Central During
Weekday						
AM	932	991	1462	1507	3480	3355
Midday	1044	1123	1755	1918	3457	3825
PM	1232	1082	2280	2091	3715	3510
Total	5915	5917	9811	9882	19250	19482
Saturday						
Midday	1314	1177*	1989	1721*	3662	3587*
* Rain						

CEA Group, Inc. and Associates, Inc.

PM Peak Queuing



CEA Group, Inc. and Associates, Inc.

PM Peak Queuing



CEA Group, Inc. and Associates, Inc.

Knox Bicycle & Pedestrian Counts

	Travis Before	Travis During	McKinney Before	McKinney During	N. Central Before	N. Central During
Weekday						
Combined Peak Periods	30/312	42/415	35/496	47/681	33/44	35/73
Saturday						
Midday Peak Period	43/385	6/216*	15/567	2/356*	21/46	2/18*
*Rain						
XX/YY =	Bicycles/	Pedestrians				

CEA Group, Inc. and Associates, Inc.

Knox Speed Observations

	During (Thursday 9/27)	After (Thursday 10/4)
Number of Observations	36	46
High (MPH)	26	37
Low (MPH)	11	19
Average (MPH)	21.0	26.1
85 th % (MPH)	24	30

Note: All observations WB mid-afternoon.

CEA Group, Inc. and Associates, Inc.

Feedback Property Owners & Tenants

- The bike lanes are not a good idea on Knox. I think the Demonstration was a great idea but simply pointed out the problems, especially with the traffic problems it creates. Vehicular traffic through the area is challenging enough. We do not want to deter traffic flow in any way. This would be a disservice to the merchants in the area that rely on customers getting there as easily as possible.

CEA Group, Inc. and Associates, Inc.

Feedback Property Owners & Tenants

- In my opinion the bike lane along Knox Street just doesn't work. I watched very carefully to see what benefits it could have versus the issues that it causes. Change is never easy, but I believe this really is not it.
- I like the concept, but unfortunately I heard a lot of negative comments from customers about traffic, parking, etc. I hate that it rained, but numbers are consistent with last year.

CEA Group, Inc. and Associates, Inc.

Feedback Property Owners & Tenants

- When I asked my customers how they liked the experiment, several replied that once they realized the new setup, they avoided the street and came a different route. I thought that was a telling example. I also feel we need to install areas to park and lock a bike, preferably around restaurants, so when they do ride to a destination, they have somewhere to park.



Feedback Property Owners & Tenants

- Everyone complained about traffic and having to wait longer (thought the middle lane was distracting). On the other hand, they liked the angled parking spaces on Knox because this did make it easier to get in and out of those stores.
- I found it difficult to get out of the parking lot at Starbucks which is a main entrance used for our shopping strip. I have concerns with losing business due to the difficulties of getting in the entrance around the cyclists as well. This being said, I think it brings a different generation to our area.



Feedback Property Owners & Tenants

- While I tried to have an open mind, I thought that this was a disaster...the bike lanes made it difficult for people to turn into local businesses as well as being able to park on Knox. There was more street congestion due to the fact that it was one lane in certain areas. To do this correctly you would have to totally redesign the sidewalks.



Feedback Property Owners & Tenants

- The bicycle lanes did have a pretty big impact on traffic causing gridlock and back up in the intersections from the time of the install. When the lanes were removed this morning, the flow eased up again.
- I know cross at the cross walks, but customers do not do that. I just do not see us gaining business from the bike lane because they are just using it to get to Katy Trail. It would be safer for them to use a side street to get to the trail and we keep a lane of traffic going.



Feedback Pedestrians and Bicyclists

- A Local Bicyclist: This is a busy narrow street. There are 6-lanes on Fitzhugh and a new connection at McCommas. Too much traffic in and out...angry drivers.
- Jason Roberts: "If we're wrong, at least you're using the scientific method as opposed to assuming and going to public meetings nonstop and debating whether it will work or not," he said.
- "We can go back and see: Did people bicycle more? Did they walk more? Was there a better pedestrian experience? ... At least let us try it. Maybe they're right. Maybe they'll come back Saturday and it'll be a nightmare. But it was just four days. Let us just try it at the very least."



Lessons Learned

Design concept

- ✓ Need extra room for angle parking and maneuvering,
- ✓ Signs and pavement markings conform to MUTCD,
- ✓ Need more advance warning for a lane drop,
- ✓ Created bulb-out to direct drivers,
- ✓ Stop bars set back to accommodate turning vehicles,
- ✓ Lose a few parking spaces converting from 90-degree to angle,
- ✓ Yellow delineator at ends of cycle track worked out,
- ✓ Utility poles and concrete bases are a barrier,
- ✓ Use lower traffic barriers (water filled).



Lessons Learned

Installation

- ✓ Black out tape or paint is a necessity,
- ✓ Tape stands up to rain & normal traffic, but not turning and parking maneuvers,
- ✓ Be vigilant & flexible - created bulb-out to direct drivers,
- ✓ Restriping of parking requires additional cleaning & care,
- ✓ Installation during off-peak vs. peak traffic periods,
- ✓ The weather determines what materials you use,
- ✓ 4-foot bike lane (8-foot cycle track) next to a gutter is tight,
- ✓ Confusing signs & markings for bicyclists at east terminus,
- ✓ Start the design earlier working with contractor re: availability and cost of materials.



Lessons Learned

- ✓ Unique uses like valet parking stands need to be identified & accommodated,
- ✓ Take TCP to meeting(s) of all business and property owners before finalizing,
- ✓ Provide more advance advertising,
- ✓ Develop a Demonstration Primer & Checklist
- ✓ Other lessons learned...



Conclusions

- Demonstrations can model permanent Complete Streets installations for a reasonable cost
- Demonstrations can ensure input from all stakeholders
- Knox can "technically" be converted to a 3-lane section and accommodate the existing vehicle demand
- The allocation of space in the existing ROW would still need to be perfected:
 - Recommend removal of utility poles and concrete bases
 - If you have additional 4'-8' you could accommodate other stakeholder needs



PROPOSED MAJOR THOROUGHFARE PLAN CHANGE PRIORITIES



POTENTIAL REVISIONS TO MANUALS

POTENTIAL REVISIONS TO MANUALS

POTENTIALLY AFFECTED SECTIONS IN:

THE PAVING DESIGN MANUAL

251D – STANDARD CONSTRUCTION DETAILS

DRAINAGE DESIGN MANUAL

DALLAS WATER UTILITIES DESIGN MANUAL

Following is a summary of the City of Dallas Design Manuals and a list and general description of section changes that should be explored.

THE PAVING DESIGN MANUAL

Section	Comment	Recommendation
Overall comment	<p>Much of the information in the manual is an overview of AASHTO design criteria and TDLR guidelines. This information has been supplemented with city-specific requirements.</p> <p>As AASHTO and TDLR guidelines are updated, the Paving Design Manual has not been updated, resulting in out-dated standards in some instances.</p>	<ul style="list-style-type: none"> Consider revising the paving manual to reference AASHTO and TDLR criteria rather than repeat the information (remove redundancies and out-dated references) Update the Paving Design Manual to incorporate the Complete Streets guidelines, as noted below
Section I – Introduction	Describes overview, purpose and scope of Manual and references other standards, ordinances, and studies	<ul style="list-style-type: none"> Add new Dallas Complete Streets Manual and Vision Map to list of references
Section II – Functional and Dimensional Classification 2. Functional Classification 1. General 2. Principal Arterial Freeways 3. Arterial Thoroughfares	Describes City Thoroughfare plan; Defines Functional Class (Arterial, Collector, Local); Defines Dimension Class (Standard, Minimum, Existing, Special Roadway Sections)	<ul style="list-style-type: none"> Reference new Vision Map and street typologies. Address how they are tied to the Thoroughfare Plan Update Dimensional Class section OR add more guidance to “Special Roadway Sections” OR simply reference the new Dallas CS Manual

THE PAVING DESIGN MANUAL



Section	Comment	Recommendation
4. Collector Thoroughfares 5. Local Streets 6. Alleys 3. Dimensional Classification 1. General 2. Arterial Thoroughfares 3. Collector Thoroughfares 4. Local Streets 5. Alleys		
Section II, Figures and Tables	Figure II-1 Functional Classification: Relationship of Access to Mobility Table II-2A Typical Characteristics of Functional Classifications Table II-2B Description of Categories Used to Define Functional Classes Table II-3 Typical Daily Volumes of Functionally Designated Thoroughfares Table II-4 Street & Thoroughfare Geometric Standards	<ul style="list-style-type: none"> Review for consistency and updating
Section III – Access Control 2. Streets 1. Intersections 2. Traffic Barriers 3. Median Openings 4. Driveway Approaches and Curb Openings 1. General 2. Spacing 3. Intersections 4. Freeways and Expressways	Documents City's access requirements (intersection spacing, median openings, driveway spacing, alleys)	<ul style="list-style-type: none"> Review to target influence of complete streets initiative
Section IV – Geometric Design	Describes geometric design for city roadways and sidewalks. Consistent with AASHTO Criteria.	<ul style="list-style-type: none"> No change to 95% of chapter

THE PAVING DESIGN MANUAL

Section	Comment	Recommendations
2. Design Criteria 1. Design Vehicles 2. Design Speed 3. Design Traffic Volumes 3. Design Elements 1. Typical Cross Sections 7. Arterial Thoroughfares 8. Sidewalks 9. Parkways 10. Medians 11. Median Openings 12. Driveways and Curb Openings 13. Street Lighting and Traffic Control Devices 3.15 Utilities 4. ADA Requirements 1. General 2. Public Rights-of-Ways	Addresses design vehicle, speed, traffic volumes; Addresses design elements (cross section, horizontal curves, vertical profiles, storm drain criteria, crosswalks, sidewalks/ADA, parkways, street lighting, utility assignments, on-street parking, etc)	<ul style="list-style-type: none"> • Incorporate recommendations from the Dallas CS Manual with technical information (standard details, minimum criteria, etc) • Address utilities in ROW to possibly allow more flexibility
Section IV, Figures and Tables	Table IV-4 Design Vehicle Criteria Table IV-6 Typical Volumes and Capacities for Streets of Given Design Table IV-7 Design Traffic Volumes for Streets Figure IV-19 Driveway Standards Figure IV-20 Utility Zones in Typical Streets Figure IV-21 Utility Zones in Alleys Figure IV-22 Accessible Route Figure IV-23 Protruding Objects Figure IV-25 Public Sidewalk Curb Ramps Various Concepts Figure IV-26 Public Sidewalk Curb Ramps at Marked Crossings Figure IV-28 Examples of Accessible Parallel On-Street Parking Figure IV-29 Dimensions of Parking Spaces Figure IV-30 Access Aisle at Passenger Loading Zones	<ul style="list-style-type: none"> • Review for consistency and updating

THE PAVING DESIGN MANUAL

Section	Comment	Recommendation
Section V – Pavement Structure Alternative Paving Design Traffic Subgrade Soils Pavement Widening	Defines minimum requirements for pavement structure for roadways, alleys, and sidewalks. Based on roadway classification, traffic volumes, truck percentages, and soil type for 30-year design life. Allows for alternate pavement designs subject to City approval.	<ul style="list-style-type: none"> • No significant changes • However, the engineering side of hardscape (stamped, pavers, etc), aesthetic treatment, green paving improvements should be addressed/incorporated into this chapter
Section VI – Construction Plan Preparation	Defines the submittal requirements for conceptual, preliminary, and final design phases. Includes checklists, etc. Does not detail process.	<ul style="list-style-type: none"> • Reference Dallas CS Manual for PROCESS prior to and/or concurrent with conceptual and preliminary phases. • No changes to final design phase.
Appendices D. Street Centerline and Corner Curb Rerun	Radii Determinations-Examples	

251D – STANDARD CONSTRUCTION DETAILS

Section	Recommendation
Overall comment	<ul style="list-style-type: none"> Of the 36 Standard Details, most can remain as they are. Following are sections that should be reviewed for potential revisions. A few details, as noted below, will need to be revised and/or supplemented with additional information
Paving 1001 Paving Sections and Street Layouts with Medial Details Monolithic Median Noise	<ul style="list-style-type: none"> Evaluate and revise typical sections for potential context-specific adjustment/alteration, , lane width, etc to be consistent with new street typologies
Paving 1004 and 1004A Pavement Joints and Bridge Approach Slab Driveway Turnouts Special Driveway Turnout Details	<ul style="list-style-type: none"> May need to supplement with other driveway scenarios
Paving 1007	<ul style="list-style-type: none"> Supplement sidewalk/ramp details
Paving	<ul style="list-style-type: none"> Consider additional Complete Street – Paving Details (hardscape, stamped/colored concrete, pavers, etc)
Drainage Lined channels	<ul style="list-style-type: none"> iSWM influence could add alternative channel treatments
Structures	<ul style="list-style-type: none"> No changes to existing details. Consider making these obsolete and reference TxDOT standards.
Alley Intersections	<ul style="list-style-type: none"> Evaluate at roadway intersections
Traffic Control 5003 and 5004 Traffic Signal Foundation Details Pavement Makrings	<ul style="list-style-type: none"> Supplement pavement marking details to incorporate bicycle facilities, etc. Should alternative signal poles be used?
Miscellaneous 9004, 9005, 9007, 9008, 9009 Aluminum Bridge Railing Details Miscellaneous Construction Items Bicycle Paths Reinforced Sidewalk and Barrier Free Ramps Barrier Free Ramps Paving Details Steps and Handrail	<ul style="list-style-type: none"> Review in more detail for areas to refine or supplement Update/revise/supplement sidewalk/ramp/step/handrail details for consistency Supplement median details with other treatments Update/revise/supplement street lighting details for consistency Multiple options could be added to this set of base details

DRAINAGE DESIGN MANUAL

Section	Recommendation
<p>Section II - Drainage Design Criteria</p> <p>3. Hydraulic Design Criteria for Drainage Related Structures</p> <p>3.1 Design of Enclosed Storm Drain Systems</p> <p>*3.1.2 Gutter Flow/Inlet Location</p> <p>*3.1.3 Street Capacity</p> <p>*3.1.5 Flow in Alleys</p> <p>*3.1.8 Manhole Placement and Design</p> <p>*3.1.9 Outfall Design</p>	<ul style="list-style-type: none"> Review for potential adjustments
<p>Section III - Construction Plan Preparation</p> <p>*Platting/Dedication of Water Course and Basins – P5</p>	<ul style="list-style-type: none"> Potential iSWM Influence
<p>Section IV - Appendix</p> <p>*Storm Drain Inlet Chart - p</p> <p>*Gutter Flow/Inlet Computation Table – p77.</p> <p>*Ratio of Intercepted to Total Flow Inlets on Grade - p8.</p> <p>*Capacity of Triangular Gutters – p9.</p> <p>*Capacity of Parabolic Gutters - p10.- 11.</p> <p>*Alley Conveyance - p. 11A - 11B</p> <p>*Detail of Alley Paving at a Turn - p24.</p>	<ul style="list-style-type: none"> Review for potential adjustments

DALLAS WATER UTILITIES DESIGN MANUAL (OCTOBER 2011)

Section	Recommendation
<p>Part II - Water Main Construction</p> <p>*3/4" Water Service Installations (Sidewalk Adjacent to Curb) 201</p> <p>*1" Water Service Installations (Sidewalk Adjacent to Curb) 202</p> <p>*1 1/2" or 2" Water Service Installations (Sidewalk Adjacent to Curb) 203</p> <p>*3/4" Water Service Installations (Sidewalk 5' from Curb) 204</p> <p>*1" Water Service Installations (Sidewalk 5' from Curb) 205</p> <p>*1 1/2" or 2" Water Service Installations (Sidewalk 5' from Curb) 206</p> <p>*Bull Head Water Services 206A</p> <p>*Flush Point Installation - Type 1 207</p> <p>*Automatic Flush Point 207A</p> <p>*Air Release Valve - Type 1 208</p> <p>*Air Release Valve - Type 2 (Elevation) 209</p> <p>*Air Release Valve - Type 2 (Details) 210</p>	<ul style="list-style-type: none"> Review for potential adjustments
<p>*Methods for Setting Fire Hydrants 224</p> <p>*Pipe-to-Soil Potential Test Station (Post Mounted) 226</p>	<ul style="list-style-type: none"> Review for potential adjustments

DALLAS WATER UTILITIES DESIGN MANUAL (OCTOBER 2011)

Section	Recommendation
*Pipe-to-Soil Potential Test Station (Buried Configuration) 227 *Guard Post Protection for Fire Hydrants 237 *Guard Post Protection for Water Meters 238 *Single PRV Assembly 239 *Dual PRV Assembly 240, 241 - 243	
Part III - Wastewater Main Construction *Wastewater Lateral Width 319	<ul style="list-style-type: none"> Review for potential adjustments
Part IV - Water and Wastewater Adjustments *Meter Box Placement 409	<ul style="list-style-type: none"> Review for potential adjustments
Part V - Large Water Service Installations *Large Service Installation Details and Plan Views 502 *Minimum Easement Sizes for Meter Installation 502A *Large Service Installation Detail - Elevation View 503 *Suspended Vault Installation Details - Plan View 523 *Suspended Vault Installation Details - Elevation View 524	<ul style="list-style-type: none"> Review for potential adjustments

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A blue-tinted photograph of a city street scene, likely in Dallas, featuring tall skyscrapers and a pedestrian walkway. A large, faint, stylized flower graphic is overlaid on the right side of the image.

 Dallas

AGENDA ITEM # 22

KEY FOCUS AREA: Public Safety

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Police

CMO: Eric Campbell, 670-3255

MAPSCO: N/A

SUBJECT

Authorize a Memorandum of Understanding between the City of Dallas and the Texas Department of Public Safety authorizing the Dallas Police Department to perform commercial motor vehicle inspections and enforcement for the period March 1, 2016 through February 28, 2017 - Financing: No cost consideration to the City

BACKGROUND

The goal of the Motor Carrier Safety Assistance Program is to reduce commercial motor vehicle (CMV) involved crashes, fatalities, and injuries through consistent, uniform, and effective CMV safety programs. The CMV program detects, corrects or temporarily puts out of service commercial vehicles that exhibit safety defects, driver deficiencies, and unsafe motor carrier practices before they become contributing factors to crashes and a hazard to the citizens of Dallas.

Police officers of the Traffic Section of the Dallas Police Department are responsible for CMV inspections and enforcement within the City of Dallas. These officers are trained and certified by the Texas Department of Public Safety (DPS) to conduct CMV inspections and enforcement. DPS requires a memorandum of understanding between them and the City of Dallas in order to ensure uniformity with state and federal enforcement and inspection guidelines.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSION)

Authorized a Memorandum of Understanding and acceptance of the Motor Carrier Safety Assistance Program, from the Texas Department of Public Safety on October 8, 2014, by Resolution No. 14-1683.

Information about this item was provided to the Public Safety Committee on January 11, 2016.

FISCAL INFORMATION

No cost consideration to the City

January 27, 2016

WHEREAS, the City desires to enter into a Memorandum of Understanding with the Texas Department of Public Safety for the Dallas Police Department to conduct commercial motor vehicle inspections and enforcement; and

WHEREAS, it is the best interest of the City to enter into this Memorandum of Understanding.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to enter into a Memorandum of Understanding with the Texas Department of Public Safety, State of Texas regarding commercial motor vehicle inspections and enforcement to be performed by the Dallas Police Department for the period March 1, 2016 through February 28, 2017.

Section 2. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 23

KEY FOCUS AREA: Public Safety

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Police

CMO: Eric Campbell, 670-3255

MAPSCO: N/A

SUBJECT

Authorize a Memorandum of Understanding between the City of Dallas and the Texas Department of Public Safety authorizing the Dallas Police Department to perform commercial motor vehicle weight enforcement for the period March 1, 2016 through December 31, 2017 - Financing: No cost consideration to the City

BACKGROUND

The goal of the Motor Carrier Safety Assistance Weight Enforcement Program is to reduce commercial motor vehicle (CMV) involved crashes, fatalities, and injuries through consistent, uniform, and effective CMV safety programs. The CMV program detects, corrects or temporarily puts out of service commercial vehicles that exhibit weight defects, before they become contributing factors to crashes and a hazard to the citizens of Dallas.

Police officers of the Traffic Section of the Dallas Police Department are responsible for CMV weight enforcement within the City of Dallas. These officers are trained and certified by the Texas Department of Public Safety (DPS) to conduct CMV weight enforcement. DPS requires a memorandum of understanding between them and the City of Dallas in order to ensure uniformity with state and federal enforcement and inspection guidelines.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSION)

Information about this item was provided to the Public Safety Committee on January 11, 2016.

FISCAL INFORMATION

No cost consideration to the City

January 27, 2016

WHEREAS, the City desires to enter into a Memorandum of Understanding with the Texas Department of Public Safety for the Dallas Police Department to conduct commercial motor vehicle weight enforcement; and

WHEREAS, it is the best interest of the City to enter into this Memorandum of Understanding.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to enter into a Memorandum of Understanding with the Texas Department of Public Safety, State of Texas regarding commercial motor vehicle weight enforcement to be performed by the Dallas Police Department for the period March 1, 2016 through December 31, 2017.

Section 2. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 24

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 12

DEPARTMENT: Public Works Department

CMO: Jill A. Jordan, P.E., 670-5299

MAPSCO: 5P Q S U V 6S

SUBJECT

Authorize a contract with RBR Infrastructure & Road, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage and drive approaches for Alley Reconstruction Group 12-2037 (list attached) - Not to exceed \$1,436,079 - Financing: 2012 Bond Funds

BACKGROUND

Alley Reconstruction Group 12-2037 was approved in the 2012 Bond Program. On September 25, 2013, Resolution No. 13-1731 authorized a professional services contract with Michael Baker Jr., Inc. for engineering design services. This action will authorize a contract for the reconstruction of alley paving, storm drainage, and drive approaches for Alley Reconstruction Group 12-2037, which consists of alley between Amberwood Road (16101-16239) and Wickerwood Drive (6401-6535) from Shadybank Drive to Shadybank Drive; the alley between Caulfield Drive (6701-6805) and Spanky Branch Court (6904-7000) from Brushfield Drive to Caulfield Drive; the alley between La Cabeza Drive (7707-7765) and Scotia Drive (7702-7718) from Salado Drive to Spring Creek Road; the alley between La Manga Drive (7707-7765) and La Cabeza Drive (7708-7764) from Salado Drive to Spring Creek Road; the alley between Rustic Valley Drive (7238-7420) and Echo Bluff Drive (7104-7132) from Hillcrest Road to Meandering Way; and the alley between Timber Creek Lane (6001-6037) and Gentle Knoll Lane (6002-6030) from Rustic Meadows Drive to Keller Springs Road.

The projects consist of reconstructing the existing alley pavement with 6-inch thick reinforced concrete pavement, drive approaches and storm drainage.

RBR Infrastructure & Road, LLC has no completed contractual activities with the City of Dallas within the past three years.

ESTIMATED SCHEDULE OF PROJECT

Began Design	January 2014
Completed Design	October 2015
Begin Construction	March 2016
Complete Construction	September 2017

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a professional services contract for engineering design services on September 25, 2013, by Resolution No. 13-1731.

Information about this item was provided to the Transportation and Trinity River Project Committee on January 11, 2016.

FISCAL INFORMATION

2012 Bond Funds - \$1,436,079

Design - PBW	\$ 213,946.00
Construction (this action)	
Paving & Drainage - PBW	<u>\$1,436,079.00</u>
Total Project Cost	\$1,650,025.00

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

RBR Infrastructure & Road, LLC

Hispanic Female	2	Hispanic Male	15
African-American Female	0	African-American Male	0
Other Female	0	Other Male	0
White Female	0	White Male	0

BID INFORMATION

The following bids with quotes were received and opened on November 20, 2015:

*Denotes successful bidder(s)

<u>BIDDERS</u>	<u>BID AMOUNT</u>
*RBR Infrastructure & Road, LLC 13140 Coit Road, Suite 230 Dallas, TX 75240	\$1,436,079.00
Ark Contracting Services	\$1,691,452.00

Original estimate: PBW - \$1,248,789.50

OWNER

RBR Infrastructure & Road, LLC

Julio Rafael Betances, President

MAPS

Attached.

Alley Reconstruction Group 12-2037

<u>Alley Reconstruction Group 12-2037</u>	<u>Council District</u>
Alley between Amberwood Road (16101-16239) and Wickerwood Drive (6401-6535) from Shadybank Drive to Shadybank Drive	12
Alley between Caulfield Drive (6701-6805) and Spanky Branch Court (6904-7000) from Brushfield Drive to Caulfield Drive	12
Alley between La Cabeza Drive (7707-7765) and Scotia Drive (7702-7718) from Salado Drive to Spring Creek Road	12
Alley between La Manga Drive (7707-7765) and La Cabeza Drive (7708-7764) from Salado Drive to Spring Creek Road	12
Alley between Rustic Valley Drive (7238-7420) and Echo Bluff Drive (7104-7132) from Hillcrest Road to Meandering Way	12
Alley between Timber Creek Lane (6001-6037) and Gentle Knoll Lane (6002-6030) from Rustic Meadows Drive to Keller Springs Road	12

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a contract with RBR Infrastructure & Road, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage and drive approaches for Alley Reconstruction Group 12-2037 (list attached) - Not to exceed \$1,436,079 - Financing: 2012 Bond Funds

RBR Infrastructure & Road, LLC is a local, minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Construction

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$1,371,079.00	95.47%
Total non-local contracts	\$65,000.00	4.53%
TOTAL CONTRACT	\$1,436,079.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
RBR Infrastructure & Road, LLC	HMMB63300N0816	\$1,371,079.00	100.00%
Total Minority - Local		\$1,371,079.00	100.00%

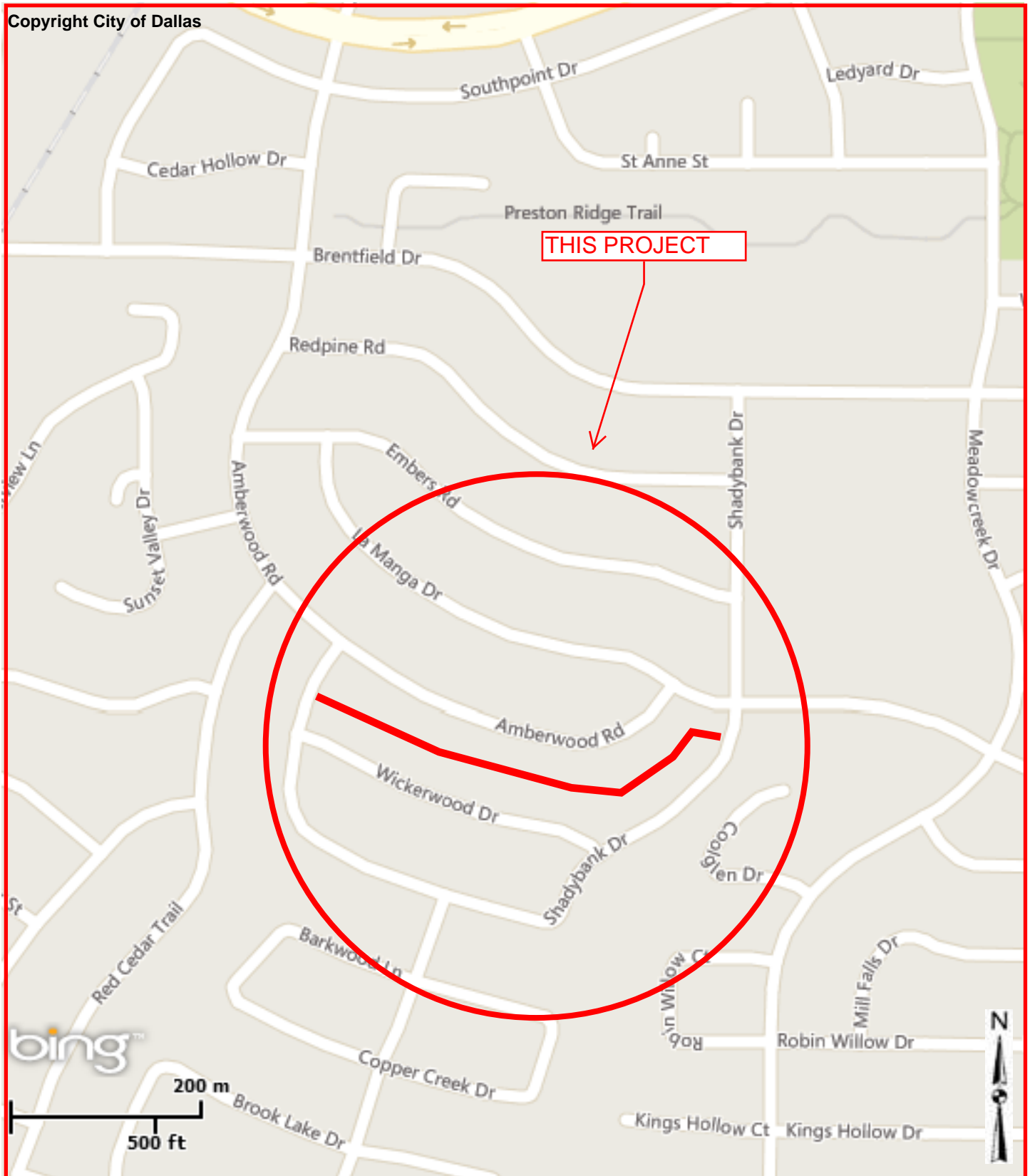
Non-Local Contractors / Sub-Contractors

<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Champion Fuel Solutions	WFDB79263Y0416	\$65,000.00	100.00%
Total Minority - Non-local		\$65,000.00	100.00%

TOTAL M/WBE CONTRACT PARTICIPATION

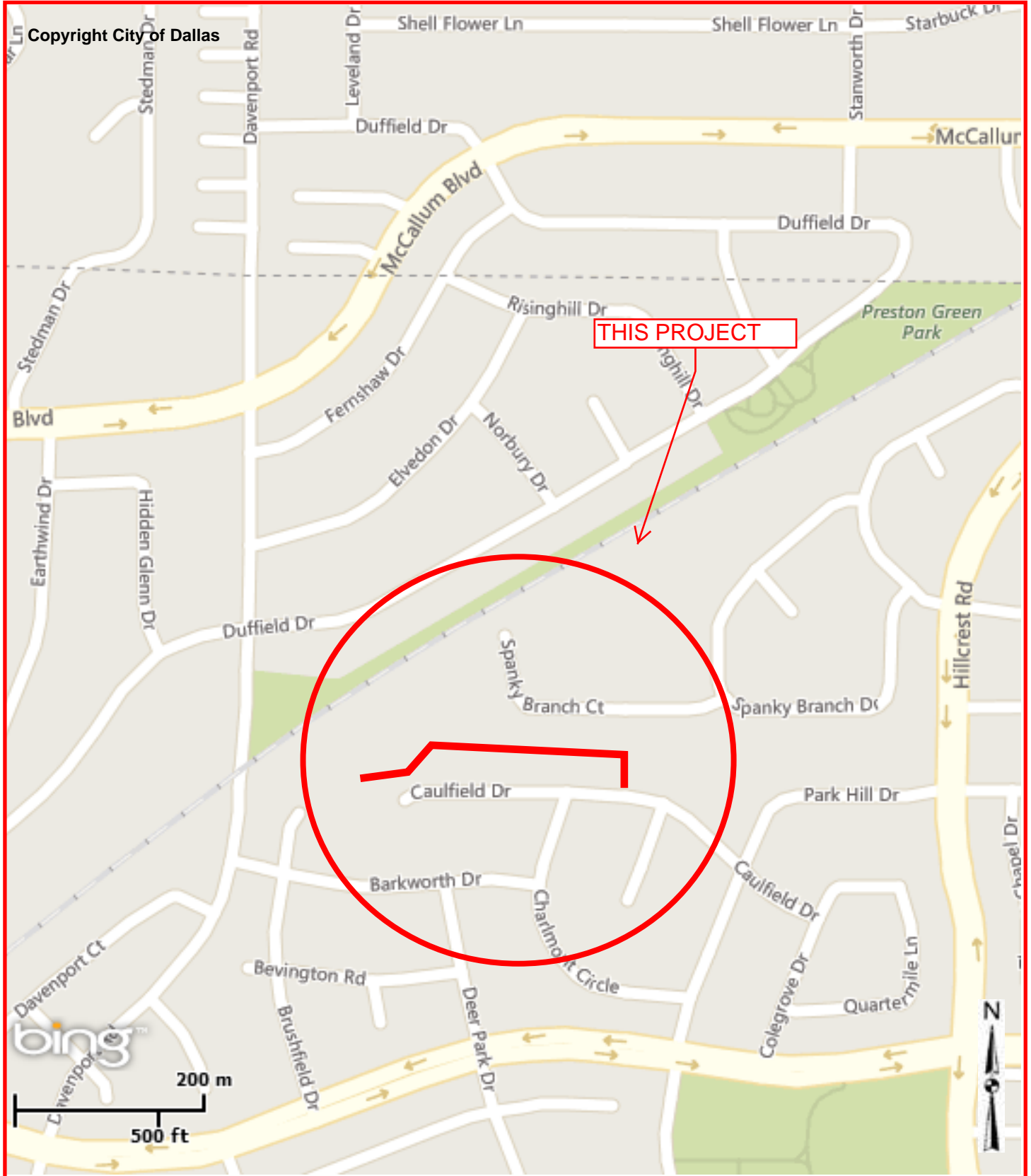
	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$1,371,079.00	100.00%	\$1,371,079.00	95.47%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$65,000.00	4.53%
Total	\$1,371,079.00	100.00%	\$1,436,079.00	100.00%

ALLEY RECONSTRUCTION GROUP 12-2037
ALLEY BETWEEN AMBERWOOD ROAD AND WICKERWOOD DRIVE
FROM SHADYBANK DRIVE TO SHADYBANK DRIVE



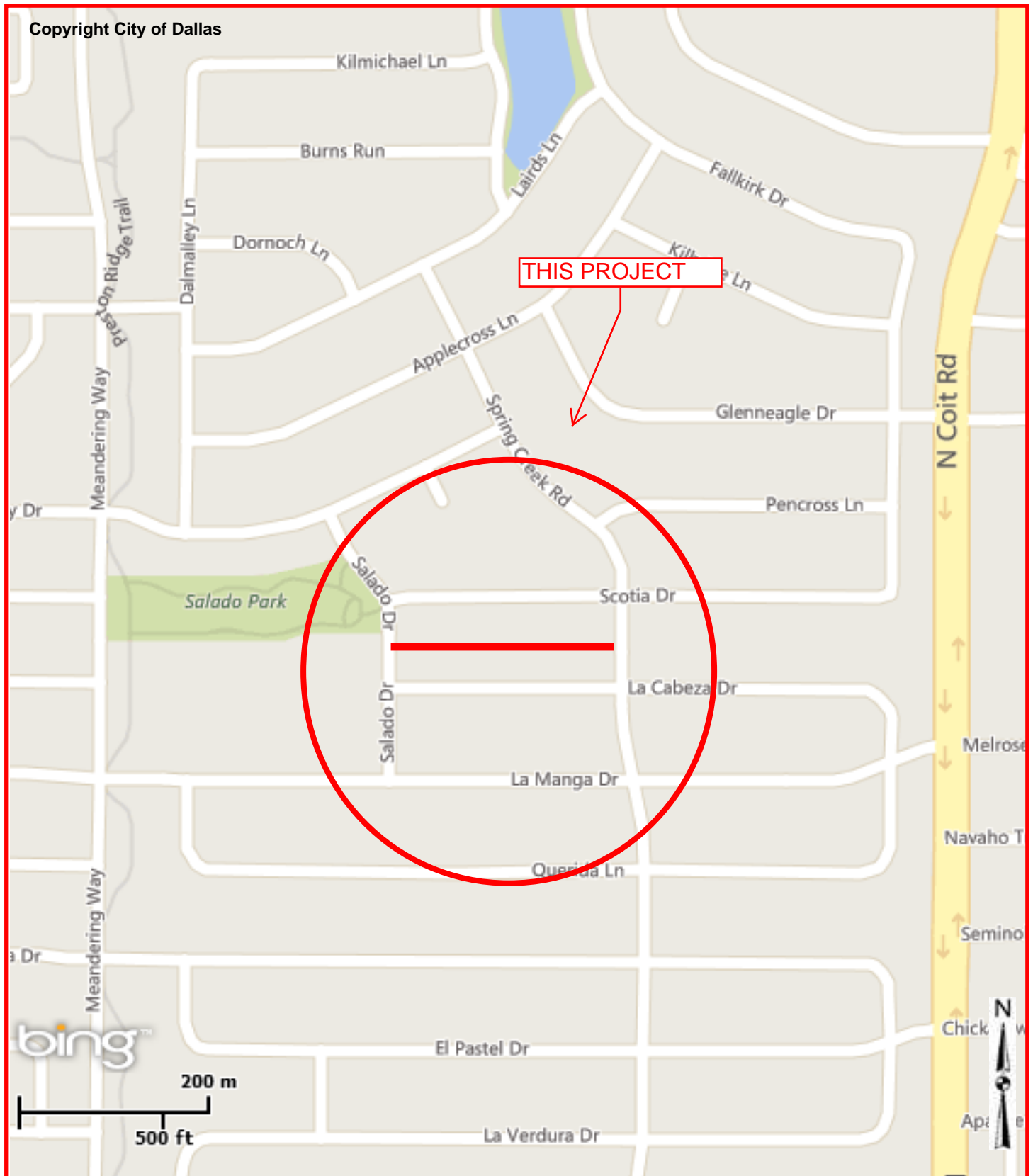
MAPSCO 5U

ALLEY RECONSTRUCTION GROUP 12-2037
ALLEY BETWEEN CAULFIELD DRIVE AND SPANKY BRANCH COURT
FROM BRUSHFIELD DRIVE TO CAULFIELD DRIVE



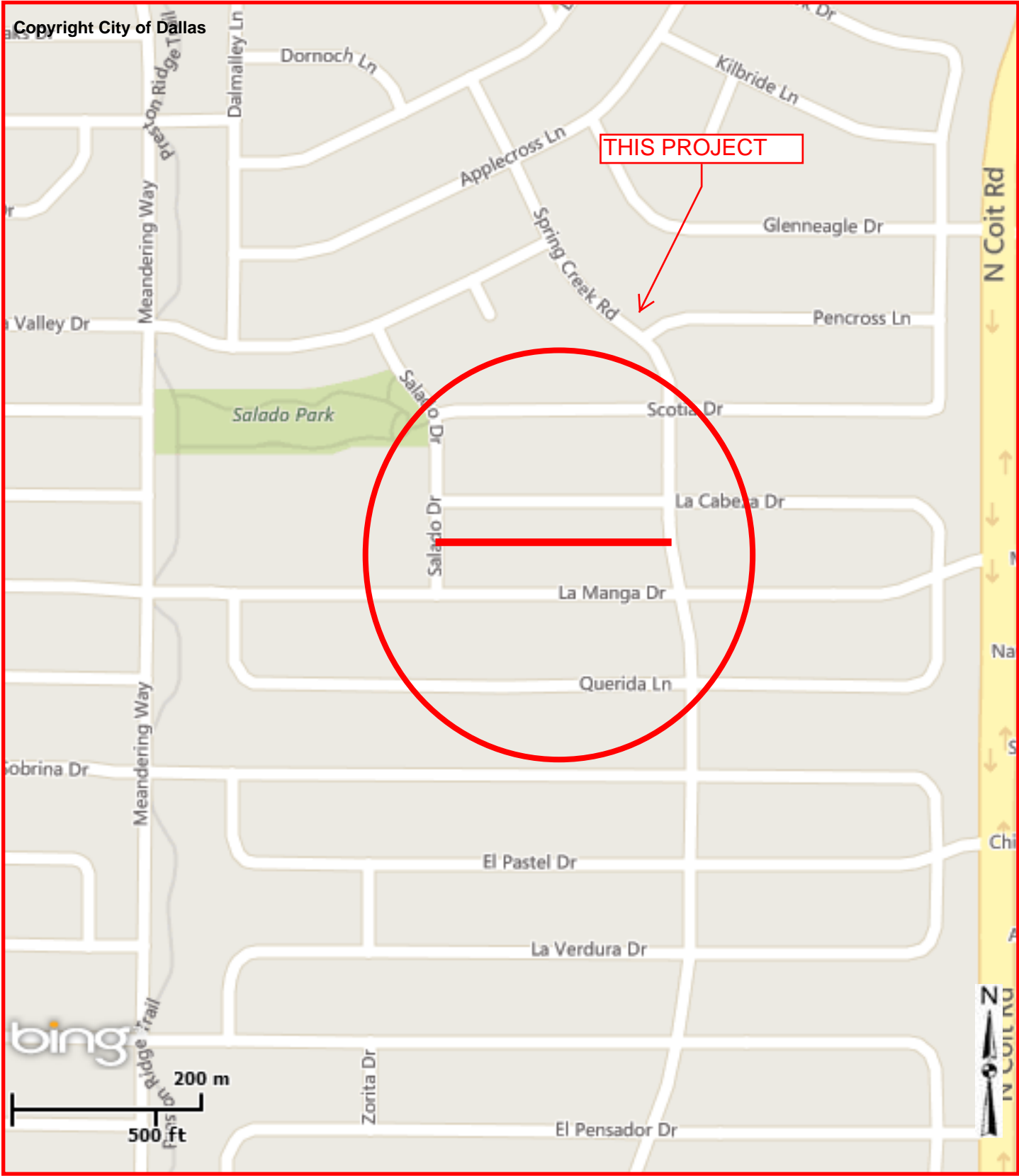
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ALLEY RECONSTRUCTION GROUP 12-2037
ALLEY BETWEEN LA CABEZA DRIVE AND SCOTIA DRIVE
FROM SALADO DRIVE TO SPRING CREEK ROAD

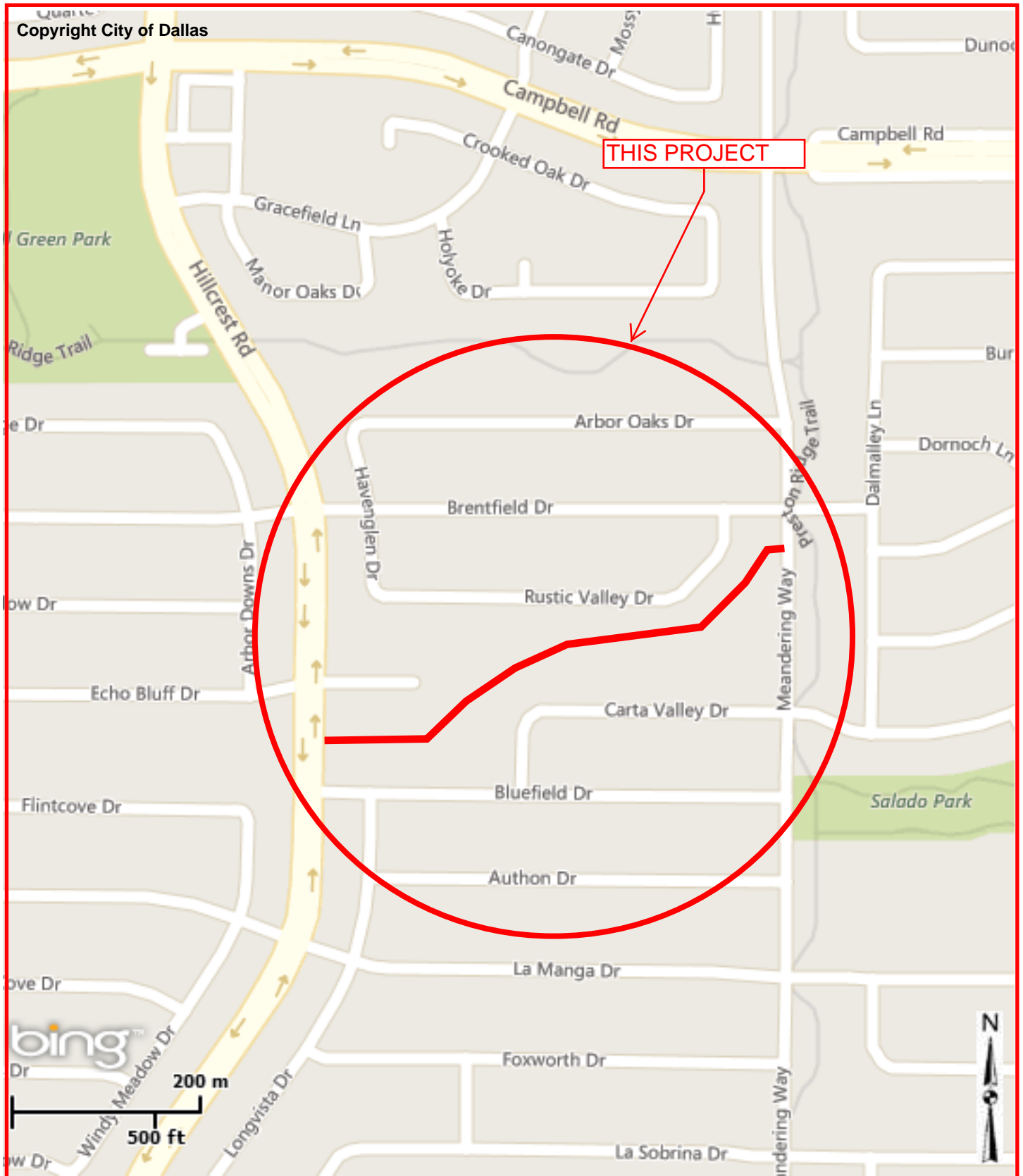


MAPSCO 6S

ALLEY RECONSTRUCTION GROUP 12-2037
ALLEY BETWEEN LA MANGA DRIVE AND LA CABEZA DRIVE
FROM SALADO DRIVE TO SPRING CREEK ROAD

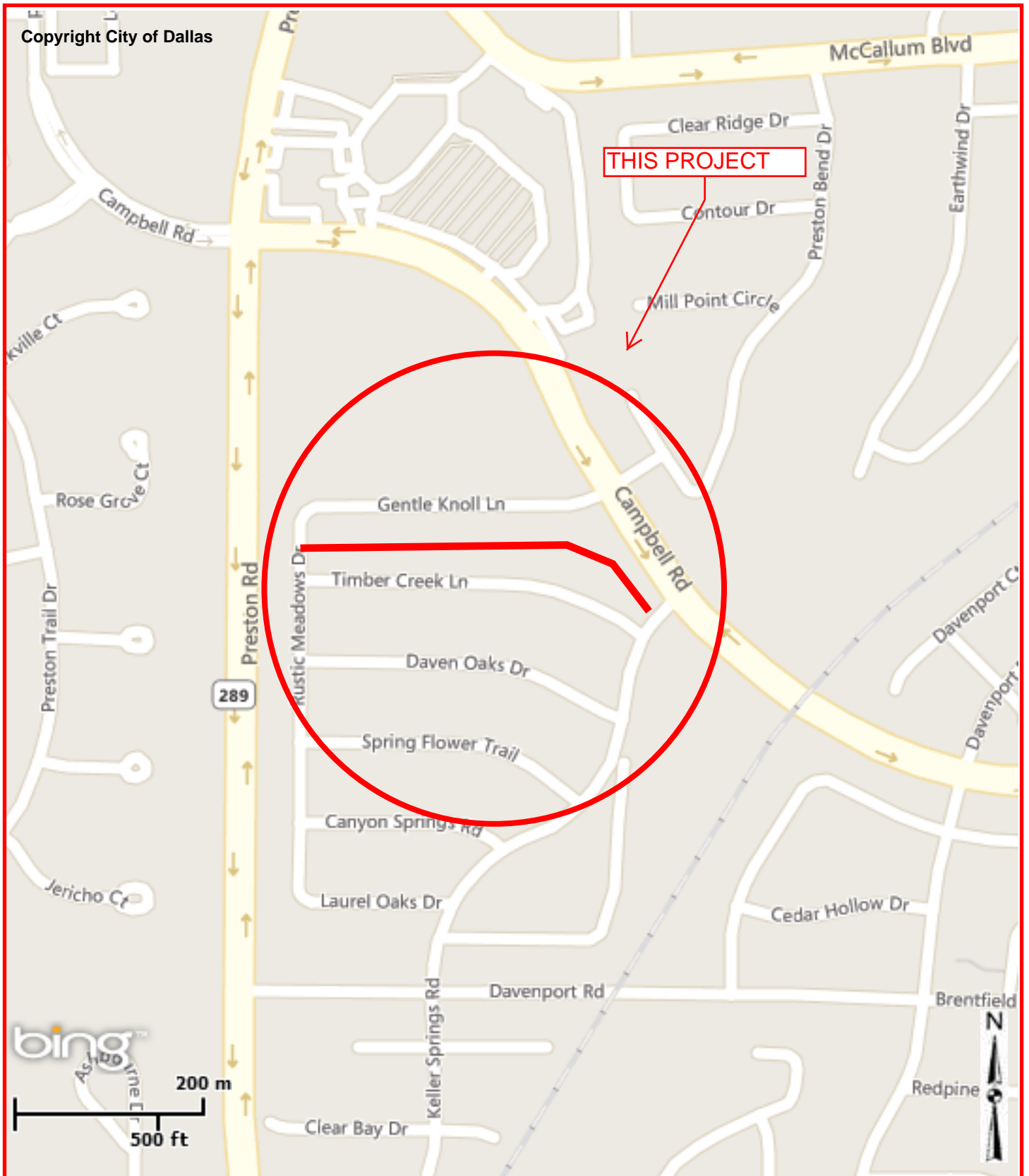


ALLEY RECONSTRUCTION GROUP 12-2037
ALLEY BETWEEN RUSTIC VALLEY DRIVE AND ECHO BLUFF DRIVE
FROM HILLCREST ROAD TO MEANDERING WAY



MAPSCO 5V, S

ALLEY RECONSTRUCTION GROUP 12-2037
ALLEY BETWEEN TIMBER CREEK LANE AND GENTLE KNOLL LANE
FROM RUSTIC MEADOWS DRIVE TO KELLER SPRINGS ROAD



MAPSCO 5P

January 27, 2016

WHEREAS, on September 25, 2013, Resolution No. 13-1731 authorized a professional services contract with Michael Baker Jr., Inc. for Alley Reconstruction Group 12-2037 for engineering design services; and,

WHEREAS, bids were received on November 20, 2015, for the reconstruction of alley paving, storm drainage, and drive approaches for Alley Reconstruction Group 12-2037, as follows:

<u>BIDDERS</u>	<u>BID AMOUNT</u>
RBR Infrastructure & Road, LLC	\$1,436,079.00
Ark Contracting Services, LLC	\$1,691,452.00

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to enter into a contract with RBR Infrastructure & Road, LLC for the reconstruction of alley paving, storm drainage, and drive approaches for Alley Reconstruction Group 12-2037 in an amount not to exceed \$1,436,079.00, this being the lowest responsible bid received as indicated by the tabulation of bids, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S215, Act. AREC Obj. 4510, Program #PB12S215, CT PBW12S215D1 Vendor #VS0000085267, in amount not to exceed	\$ 302,281.50
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Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S228, Act. AREC Obj. 4510, Program #PB12S228, CT PBW12S215D1 Vendor #VS0000085267, in amount not to exceed	\$ 157,338.50
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Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S269, Act. AREC Obj. 4510, Program #PB12S269, CT PBW12S215D1 Vendor #VS0000085267, in amount not to exceed	\$ 191,246.00
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January 27, 2016

Street and Transportation Improvements Fund
Fund 3U22, Department PBW, Unit S273, Act. AREC
Obj. 4510, Program #PB12S273, CT PBW12S215D1
Vendor #VS0000085267, in amount not to exceed \$ 196,090.00

Street and Transportation Improvements Fund
Fund 3U22, Department PBW, Unit S298, Act. AREC
Obj. 4510, Program #PB12S298, CT PBW12S215D1
Vendor #VS0000085267, in amount not to exceed \$ 337,950.00

Street and Transportation Improvements Fund
Fund 3U22, Department PBW, Unit S304, Act. AREC
Obj. 4510, Program #PB12S304, CT PBW12S215D1
Vendor #VS0000085267, in amount not to exceed \$ 251,173.00

Total amount not to exceed \$1,436,079.00

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 6, 13

DEPARTMENT: Public Works Department
Water Utilities

CMO: Jill A. Jordan, P.E., 670-5299
Mark McDaniel, 670-3256

MAPSCO: 13V Y 14S W 23D F G L

SUBJECT

Authorize a contract with Ark Contracting Services, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2041 and 12-2043 (list attached) - Not to exceed \$2,871,476 - Financing: 2012 Bond Funds (\$2,566,981), Water Utilities Capital Improvement Funds (\$290,895) and Water Utilities Capital Construction Funds (\$13,600)

BACKGROUND

Alley Reconstruction Groups 12-2041 and 12-2043 were approved in the 2012 Bond Program. On September 25, 2013, Resolution No. 13-1731 authorized professional services contracts for engineering design services with Bury - DFW, Inc. f/k/a Bury + Partners, Inc. for Alley Reconstruction Group 12-2041 and GarzaBury, LLC for Alley Reconstruction Group 12-2043. This action will authorize a construction contract for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Group 12-2041, which consists of the alley between Cedar Bend Drive (12317-12417) and LBJ Freeway from Marsh Lane to Cedar Bend Drive; the alley between Cedarbrush Drive (3809-3951) and Goodfellow Drive (3810-3948) from Cox Lane to Rosser Road; the alley between Coral Gables Drive (3608-3660) and Pallos Verdas Drive (3627-3675) from Pallos Verdas Drive to Coral Gables Drive; the alley between Cromwell Drive (12112-12132) and High Mesa Drive (3403-3416) from High Meadow Drive and High Bluff Drive to High Mesa Drive; the alley between Deep Valley Drive (3920-3890) and Clear Cove Lane (3911-3951) from Rosser Road to Rosser Road; and Alley Reconstruction Group 12-2043, which consists of the alley behind Beauty Lane (10804-11030) from Royal Lane to Townsend Drive; the alley between Catamore Lane (3117-3185) and Flowerdale Lane (3112-3162) from alley between Channel Drive and Dundee Drive to Dundee Drive; and the alley between Chapel Downs Drive (3108-3256)

BACKGROUND (Continued)

and Timberview Road (3111-3253) from Dale Crest Drive to Chapel Downs Drive.

The project includes reconstruction of a 10-foot wide reinforced concrete alley pavement and drive approaches, storm drainage, and wastewater main and adjustment improvements.

Ark Contracting Services, LLC has no completed contractual activities with the City of Dallas within the past three years.

ESTIMATED SCHEDULE OF PROJECT

Began Design	January 2014
Completed Design	September 2015
Begin Construction	February 2016
Complete Construction	February 2017

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a professional services contract with Bury - DFW, Inc. f/k/a Bury + Partners, Inc. and GarzaBury, LLC for engineering design services on September 25, 2013, by Resolution No. 13-1731.

Information about this item was provided to the Transportation and Trinity River Project Committee on January 11, 2016.

FISCAL INFORMATION

2012 Bond Funds - \$2,566,981

Water Utilities Capital Improvement Funds - \$290,895

Water Utilities Capital Construction Funds - \$13,600

Design - PBW	\$ 282,381.50
Design - DWU	\$ 9,195.00
Construction (this action)	
Paving & Drainage - PBW	\$2,566,981.00
Wastewater - DWU	<u>\$ 304,495.00</u>
Total Project Cost	\$3,163,052.50

FISCAL INFORMATION (Continued)**Council District****Amount**

6	\$ 249,220.00
13	<u>\$2,622,256.00</u>
Total	\$2,871,476.00

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION**Ark Contracting Services, LLC**

Hispanic Female	0	Hispanic Male	131
African-American Female	0	African-American Male	0
Other Female	0	Other Male	0
White Female	5	White Male	14

BID INFORMATION

The following bids with quotes were received and opened on October 30, 2015:

*Denotes successful bidder(s)

BIDDERS**BID AMOUNT**

*Ark Contracting Services, LLC 420 S. Dick Price Road Kennedale, Texas 76060	\$2,871,476.00
RBR Infrastructure & Road, LLC	\$2,999,512.00

Original estimate: PBW - \$1,778,157.00
WTR - \$ 148,717.00

Total \$1,926,874.00

OWNER

Ark Contracting Services, LLC

Steven C. Bowman, President

MAPS

Attached.

Alley Reconstruction Groups 12-2041 and 12-2043

<u>Alley Reconstruction Group 12-2041</u>	<u>Council District</u>
Alley between Cedar Bend Drive (12317-12417) and LBJ Freeway from Marsh Lane to Cedar Bend Drive	13
Alley between Cedarbrush Drive (3809-3951) and Goodfellow Drive (3810-3948) from Cox Lane to Rosser Road	13
Alley between Coral Gables Drive (3608-3660) and Pallos Verdas Drive (3627-3675) from Pallos Verdas Drive to Coral Gables Drive	13
Alley between Cromwell Drive (12112-12132) and High Mesa Drive (3403-3416) from High Meadow Drive and High Bluff Drive to High Mesa Drive	6
Alley between Deep Valley Drive (3920-3890) and Clear Cove Lane (3911-3951) from Rosser Road to Rosser Road	13
<u>Alley Reconstruction Group 12-2043</u>	
Alley behind Beauty Lane (10804-11030) from Royal Lane to Townsend Drive	13
Alley between Catamore Lane (3117-3185) and Flowerdale Lane (3112-3162) from alley between Channel Drive and Dundee Drive to Dundee Drive	13
Alley between Chapel Downs Drive (3108-3256) and Timberview Road (3111-3253) from Dale Crest Drive to Chapel Downs Drive	13

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a contract with Ark Contracting Services, LLC, lowest responsible bidder of two, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2041 and 12-2043 (list attached) - Not to exceed \$2,871,476 - Financing: 2012 Bond Funds (\$2,566,981), Water Utilities Capital Improvement Funds (\$290,895) and Water Utilities Capital Construction Funds (\$13,600)

Ark Contracting Services, LLC is a non-local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Construction

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$699,200.00	24.35%
Total non-local contracts	\$2,172,276.00	75.65%
TOTAL CONTRACT	\$2,871,476.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
J & A Trucking	HMMB64410N0317	\$122,000.00	17.45%
LKT & Associates	WFDB26473Y1016	\$34,200.00	4.89%
Magnum Manhole & Underground	WFDB64723Y0416	\$5,000.00	0.72%
Total Minority - Local		\$161,200.00	23.05%

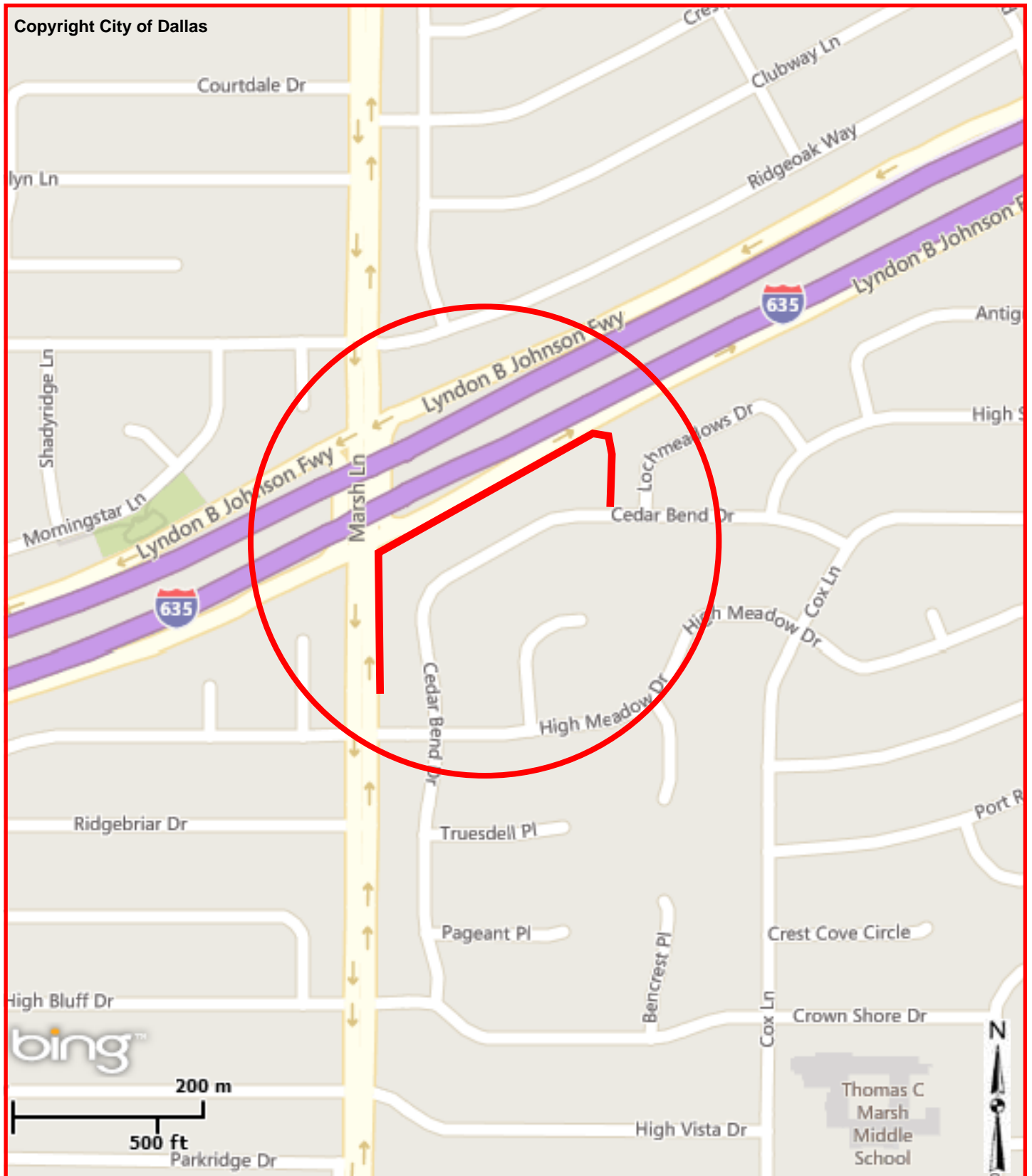
Non-Local Contractors / Sub-Contractors

<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Viking Fence Co.	HMMB31501N0617	\$88,600.00	4.08%
Cowtown Redi Mix	WFWB06682Y0916	\$473,000.00	21.77%
ANA Consultants, LLC	WFDB57431Y0516	\$12,000.00	0.55%
Total Minority - Non-local		\$573,600.00	26.41%

TOTAL M/WBE CONTRACT PARTICIPATION

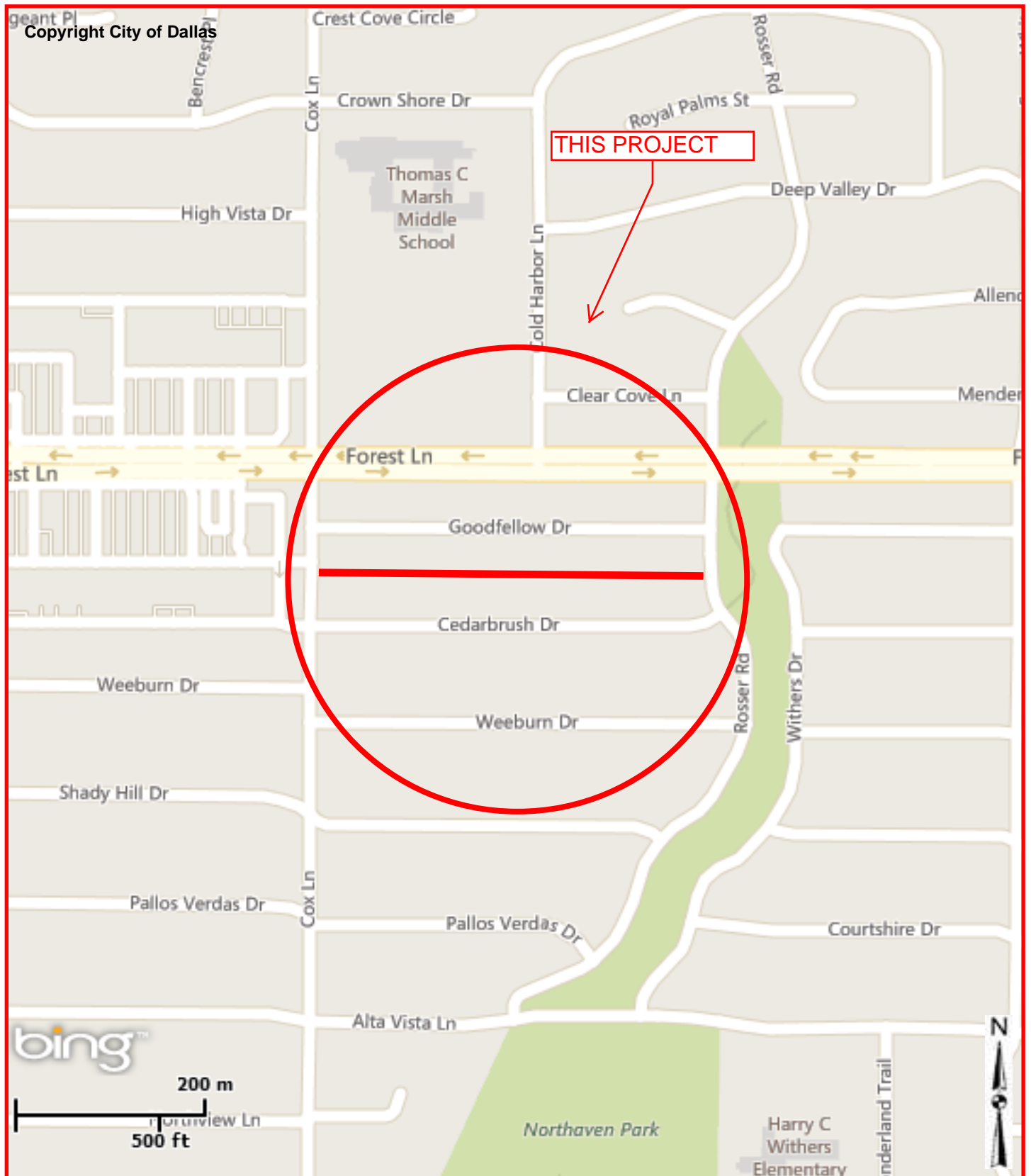
	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$122,000.00	17.45%	\$210,600.00	7.33%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$39,200.00	5.61%	\$524,200.00	18.26%
	<hr/>	<hr/>	<hr/>	<hr/>
Total	\$161,200.00	23.05%	\$734,800.00	25.59%

ALLEY RECONSTRUCTION GROUP 12-2041
ALLEY BETWEEN CEDAR BEND DRIVE AND LBJ FREEWAY
FROM MARSH LANE TO CEDAR BEND DRIVE



MAPSCO 13V & 14S

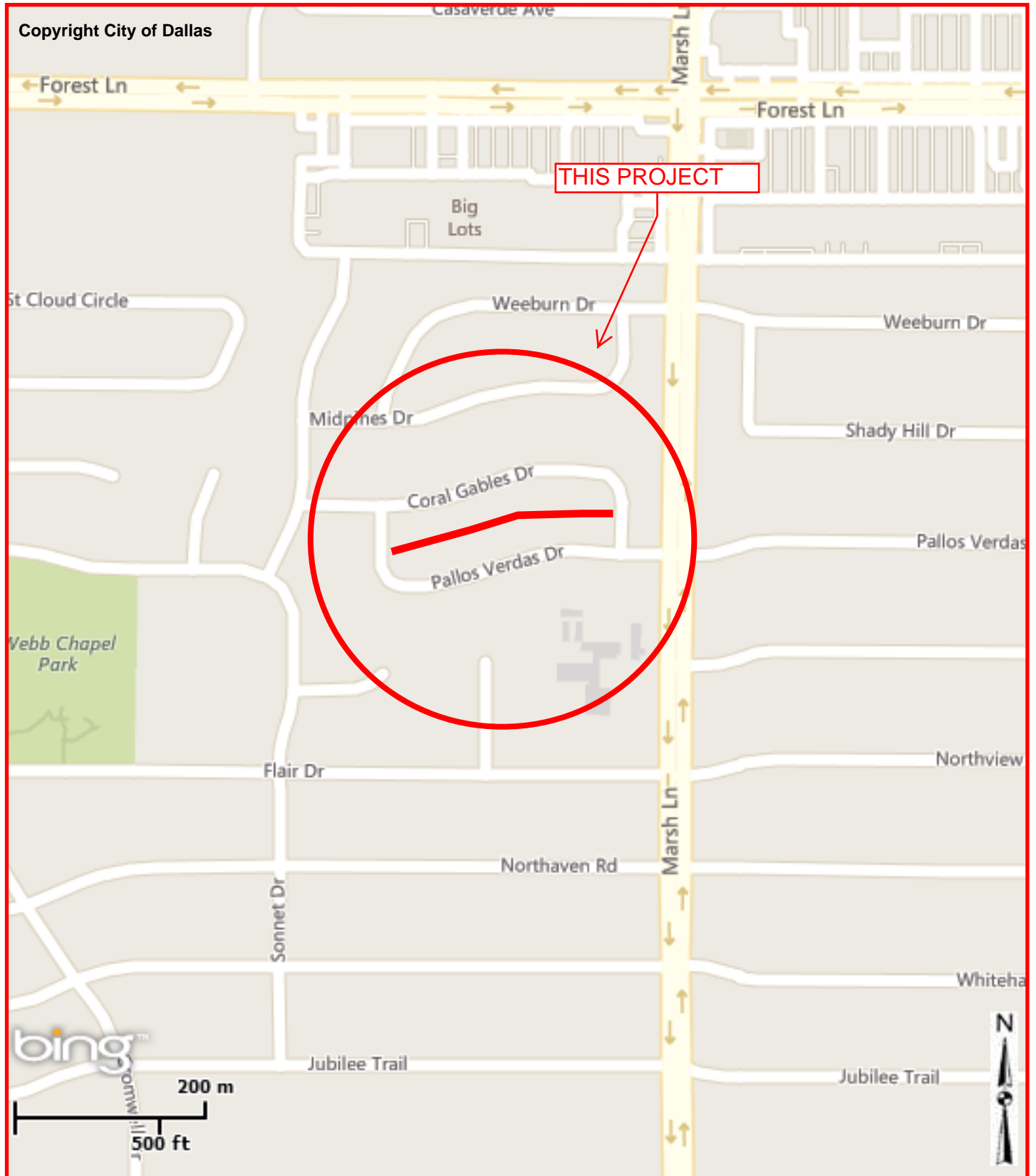
ALLEY RECONSTRUCTION GROUP 12-2041
ALLEY BETWEEN CEDARBRUSH DRIVE AND GOODFELLOW DRIVE FROM
COX LANE TO ROSSER ROAD



MAPSCO 14W

ALLEY RECONSTRUCTION GROUP 12-2041

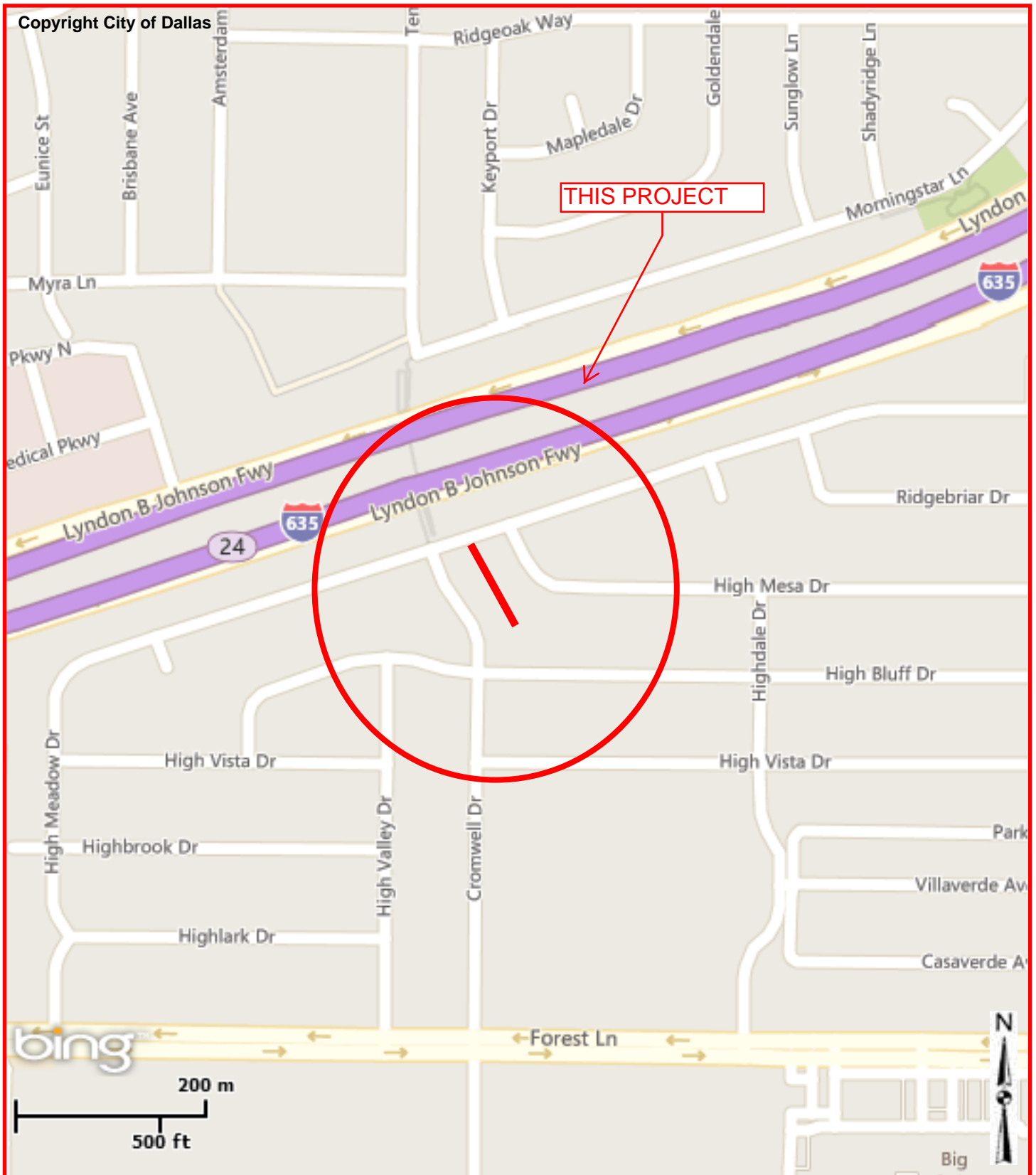
ALLEY BETWEEN CORAL GABLES DRIVE AND PALLOS VERDAS DRIVE
FROM PALLOS VERDAS DRIVE TO CORAL GABLES DRIVE



MAPSCO 23D

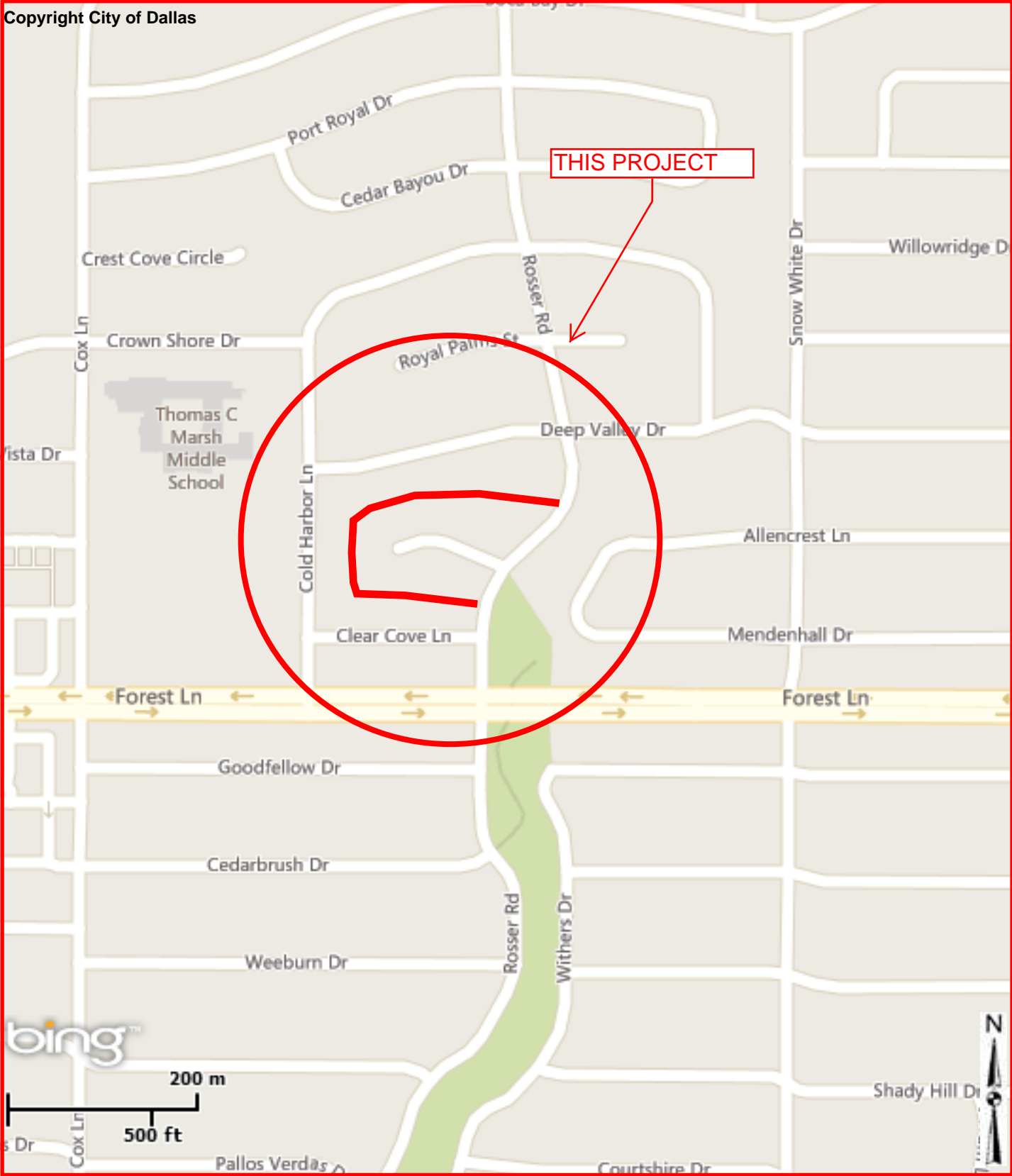
ALLEY RECONSTRUCTION GROUP 12-2041

ALLEY BETWEEN CROMWELL DRIVE AND HIGH MESA DRIVE FROM
HIGH MEADOW DRIVE AND HIGH BLUFF DRIVE TO HIGH MESA DRIVE

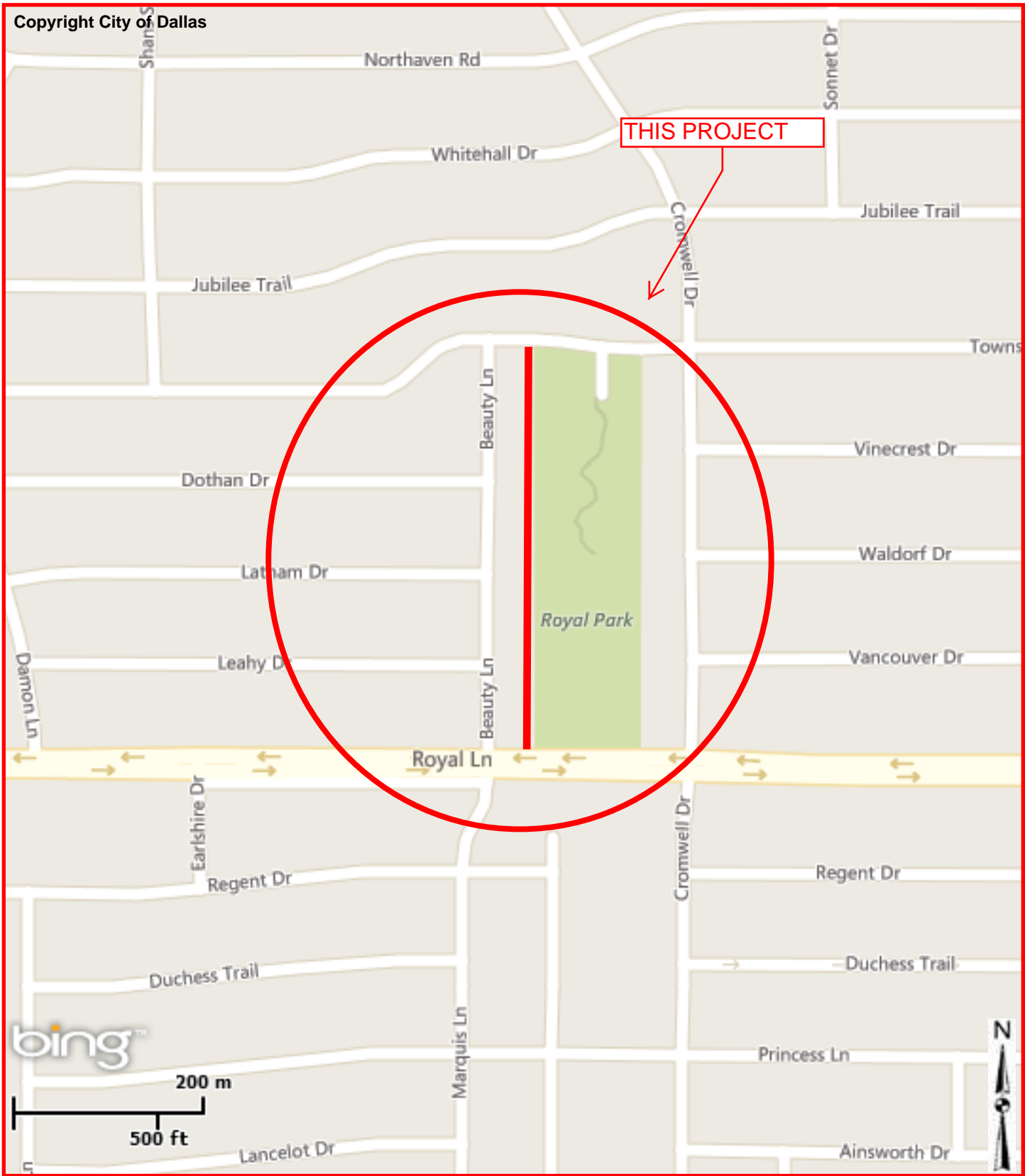


MAPSCO 13Y

ALLEY RECONSTRUCTION GROUP 12-2041
ALLEY BETWEEN DEEP VALLEY DRIVE AND CLEAR COVE LANE FROM ROSSER ROAD
TO ROSSER ROAD



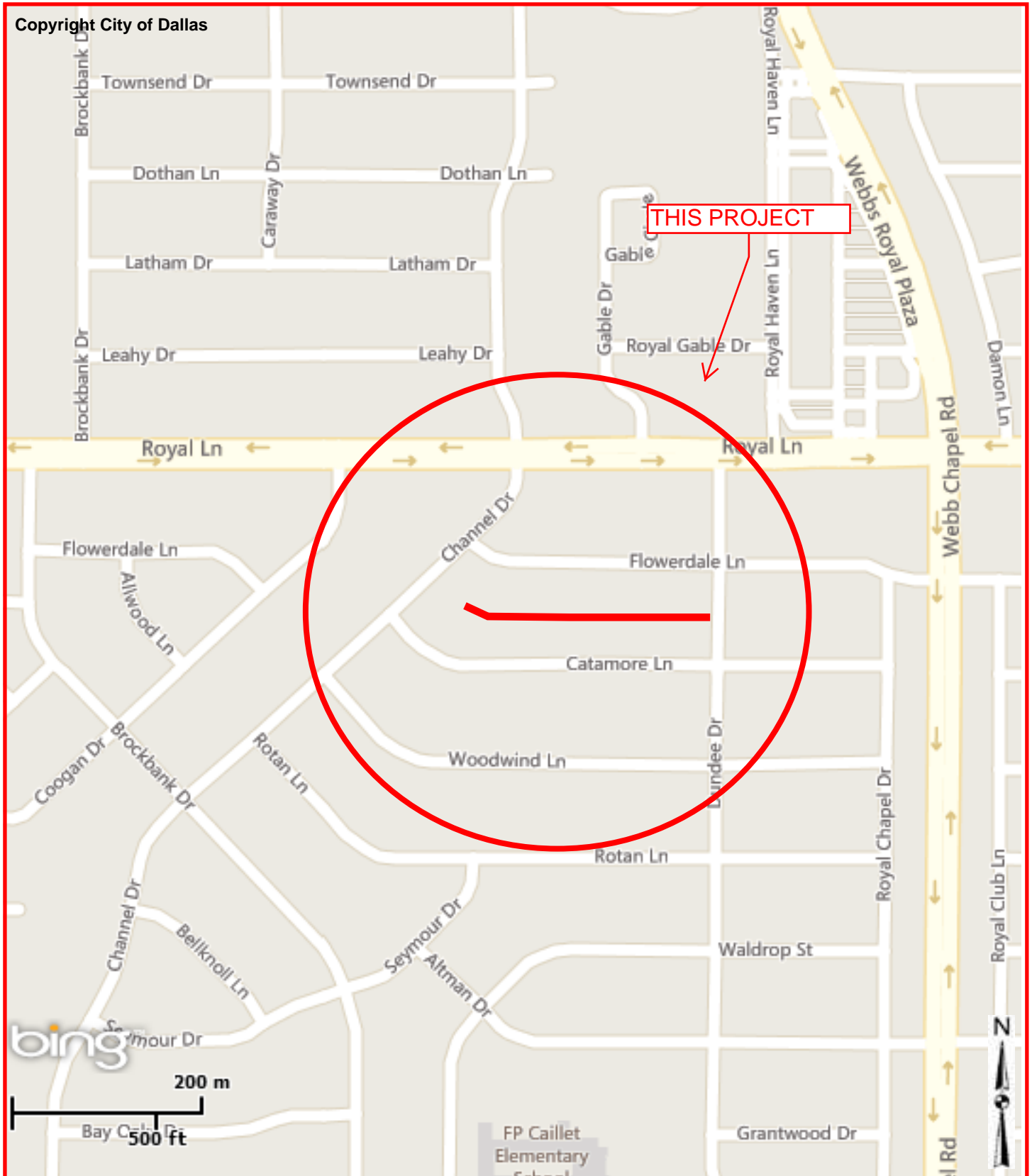
ALLEY RECONSTRUCTION GROUP 12-2043
ALLEY BEHIND BEAUTY LANE FROM ROYAL LANE
TO TOWNSEND DRIVE



MAPSCO 23G

ALLEY RECONSTRUCTION GROUP 12-2043

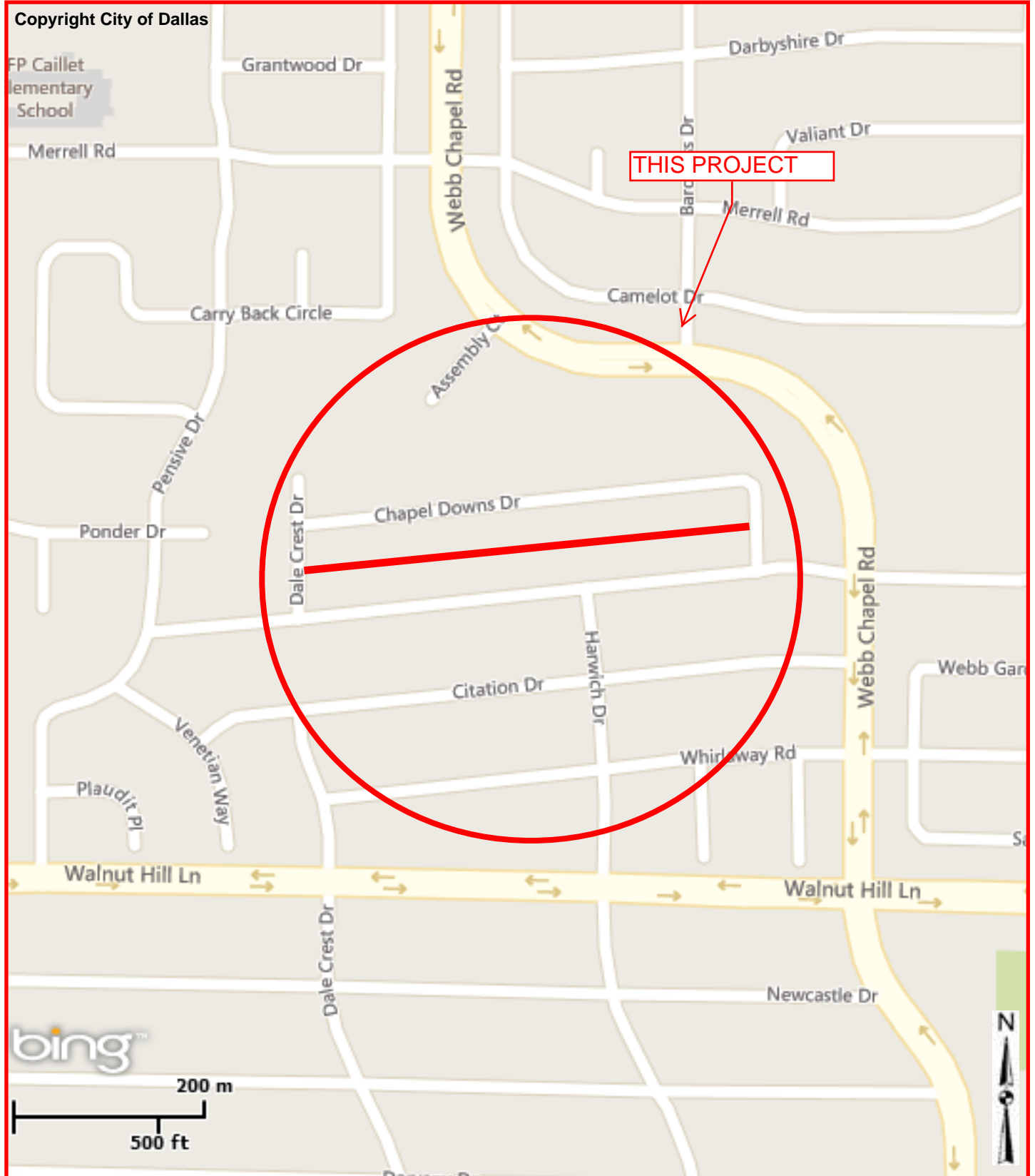
ALLEY BETWEEN CATAMORE LANE AND FLOWERDALE LANE FROM
ALLEY BETWEEN CHANNEL DRIVE AND DUNDEE DRIVE TO DUNDEE DRIVE



MAPSCO 23F

ALLEY RECONSTRUCTION GROUP 12-2043

ALLEY BETWEEN CHAPEL DOWNS DRIVE AND TIMBERVIEW ROAD FROM
DALE CREST DRIVE TO CHAPEL DOWNS DRIVE



MAPSCO 23L

January 27, 2016

WHEREAS, on September 25, 2013, Resolution No. 13-1731 authorized professional services contracts for engineering design services with Bury - DFW, Inc. f/k/a Bury + Partners, Inc. for Alley Reconstruction Group 12-2041 and GarzaBury, LLC for Alley Reconstruction Group 12-2043; and,

WHEREAS, bids were received on October 30, 2015, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2041 and 12-2043, as follows:

<u>BIDDERS</u>	<u>BID AMOUNT</u>
Ark Contracting Services, LLC	\$2,871,476.00
RBR Infrastructure & Road, LLC	\$2,999,512.00

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to enter into a contract with Ark Contracting Services, LLC for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2041 and 12-2043 in an amount not to exceed \$2,871,476.00, this being the lowest responsible bid received as indicated by the tabulation of bids, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Street and Transportation Improvements Fund Fund 2U22, Department PBW, Unit S229, Act. AREC Obj. 4510, Program #PB12S229, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 150,131.00
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Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S229, Act. AREC Obj. 4510, Program #PB12S229, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 153,955.00
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January 27, 2016

Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S230, Act. AREC Obj. 4510, Program #PB12S230, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 291,164.00
Street and Transportation Improvements Fund Fund 2U22, Department PBW, Unit S240, Act. AREC Obj. 4510, Program #PB12S240, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 159,179.00
Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S240, Act. AREC Obj. 4510, Program #PB12S240, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 156,395.00
Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S243, Act. AREC Obj. 4510, Program #PB12S243, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 160,915.00
Street and Transportation Improvements Fund Fund 2U22, Department PBW, Unit S245, Act. AREC Obj. 4510, Program #PB12S245, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 207,660.00
Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S245, Act. AREC Obj. 4510, Program #PB12S245, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 270,895.00
Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S220, Act. AREC Obj. 4510, Program #PB12S220, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 341,358.00
Street and Transportation Improvements Fund Fund 2U22, Department PBW, Unit S227, Act. AREC Obj. 4510, Program #PB12S227, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 101,333.00

January 27, 2016

Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S227, Act. AREC Obj. 4510, Program #PB12S227, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 166,203.00
Street and Transportation Improvements Fund Fund 2U22, Department PBW, Unit S231, Act. AREC Obj. 4510, Program #PB12S231, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 111,143.00
Street and Transportation Improvements Fund Fund 3U22, Department PBW, Unit S231, Act. AREC Obj. 4510, Program #PB12S231, CT PBW12S220D1 Vendor #VS0000017816, in amount not to exceed	\$ 296,650.00
Wastewater Capital Improvement Fund Fund 2116, Department DWU, Unit PS42 Obj. 4560, Program #716034, CT PBW716034CP Vendor #VS0000017816, in amount not to exceed	\$ 290,895.00
Wastewater Construction Fund Fund 0103, Department DWU, Unit CS42 Obj. 3222, Program #716034X, CT PBW716034EN Vendor #VS0000017816, in amount not to exceed	<u>\$ 13,600.00</u>
Total amount not to exceed	\$2,871,476.00

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): All

DEPARTMENT: Public Works Department
Water Utilities

CMO: Jill A. Jordan, P.E., 670-5299
Mark McDaniel, 670-3256

MAPSCO: Various

SUBJECT

Authorize a twenty-four month sidewalk and barrier free ramp installation paving services contract at various locations throughout the city - Vescorp Construction, LLC dba Chavez Concrete Cutting, lowest responsible bidder of four - Not to exceed \$3,501,400 - Financing: Capital Assessment Funds (\$351,000), 2003 Bond Funds (\$276,687), 2006 Bond Funds (\$1,489,973), General Obligation Commercial Paper Funds (\$869,150), 2012 Bond Funds (\$439,715) and Water Utilities Capital Construction Funds (\$74,875)

BACKGROUND

This action will authorize a twenty-four month sidewalk and barrier free ramp installation paving services contract for the sidewalk replacement and barrier free ramp program. The installation of barrier free ramps throughout the city is required in order to be in compliance with the Americans with Disabilities Act (ADA). This contract will also include water and wastewater adjustments. This is a citizen-driven program based on demand. Work orders are created as citizens pay to get their sidewalk replaced.

ESTIMATED SCHEDULE OF PROJECT

Begin Construction	March 2016
Complete Construction	March 2018

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item was provided to the Transportation and Trinity River Project Committee on January 11, 2016.

FISCAL INFORMATION

Capital Assessment Funds - \$350,999.80

2003 Bond Funds - \$276,687.00

2006 Bond Funds - \$1,489,972.66

2006 Bond Program (General Obligation Commercial Paper Funds) - \$869,150.00

2012 Bond Funds - \$439,715.54

Water Utilities Capital Construction Funds - \$74,875.00

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

Vescorp Construction, LLC dba Chavez Concrete Cutting

Hispanic Female	4	Hispanic Male	33
African-American Female	0	African-American Male	0
Other Female	0	Other Male	0
White Female	0	White Male	0

BID INFORMATION

The following bids with quotes were received and opened on November 6, 2015.

*Denotes successful bidder

<u>BIDDERS</u>	<u>BID AMOUNT</u>
*Vescorp Construction, LLC dba Chavez Concrete Cutting 4422 S. Peachtree Road Balch Springs, TX 75180	\$3,501,400.00
Omega Contracting, Inc.	\$4,253,708.97
RBR Infrastructure Road	\$4,984,600.00
NPL Construction Co.	\$5,340,000.00

OWNER

Vescorp Construction, LLC dba Chavez Concrete Cutting

Hugo Chavez, President

MAP

Attached.

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a twenty-four month sidewalk and barrier free ramp installation paving services contract at various locations throughout the city - Vescorp Construction, LLC dba Chavez Concrete Cutting, lowest responsible bidder of four - Not to exceed \$3,501,400 - Financing: Capital Assessment Funds (\$351,000), 2003 Bond Funds (\$276,687), 2006 Bond Funds (\$1,489,973), General Obligation Commercial Paper Funds (\$869,150), 2012 Bond Funds (\$439,715) and Water Utilities Capital Construction Funds (\$74,875)

Vescorp Construction, LLC dba Chavez Concrete Cutting is a local, minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Construction

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$3,501,400.00	100.00%
Total non-local contracts	\$0.00	0.00%
TOTAL CONTRACT	\$3,501,400.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Vescorp Construction	HMDB63992Y0116	\$2,626,050.00	75.00%
Thousand Mile South Construction Inc	HMMB64788Y0417	\$875,350.00	25.00%
Total Minority - Local		\$3,501,400.00	100.00%

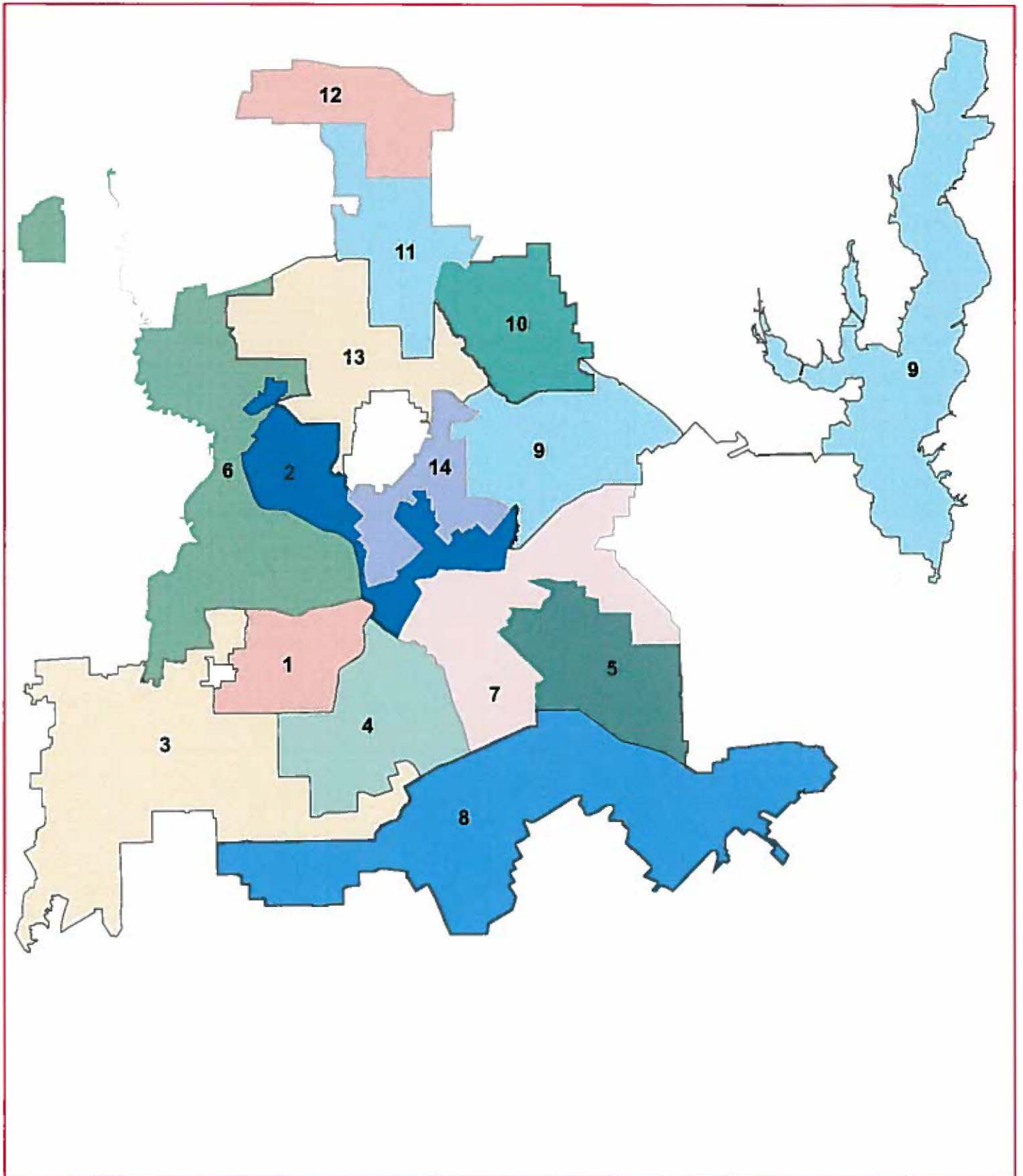
Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$3,501,400.00	100.00%	\$3,501,400.00	100.00%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$0.00	0.00%
Total	\$3,501,400.00	100.00%	\$3,501,400.00	100.00%

PAVING SERVICES CONTRACT
Infrastructure at Various Locations



January 27, 2016

WHEREAS, bids were received on November 6, 2015, for sidewalk and barrier free ramp installation paving services contract, water and wastewater adjustments at various locations throughout the city:

<u>BIDDERS</u>	<u>BID AMOUNT</u>
Vescorp Construction, LLC dba Chavez Concrete Cutting	\$3,501,400.00
Omega Contracting, Inc.	\$4,253,708.97
RBR Infrastructure Road	\$4,984,600.00
NPL Construction Co.	\$5,340,000.00

WHEREAS, the bid submitted by Vescorp Construction, LLC dba Chavez Concrete Cutting, in the amount of \$3,501,400.00 is the lowest and best of all bids received.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute a twenty-four month contract with Vescorp Construction, LLC dba Chavez Concrete Cutting for sidewalk and barrier free ramp installation paving services contract, water and wastewater adjustments, in an amount not to exceed \$3,501,400.00, this being the lowest responsive bid received as indicated by the tabulation of bids, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Capital Assessment Fund Fund L098, Department PBW, Unit P414, Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$350,999.80
Street and Transportation Improvements Fund Fund 4R22, Department PBW, Unit R474, Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$94,580.00
Street and Transportation Improvements Fund Fund 5R22, Department PBW, Unit R474, Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$94,580.00

January 27, 2016

Street and Transportation Improvements Fund Fund 6R22, Department PBW, Unit R474, Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$87,527.00
Street and Transportation Improvements Fund Fund 2T22, Department PBW, Unit Various, Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$1,391,743.26
Street and Transportation Improvements Fund Fund 4T22, Department PBW, Unit W009 Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$869,150.00
Street and Transportation Improvements Fund Fund 7T22, Department PBW, Unit R474 Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$98,229.40
Street and Transportation Improvements Fund Fund 1U22, Department PBW, Unit S410 Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$13,063.40
Street and Transportation Improvements Fund Fund 2U22, Department PBW, Unit Various, Act. SIDI, Obj. 4510, Vendor # VS89922 in an amount not to exceed	\$426,652.14
Water Construction Fund Fund 0102, Department DWU, Unit CW42, Obj. 3221, Program# 7A1311X, Vendor #VS89922 in an amount not to exceed	\$70,375.00
Wastewater Construction Fund Fund 0103, Department DWU, Unit CS42, Obj. 3222, Program# 7A1310X, Vendor #VS89922 in an amount not to exceed	<u>\$4,500.00</u>
Total in an amount not to exceed	\$3,501,400.00

January 27, 2016

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

AGENDA ITEM # 27

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 3

DEPARTMENT: Public Works Department
Office of Economic Development

CMO: Jill A. Jordan, P.E., 670-5299
Ryan S. Evans, 671-9837

MAPSCO: 43T X

SUBJECT

Authorize an increase in the contract with North Texas Contracting, Inc. for the removal of unsuitable subgrade material discovered during excavation and replacing the void with select fill material, pavement marking modifications and to provide necessary slope stabilization measures to prevent erosion along the Colorado Boulevard extension from Interstate Highway 30 eastbound service road to Westmoreland Road - Not to exceed \$218,406, from \$4,219,951 to \$4,438,357 - Financing: 2012 Bond Funds

BACKGROUND

On June 25, 2014, Resolution No. 14-1018 authorized a contract with North Texas Contracting, Inc. for the construction of utilities, roadway and streetscape improvements for the Colorado Boulevard Extension from IH30 eastbound service road to approximately 1900 feet south, as well as construction of water line from 1900 feet south of IH30 to Westmoreland Road. This action will authorize an increase in the contract with North Texas Contracting, Inc. for the removal of unsuitable subgrade material discovered during excavation and replacing the void with select fill material, pavement marking modifications and to provide necessary slope stabilization measures to prevent erosion along the Colorado Boulevard extension from Interstate Highway 30 eastbound service road to Westmoreland Road.

BACKGROUND (Continued)

During construction, isolated pockets of contaminated soil were encountered and had to be disposed of properly. The resulting voids created by the removal of the contaminated soil were filled with approximately 1,016 cubic yard of fill material suitable for a roadway subgrade. Additionally, approximately 1,440 tons of rocks and boulders not suitable for subgrade material were hauled off. In order to stabilize steep slope areas in the parkway and prevent erosion during rain events, it was also necessary to provide 24,600 square yards of erosion control blankets. In order to minimize future maintenance, pavement marking modifications that entailed replacing the originally proposed thermo lane markings with an adhesive tape product were incorporated into the construction project in areas where the roadway consisted of brick pavers.

ESTIMATED SCHEDULE OF PROJECT

Began Design	March 2013
Completed Design	April 2014
Began Construction	July 2014
Complete Construction	July 2016

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a Chapter 380 economic development grant agreement with SLF III - THE CANYON TIF, LP to support the proposed Canyon Development on March 27, 2013, by Resolution No. 13-0551.

Authorized a construction contract with North Texas Contracting, Inc. on June 25, 2014, by Resolution No. 14-1018.

Authorized Change Order No. 1 to the construction contract with North Texas Contracting, Inc. on January 28, 2015, by Resolution No. 15-0173.

Information about this item was provided to the Transportation and Trinity River Project Committee on January 11, 2016.

FISCAL INFORMATION

2012 Bond Funds - \$218,406.00

Design (SLF III)	\$ 327,140.00
Construction	
Paving & Drainage - PBW	\$3,713,015.70
Change Order No. 1	\$ 506,934.50
Change Order No. 2 (this action)	<u>\$ 218,406.00</u>
Total Project Cost	\$4,765,496.20

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

North Texas Contracting, Inc.

Hispanic Female	3	Hispanic Male	311
African-American Female	0	African-American Male	2
Other Female	0	Other Male	0
White Female	3	White Male	46

OWNER

North Texas Contracting, Inc.

Zach Fusilier, Vice President

MAP

Attached.

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize an increase in the contract with North Texas Contracting, Inc. for the removal of unsuitable subgrade material discovered during excavation and replacing the void with select fill material, pavement marking modifications and to provide necessary slope stabilization measures to prevent erosion along the Colorado Boulevard extension from Interstate Highway 30 eastbound service road to Westmoreland Road - Not to exceed \$218,406, from \$4,219,951 to \$4,438,357 - Financing: 2012 Bond Funds

North Texas Contracting, Inc. is a non-local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Construction

LOCAL/NON-LOCAL CONTRACT SUMMARY - THIS ACTION ONLY

	<u>Amount</u>	<u>Percent</u>
Local contracts	\$0.00	0.00%
Non-local contracts	\$218,406.00	100.00%
TOTAL THIS ACTION	\$218,406.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION THIS ACTION

Local Contractors / Sub-Contractors

None

Non-Local Contractors / Sub-Contractors

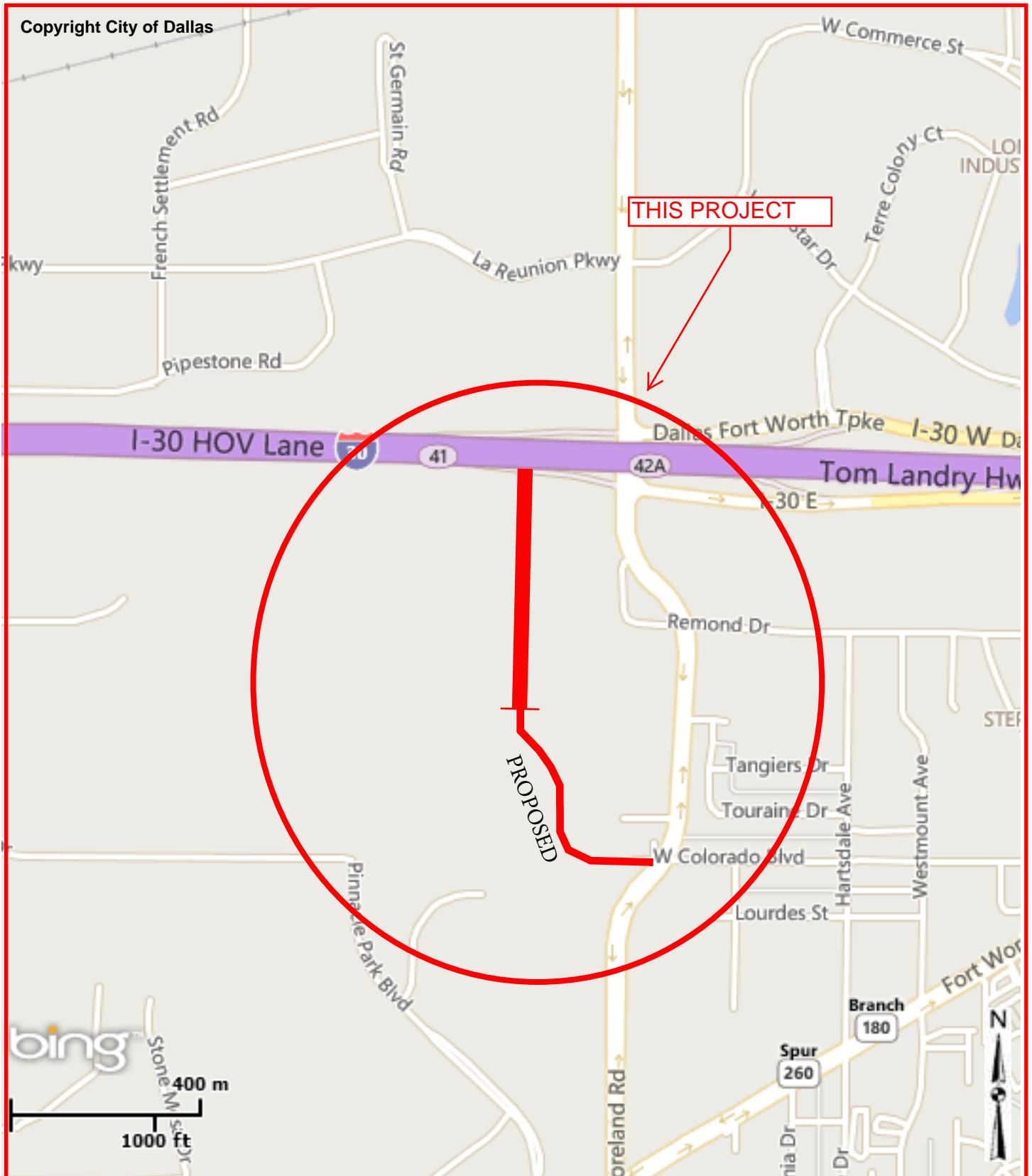
<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Brenda Price Trucking	WFDB22413Y0516	\$15,260.00	6.99%
Total Minority - Non-local		\$15,260.00	6.99%

TOTAL M/WBE PARTICIPATION

	<u>This Action</u>		<u>Participation to Date</u>	
	<u>Amount</u>	<u>Percent</u>	<u>Amount</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$0.00	0.00%	\$397,302.00	8.95%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$15,260.00	6.99%	\$687,844.50	15.50%
Total	\$15,260.00	6.99%	\$1,085,146.50	24.45%

COLORADO BOULEVARD EXTENSION

FROM IH 30 EASTBOUND SERVICE ROAD TO APPROX. 1900 FEET SOUTH



MAPSCO 43T, X

January 27, 2016

WHEREAS, on March 27, 2013, Resolution No. 13-0551 authorized the City to enter into a Chapter 380 economic development grant agreement with SLF III - THE CANYON TIF, LP to reimburse the developer for the design and engineering costs for public infrastructure in the amount of \$327,140.00; and,

WHEREAS, on June 25, 2014, Resolution No. 14-1018 authorized a construction contract with North Texas Contracting, Inc. for the construction of utilities, roadway and streetscape improvements for the Colorado Boulevard Extension Project in the amount of \$3,713,015.70; and,

WHEREAS, on January 28, 2015, Resolution No. 15-0173 authorized Change Order No. 1 to the contract with North Texas Contracting, Inc. for construction of utilities, roadway and streetscape improvements for the Colorado Boulevard Extension from Interstate Highway 30 eastbound service road to Westmoreland Road to add new pay items and increase the quantity of some of the existing pay items related to grading, earthwork, drainage and utility infrastructure necessary to complete the project in the amount of \$506,934.50, increasing the contract from \$3,713,015.70 to \$4,219,950.20; and,

WHEREAS, it is now necessary to authorize Change Order No. 2 to the contract with North Texas Contracting, Inc. for the removal of unsuitable subgrade material discovered during excavation and replacing the void with select fill material, pavement marking modifications and to provide necessary slope stabilization measures to prevent erosion along the Colorado Boulevard extension from Interstate Highway 30 eastbound service road to Westmoreland Road in the amount of \$218,406.00, increasing the contract from \$4,219,950.20 to \$4,438,356.20.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute Change Order No. 2 to the contract with North Texas Contracting, Inc. for the removal of unsuitable subgrade material discovered during excavation and replacing the void with select fill material, pavement marking modifications and to provide necessary slope stabilization measures to prevent erosion along the Colorado Boulevard extension from Interstate Highway 30 eastbound service road to Westmoreland Road, in an amount not to exceed \$218,406.00, increasing the contract from \$4,219,950.20 to \$4,438,356.20, after it has been approved as to form by the City Attorney.

January 27, 2016

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Economic & S. Area of City Transit-Oriented Development Fund
Fund 2U52, Department ECO, Unit P889, Act. ECNR
Obj. 4599, Program #ECO12P889, CT ECOP889H272
Vendor #514455, in an amount not to exceed \$218,406.00

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2

DEPARTMENT: Public Works Department
Aviation

CMO: Jill A. Jordan, P.E., 670-5299
Ryan S. Evans, 671-9837

MAPSCO: 34K

SUBJECT

Authorize Supplemental Agreement No. 1 to the contract with O'Brien Engineering, Inc. to provide engineering services for design and construction documents for a parking area adjacent to the Tom Braniff Channel Cover project at Dallas Love Field - Not to exceed \$49,874, from \$195,596 to \$245,470 - Financing: Aviation Capital Construction Funds

BACKGROUND

This action will authorize Supplemental Agreement No. 1 to the contract with O'Brien Engineering, Inc., to provide engineering services for design and construction documents for a parking area adjacent to the Tom Braniff Channel Cover project at Dallas Love Field.

The Department of Aviation has identified an opportunity to expand the area located adjacent to the Tom Braniff Channel project for additional surface lot parking.

ESTIMATED SCHEDULE OF PROJECT

Began Design	May 2015
Complete Design	October 2016
Begin Construction	January 2017
Complete Construction	January 2018

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a professional services contract for the design and preparation of construction documents for the Tom Braniff Channel Cover project at Dallas Love Field on June 17, 2015, by Resolution No. 15-1160.

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS) (Continued)

Information about this item was provided to the Transportation and Trinity River Project Committee on January 11, 2016.

FISCAL INFORMATION

Aviation Capital Construction Funds - \$49,874.00

Design	\$195,595.60
Supplemental Agreement No. 1 (this action)	<u>\$ 49,874.00</u>
Total	\$245,469.60

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

O'Brien Engineering, Inc.

Hispanic Female	0	Hispanic Male	1
African-American Female	0	African-American Male	0
Other Female	0	Other Male	0
White Female	3	White Male	6

OWNER

O'Brien Engineering, Inc.

Jim O'Brien, P.E., Certified Floodplain Manager, Fellow Society of American Military Engineers

MAP

Attached

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize Supplemental Agreement No. 1 to the contract with O'Brien Engineering, Inc. to provide engineering services for design and construction documents for a parking area adjacent to the Tom Braniff Channel Cover project at Dallas Love Field - Not to exceed \$49,874, from \$195,596 to \$245,470 - Financing: Aviation Capital Construction Funds

O'Brien Engineering, Inc. is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Architecture & Engineering

LOCAL/NON-LOCAL CONTRACT SUMMARY - THIS ACTION ONLY

	<u>Amount</u>	<u>Percent</u>
Local contracts	\$49,874.00	100.00%
Non-local contracts	\$0.00	0.00%
TOTAL THIS ACTION	\$49,874.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION THIS ACTION

Local Contractors / Sub-Contractors

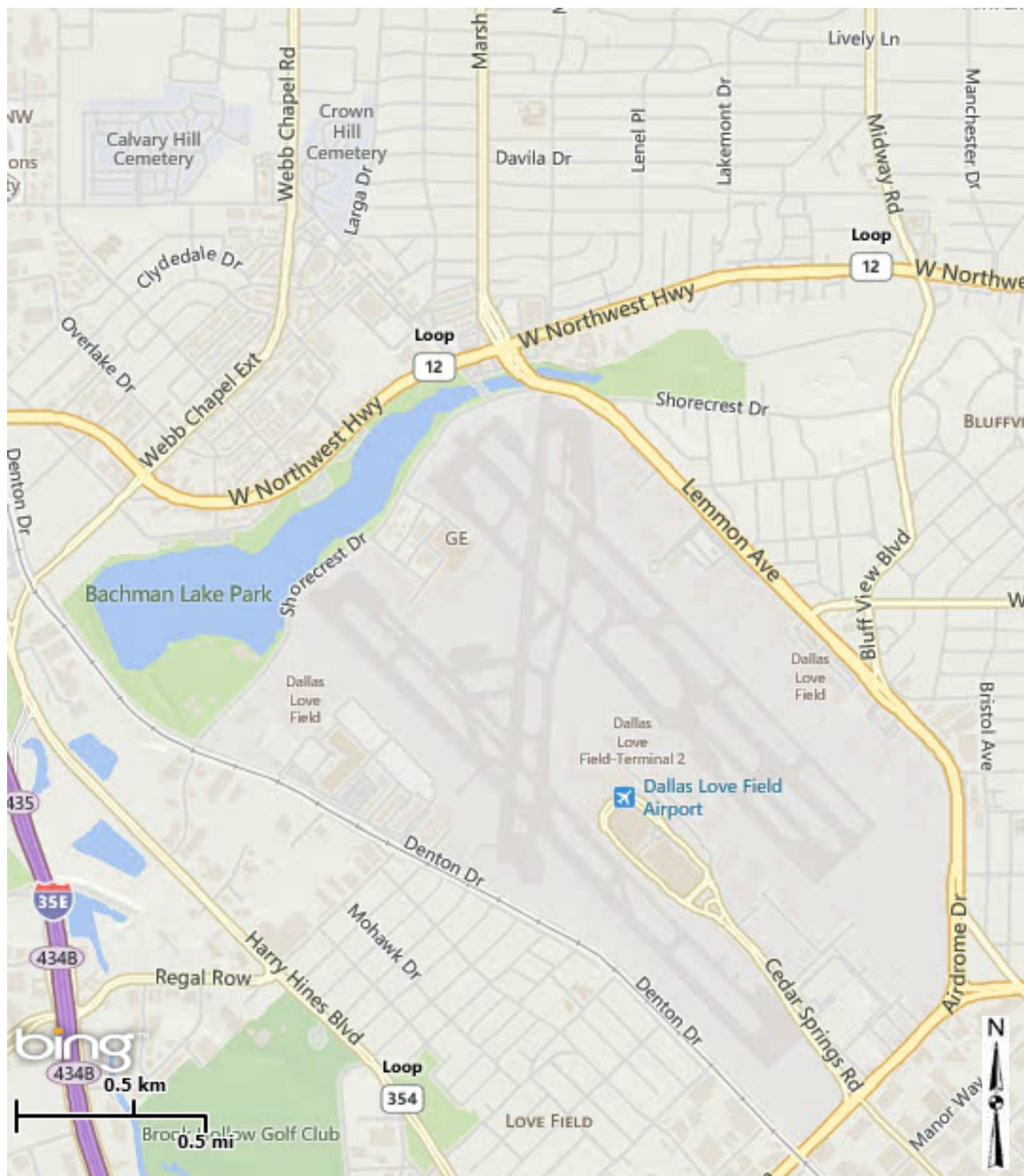
<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Alliance Geotechnical Group, Inc.	BMDB63934Y0116	\$5,000.00	10.03%
CP&Y, Inc.	PMMB64115Y0217	\$2,400.00	4.81%
Craig Design Group, Inc.	WFDB25886Y0916	\$5,950.00	11.93%
Total Minority - Local		\$13,350.00	26.77%

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE PARTICIPATION

	<u>This Action</u>		<u>Participation to Date</u>	
	<u>Amount</u>	<u>Percent</u>	<u>Amount</u>	<u>Percent</u>
African American	\$5,000.00	10.03%	\$5,000.00	2.04%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$2,400.00	4.81%	\$47,020.00	19.16%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$5,950.00	11.93%	\$23,200.00	9.45%
Total	\$13,350.00	26.77%	\$75,220.00	30.64%



Dallas Love Field

January 27, 2016

WHEREAS, Dallas Love Field has determined a need for additional parking for the Taxicab Queuing lot located adjacent to the Tom Braniff Channel Cover project; and,

WHEREAS, covering the existing Tom Braniff open channel will result in additional parking areas for the Taxicab Queuing Lot; and,

WHEREAS, O'Brien Engineering, Inc. was selected as the most qualified proposer of four, as a result of a qualifications-based selection process in accordance with City of Dallas procurement guidelines for the professional services for the Tom Braniff Channel Cover project at Dallas Love Field; and,

WHEREAS, on June 17, 2015, Resolution No. 15-1160 authorized a contract with O'Brien Engineering, Inc. to provide engineering services for design and construction documents for the Tom Braniff Channel Cover project at Dallas Love Field, in the amount of \$195,595.60; and,

WHEREAS, an area adjacent to the Tom Braniff Channel Cover project was identified as an area that can be utilized for additional parking; and,

WHEREAS, it is desirable to authorize Supplemental Agreement No. 1 with O'Brien Engineering, Inc. to provide engineering services for design and construction documents for a parking area adjacent to the Tom Braniff Channel Cover Project at Dallas Love Field, in an amount not to exceed \$49,874.00, from \$195,595.60 to \$245,469.60.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute Supplemental Agreement No. 1 to the contract with O'Brien Engineering, Inc. to provide engineering services for design and construction documents for a parking area adjacent to the Tom Braniff Channel Cover project at Dallas Love Field, in an amount not to exceed \$49,874.00, from \$195,595.60 to \$245,469.60, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds, in an amount not to exceed \$49,874.00, in accordance with the terms and conditions of the contract from:

Aviation Capital Construction Fund
Fund 0131, Department AVI, Unit P949, Activity AAIP,
Object 4111, Program AVTBCCP, CT AVIOEIP949
Vendor # 352724 in an amount not to exceed

\$49,874.00

January 27, 2016

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2

DEPARTMENT: Public Works Department
Aviation

CMO: Jill A. Jordan, P.E., 670-5299
Ryan S. Evans, 671-9837

MAPSCO: 23Z; 24W; 33D H; 34A E

SUBJECT

Authorize Supplemental Agreement No. 2 to the professional services contract with HNTB Corporation to provide design, construction documents and bidding services for a Runway Incursion Project identified by the Federal Aviation Administration to be part of the Runway 18-36 Conversion Project at Dallas Love Field - Not to exceed \$287,434, from \$762,421 to \$1,049,855 - Financing: Aviation Capital Construction Funds

BACKGROUND

This action will authorize Supplemental Agreement No. 2 to the professional services contract with HNTB Corporation to provide design, construction documents and bidding services for a Runway Incursion Project identified by the Federal Aviation Administration (FAA) to be part of the Runway 18-36 Conversion Project at Dallas Love Field.

Dallas Love Field has a high incident rate of runway incursions on the approach end of Runway 13L. A runway incursion occurs when an aircraft does not stop at the hold sign before it has been cleared to enter the runway for takeoff. The FAA requested this project, which will simplify the geometry in that area, to mitigate future runway incursions. The FAA requested that the construction bids be procured by the summer of 2016. The design and construction documents will delineate the work required to physically remove a section of Taxiway A from Runway 18/36.

ESTIMATED SCHEDULE OF PROJECT

Began Design	April 2015
Complete Design	August 2016
Begin Construction	July 2016
End Construction	July 2017

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a professional services contract with HNTB Corporation on April 22, 2015, by Resolution No. 15-0806.

Authorized Supplemental Agreement No. 1 with HNTB Corporation on October 14, 2015, by Resolution No. 15-1880.

Information about this item was presented to the Transportation and Trinity River Project Committee on January 11, 2016.

FISCAL INFORMATION

Aviation Capital Construction Funds - \$287,434.00

Design	\$ 236,929.00
Project administration costs	\$ 50,000.00
Supplemental Agreement No. 1	\$ 525,492.00
Supplemental Agreement No. 2 (this action)	<u>\$ 287,434.00</u>
Total	\$1,099,855.00

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

HNTB Corporation

Hispanic Female	6	Hispanic Male	7
African-American Female	2	African-American Male	1
Other Female	6	Other Male	7
White Female	22	White Male	46

OWNER

HNTB Corporation

Kevin L. Wallace, P.E., Vice President

MAP

Attached

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize Supplemental Agreement No. 2 to the professional services contract with HNTB Corporation to provide design, construction documents and bidding services for a Runway Incursion Project identified by the Federal Aviation Administration to be part of the Runway 18-36 Conversion Project at Dallas Love Field - Not to exceed \$287,434, from \$762,421 to \$1,049,855 - Financing: Aviation Capital Construction Funds

HNTB Corporation is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Architecture & Engineering

LOCAL/NON-LOCAL CONTRACT SUMMARY - THIS ACTION ONLY

	<u>Amount</u>	<u>Percent</u>
Local contracts	\$208,900.00	72.68%
Non-local contracts	\$78,534.00	27.32%
TOTAL THIS ACTION	\$287,434.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION THIS ACTION

Local Contractors / Sub-Contractors

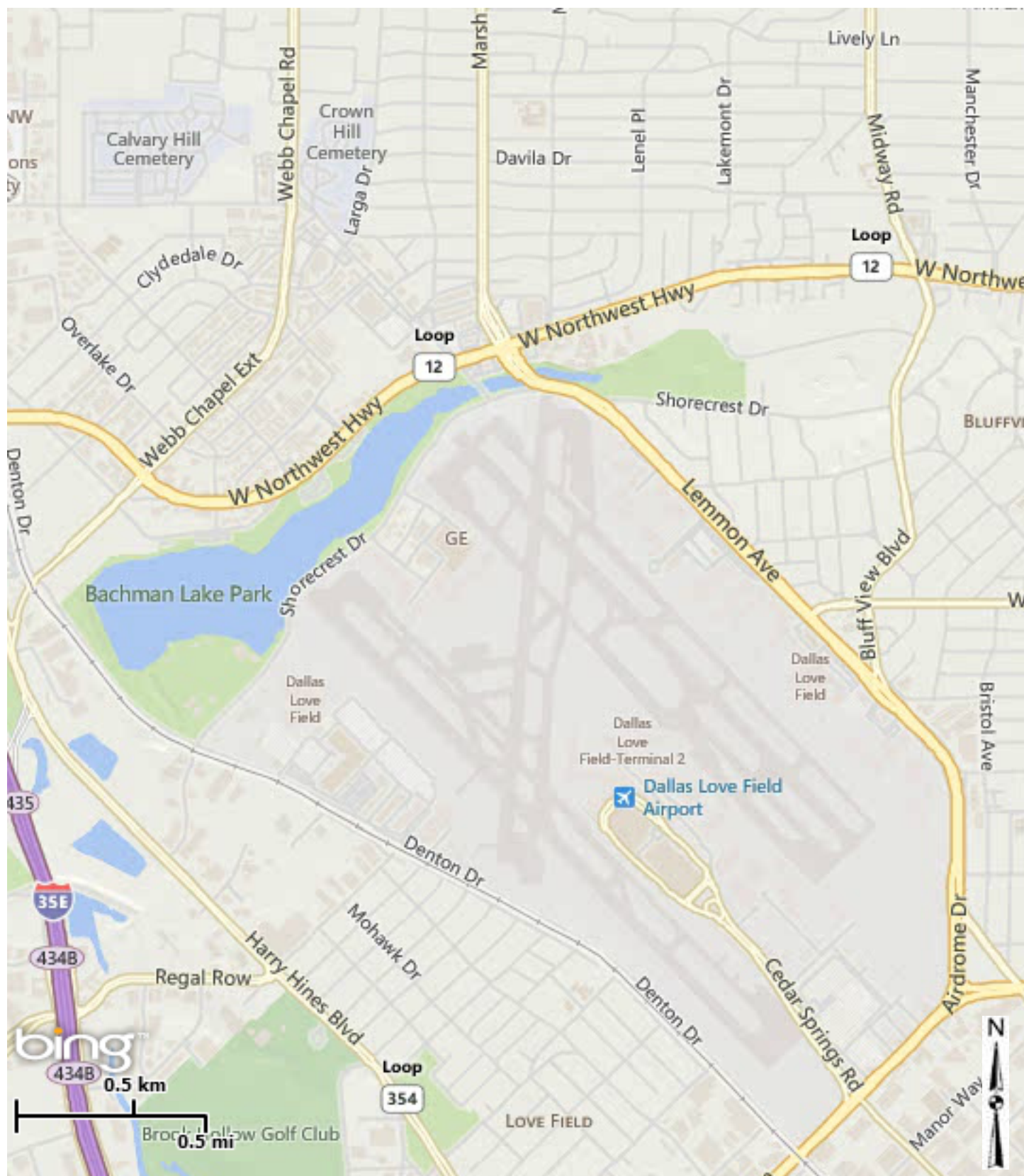
<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
ARS Engineers, Inc.	IMDB14605Y0516	\$28,947.08	13.86%
Total Minority - Local		\$28,947.08	13.86%

Non-Local Contractors / Sub-Contractors

<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Aviation Alliance, Inc.	WFDB85002Y0516	\$51,534.00	65.62%
Williams CM Group, LLC DBE	WFDB33386Y0716	\$27,000.00	34.38%
Total Minority - Non-local		\$78,534.00	100.00%

TOTAL M/WBE PARTICIPATION

	This Action		Participation to Date	
	<u>Amount</u>	<u>Percent</u>	<u>Amount</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$11,585.00	1.10%
Hispanic American	\$0.00	0.00%	\$0.00	0.00%
Asian American	\$28,947.08	10.07%	\$68,146.89	6.49%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$78,534.00	27.32%	\$258,116.00	24.59%
Total	\$107,481.08	37.39%	\$337,847.89	32.18%



Dallas Love Field

January 27, 2016

WHEREAS, Runway 18/36 does not meet Federal Aviation Administration (FAA) standards for runway safety areas or for visual approach slope indicator lights; and,

WHEREAS, the Department of Aviation has determined that decommissioning the runway is more prudent than correcting the deficiencies; and,

WHEREAS, HNTB Corporation was selected as the most qualified proposer of the three proposers as a result of a qualifications-based selection process, in accordance with the City of Dallas procurement guidelines; and,

WHEREAS, on April 22, 2015, Resolution No. 15-0806 authorized a contract with HNTB Corporation to provide engineering services for the Runway 18-36 Conversion Project at Dallas Love Field, in an amount not to exceed \$236,929.00; and,

WHEREAS, HNTB Corporation, as part of the evaluation phase, developed a preferred alignment that has been approved by the Department of Aviation and the Federal Aviation Administration; and,

WHEREAS, on October 14, 2015, Resolution No. 15-1880 authorized Supplemental Agreement No. 1 to the contract with HNTB Corporation to provide design, construction documents and bidding services for the physical improvements necessary to convert the existing Runway 18/36 to a taxiway for the Runway 18-36 Conversion Project at Dallas Love Field in an amount not to exceed \$525,492.00, from \$236,929.00 to \$762,421.00; and,

WHEREAS, The Federal Aviation Administration has initiated a nationwide Runway Incursion Mitigation Program with the purpose of reducing the number of runway incursions throughout the air traffic system; and,

WHEREAS, The Federal Aviation Administration has identified an area located at Dallas Love Field that has a high number of runway incursions; and,

WHEREAS, The Federal Aviation Administration has identified improvements to the airfield geometry whose purpose is to reduce the number of runway incursions at Dallas Love Field; and,

WHEREAS, The Federal Aviation Administration has requested Dallas Love Field include this Runway Incursion Mitigation Project in the 2016 Capital Improvement Program as a construction project; and,

January 27, 2016

WHEREAS, it is now necessary to authorize Supplemental Agreement No. 2 to the professional services contract with HNTB Corporation to provide design, construction documents and bidding services for a Runway Incursion Project identified by the Federal Aviation Administration to be part of the Runway 18-36 Conversion Project at Dallas Love Field, in an amount not to exceed \$287,434.00, from \$762,421.00 to \$1,049,855.00.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute Supplemental Agreement No. 2 to the professional services contract with HNTB Corporation to provide design, construction documents and bidding services for a Runway Incursion Program Project identified by the Federal Aviation Administration to be part of the Runway 18-36 Conversion Project at Dallas Love Field, in an amount not to exceed \$287,434.00, from \$762,421.00 to \$1,049,855.00, after it has been approved as to form by the City Attorney.

Section 2. That the City will apply for Passenger Facility Charge (PFC) funding at a later date for the eligible design of the Runway Incursion Mitigation Project in an amount not to exceed \$71,856.00; and upon approval of the PFC for these projects, the Chief Financial Officer is hereby authorized to transfer an amount not to exceed \$71,856.00 from the PFC Fund 0477, Dept. AVI, Balance Sheet Account 0001 to the Aviation Capital Construction Fund 0131, Dept. AVI, Balance Sheet Account 0001. The transfer of cash is contingent upon the approval of Passenger Facility Charge (PFC) Funds by the Federal Aviation Administration.

Section 3. That the City will apply for grant funds from the FAA at a later date and upon approval and deposit of the grant funds, the Chief Financial Officer is hereby authorized to reclassify eligible design and construction costs for the 18-36 Runway Incursion Mitigation Project to the AIP Grant Fund Program (AIP) Grant Fund F482, Department AVI, Unit W047, OBJ 4111, from Aviation Capital Construction Fund 0131, Department AVI, Unit W047, OBJ 4111 in an amount not to exceed \$215,578.00.

Section 4. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$287,434.00 to be paid to HNTB Corporation in accordance with the terms and conditions of the contract:

Aviation Capital Construction Fund
Fund 0131, Department AVI, Unit W047, Act. AAIP, Comm. 92500,
Object 4111, Program # W047, CT AVIW047HNTFY16,
Vendor #352433, in an amount not to exceed \$287,434.00

January 27, 2016

Section 5. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 1, 2, 3, 4, 5, 6, 7, 8, 13

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 23P 42H L Q 43E F J K N P 44P 45Z 46M N R T U V W X Y Z
47J L N S W 48T V 49T 54K M V Z 55B F H K L N P T W X
56B C D G S T W X 57R 58C J L R Y 64F H Q V 65B F K 66E
N S W X 68C D G 69A-G P Q 70W 74 H 76A L

SUBJECT

Authorize the quitclaim of 120 properties acquired by the taxing authorities from the Tax Foreclosure Sheriff's Sale to the highest bidders; and authorize the execution of release of liens for any non-tax liens that may have been filed by the City and were included in the foreclosure judgment (list attached) - Revenue: \$798,707

BACKGROUND

This item authorizes the quitclaim of 120 properties that were foreclosed by the Sheriff's Department for unpaid taxes pursuant to judgments or seizure warrants from a District Court and the release of liens for any non-tax liens that may have been filed by the City and were included in the foreclosure judgment. These properties are being sold to the highest bidders and will return to the tax rolls upon conveyance.

Successful bidders are required to sign a certification stating that they are not purchasing these properties on behalf of the foreclosed owners and that they have no debts owed to the City, no pending code violations, and are not chronic code violators.

All properties were reviewed by the Housing Department for infill housing and were not desired for that program.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

Revenue: \$798,706.39

OWNERS

Camden Homes, LLC

MT Akhavizadeh, Managing Member

County Land and Water LLC

Wayne Prokay, Managing Member

JCRB Bucker Realty, L.P.

Juanky Enterprises, LLC, General Partner

Juan Ruvalcaba, Managing Member

King Home Builders, LLC

Jorge Lariz, Managing Member

Nextlots Now L.L.C.

Lee J. Schmitt, Managing Member

SDE Texas LLC

Samuel Aflalo, Managing Member

David Eitches, Managing Member

Everett Fujii, Managing Member

Abraham Galdian

Albert Almanza

Anthony Boyd

Aubrey Quarles

Benita Beltran

Benito Mojica

Constance Armstrong

DaPorscha Kelley

David Menn

Demond Thomas

OWNERS (Continued)

Devan Earle

Doric Earle

Edgar Milton Pineda

Friew Zerihun

J. Santos Coria

Jose Alfredo Ramirez

Juan Roberto Leon

Keith Marshall

Kimiaki Itamura

Lilia Jimenez

Luis Ramirez

M.W. Resnick

Maria Ospina

Maria Schneider

Nicholas A. Barnett Sr.

Omar Correa

Rolando Cobos

Sabrina Sutton

Saul Vargas

Temesgen Wukaye

William Baker

Yesenia Villela

MAP

Attached

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
PROPERTY LIST**

ITEM #	STREET ADDRESS	VAC/IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
1	2606 52ND	I	04	R-7.5(A)	.1721	\$18,961.00	3	\$8,000.00	\$9,999.99	\$146,190.00	Anthony Boyd
2	1706 ALASKA	V	04	R-7.5(A)	.1790	\$13,000.00	9	\$1,000.00	\$10,000.00	\$13,000.00	Camden Homes, LLC
3	3835 ATLANTA	I	07	PD-595	.0851	\$19,538.00	7	\$5,000.00	\$13,000.00	\$26,780.00	Demond Thomas
4	506 AVENUE	V	04	D(A)	.1261	\$9,500.00	2	\$500.00	\$1,451.00	\$9,500.00	Rolando Cobos
5	3302 BEAUCHAMP	V	04	R-5(A)	.2517	\$11,500.00	1	\$1,000.00	\$1,350.00	\$11,500.00	Nextlots Now L.L.C.
6	3042 S. BECKLEY	V	04	R-7.5(A)	.1640	\$13,500.00	1	\$1,000.00	\$10,000.00	\$13,500.00	Camden Homes, LLC
7	3614 BERTRAND	V	07	PD-595	.1492	\$1,724.00	1	\$500.00	\$608.00	\$3,250.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
8	4431 BONNIE VIEW	V	04	R-5(A)	.3523	\$5,920.00	1	\$250.00	\$795.00	\$6,000.00	Nextlots Now L.L.C.
9	3508 BOOKER	V	07	PD-595	.1030	\$17,310.00	1	\$250.00	\$449.00	\$6,000.00	Anthony Boyd
10	2702 BRIGHAM	V	07	PD-595	.1180	\$1,940.00	2	\$250.00	\$901.00	\$4,000.00	DaPorscha Kelley
11	15319 BUDEUDY	V	08	MH-(A)	.0601	\$5,000.00	6	\$500.00	\$8,475.00	\$4,500.00	Nextlots Now L.L.C.
12	1210 CABOT	V	08	R-7.5(A)	.3233	\$20,226.00	2	\$1,000.00	\$4,275.00	\$17,500.00	Nextlots Now L.L.C.
13	1613 CALDWELL	V	07	D(A)	.0876	\$3,820.00	2	\$500.00	\$2,175.00	\$3,820.00	Nextlots Now L.L.C.
14	3837 CANADA	V	06	R-5(A)	.2509	\$12,000.00	10	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
15	1527 CARSON	V	04	R-7.5(A)	.1592	\$1,535.00	2	\$1,000.00	\$2,161.80	\$11,500.00	Luis Ramirez
16	5815 CARY	V	07	R-7.5(A)	.1380	\$15,000.00	4	\$1,000.00	\$5,660.99	\$15,000.00	Abraham Galdian
17	315 E. CHERRY POINT	V	03	R-7.5(A)	.4713	\$3,500.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
PROPERTY LIST**

ITEM #	STREET ADDRESS	VAC/IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
18	307 E. CHERRY POINT	V	03	R-7.5(A)	.4589	\$3,500.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
19	339 E. CHERRY POINT	V	03	R-7.5(A)	.4572	\$,3500.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
20	323 E. CHERRY POINT	V	03	R-7.5(A)	.4564	\$4,000.00	4	\$500.00	\$1,289.00	\$4,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
21	6404 CINNAMON OAKS	V	08	R-5(A)	.1640	\$11,334.00	1	\$1,000.00	\$1,508.00	\$8,500.00	Devan Earle
22	1403 E. CLARENDON	V	04	PD-388	.0910	\$4,000.00	2	\$500.00	\$562.00	\$4,000.00	William Baker
23	3513 CLEVELAND	V	07	PD-595	.1917	\$12,530.00	1	\$1,000.00	\$2,475.00	\$12,530.00	Nextlots Now L.L.C.
24	3600 COLONIAL	V	07	PD-595	.1756	\$6,890.00	3	\$1,000.00	\$2,851.00	\$6,890.00	Rolando Cobos
25	5003 COLONIAL	V	07	PD-595	.0990	\$5,470.00	1	\$250.00	\$899.00	\$3,570.00	Devan Earle
26	2046 COOL MIST	V	08	R-7.5(A)	.1706	\$15,750.00	2	\$1,000.00	\$1,788.00	\$8,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
27	3943 COOLIDGE	V	07	PD-595	.1046	\$1,662.00	1	\$500.00	\$608.00	\$5,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
28	4006 COOLIDGE	V	07	PD-595	.0932	\$1,283.00	1	\$500.00	\$1,088.00	\$5,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
PROPERTY LIST**

ITEM #	STREET ADDRESS	VAC/IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
29	3918 COOLIDGE	V	07	PD-595	.1072	\$1,034.00	1	\$500.00	\$788.00	\$5,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
30	1104 COSTON	V	05	R-7.5(A)	.1675	\$15,000.00	3	\$1,000.00	\$7,110.00	\$15,000.00	David Menn
31	4419 CRANFILL	V	04	R-7.5(A)	.4685	\$12,000.00	3	\$1,000.00	\$6,510.00	\$12,000.00	David Menn
32	2238 DATHE	V	07	PD-595	.1459	\$1,646.00	1	\$500.00	\$1,288.00	\$6,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
33	2403 DATHE	V	07	R-5(A)	.1171	\$2,930.00	1	\$250.00	\$688.00	\$2,930.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
34	4019 S. DENLEY	V	04	R-7.5(A)	.7244	\$15,750.00	1	\$1,000.00	\$3,361.00	\$15,750.00	Anthony Boyd
35	2194 ECHO LAKE	V	08	R-7.5(A)	.1648	\$8,000.00	1	\$500.00	\$1,000.00	\$8,000.00	JCRB Bucker Realty, L.P.
36	2111 EDD	V	08	R-7.5(A)	.3473	\$12,000.00	6	\$1,000.00	\$4,795.00	\$12,000.00	Nextlots Now L.L.C.
37	4105 ELK HORN	V	04	R-7.5(A)	.2080	\$12,000.00	5	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
38	659 ELLA	V	05	R-7.5(A)	.1646	\$15,000.00	4	\$1,000.00	\$3,575.00	\$15,000.00	Nextlots Now L.L.C.
39	231 ELMORE	V	04	R-7.5(A)	.1335	\$15,000.00	10	\$1,000.00	\$10,000.00	\$15,000.00	Camden Homes, LLC
40	4107 ESMALDA	V	06	R-5(A)	.1434	\$10,000.00	8	\$1,000.00	\$8,100.00	\$12,000.00	J. Santos Coria
41	2131 S. EWING	V	04	R-7.5(A)	.1940	\$13,000.00	5	\$1,000.00	\$7,510.00	\$12,500.00	David Menn
42	2810 FARRAGUT	V	07	PD-595	.0811	\$2,019.00	1	\$500.00	\$901.00	\$3,200.00	DaPorscha Kelley
43	3217 FORDHAM	V	04	R-7.5(A)	.3812	\$10,000.00	3	\$1,000.00	\$4,575.00	\$10,000.00	Nextlots Now L.L.C.
44	1726 FORDHAM	V	04	R-7.5(A)	.1382	\$9,000.00	1	\$1,000.00	\$1,099.00	\$9,000.00	Anthony Boyd

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
PROPERTY LIST**

ITEM #	STREET ADDRESS	VAC/IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
45	2910 GAY	V	07	PD-595	.0785	\$9,750.00	1	\$500.00	\$1,000.00	\$5,000.00	Benita Beltran
46	3206 GOLDSPIER	V	07	PD-595	.1721	\$1,540.00	1	\$500.00	\$1,499.00	\$6,000.00	Devan Earle
47	3201 S. HASKELL	V	07	PD-305	.1358	\$17,448.00	4	\$3,000.00	\$8,200.00	\$17,750.00	Benito Mojica
48	4410 HELEN	V	07	CS	.1080	\$4,710.00	1	\$500.00	\$2,050.00	\$4,710.00	Nextlots Now L.L.C.
49	3623 HUMPHREY	V	04	R-5(A)	.3298	\$3,147.00	1	\$1,000.00	\$1,250.00	\$11,500.00	Nextlots Now L.L.C.
50	1230 HUTCHINS	V	04	R-7.5(A)	.1236	\$9,500.00	1	\$1,000.00	\$10,000.00	\$9,500.00	Camden Homes, LLC
51	3114 INDIANOLA	V	07	R-7.5(A)	.2321	\$9,318.00	4	\$1,000.00	\$6,600.00	\$15,000.00	Abraham Galdian
52	2434 INGERSOLL	V	06	R-5(A)	.1721	\$12,000.00	9	\$1,000.00	\$12,500.00	\$12,000.00	Omar Correa
53	704 JERAN	V	05	R-7.5(A)	.1919	\$12,500.00	3	\$1,000.00	\$7,110.00	\$12,500.00	David Menn
54	3050 KINKAID	V	06	R-7.5(A)	.1757	\$29,565.00	7	\$4,000.00	\$41,900.00	\$33,750.00	Aubrey Quarles
55	8322 LAKE ANNA	V	08	R-7.5(A)	.2367	\$15,500.00	3	\$1,000.00	\$3,875.00	\$17,500.00	Nextlots Now L.L.C.
56	4506 LELAND	V	07	PD-595	.1148	\$1,944.00	1	\$250.00	\$308.00	\$6,000.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
57	2262 LOLITA	V	05	R-7.5(A)	.4224	\$15,000.00	6	\$1,000.00	\$8,375.00	\$15,000.00	Nextlots Now L.L.C.
58	1311 LOTUS	V	04	LI	.0681	\$4,460.00	1	\$500.00	\$1,050.00	\$4,460.00	Rolando Cobos
59	1222 E. LOUISIANA	I	04	R-7.5(A)	.1973	\$31,240.00	22	\$5,000.00	\$30,000.00	\$31,240.00	Edgar Milton Pineda
60	2211 MARBURG	V	07	PD-595	.1699	\$9,996.00	1	\$1,250.00	\$2,345.00	\$6,000.00	County Land & Water LLC
61	2627 MARBURG	V	07	PD-595	.1723	\$6,000.00	1	\$500.00	\$600.00	\$6,000.00	Constance Armstrong
62	2823 S. MARSALIS	V	04	R-7.5(A)	.1881	\$13,500.00	3	\$1,000.00	\$8,100.00	\$13,500.00	King Home Builders, LLC

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
PROPERTY LIST**

ITEM #	STREET ADDRESS	VAC/IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
63	2923 S. MARSALIS	V	04	R-7.5(A)	.1886	\$13,500.00	4	\$1,000.00	\$8,100.00	\$13,500.00	King Home Builders, LLC
64	3723 MCBROOM	V	06	R-5(A)	.1194	\$2,500.00	7	\$250.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
65	4921 MEADOW VIEW	V	07	CA-1(A)	.1120	\$1,221.00	4	\$250.00	\$1,005.62	\$2,440.00	Nicholas A. Barnett Sr.
66	2515 MEYERS	V	07	PD-363	.1721	\$3,750.00	2	\$500.00	\$800.00	\$3,750.00	Jose Alfredo Ramirez
67	11135 MIDWAY	I	13	R-16(A)	.3673	\$311,580.00	17	\$40,000.00	\$231,100.00	\$367,890.00	SDE Texas LLC
68	3539 MINGO	V	07	R-7.5(A)	.0771	\$4,200.00	1	\$500.00	\$2,475.00	\$4,200.00	Nextlots Now L.L.C.
69	2724 MOJAVE	V	08	R-7.5(A)	.4617	\$18,500.00	5	\$1,000.00	\$7,250.00	\$18,500.00	Nextlots Now L.L.C.
70	1522 MONTAGUE	V	04	R-7.5(A)	.1731	\$6,000.00	2	\$1,000.00	\$1,475.00	\$6,000.00	Nextlots Now L.L.C.
71	3107 MORGAN	V	08	R-7.5(A)	.3091	\$17,550.00	1	\$1,000.00	\$3,675.00	\$9,000.00	Nextlots Now L.L.C.
72	3602 MORRIS	V	06	R-5(A)	.1579	\$2,130.00	9	\$500.00	\$10,000.00	\$10,200.00	Camden Homes, LLC
73	1106 MUNCIE	V	06	TH-3(A)	.1147	\$11,695.00	10	\$1,000.00	\$10,600.00	\$8,000.00	William Baker
74	609 MURDOCK	V	08	R-7.5(A)	.1675	\$14,866.00	4	\$1,000.00	\$10,000.00	\$17,500.00	Camden Homes, LLC
75	605 MURDOCK	V	08	R-7.5(A)	.1649	\$11,314.00	5	\$1,000.00	\$10,000.00	\$17,500.00	Camden Homes, LLC
76	1624 MURDOCK	V	08	R-7.5(A)	.3046	\$17,500.00	2	\$1,000.00	\$1,775.00	\$17,500.00	Nextlots Now L.L.C.
77	3411 MYRTLE	V	07	PD-595	.1615	\$12,489.00	1	\$250.00	\$703.00	\$6,000.00	Friew Zerihun
78	1334 OAKLEY	V	04	R-7.5(A)	.1754	\$10,000.00	2	\$1,000.00	\$1,875.00	\$10,000.00	Nextlots Now L.L.C.
79	3431 ODESSA	V	06	R-5(A)	.1917	\$12,000.00	6	\$1,000.00	\$13,050.00	\$12,000.00	Albert Almanza
80	4507 N. OTTAWA	V	06	R-5(A)	.1511	\$12,000.00	5	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
81	1446 OWEGA	V	04	R-7.5(A)	.1763	\$6,000.00	2	\$1,000.00	\$3,175.00	\$6,000.00	Nextlots Now L.L.C.
82	1409 PARK ROW	V	07	PD-314	.0918	\$6,000.00	3	\$1,000.00	\$2,810.00	\$10,000.00	William Baker

TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES PROPERTY LIST

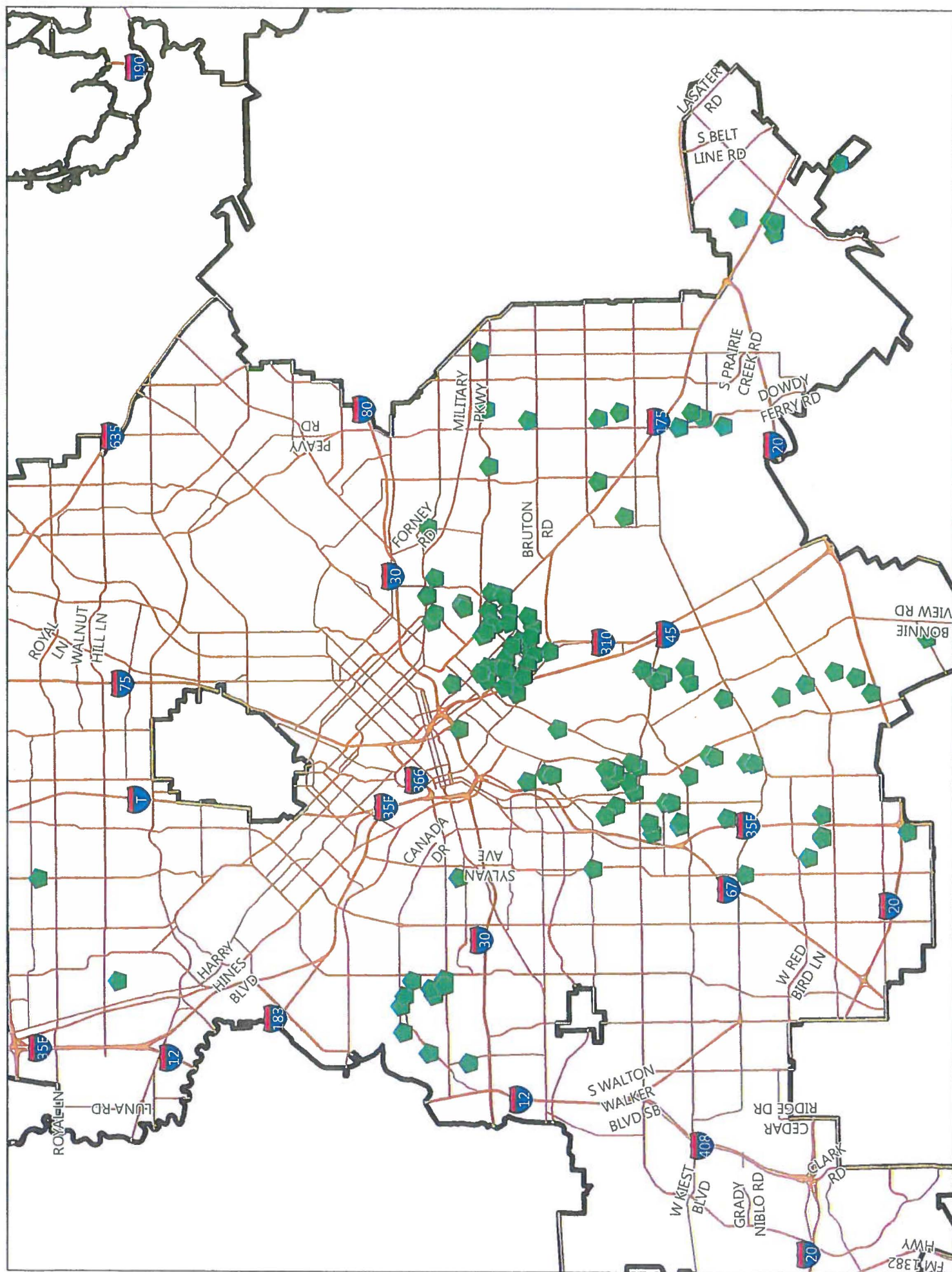
ITEM #	STREET ADDRESS	VAC/IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
83	2504 PEABODY	V	07	PD-595	.1377	\$6,000.00	2	\$1,000.00	\$2,050.00	\$6,000.00	Kimiaki Itamura
84	2724 PEABODY	V	07	PD-595	.1442	\$18,296.00	2	\$500.00	\$2,801.00	\$33,790.00	Lilia Jimenez
85	2603 PINE	V	07	PD-595	.1128	\$12,770.00	1	\$500.00	\$1,300.00	\$6,000.00	Kimiaki Itamura
86	3319 PINE	V	07	PD-595	.1779	\$6,000.00	1	\$1,000.00	\$2,004.00	\$6,000.00	Temesgen Wukaye
87	1725 PINE	V	07	PD-595	.1652	\$6,480.00	4	\$1,000.00	\$3,201.00	\$6,480.00	Lilia Jimenez
88	2616 PINE	V	07	R-5(A)	.1530	\$21,250.00	2	\$250.00	\$500.00	\$6,000.00	Benita Beltran
89	4483 N. POLK	V	03	R-7.5(A)	3.7290	\$115,200.00	2	\$5,000.00	\$35,500.00	\$129,960.00	Maria Ospina
90	3915 POLLY	V	07	PD-595	.1007	\$10,993.00	1	\$250.00	\$308.00	\$2,190.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
91	3220 REED	V	07	PD-595	.1916	\$1,944.00	2	\$500.00	\$1,002.00	\$6,000.00	Keith Marshall
92	3335 REED	V	07	PD-595	.1721	\$1,665.00	2	\$500.00	\$2,560.00	\$6,000.00	Temesgen Wukaye
93	3228 REED	V	07	PD-595	.2721	\$1,667.00	3	\$500.00	\$2,501.00	\$6,000.00	Temesgen Wukaye
94	3600 REESE	V	07	PD-595	.1730	\$1,692.00	1	\$250.00	\$308.00	\$3,770.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
95	1126 RIDGEWOOD	V	05	R-7.5(A)	.1823	\$51,705.00	3	\$1,000.00	\$4,375.00	\$12,000.00	Nextlots Now L.L.C.
96	2637 RIPPLE	V	08	R-7.5(A)	.1414	\$4,000.00	1	\$500.00	\$849.00	\$4,000.00	Anthony Boyd
97	3310 RUTLEDGE	V	07	PD-595	.1721	\$5,630.00	2	\$250.00	\$400.00	\$5,630.00	Saul Vargas
98	3054 SEEVERS	V	04	R-7.5(A)	.1688	\$15,000.00	10	\$1,000.00	\$10,000.00	\$15,000.00	Camden Homes, LLC
99	2518 SEEVERS	V	04	R-7.5(A)	.1672	\$14,000.00	8	\$1,000.00	\$10,000.00	\$14,000.00	Camden Homes, LLC

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
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ITEM #	STREET ADDRESS	VAC/IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
100	1703 SMOKE TREE	V	08	PD-258	.1871	\$9,000.00	4	\$1,000.00	\$2,575.00	\$9,000.00	Nextlots Now L.L.C.
101	1526 SOUTHERLAND	V	04	R-7.5(A)	.1080	\$1,846.00	3	\$500.00	\$3,195.00	\$11,900.00	Nextlots Now L.L.C.
102	3022 SOUTHLAND	V	07	PD-595	.1399	\$14,758.00	1	\$500.00	\$600.00	\$6,000.00	Saul Vargas
103	3835 SPENCE	V	07	PD-595	.1377	\$1,611.00	2	\$500.00	\$600.00	\$5,400.00	Constance Armstrong
104	3809 SPENCE	V	07	PD-595	.1377	\$1,572.00	2	\$500.00	\$508.00	\$5,400.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
105	4304 SPRING	V	07	PD-595	.1678	\$1,617.00	2	\$500.00	\$1,800.00	\$5,000.00	Benita Beltran
106	3240 SPRINGVIEW	V	04	R-7.5(A)	.2503	\$29,670.00	1	\$500.00	\$576.00	\$10,000.00	Sabrina Sutton
107	1406 STELLA	V	04	R-7.5(A)	.1743	\$11,500.00	3	\$1,000.00	\$4,110.00	\$11,500.00	Yesenia Villela
108	2521 STEPHENSON	V	07	PD-595	.1607	\$6,310.00	1	\$500.00	\$600.00	\$6,260.00	Saul Vargas
109	1444 STIRLING	V	04	TH-3(A)	.5498	\$2,838.00	1	\$1,500.00	\$4,219.00	\$17,250.00	Devan Earle
110	2802 SWANSON	V	07	PD-595	.7940	\$10,414.00	1	\$500.00	\$1,000.00	\$3,750.00	County Land & Water LLC
111	3418 TORONTO	V	06	R-5(A)	.1370	\$12,000.00	14	\$1,000.00	\$7,100.00	\$12,000.00	J. Santos Coria
112	3540 TORONTO	V	06	R-5(A)	.1421	\$24,630.00	7	\$1,000.00	\$7,600.00	\$10,200.00	J. Santos Coria
113	6214 TRACY	V	08	R-5(A)	.1434	\$8,000.00	1	\$250.00	\$651.00	\$8,000.00	Anthony Boyd
114	9627 TRAVIS	V	08	A(A)	.1780	\$8,481.00	1	\$1,000.00	\$1,475.00	\$12,680.00	Nextlots Now L.L.C.
115	1110 S. TYLER	V	04	R-7.5(A)	.1807	\$19,111.00	3	\$2,000.00	\$7,150.00	\$20,000.00	Juan Roberto Leon
116	3007 URBAN	V	05	R-7.5(A)	.2978	\$65,870.00	2	\$1,000.00	\$3,001.99	\$12,000.00	Abraham Galdian

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
PROPERTY LIST**

ITEM #	STREET ADDRESS	VAC/ IMP	COUNCIL DISTRICT	ZONING	PARCEL SIZE	STRUCKOFF AMOUNT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	DCAD	HIGHEST BIDDER
117	9652 VALLEY MILLS	V	07	PD-415	.0891	\$13,000.00	3	\$1,000.00	\$2,804.00	\$13,000.00	Devan Earle
118	3922 VINEYARD	V	06	R-5(A)	.1588	\$12,000.00	6	\$1,000.00	\$10,000.00	\$12,000.00	Camden Homes, LLC
119	3317 WENDELKIN	V	07	PD-595	.1056	\$4,500.00	2	\$500.00	\$606.00	\$4,500.00	County Land & Water LLC
120	1627 E. WOODIN	V	04	R-7.5(A)	.1684	\$11,500.00	1	\$1,000.00	\$2,709.00	\$11,500.00	Devan Earle



January 27, 2016

WHEREAS, the City of Dallas ("City"), the State of Texas ("State"), the County of Dallas, ("County"), and/or Dallas Independent School District ("DISD") acquired Sheriff Deeds to properties ("Properties") at a sheriff tax sale ("the First Sale") authorized by a Judicial Foreclosure ("Judgment") in a District Court in Dallas County, Texas. The Sheriff's Deeds were recorded in the real property records of Dallas County, Texas as described on "Exhibit A," attached herein and incorporated by reference; and

WHEREAS, pursuant to the Texas Attorney General Opinion No. JM-1232 and Section 34.05(a) of the Texas Property Tax Code, the City may re-sell the Properties ("the Second Sale") subject to any right of redemption existing at the time of the Second Sale; and

WHEREAS, pursuant to the provisions of Chapter 34, Section 34.05 of the Texas Property Tax Code, a taxing entity is authorized to re-sell the Properties ("the Second Sale"); and

WHEREAS, by accepting its pro rata proceeds from the Second Sale, the State agrees to the transfer of Properties in which it has an interest; and

WHEREAS, the City Manager, acting on behalf of the County pursuant to a County Commissioner's Court Order, and acting on behalf of DISD pursuant to a School Board Resolution have the authority to execute Quitclaim Deeds to the purchasers of Properties at the Second Sale, and transfer any rights, title, or interests acquired or held by each taxing entity that was a party to the Judgment at the First Sale; and

WHEREAS, the Properties were advertised in the Dallas Morning News on the dates indicated on Exhibit A; and

WHEREAS, the City Council has previously approved the re-sale of other Properties where funds were not received, nor disbursed prior to the April 1, 2001 Tax Collection Consolidation with Dallas County; and

WHEREAS, the distribution of the proceeds from the resale of the Properties will be in accordance with Chapter 34, Section 34.06 of the Texas Property Tax Code; **Now, Therefore,**

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

January 27, 2016

SECTION 1. That upon receipt of the monetary consideration from the purchasers of the Properties listed on Exhibit A, and upon consent by the County and DISD, the City Manager upon approval as to form by the City Attorney and attested by the City Secretary, is hereby authorized to execute Quitclaim Deeds to the Properties, conveying to the purchasers the right, title, and interest acquired or held by each taxing entity that was a party to the Judgment, subject to any right of redemption, post-Judgment taxes and post Judgment non-municipal liens, and in accordance with the written agreement of the terms, conditions, and release of the taxing entities.

SECTION 2. That the consideration received from the Second Sale shall be distributed pursuant to Chapter 34, Section 34.06 of the Texas Property Tax Code, and applied to the payment of the court costs, interest, and cost of sale and applied to the amount of delinquent taxes, penalties, and non-tax municipal liens as set forth in the Judgment and pursuant to the order of the court.

SECTION 3. That all purchasers shall be responsible for the pro rata portion of property taxes for the remaining part of the current calendar year that will be assessed from the date of closing of the Second Sale. Purchasers shall also be responsible for any post-Judgment taxes, penalties and interest, pursuant to the Texas Property Tax Code, and post-Judgment non-municipal liens. The Properties shall be replaced on the tax rolls as of the date of execution of Quitclaim Deeds.

SECTION 4. That to the extent authorized by law, any liens securing taxes referenced in Section 2 above are hereby released. That the City Manager, upon approval as to form by the City Attorney, is authorized to execute a release(s) of lien for any non-tax municipal lien(s) which (i) are included in the Judgments issued in the foreclosure suits filed by the City on the lot(s) shown on Exhibit "A"; or (ii) arise or are filed of record post Judgment and prior to the Second Sale by the City on the lot(s) shown on Exhibit "A".

SECTION 5. That any and all proceeds from the Second Sale, including funds not received, nor disbursed prior to the April 1, 2001 Tax Collection Consolidation with Dallas County will be deposited to General Fund 0001, Department DEV, Balance Sheet Account 0519.

SECTION 6. That upon receipt of the consideration from the Second Sale, the Chief Financial Officer is authorized to disburse the proceeds in accordance with Chapter 34, Section 34.06 of the Texas Property Tax Code. Calculations for disbursements shall be provided by the Director of Sustainable Development and Construction to the City of Dallas Land Based Receivables, the Dallas County District Clerk, and the Dallas County Tax Office from the account specified in Section 5, above.

SECTION 7. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
EXHIBIT A

ITEM #	STREET ADDRESS	LEGAL DESCRIPTION	VAC/IMP	OWNED BY TAXING ENTITIES	DMN DATES ADVERTISEMENT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	HIGHEST BIDDER
1	2606 52ND	North half of Lot 14, Block 44/5845	I	1, 2, 3	11/29 & 11/30	3	\$8,000.00	\$9,999.99	Anthony Boyd
2	1706 ALASKA	Lot 17, Block 6/3628	V	1, 2, 3	11/29 & 11/30	9	\$1,000.00	\$10,000.00	Camden Homes, LLC
3	3835 ATLANTA	Lot 10, Block 2/1727	I	1, 2, 3	11/29 & 11/30	7	\$5,000.00	\$13,000.00	Demond Thomas
4	506 AVENUE	Lot 5, Block 9/4970	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$1,451.00	Rolando Cobos
5	3302 BEAUCHAMP	Lot 1, Block H/6094	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$1,350.00	Nextlots Now L.L.C.
6	3042 S. BECKLEY	Lot 11, Block 15/4111	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$10,000.00	Camden Homes, LLC
7	3614 BERTRAND	Lot 12, Block 2/2128	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$608.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
8	4431 BONNIE VIEW	Lot 37, Block 1/6084	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$795.00	Nextlots Now L.L.C.
9	3508 BOOKER	Lot 21, Block G/1698	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$449.00	Anthony Boyd
10	2702 BRIGHAM	Lot 9A, Block G/2490	V	1, 2, 3	11/29 & 11/30	2	\$250.00	\$901.00	DaPorscha Kelley
11	15319 BUDEUDY	Part of Lot 82, Block D/8808	V	1, 2, 3	11/29 & 11/30	6	\$500.00	\$8,475.00	Nextlots Now L.L.C.
12	1210 CABOT	Lot 8, Block 6/7860	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$4,275.00	Nextlots Now L.L.C.
13	1613 CALDWELL	Lot 7, Block B/1444	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$2,175.00	Nextlots Now L.L.C.
14	3837 CANADA	Lot 36, Block 1/7155	V	1, 2, 3	11/29 & 11/30	10	\$1,000.00	\$10,000.00	Camden Homes, LLC
15	1527 CARSON	Lot 18, Block 12/3572	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$2,161.80	Luis Ramirez
16	5815 CARY	Lot 10, Block B/5805	V	1, 2, 3	11/29 & 11/30	4	\$1,000.00	\$5,660.99	Abraham Galdian

TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
EXHIBIT A

ITEM #	STREET ADDRESS	LEGAL DESCRIPTION	VAC/IMP	OWNED BY TAXING ENTITIES	DMN DATES ADVERTISEMENT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	HIGHEST BIDDER
17	315 E. CHERRY POINT	Lot 14, Block H/6627	V	1, 2, 3	11/29 & 11/30	4	\$500.00	\$1,289.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
18	307 E. CHERRY POINT	Lot 15, Block H/6627	V	1, 2, 3	11/29 & 11/30	4	\$500.00	\$1,289.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
19	339 E. CHERRY POINT	Lot 11, Block H/6627	V	1, 2, 3	11/29 & 11/30	4	\$500.00	\$1,289.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
20	323 E. CHERRY POINT	Lot 13, Block H/6627	V	1, 2, 3	11/29 & 11/30	4	\$500.00	\$1,289.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
21	6404 CINNAMON OAKS	Lot 21, Block 1/6883	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$1,508.00	Devan Earle
22	1403 E. CLARENDON	Lot 1, Block 4/3923	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$562.00	William Baker
23	3513 CLEVELAND	Lot 4, Block 5/1182	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$2,475.00	Nextlots Now L.L.C.
24	3600 COLONIAL	Lot 1, Block 1/1201	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$2,851.00	Rolando Cobos
25	5003 COLONIAL	Part of Lot 56 and 57, Block 2247	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$899.00	Devan Earle
26	2046 COOL MIST	Lot 9, Block A/8800	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$1,788.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
EXHIBIT A**

ITEM #	STREET ADDRESS	LEGAL DESCRIPTION	VAC/IMP	OWNED BY TAXING ENTITIES	DMN DATES ADVERTISEMENT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	HIGHEST BIDDER
27	3943 COOLIDGE	Lot 37, Block 1783	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$608.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
28	4006 COOLIDGE	Lot 3, Block 2/1783	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,088.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
29	3918 COOLIDGE	Lot 48, Block 1784	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$788.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
30	1104 COSTON	Lot 41, Block 6331	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$7,110.00	David Menn
31	4419 CRANFILL	Tract 1, Block 26/8617	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$6,510.00	David Menn
32	2238 DATHE	Lot 5, Block 1705	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,288.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
33	2403 DATHE	Lot 17, Block A/1711	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$688.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
34	4019 S. DENLEY	Lot 17, Block 26/4322	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$3,361.00	Anthony Boyd
35	2194 ECHO LAKE	Lot 1, Block K/8800	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,000.00	JCRB Bucker Realty, L.P.
36	2111 EDD	Lots 29 and 30, Block M/8800	V	1, 2, 3	11/29 & 11/30	6	\$1,000.00	\$4,795.00	Nextlots Now L.L.C.

TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
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37	4105 ELK HORN	Lot 19, Block 3/6002	V	1, 2, 3	11/29 & 11/30	5	\$1,000.00	\$10,000.00	Camden Homes, LLC
38	659 ELLA	Lot 18, Block O/6256	V	1, 2, 3	11/29 & 11/30	4	\$1,000.00	\$3,575.00	Nextlots Now L.L.C.
39	231 ELMORE	Lot 8, Block 2/4036	V	1, 2, 3	11/29 & 11/30	10	\$1,000.00	\$10,000.00	Camden Homes, LLC
40	4107 ESMALDA	Lot 26, Block 3/7150	V	1, 2, 3	11/29 & 11/30	8	\$1,000.00	\$8,100.00	J. Santos Coria
41	2131 S. EWING	Lot 11, Block 21/3643	V	1, 2, 3	11/29 & 11/30	5	\$1,000.00	\$7,510.00	David Menn
42	2810 FARRAGUT	Lot 5, Block 2/4431	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$901.00	DaPorscha Kelley
43	3217 FORDHAM	Lot 4, Block E/6088	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$4,575.00	Nextlots Now L.L.C.
44	1726 FORDHAM	Lot 9D, Block 28/4323	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$1,099.00	Anthony Boyd
45	2910 GAY	Lot 19, Block B/1855	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,000.00	Benita Beltran
46	3206 GOLDSPIER	Lot 6, Block C/4446	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,499.00	Devan Earle
47	3201 S. HASKELL	Lot 11, Block 2619	V	1, 2, 3	11/29 & 11/30	4	\$3,000.00	\$8,200.00	Benito Mojica
48	4410 HELEN	Lot 4, Block 1448	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$2,050.00	Nextlots Now L.L.C.
49	3623 HUMPHREY	Lot 29, Block G/6094	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$1,250.00	Nextlots Now L.L.C.
50	1230 HUTCHINS	Lot 8, Block 3/8558	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$10,000.00	Camden Homes, LLC
51	3114 INDIANOLA	Lot 6, Block 2/6215	V	1, 2, 3	11/29 & 11/30	4	\$1,000.00	\$6,600.00	Abraham Galdian
52	2434 INGERSOLL	Lot 10, Block 1/7180	V	1, 2, 3	11/29 & 11/30	9	\$1,000.00	\$12,500.00	Omar Correa
53	704 JERAN	Lot 7, Block C/6346	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$7,110.00	David Menn

TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
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54	3050 KINKAID	Lot 4, Block E/6450	V	1, 2, 3	11/29 & 11/30	7	\$4,000.00	\$41,900.00	Aubrey Quarles
55	8322 LAKE ANNA	Lot 3, Block 7864	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$3,875.00	Nextlots Now L.L.C.
56	4506 LELAND	Lot 2, Block 2/1762	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$308.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
57	2262 LOLITA	Lot 2, Block B/6182	V	1, 2, 3	11/29 & 11/30	6	\$1,000.00	\$8,375.00	Nextlots Now L.L.C.
58	1311 LOTUS	Block 3527	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,050.00	Rolando Cobos
59	1222 E. LOUISIANA	Lot 4, Block 37/3709	I	1, 2, 3	11/29 & 11/30	22	\$5,000.00	\$30,000.00	Edgar Milton Pineda
60	2211 MARBURG	Lot 19, Block B/1757	V	1, 2, 3	11/29 & 11/30	1	\$1,250.00	\$2,345.00	County Land & Water LLC
61	2627 MARBURG	Lot 20, Block C/1955	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$600.00	Constance Armstrong
62	2823 S. MARSALIS	Lot 18, Block 7/4117	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$8,100.00	King Home Builders, LLC
63	2923 S. MARSALIS	Lot 18, Block 14/4120	V	1, 2, 3	11/29 & 11/30	4	\$1,000.00	\$8,100.00	King Home Builders, LLC
64	3723 MCBROOM	Lot 3, Block K/7152	V	1, 2, 3	11/29 & 11/30	7	\$250.00	\$10,000.00	Camden Homes, LLC
65	4921 MEADOW VIEW	Lot 5, Block D/4467	V	1, 2, 3	11/29 & 11/30	4	\$250.00	\$1,005.62	Nicholas A. Barnett Sr.
66	2515 MEYERS	Lot 4, Block 5/854	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$800.00	Jose Alfredo Ramirez
67	11135 MIDWAY	Lot 13, Block 5/6402	I	1, 2, 3	11/29 & 11/30	17	\$40,000.00	\$231,100.00	SDE Texas LLC
68	3539 MINGO	Lot 36, Block A/2636	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$2,475.00	Nextlots Now L.L.C.

**TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
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ITEM #	STREET ADDRESS	LEGAL DESCRIPTION	VAC/IMP	OWNED BY TAXING ENTITIES	DMN DATES ADVERTISEMENT	# BIDS	MINIMUM BID	HIGHEST BID AMOUNT	HIGHEST BIDDER
69	2724 MOJAVE	Lot 2, Block D/7615	V	1, 2, 3	11/29 & 11/30	5	\$1,000.00	\$7,250.00	Nextlots Now L.L.C.
70	1522 MONTAGUE	Block 4/4345	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$1,475.00	Nextlots Now L.L.C.
71	3107 MORGAN	Lot 14, Block 24/7614	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$3,675.00	Nextlots Now L.L.C.
72	3602 MORRIS	Lot 6, Block 1/7148	V	1, 2, 3	11/29 & 11/30	9	\$500.00	\$10,000.00	Camden Homes, LLC
73	1106 MUNCIE	Lot 4, Block 10/7265	V	1, 2, 3	11/29 & 11/30	10	\$1,000.00	\$10,600.00	William Baker
74	609 MURDOCK	Tract 1, Block 7970	V	1, 2, 3	11/29 & 11/30	4	\$1,000.00	\$10,000.00	Camden Homes, LLC
75	605 MURDOCK	Tract 2, Block 7970	V	1, 2, 3	11/29 & 11/30	5	\$1,000.00	\$10,000.00	Camden Homes, LLC
76	1624 MURDOCK	Lot 1, Block 4/7855	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$1,775.00	Nextlots Now L.L.C.
77	3411 MYRTLE	Lot 10, Block A/1694	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$703.00	Friew Zerihun
78	1334 OAKLEY	Block 7/4059	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$1,875.00	Nextlots Now L.L.C.
79	3431 ODESSA	Lot 7, Block 9/7162	V	1, 2, 3	11/29 & 11/30	6	\$1,000.00	\$13,050.00	Albert Almanza
80	4507 N. OTTAWA	Lot 17, Block 18/7161	V	1, 2, 3	11/29 & 11/30	5	\$1,000.00	\$10,000.00	Camden Homes, LLC
81	1446 OWEGA	Lot 12, Block 9/4650	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$3,175.00	Nextlots Now L.L.C.
82	1409 PARK ROW	Northeast part of Lot 17 and 18, Block 4/1117	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$2,810.00	William Baker
83	2504 PEABODY	Lots 3 and 4, Block 25/1306	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$2,050.00	Kimiaki Itamura
84	2724 PEABODY	Lots 13 and 14, Block 27/1310	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$2,801.00	Lilia Jimenez
85	2603 PINE	Lot 13, Block 2/1742	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,300.00	Kimiaki Itamura
86	3319 PINE	Lot 11, Block A/1791	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$2,004.00	Temesgen Wukaye
87	1725 PINE	Lot 8, Block F/1606	V	1, 2, 3	11/29 & 11/30	4	\$1,000.00	\$3,201.00	Lilia Jimenez

TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
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88	2616 PINE	Part of Lots 10 and 11, Block /1745	V	1, 2, 3	11/29 & 11/30	2	\$250.00	\$500.00	Benita Beltran
89	4483 N. POLK	Block 6048	V	1, 2, 3	11/29 & 11/30	2	\$5,000.00	\$35,500.00	Maria Ospina
90	3915 POLLY	Lot 10, Block A/4467	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$308.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
91	3220 REED	Lot 3, Block /1780	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$1,002.00	Keith Marshall
92	3335 REED	Lot 27, Block B/1792	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$2,560.00	Temesgen Wukaye
93	3228 REED	Lot 1, Block /1780	V	1, 2, 3	11/29 & 11/30	3	\$500.00	\$2,501.00	Temesgen Wukaye
94	3600 REESE	Lot 10, Block 3/4455	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$308.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
95	1126 RIDGEWOOD	Lot 19, Block A/6245	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$4,375.00	Nextlots Now L.L.C.
96	2637 RIPPLE	Lot 11, Block B/6881	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$849.00	Anthony Boyd
97	3310 RUTLEDGE	Lot 7, Block 5/1789	V	1, 2, 3	11/29 & 11/30	2	\$250.00	\$400.00	Saul Vargas
98	3054 SEEVERS	Lot 14, Block 16/4112	V	1, 2, 3	11/29 & 11/30	10	\$1,000.00	\$10,000.00	Camden Homes, LLC
99	2518 SEEVERS	Lot 5, Block 10/4196	V	1, 2, 3	11/29 & 11/30	8	\$1,000.00	\$10,000.00	Camden Homes, LLC
100	1703 SMOKE TREE	Lot 18A, Block A/8827	V	1, 2, 3	11/29 & 11/30	4	\$1,000.00	\$2,575.00	Nextlots Now L.L.C.
101	1526 SOUTHERLAND	Lot 5, Block I/4239	V	1, 2, 3	11/29 & 11/30	3	\$500.00	\$3,195.00	Nextlots Now L.L.C.
102	3022 SOUTHLAND	Lot 7, Block 4/1774	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$600.00	Saul Vargas
103	3835 SPENCE	Lot 12, Block A/1260 1/2	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$600.00	Constance Armstrong

TAX FORECLOSED AND SEIZURE WARRANT PROPERTY RESALES
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104	3809 SPENCE	Lot 18, Block A/1260	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$508.00	Maria Schneider, Doric Earle, Devan Earle, and M. W. Resnick
105	4304 SPRING	Lot 1, Block 2B/1855	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$1,800.00	Benita Beltran
106	3240 SPRINGVIEW	Lot 5, Block C/6088	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$576.00	Sabrina Sutton
107	1406 STELLA	Lot 2, Block 1/3561	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$4,110.00	Yesenia Villela
108	2521 STEPHENSON	Lot 12, Block C/2486	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$600.00	Saul Vargas
109	1444 STIRLING	Lots 9, 10 and 11, Block 37/5880	V	1, 2, 3	11/29 & 11/30	1	\$1,500.00	\$4,219.00	Devan Earle
110	2802 SWANSON	Lot 16, Block 2/1855	V	1, 2, 3	11/29 & 11/30	1	\$500.00	\$1,000.00	County Land & Water LLC
111	3418 TORONTO	Lot 10, Block 5/7144	V	1, 2, 3	11/29 & 11/30	14	\$1,000.00	\$7,100.00	J. Santos Coria
112	3540 TORONTO	Lot 18, Block 16/7147	V	1, 2, 3	11/29 & 11/30	7	\$1,000.00	\$7,600.00	J. Santos Coria
113	6214 TRACY	North half of Lot 7, Block 24	V	1, 2, 3	11/29 & 11/30	1	\$250.00	\$651.00	Anthony Boyd
114	9627 TRAVIS	Lot 25, Block 1/8317	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$1,475.00	Nextlots Now L.L.C.
115	1110 S. TYLER	Lot 1, Block B/4259	V	1, 2, 3	11/29 & 11/30	3	\$2,000.00	\$7,150.00	Juan Roberto Leon
116	3007 URBAN	Lot 16, Block 5821	V	1, 2, 3	11/29 & 11/30	2	\$1,000.00	\$3,001.99	Abraham Galdian
117	9652 VALLEY MILLS	Lot 48, Block C/6795	V	1, 2, 3	11/29 & 11/30	3	\$1,000.00	\$2,804.00	Devan Earle
118	3922 VINEYARD	Lot 23, Block 21/7146	V	1, 2, 3	11/29 & 11/30	6	\$1,000.00	\$10,000.00	Camden Homes, LLC
119	3317 WENDELKIN	Part of Lot 20, Block K/1166	V	1, 2, 3	11/29 & 11/30	2	\$500.00	\$606.00	County Land & Water LLC
120	1627 E. WOODIN	Lot 8, Block 55/3729	V	1, 2, 3	11/29 & 11/30	1	\$1,000.00	\$2,709.00	Devan Earle

AGENDA ITEM # 31

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 7, 8

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 56U V 57S 66T U V

SUBJECT

A resolution consenting to the sale of 7 tax foreclosed properties by Dallas County, acquired by the taxing authorities from a Sheriff's Sale (list attached) – Financing: No cost consideration to the City

BACKGROUND

As required under Tax Code 34.05(i) and 34.05(j), this item will consent to the sale of 7 tax foreclosed properties (list attached) by Dallas County. Pursuant to the provisions of Section 34.05(c) of the Tax Property Code, the properties were offered for sale by the Sheriff of Dallas County at public auction. No bids were received and the properties were subsequently struck off to the Wilmer Hutchins Independent School District, pursuant to tax judgments (list attached) for the non-payment of delinquent taxes. The County of Dallas holds the properties in trust for each taxing entity and is seeking the consent for the sale of the properties.

The properties will return to the tax rolls upon sale and conveyance.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

No cost consideration to the City.

MAP

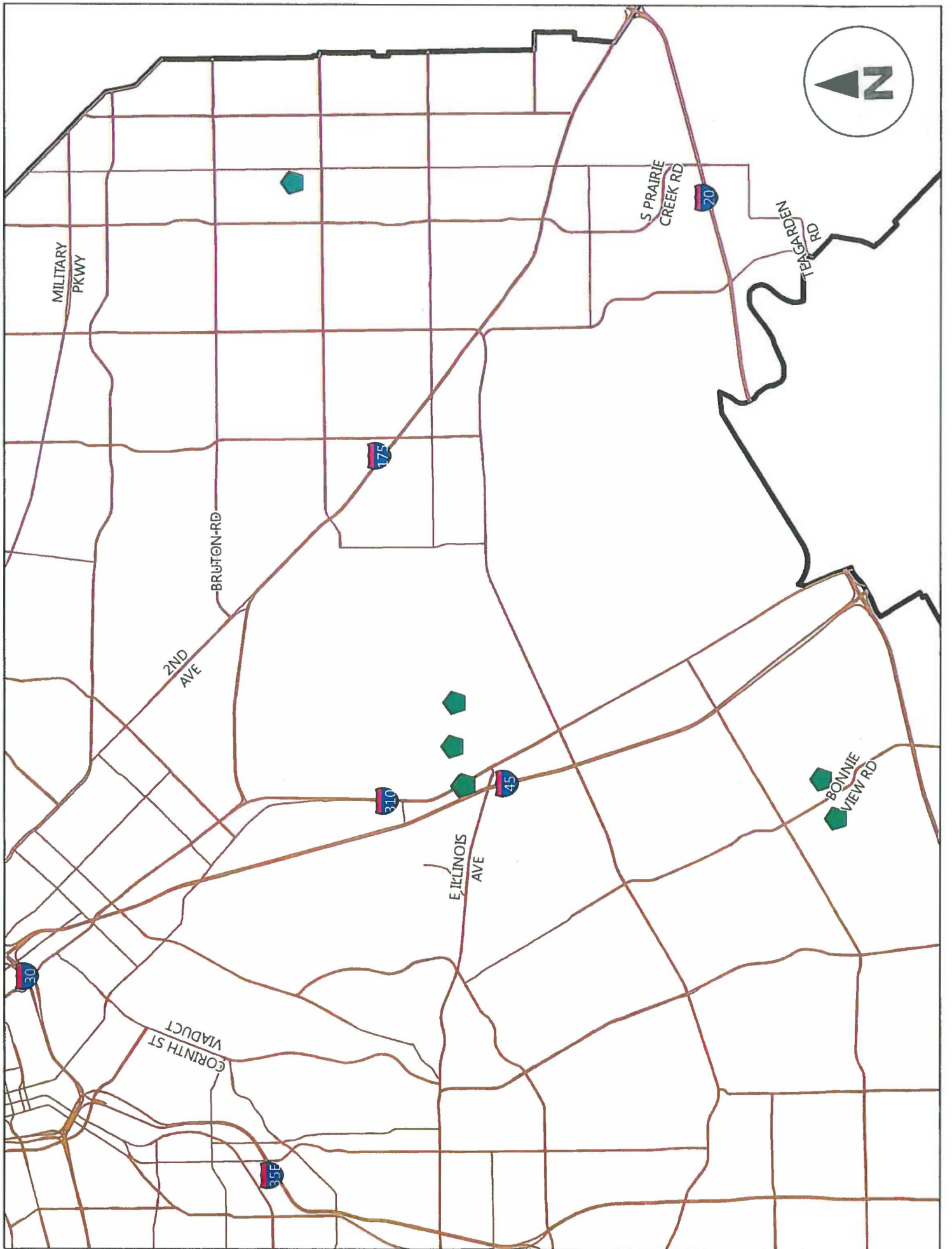
Attached

PROPERTY LIST

TAX FORECLOSURE PROPERTIES STRUCK OFF TO WILMER HUTCHINS I.S.D.

DALLAS COUNTY AS TRUSTEE FOR CITY OF DALLAS AND DALLAS I.S.D.

STREET ADDRESS	CAUSE #/ JUDGMENT DATE	IMPROVED OR UNIMPROVED	TAX YEARS INCLUDED IN JUDGMENT (COUNTY/CITY/ SCHOOL)	DATE OF SHERIFF'S SALE
7615 S. Central Expy, Dallas, Texas	TX-92-40409-TK consolidated with TX-95-30537-TD 10/28/99 (Tr. 2)	Unimproved	WHISD: 1990-1998 County: 1990-1998 City: 1990-1998	7/1/03
7623 S. Central Expy, Dallas, Texas	TX-92-40409-TK consolidated with TX-95-30537-TD 10/28/99 (Tr. 1)	Unimproved	WHISD: 1990-1998 County: 1990-1998 City: 1990-1998	7/1/03
4836 Fellows Ln., Dallas, Texas	TX-91-40715 8/6/94	Unimproved	WHISD: 1984-1995 County: 1982-1995 City: 1984-1995	10/5/04
7715 Hull Ave., Dallas, Texas	TX-90-40973 6/27/91	Unimproved	WHISD: 1982-1990 County: 1982-1990 City: 1980-1990	1/4/05
4234 Memory Ln., Dallas, Texas	TX-98-41126-T-G 10/9/03	Unimproved	WHISD: 1987-2002 County: 1987-2002 City: 1987-2002	6/1/04
4020 Silverhill Dr., Dallas, Texas	TX-88-40708-T/E 2/6/90	Unimproved	WHISD: 1982-1989 County: 1984-1989 City: 1982-1989	10/5/04
3623 Softcloud Dr., Dallas, Texas	TX-94-40485-T/C 7/14/97	Unimproved	WHISD: 1990-1995 County: 1988-1995 City: 1986-1987 1989-1995	6/1/04



January 27, 2016

WHEREAS, the City of Dallas, the State of Texas, ("State"), the County of Dallas, ("County"), the Dallas County Community College District, the Parkland Hospital District, the Dallas County School Equalization Fund, and/or the Dallas County Education District, acquired a Sheriff's Deed to the properties, ("Properties"), at a sheriff's tax sale, ("the First Sale"), authorized by a District Court of Dallas County, Texas, by a Judicial Foreclosure ("the Judgment") in a tax foreclosure sale or a Seizure Warrant, ("Warrant") and the subsequent Sheriff's Deed was filed in the Real Property Records of Dallas County, Texas; and

WHEREAS, the Properties, as described in Exhibit A was struck off to the Wilmer Hutchins Independent School District; and

WHEREAS, the County holds the Properties in trust for each taxing jurisdiction party to the judgment; and

WHEREAS, pursuant to the provisions of Chapter 34, Section 34.05 of the Property Tax Code, a taxing unit is authorized to resell the Properties by public or private sale; and

WHEREAS, the County seeks to conduct a resale of the Properties by private sale which requires consent of all the taxing entities; and

WHEREAS, the distribution of the proceeds of the resales will be in accordance with Chapter 34, Section 34.06 of the Property Tax Code; and

WHEREAS, the County seeks the City of Dallas' consent to a private sale of the Properties; **Now, Therefore**,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That the City Council of the City of Dallas consents to the private sales pursuant to Chapter 34, Section 34.05(i) and 34.05(j) of the Property Tax Code of the Properties identified on Exhibit A.

January 27, 2016

SECTION 2. That the consideration received will be distributed pursuant to Chapter 34, Section 34.06 of the Property Tax Code and applied to payment of the judgment, court costs, interest, and cost of sale owed to the taxing entities by the delinquent taxpayer or the amount of delinquent taxes, penalties, the amount secured by any municipal health or safety liens on the Properties included in the Warrant application, court costs, interest and cost of seizure and sale owed to the taxing entities as set forth in the Warrant; any such amount(s) still owed by the delinquent taxpayer to any of the taxing entities shall remain the personal obligation of the delinquent taxpayer, and any excess amounts shall be distributed in the manner described in Section 34.03 of the Property Tax Code.

SECTION 3. That the purchaser shall be responsible for post judgment taxes and pro rata property taxes assessed from the date of closing for the remaining part of the then current calendar year. The Properties shall be placed back on the tax rolls effective as of the date of execution of the deed.

SECTION 4. That to the extent authorized by law the liens securing the taxes referenced in Section 2 above are hereby released from the Properties.

SECTION 5. That any and all proceeds received for the resale of the properties listed on Exhibit A be deposited to General Fund 0001, Department DEV, Balance Sheet Account 0519.

SECTION 6. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

APPROVED AS TO FORM:

WARREN M. S. ERNST, City Attorney

BY


Assistant City Attorney

EXHIBIT "A"

TAX FORECLOSURE PROPERTIES STRUCK OFF TO WILMER HUTCHINS I.S.D. DALLAS COUNTY AS TRUSTEE FOR CITY OF DALLAS AND DALLAS I.S.D.

STREET ADDRESS	TAX ACCOUNT #	CAUSE #/ JUDGMENT DATE	IMPROVED OR UNIMPROVED	LAND SIZE (APPROX.)	JUDGMENT/ STRIKE OFF AMOUNT	MARKET VALUE SPECIFIED IN JUDGMENT	2015 DCAD VALUE	TAX YEARS INCLUDED IN JUDGMENT (COUNTY/CITY/ SCHOOL)	DATE OF SHERIFF'S SALE
7615 S. Central Expy, Dallas, Texas	00000512434000000	TX-92-40409-TK consolidated with TX-95-30537-TD 10/28/99 (Tr. 2)	Unimproved	56,758 SF	\$81,170.00	\$23,600.00	\$11,070.00	WHISD: 1990-1998 County: 1990-1998 City: 1990-1998	7/1/03
7623 S. Central Expy, Dallas, Texas	00000512430000000	TX-92-40409-TK consolidated with TX-95-30537-TD 10/28/99 (Tr. 1)	Unimproved	14,754 SF	\$81,170.00	\$57,570.00	\$42,570.00	WHISD: 1990-1998 County: 1990-1998 City: 1990-1998	7/1/03
4836 Fellows Ln., Dallas, Texas	00000759811000000	TX-91-40715 8/6/94	Unimproved	7,722 SF	\$4,000.00	\$4,000.00	\$4,000.00	WHISD: 1984-1995 County: 1982-1995 City: 1984-1995	10/5/04
7715 Hull Ave., Dallas, Texas	00000758269000000	TX-90-40973 6/27/91	Unimproved	7,499 SF	\$15,549.38	\$17,840.00	\$2,000.00	WHISD: 1982-1990 County: 1982-1990 City: 1980-1990	1/4/05
4234 Memory Ln., Dallas, Texas	00000800665000000	TX-98-41126-T-G 10/9/03	Unimproved	5 Acres	\$20,000.00	\$20,000.00	\$53,750.00	WHISD: 1987-2002 County: 1987-2002 City: 1987-2002	6/1/04
4020 Silverhill Dr., Dallas, Texas	00000639526000000	TX-88-40708-T/E 2/6/90	Unimproved	63' X 140'	\$11,381.03	\$15,480.00	\$8,500.00	WHISD: 1982-1989 County: 1984-1989 City: 1982-1989	10/5/04
3623 Softcloud Dr., Dallas, Texas	00000801826880000	TX-94-40485-T/C 7/14/97	Unimproved	60' X 120'	\$21,443.36	\$30,800.00	\$8,500.00	WHISD: 1990-1995 County: 1988-1995 City: 1986-1987 1989-1995	6/1/04

AGENDA ITEM # 32

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 3

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 43S W X

SUBJECT

A resolution authorizing acceptance of the only bid received from SLF III - The Canyon in Oak Cliff, L.P. for approximately 12.993 acres of land located near the intersection of Pinnacle Park Boulevard and Falls Bluff Drive in exchange for approximately 23.78 acres of unwanted and unneeded City-owned land located near the intersection of Pinnacle Park Boulevard and Pinnacle Point Drive - Revenue: \$7,500

BACKGROUND

On May 11, 2013, in accordance with Ordinance No's. 28938 and 28939, as amended, an election was held and the majority of the voting public authorized the City Council to: (1) convey by sale or exchange the property and (2) use the proceeds of the conveyance for the enhancement of the City of Dallas Park and Recreation System.

On May 22, 2013, by Resolution No. 13-0910, the City Council accepted the results of the public's vote and said property was deemed unwanted and unneeded surplus land.

This item authorizes the acceptance of the only bid received from SLF III - The Canyon in Oak Cliff, L.P. for approximately 12.993 acres of land located near the intersection of Pinnacle Park Boulevard and Falls Bluff Drive in exchange for approximately 23.78 acres of unwanted and unneeded City-owned land located near the intersection of Pinnacle Park Boulevard and Pinnacle Point Drive.

This property was advertised for sale on January 11, 2015 and January 12, 2015 in the Dallas Morning News for a minimum bid of \$517,967 or exchange for needed land of comparable value plus an additional \$7,500 for administrative costs.

Bids were opened on January 29, 2015 and one bid was received.

The land/property was advertised in local newspapers and on the Real Estate website. This land will return to the tax rolls upon conveyance.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

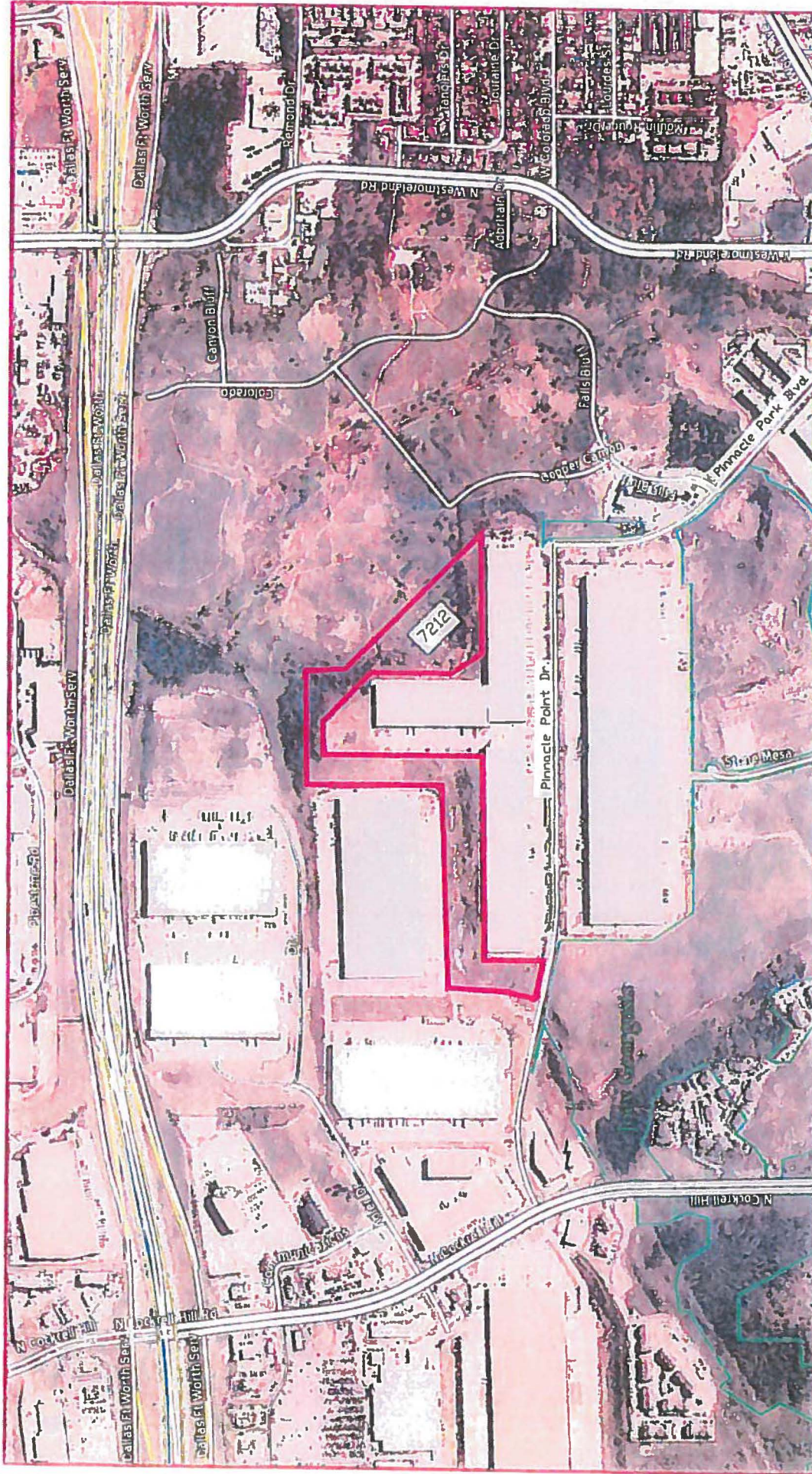
Council declared this property unwanted and unneeded on December 10, 2014, by Resolution No. 14-2200.

FISCAL INFORMATION

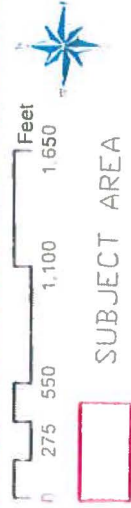
Revenue: \$7,500

MAPS

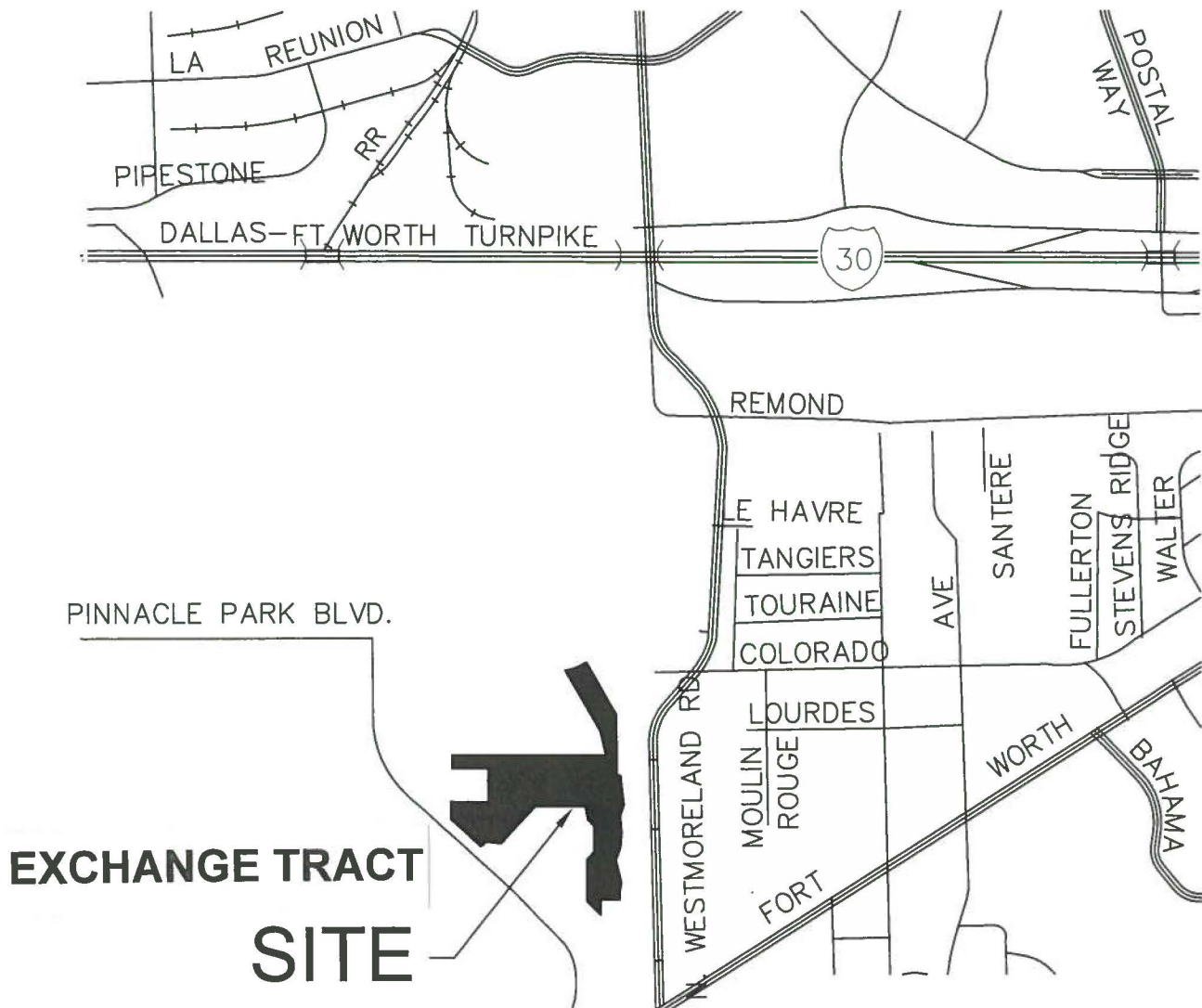
Attached



Joey Georgusis Park
Park Land Sale



SUBJECT AREA



LOCATOR MAP

NOT TO SCALE

January 27, 2016

WHEREAS, the City of Dallas is the owner of a tract of land containing approximately 23.78 acres of land in Block 7212, Dallas County, Texas, located at 4524 W. Davis Street, near the intersection of Pinnacle Park Boulevard and Pinnacle Point Drive (the "Property"); and

WHEREAS, on May 11, 2013, in accordance with Ordinance No's. 28938 and 28939, as amended, an election was held and the majority of the voting public authorized the City Council to: (1) convey by sale or exchange the property and (2) use the proceeds of the conveyance for the enhancement of the City of Dallas Park and Recreation System; and

WHEREAS, on May 22, 2013, by Resolution No. 13-0910, the City Council accepted the results of the public's vote and said property was deemed unwanted and unneeded surplus land; and

WHEREAS, on December 10, 2014, by Resolution No. 14-2200, the City Council authorized the Property to be advertised for sale and/or exchange of land for Park purposes pursuant to State law; provided that the minimum bid has a cash fair market value of not less than \$517,967; and

WHEREAS, on January 29, 2015, the City received one bid from SLF III - The Canyon in Oak Cliff, L.P. for the exchange of land for Park purposes with a market value not less than the equivalent value of the City of Dallas' property and has found that this bid complies with all legal requirements and is acceptable; **Now, Therefore,**

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That upon receipt from SLF III - The Canyon in Oak Cliff, L.P. of **SEVEN THOUSAND FIVE HUNDRED AND NO/100 (\$7,500.00) DOLLARS** and a Deed Without Warranty and an owner's policy of title insurance, acceptable as to form by the City Attorney conveying approximately 12.993 acres of land located near the intersection of Pinnacle Park Boulevard and Falls Bluff Drive, Dallas County, Texas, to the City of Dallas, the City Manager or designee is hereby authorized to execute a Deed Without Warranty conveying approximately 23.78 acres of City-owned land near the intersection of Pinnacle Park Boulevard and Pinnacle Point Drive, Dallas County, Texas to SLF III - The Canyon in Oak Cliff, L.P., to be attested by the City Secretary upon approval as to form by the City Attorney.

January 27, 2016

SECTION 2. That SLF III - The Canyon in Oak Cliff, L.P. shall convey good and indefeasible fee simple title to the land offered for exchange, free and clear of all liens and encumbrances and subject only to such title exceptions as shall be deemed acceptable by the City Attorney and said title into the City shall be insured by an owner's policy of title insurance issued by a title insurer acceptable to the City in an amount not less than fair market value in a form and subject to only those matters approved by the City Attorney.

SECTION 3. That the tract being conveyed to City by SLF III - The Canyon in Oak Cliff, L.P., is subject to the following:

- (a) satisfactory due diligence of the property's feasibility for City purposes, including but not limited to being environmentally satisfactory, as determined by the City of Dallas' Office of Environmental Quality;
- (b) easements for grading and installation, use, maintenance, repair and replacement of utilities, drainage improvements, hike/bike trails, landscaping and roadways, together with access in compliance with TCEQ, USACE 404 and other federal state and local governmental approvals and permits relating to The Canyon in Oak Cliff; and
- (c) that certain Drainage, Water Quality and Water Feature Easement and Designation of Common Area recorded in Dallas County Real Property Records; and
- (d) obligation of the Canyon in Oak Cliff Property Owners Association, Inc. to maintain the landscaping, water features and any other improvements on the property and present at closing; and
- (e) that, to the maximum extent allowed by law, the sale shall be strictly on an "AS IS, WHERE IS, WITH ALL FAULTS" basis; and
- (f) such other terms and requirements of the sale and/or disclaimers as the City deems necessary, convenient or appropriate.

SECTION 4. That the Deed Without Warranty of the City surplus land shall provide that the conveyance to SLF III - The Canyon in Oak Cliff, L.P., a Texas limited partnership. ("Grantee") is subject to the following:

- (a) a restriction prohibiting the placement of industrialized housing on the Property; and

January 27, 2016

- (b) reservation by the City of Dallas of all oil, gas and other minerals in and under the Property with a waiver of surface access rights relating to said same; and
- (c) any visible and apparent easements and any encroachments whether of record or not; and
- (d) any and all covenants, conditions, reservations, restrictions, exceptions, easements, rights-of-way, mineral interests, mineral leases or other instruments of record and applicable to the Property or any part thereof, including without limitation those in favor of the City of Dallas; and
- (e) standby fees, taxes and assessments, if any, by any taxing authority for the year of closing and subsequent years and assessments by any taxing authority for prior years due to changes in land usage or ownership, the payment of said standby fees, taxes, and assessments being assumed by Grantee; and
- (f) that, to the maximum extent allowed by law, the sale shall be strictly on an "AS IS, WHERE IS, WITH ALL FAULTS" basis; and
- (g) such other terms and requirements of the sale and/or disclaimers as the City deems necessary, convenient or appropriate.

SECTION 5. That as a material part of the consideration for the sale, the **GRANTEE** and the City shall acknowledge and agree and provide in any relevant instrument that, to the maximum extent allowed by law, (a) **GRANTEE** is taking the Property "AS IS, WHERE IS, WITH ALL FAULTS", (b) The City disclaims responsibility as to the accuracy or completeness of any information relating to the Property, (c) **GRANTEE** assumes all responsibility to examine all applicable building codes and zoning ordinances to determine if the Property can be used for the purposes desired and to check for outstanding or pending code enforcement actions including but not limited to repair or demolition orders, and (d) the City expressly disclaims and **GRANTEE** expressly waives, any warranty or representation, express or implied, including without limitation any warranty of condition, habitability, merchantability or fitness for a particular purpose of the Property. Without limiting the foregoing, the City makes no representations of any nature regarding the Property and specifically disclaims any warranty, guaranty or representation, oral or written, express or implied, past, present, or future, concerning:

January 27, 2016**SECTION 5.** (Continued)

(i) the nature and condition of the Property, including without limitation, the water, soil and geology, and the suitability thereof and the Property for any and all activities and uses which **GRANTEE** may elect to conduct thereon, and the existence of any environmental substances, hazards or conditions or presence of any endangered or protected species thereon or compliance with all applicable laws, rules or regulations; (ii) the nature and extent of any right-of-way, lease, possession, lien, encumbrance, license, reservation, condition or otherwise; (iii) the compliance of the Property or its operation with any law, ordinance or regulation of any federal, state, or local governmental authority; and (iv) whether or not the Property can be developed or utilized for any purpose. For purposes hereof, "environmental substances" means the following: (a) any "hazardous substance" under the Comprehensive Environmental Response, Compensation and Liability Act of 1980, 42 U.S.C.A. Section 9601 et. seq., as amended, (b) any "hazardous substance" under the Texas Hazardous Substances Spill Prevention and Control Act, Tex. Water Code, Section 26.261, et. seq., as amended, (c) petroleum or petroleum-based products (or any derivative or hazardous constituents thereof or additives thereto), including without limitation, fuel and lubrication oils, (d) any "hazardous chemicals" or "toxic chemicals" under the Occupational Safety and Health Act, 29 U.S.C.A. Section 651 et. seq., as amended, (e) any "hazardous waste" under the Resource Conservation and Recovery Act, 42 U.S.C.A. Section 6901 et. seq., as amended, (f) asbestos, (g) polychlorinated biphenyls, (h) underground storage tanks, whether empty, filled or partially filled with any substance, (i) any substance, the presence of which is prohibited by federal, state or local laws and regulations, and (j) any other substance which by federal, state or local laws and regulations requires special handling or notification of governmental authorities in its collection, storage, treatment or disposal. References to particular acts or codifications in this definition include all past and future amendments thereto, as well as applicable rules and regulation as now or hereafter promulgated.

SECTION 6. That the monetary consideration set forth in Section 1, shall be deposited into the General Fund 0001, Department DEV, Balance Sheet 0519 and Department of Sustainable Development and Construction, Real Estate Division shall be reimbursed for the cost of obtaining legal description, appraisal and other administrative costs incurred.

SECTION 7. That all closing costs and title expenses for the City Surplus land and Exchange Tract, including without limitation costs of title insurance, shall be paid by **GRANTEE**.

SECTION 8. That the City surplus land is now on the exempt tax roll, and taxes for the remaining part of the then current calendar year shall be assessed from the date of closing.

January 27, 2016

SECTION 9. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

APPROVED AS TO FORM:
WARREN M. S. ERNST, City Attorney

BY 
Assistant City Attorney

AGENDA ITEM # 33

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: January 27, 2016
COUNCIL DISTRICT(S): 8
DEPARTMENT: Sustainable Development and Construction
CMO: Ryan S. Evans, 671-9837
MAPSCO: 69M

SUBJECT

A resolution authorizing the conveyance of a tract of City-owned land and a drainage easement containing a total of approximately 5,393 square feet to the State of Texas located near the intersection of Interstate Highway 20 and Haymarket Road - Revenue: \$6,463

BACKGROUND

This item authorizes the conveyance of a tract of City-owned land and a drainage easement for a total of approximately 5,393 square feet to the State of Texas for the construction and maintenance of a highway ramp and drainage easement near the intersection of Interstate Highway 20 and Haymarket Road. This property will be used for the Interstate Highway 20 Expansion Project. The State of Texas is an entity with the power of eminent domain. The City may sell or exchange its property to a governmental entity that has the power of eminent domain without complying with the notice and bid requirements pursuant to Chapter 272 of the Local Government Code. The purchase price of \$6,463 is based on an independent appraisal.

This property will be conveyed with a reservation of all oil, gas and other minerals in and under the property.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

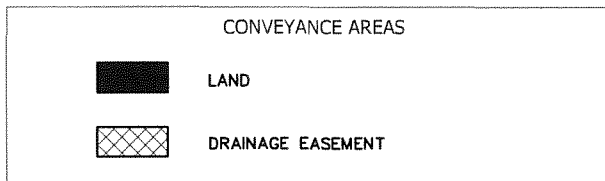
Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

Revenue: \$6,463

MAP

Attached



January 27, 2016

WHEREAS, the City of Dallas is the owner of tracts of land containing approximately 674 square feet of land (Parcel 2) and 4,719 square feet of land (Parcel 2E), being in Block 8764, Dallas County, Texas, and located near the intersection of Interstate Highway 20 and Haymarket Road, and which Parcel 2 is no longer needed for municipal use; and

WHEREAS, the Texas Department of Transportation, a State of Texas agency, has the power of eminent domain and proposes to acquire (1) fee simple title to Parcel 2; and (2) a drainage easement on Parcel 2E; both at fair market value for a state highway project for the construction and maintenance of a highway ramp and drainage facilities for Interstate Highway 20; and

WHEREAS, the City of Dallas may sell or exchange its property to a governmental entity that has the power of eminent domain, for fair market value as determined by an appraisal, without complying with the notice and bidding requirements for the sale of public lands provided for in Chapter 272, Section 272.001 of the Texas Local Government Code; and

WHEREAS, certain provisions of Section 2-24 of the Dallas City Code do not apply to the sale of land by the City of Dallas to other governmental entities as contemplated and authorized herein; **Now, Therefore,**

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That upon receipt of **SIX THOUSAND FOUR HUNDRED SIXTY THREE AND NO/100 (\$6,463.00) DOLLARS** from the State of Texas, \$3,915.00 for Parcel 2 and \$2,548.00 for a drainage easement on Parcel 2E, the City Manager or designee is authorized to execute a Special Warranty Deed for Parcel 2, to be attested by the City Secretary upon approval as to form by the City Attorney, said Special Warranty Deed being subject to the conditions contained in Section 2; and the City Manager or designee is authorized to execute a Drainage Easement for Parcel 2E, to be attested by the City Secretary upon approval as to form by the City Attorney.

SECTION 2. That the Special Warranty Deed shall provide that the conveyance of Parcel 2 to the State of Texas ("**GRANTEE**") is subject to the following:

- (a) a restriction prohibiting the placement of industrialized housing on the property; and
- (b) reservation by the City of Dallas of all oil, gas and other minerals in and under the property with a waiver of surface access rights relating to said minerals; and

January 27, 2016

- (c) any visible and apparent easements and any encroachments whether of record or not; and
- (d) any and all covenants, conditions, reservations, restrictions, exceptions, easements, rights-of-way, mineral interests, mineral leases or other instruments of record and applicable to the property or any part thereof; and
- (e) to the maximum extent allowed by law, (i) **GRANTEE** is taking the Property "AS IS, WHERE IS, WITH ALL FAULTS"; (ii) GRANTOR disclaims responsibility as to the accuracy or completeness of any information relating to the Property; (iii) **GRANTEE** assumes all responsibility to examine all applicable building codes and zoning ordinances to determine if the Property can be used for the purposes desired and to check for outstanding or pending code enforcement actions including but not limited to repair or demolition orders; and (iv) GRANTOR expressly disclaims and **GRANTEE** expressly waives, any warranty or representation, express or implied, including without limitation any warranty of condition, habitability, merchantability or fitness for a particular purpose of the Property; and
- (f) GRANTOR makes no representations of any nature regarding the Property and specifically disclaims any warranty, guaranty or representation, oral or written, express or implied, past, present, or future, concerning: (i) the nature and condition of the Property, including without limitation, the water, soil and geology, and the suitability thereof and the Property for any and all activities and uses which **GRANTEE** may elect to conduct thereon, and the existence of any environmental substances, hazards or conditions or presence of any endangered or protected species thereon or compliance with all applicable laws, rules or regulations; (ii) the nature and extent of any right-of-way, lease, possession, lien, encumbrance, license, reservation, condition or otherwise; (iii) the compliance of the Property or its operation with any law, ordinance or regulation of any federal, state, or local governmental authority; and (iv) whether or not the Property can be developed or utilized for any purpose. For purposes hereof, "environmental substances" means the following: (a) any "hazardous substance" under the Comprehensive Environmental Response, Compensation and Liability Act of 1980, 42 U.S.C.A. Section 9601 et. seq., as amended, (b) any "hazardous substance" under the Texas Hazardous Substances Spill Prevention and Control Act, Tex. Water Code, Section 26.261, et. seq., as amended, (c) petroleum or petroleum-based products (or any derivative or hazardous constituents thereof or additives thereto), including without

January 27, 2016

limitation, fuel and lubrication oils, (d) any “hazardous chemicals” or “toxic chemicals” under the Occupational Safety and Health Act, 29 U.S.C.A. Section 651 et. seq., as amended, (e) any “hazardous waste” under the Resource Conservation and Recovery Act, 42 U.S.C.A. Section 6901 et. seq., as amended, (f) asbestos, (g) polychlorinated biphenyls, (h) underground storage tanks, whether empty, filled, or partially filled with any substance, (i) any substance, the presence of which is prohibited by federal, state or local laws and regulations, and (j) any other substance which by federal, state or local laws and regulations requires special handling or notification of governmental authorities in its collection, storage, treatment or disposal. References to particular acts or codifications in this definition include all past and future amendments thereto, as well as applicable rules and regulations as now or hereafter promulgated thereunder; and

- (g) such other terms and requirements of the sale and/or disclaimers as the City deems necessary, convenient or appropriate.

SECTION 3. That the sale proceeds shall be deposited into the Water Utilities Capital Improvement Funds, Fund No. 0102, Revenue Source 8416, Dept DWU, Unit PW40, and Department of Sustainable Development and Construction, Real Estate Division shall be reimbursed for the cost of obtaining legal description, appraisal and other administrative costs incurred. The reimbursement proceeds shall be deposited in General Fund 0001, Dept DEV, Unit 1183, Object 5011 and any remaining proceeds shall be transferred to the General Capital Reserve Fund 0625, Agency BMS, Org 8888, Revenue Source 8118.

SECTION 4. That if a title policy is desired by **GRANTEE**, for either of Parcel 2 and 2E, same shall be at the expense of said **GRANTEE**.

SECTION 5. That the sale of Parcel 2 shall be subject to standby fees, taxes and assessments, if any, by any taxing authority for the year of closing and subsequent years and assessments by any taxing authority for prior years due to changes in land usage or ownership, the payment of said standby fees, taxes and assessments being assumed by grantee.

SECTION 6. That the procedures required by Section 2-24 of the Dallas City Code that are not required by state law concerning the sale of unneeded real property are waived with respect to this tract of land.

January 27, 2016

SECTION 7. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

APPROVED AS TO FORM:
WARREN M. S. ERNST, City Attorney

BY 
Assistant City Attorney

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 8

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 69M

SUBJECT

A resolution authorizing the conveyance of a tract of City-owned land and a drainage easement containing a total of approximately 4,624 square feet to the State of Texas located near the intersection of Interstate Highway 20 and Haymarket Road - Revenue: \$3,730

BACKGROUND

This item authorizes the conveyance of a tract of City-owned land and a drainage easement containing a total of approximately 4,624 square feet to the State of Texas for the construction and maintenance of a highway ramp and drainage easement near the intersection of Interstate Highway 20 and Haymarket Road. This property will be used for the Interstate Highway 20 Expansion Project. The State of Texas is an entity with the power of eminent domain. The City may sell or exchange its property to a governmental entity that has the power of eminent domain without complying with the notice and bid requirements pursuant to Chapter 272 of the Local Government Code. The purchase price of \$3,730 is based on an independent appraisal.

This property will be conveyed with a reservation of all oil, gas and other minerals in and under the property.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

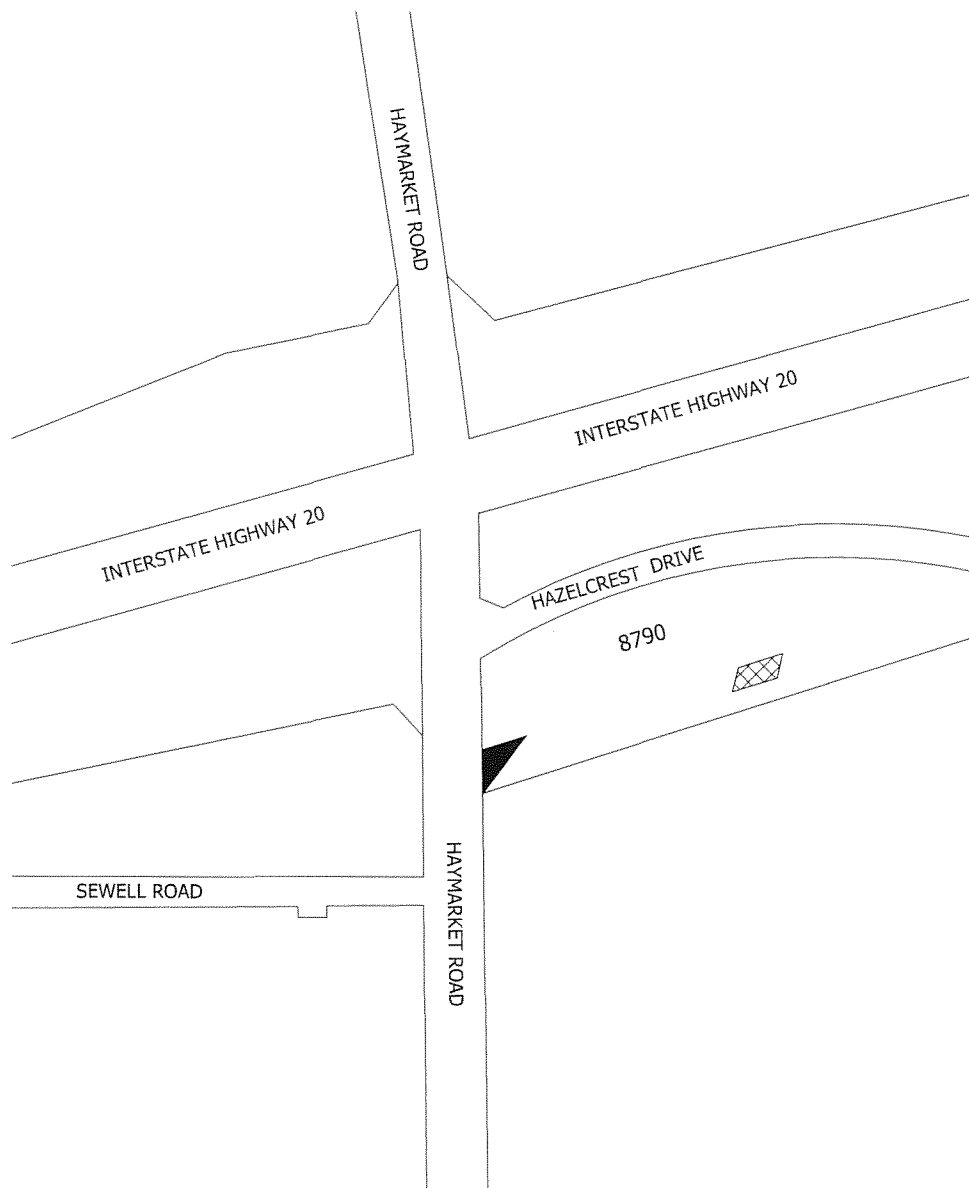
Information about this item will be provided to the Economic Development Committee on January 19, 2016.



FISCAL INFORMATION

Revenue: \$3,730

MAP

Attached



CONVEYANCE AREAS	
	LAND
	DRAINAGE EASEMENT

January 27, 2016

WHEREAS, the City of Dallas is the owner of tracts of land containing approximately 1,716 square feet of land (Parcel 3) and 2,908 square feet of land (Parcel 3E), being in Block 8790, Dallas County, Texas, and located near the intersection of Interstate Highway 20 and Haymarket Road, and which Parcel 3 is no longer needed for municipal use; and

WHEREAS, the Texas Department of Transportation, a State of Texas agency, has the power of eminent domain and proposes to acquire (1) fee simple title to Parcel 3; and (2) a drainage easement on Parcel 3E, both at fair market value for a state highway project and the construction and maintenance of a highway ramp and drainage facilities for Interstate Highway 20; and

WHEREAS, the City of Dallas may sell or exchange its property to a governmental entity that has the power of eminent domain, for fair market value as determined by an appraisal, without complying with the notice and bidding requirements for the sale of public lands provided for in Chapter 272, Section 272.001 of the Texas Local Government Code; and

WHEREAS, certain provisions of Section 2-24 of the Dallas City Code do not apply to the sale of land by the City of Dallas to other governmental entities as contemplated and authorized herein; **Now, Therefore,**

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That upon receipt of **THREE THOUSAND SEVEN HUNDRED THIRTY AND NO/100 (\$3,730.00) DOLLARS** from the State of Texas, \$2,377.00 for Parcel 3 and \$1,353.00 for a drainage easement on Parcel 3E, the City Manager or designee is authorized to execute a Special Warranty Deed for Parcel 3, to be attested by the City Secretary upon approval as to form by the City Attorney, said Special Warranty Deed being subject to the conditions contained in Section 2; and the City Manager or designee is authorized to execute a Drainage Easement for Parcel 3E, to be attested by the City Secretary upon approval as to form by the City Attorney.

SECTION 2. That the Special Warranty Deed shall provide that the conveyance of Parcel 3 to the State of Texas ("**GRANTEE**") is subject to the following:

- (a) a restriction prohibiting the placement of industrialized housing on the property; and
- (b) reservation by the City of Dallas of all oil, gas and other minerals in and under the property with a waiver of surface access rights relating to said minerals; and

January 27, 2016

- (c) any visible and apparent easements and any encroachments whether of record or not; and
- (d) any and all covenants, conditions, reservations, restrictions, exceptions, easements, rights-of-way, mineral interests, mineral leases or other instruments of record and applicable to the property or any part thereof; and
- (e) to the maximum extent allowed by law, (i) **GRANTEE** is taking the Property "AS IS, WHERE IS, WITH ALL FAULTS"; (ii) GRANTOR disclaims responsibility as to the accuracy or completeness of any information relating to the Property; (iii) **GRANTEE** assumes all responsibility to examine all applicable building codes and zoning ordinances to determine if the Property can be used for the purposes desired and to check for outstanding or pending code enforcement actions including but not limited to repair or demolition orders; and (iv) GRANTOR expressly disclaims and **GRANTEE** expressly waives, any warranty or representation, express or implied, including without limitation any warranty of condition, habitability, merchantability or fitness for a particular purpose of the Property; and
- (f) GRANTOR makes no representations of any nature regarding the Property and specifically disclaims any warranty, guaranty or representation, oral or written, express or implied, past, present, or future, concerning: (i) the nature and condition of the Property, including without limitation, the water, soil and geology, and the suitability thereof and the Property for any and all activities and uses which **GRANTEE** may elect to conduct thereon, and the existence of any environmental substances, hazards or conditions or presence of any endangered or protected species thereon or compliance with all applicable laws, rules or regulations; (ii) the nature and extent of any right-of-way, lease, possession, lien, encumbrance, license, reservation, condition or otherwise; (iii) the compliance of the Property or its operation with any law, ordinance or regulation of any federal, state, or local governmental authority; and (iv) whether or not the Property can be developed or utilized for any purpose. For purposes hereof, "environmental substances" means the following: (a) any "hazardous substance" under the Comprehensive Environmental Response, Compensation and Liability Act of 1980, 42 U.S.C.A. Section 9601 et. seq., as amended, (b) any "hazardous substance" under the Texas Hazardous Substances Spill Prevention and Control Act, Tex. Water Code, Section 26.261, et. seq., as amended, (c) petroleum or petroleum-based products (or any derivative or hazardous constituents thereof or additives thereto), including without

January 27, 2016

limitation, fuel and lubrication oils, (d) any “hazardous chemicals” or “toxic chemicals” under the Occupational Safety and Health Act, 29 U.S.C.A. Section 651 et. seq., as amended, (e) any “hazardous waste” under the Resource Conservation and Recovery Act, 42 U.S.C.A. Section 6901 et. seq., as amended, (f) asbestos, (g) polychlorinated biphenyls, (h) underground storage tanks, whether empty, filled, or partially filled with any substance, (i) any substance, the presence of which is prohibited by federal, state or local laws and regulations, and (j) any other substance which by federal, state or local laws and regulations requires special handling or notification of governmental authorities in its collection, storage, treatment or disposal. References to particular acts or codifications in this definition include all past and future amendments thereto, as well as applicable rules and regulations as now or hereafter promulgated thereunder; and

- (g) such other terms and requirements of the sale and/or disclaimers as the City deems necessary, convenient or appropriate.

SECTION 3. That the sale proceeds shall be deposited into the Water Utilities Capital Improvement Funds, Fund No. 0102, Revenue Source 8416, Dept DWU, Unit PW40, and Department of Sustainable Development and Construction, Real Estate Division shall be reimbursed for the cost of obtaining legal description, appraisal and other administrative costs incurred. The reimbursement proceeds shall be deposited in General Fund 0001, Dept DEV, Unit 1183, Object 5011 and any remaining proceeds shall be transferred to the General Capital Reserve Fund 0625, Agency BMS, Org 8888, Revenue Source 8118.

SECTION 4. That if a title policy is desired by **GRANTEE**, for either of Parcel 3 and Parcel 3E, same shall be at the expense of said **GRANTEE**.

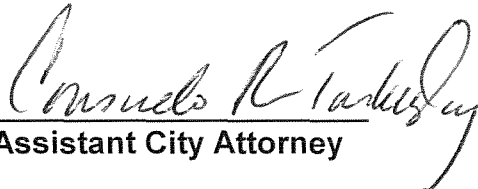
SECTION 5. That the sale of Parcel 3 shall be subject to standby fees, taxes and assessments, if any, by any taxing authority for the year of closing and subsequent years and assessments by any taxing authority for prior years due to changes in land usage or ownership, the payment of said standby fees, taxes and assessments being assumed by grantee.

SECTION 6. That the procedures required by Section 2-24 of the Dallas City Code that are not required by state law concerning the sale of unneeded real property are waived with respect to this tract of land.

January 27, 2016

SECTION 7. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

APPROVED AS TO FORM:
WARREN M. S. ERNST, City Attorney

BY 
Assistant City Attorney

AGENDA ITEM # 35

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: January 27, 2016
COUNCIL DISTRICT(S): 6
DEPARTMENT: Sustainable Development and Construction
CMO: Ryan S. Evans, 671-9837
MAPSCO: 44H

SUBJECT

An ordinance granting a private license to DD Dunhill, LLC and DE Design Borrower LLC for the use of approximately 4,350 square feet to install, maintain and use a statue, electrical conduit and wiring, lighting, sensors, paving and landscaping on a portion of Hi Line Drive right-of-way, near its intersection with Oak Lawn Avenue – Revenue: \$2,000 one-time fee, plus the \$20 ordinance publication fee

BACKGROUND

This item grants a private license to DD Dunhill, LLC and DE Design Borrower LLC for the use of approximately 4,350 square feet to install, maintain and use a statue, electrical conduit and wiring, lighting, sensors, paving and landscaping on a portion of Hi Line Drive right-of-way, near its intersection with Oak Lawn Avenue. The use of this area will not impede pedestrian or vehicular traffic.

The licensee will indemnify the City and carry general liability insurance naming the City as an additional insured.

PRIOR ACTION/REVIEW (Council, Boards, Commissions)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

Revenue: \$2,000 one-time fee, plus the \$20 ordinance publication fee

OWNERS

DD Dunhill, LLC

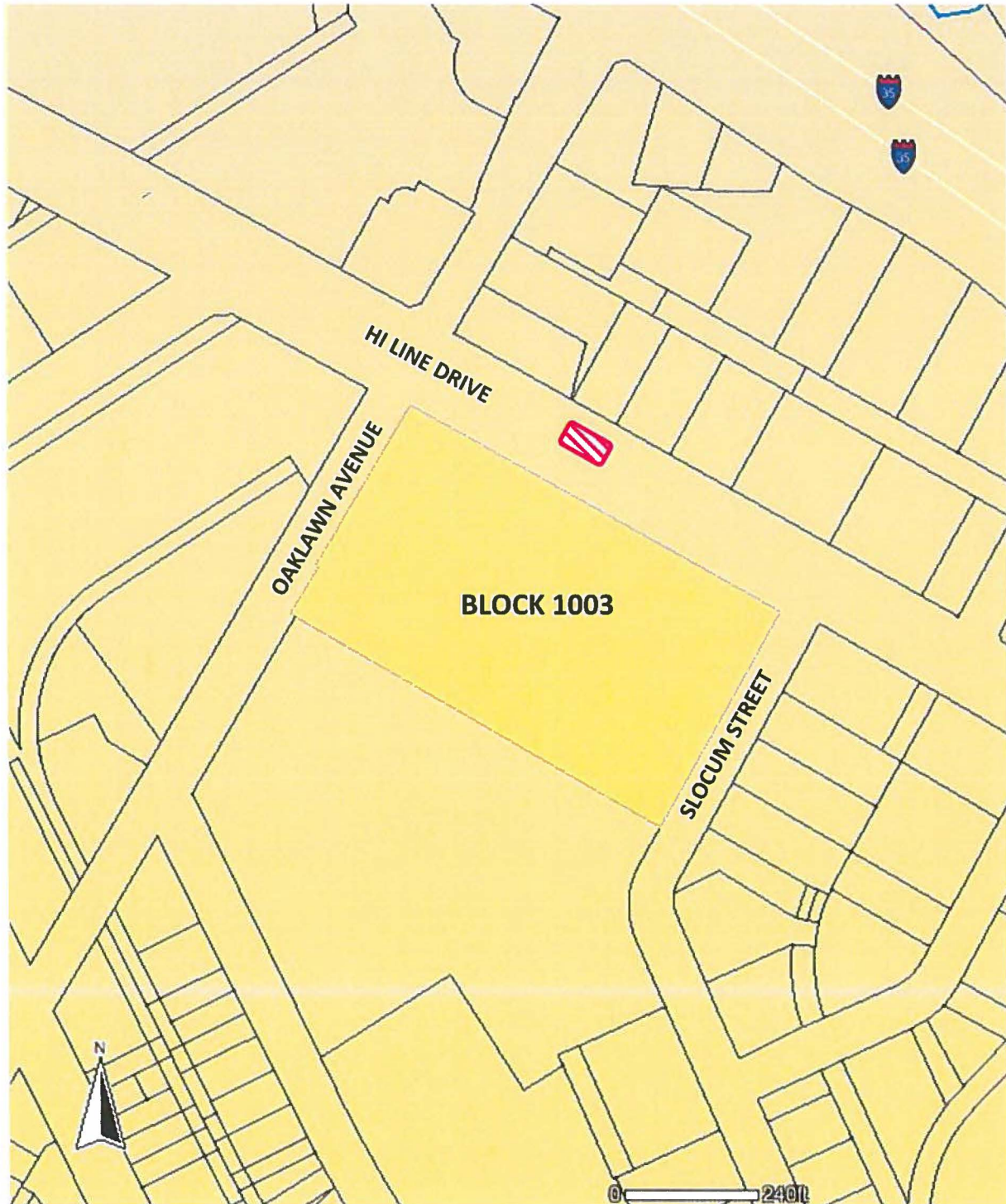
William L. Hutchinson, Governing Person

DE Design Borrower LLC

Donald Engle, Director

MAP

Attached



 LICENSING AREA

ORDINANCE NO. _____

An ordinance granting a private license to DD Dunhill, LLC and DE Design Borrower LLC to occupy, maintain, and utilize a portion of Hi Line Drive right-of-way located near the intersection of Oak Lawn Avenue and Hi Line Drive adjacent to City Block 46/1003 within the limits hereinafter more fully described, for the purpose of installing streetscape improvements consisting of a statue, electrical conduit and wiring, lighting, sensors, paving and landscaping; providing for the terms and conditions of this license; providing for the one-time fee to be paid to the City of Dallas; providing for payment of the publication fee; and providing an effective date of this license and ordinance.

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That a private license, hereinafter referred to as "license", subject to the restrictions and conditions of this ordinance, is hereby granted to DD Dunhill, LLC, a Delaware limited liability company and DE Design Borrower LLC, a Delaware limited liability company, their successors and assigns, hereinafter referred to as "**GRANTEE**", to occupy, maintain and utilize for the purpose set out hereinbelow the tract of land described in Exhibit A, hereinafter referred to as "licensed area" which is attached hereto and made a part hereof.

SECTION 2. That this license is granted for a term of forty (40) years, unless sooner terminated according to other terms and provisions herein contained.

SECTION 3. That **GRANTEE** shall pay to the City of Dallas a one-time license fee in the sum of **TWO THOUSAND AND NO/100 (\$2,000.00) DOLLARS**, said sum to become due and payable upon the final passage of this ordinance and shall cover the consideration for the license term. Such consideration shall be in addition to and exclusive of any other taxes or special assessments required by law to be paid by **GRANTEE**. Should **GRANTEE** fail to pay the above stated fee within sixty (60) days of the due date, the Director of Department of Sustainable Development and Construction may terminate this license. All sums payable to the City of Dallas hereunder shall be paid to the Chief Financial Officer of the City of Dallas and deposited in Fund 0001, Department DEV, Unit 1181, Revenue Source 8200.

In the event **GRANTEE's** check for the license fee is dishonored, **GRANTEE** shall pay to the City a processing fee of \$25.00 for each dishonored check. Additionally, all monies owed to the City under this license shall be subject to the assessment of interest at a rate of 10% a year from the day after any monies become due until it is paid in full, in accordance with Section 2-1.1 of the Dallas City Code.

SECTION 4. That the licensed area shall be used by **GRANTEE** for the following purpose under the direction of the Director of Department of Sustainable Development and Construction of the City of Dallas: to install use and maintain a statue, electrical conduit and wiring, lighting, sensors, paving and landscaping.

SECTION 5. That this license is subject to the provisions set forth in EXHIBIT B, attached hereto and made a part hereof.

SECTION 6. That this license is nonexclusive and is made expressly subject and subordinate to the right of the City to use the licensed area for any public purpose. The Governing Body of the City of Dallas reserves the right by resolution duly passed by said Governing body, to terminate and cancel this license, upon giving **GRANTEE** sixty (60) days notice of its intent to cancel. Upon termination, all rights granted hereunder shall thereupon be considered fully terminated and cancelled and the City of Dallas shall not be held liable by reason thereof. Said resolution shall be final and shall not be subject to review by the Courts. **GRANTEE** shall have the right of cancellation upon giving the City of Dallas sixty (60) days written notice of its intention to cancel, and in either event upon the termination or cancellation by the City or **GRANTEE**, as the case may be, this license shall become null and void and **GRANTEE** or anyone claiming any rights under this instrument shall remove, to the extent required by the Director of Department of Sustainable Development and Construction, any improvements and encroachments from the licensed area at **GRANTEE's** expense. Failure to do so shall subject **GRANTEE** to the provisions contained in EXHIBIT B, Subsection (a). All work shall be done at the sole cost of **GRANTEE** and to the satisfaction of the Director of Department of Sustainable Development and Construction.

SECTION 7. That the license is subject to the following conditions, terms and reservations:

- a) **GRANTEE** shall obtain approval for all paving and drainage plans.
- b) **GRANTEE** shall obtain cut permit from Public Works.
- c) **GRANTEE** shall enter into a three way contract between the City, the developer and the contractor for performance of work within the public right-of-way.
- d) **GRANTEE** acknowledges that ATMOS Energy has active facilities within Hi Line Drive right-of-way. If there are conflicts with active facilities, **GRANTEE** must coordinate with Thomas Hunter at 214-426-7074 or Thomas.hunter@atmosenergy.com.
- e) **GRANTEE** shall obtain building and right-of-way permit review and approval for all work.
- f) **GRANTEE** shall ensure all work meets Americans with Disabilities Act and Texas Accessibility Standard requirements.
- g) **GRANTEE** shall ensure all improvements meet sight distance requirements and visibility triangles.
- h) **GRANTEE** acknowledges Time Warner Cable has coax and/or fiber facilities near the area.
- i) **GRANTEE** acknowledges Hi Line Drive requires 80 feet right-of-way which includes 15 feet for the median, per the City of Dallas thoroughfare plan.
- j) **GRANTEE** shall ensure there are no raised crosswalks of any type.

SECTION 8. That the license granted hereby shall not become effective until and unless **GRANTEE** files a final acceptance, in writing, to the terms and conditions of this ordinance with the Director of Department of Sustainable Development and Construction and said written acceptance shall be forwarded to the City Secretary of the City of Dallas. In the event said written final acceptance is not filed within six (6) months after the passage of this ordinance as provided for herein, then the Director of Department of Sustainable Development and Construction, or designee, may terminate this license.

SECTION 9. That upon receipt of **GRANTEE's** final written acceptance, the Director of Department of Sustainable Development and Construction, or designee, is hereby authorized to execute a NOTICE OF LICENSE and to file same in the deed records of Dallas County, Texas. Additionally, the Director of Department of Sustainable Development and Construction, or designee, is hereby authorized to execute a cancellation of Notice of License upon termination by the City or **GRANTEE** and to file such cancellation of Notice of License in the deed records of Dallas County, Texas.

SECTION 10. That the terms and conditions contained in this ordinance shall be binding upon **GRANTEE**, their successors and assigns.

SECTION 11. That this license may not be assigned without prior written approval from the Director of Department of Sustainable Development and Construction, or designee. Such assignment shall recite that it is subject to the terms, restrictions, and conditions contained in this ordinance. The assignee shall deliver evidence of ownership of property abutting the licensed area and a copy of the assignment, along with the assignee's written acceptance of the provisions of this ordinance, to the Director of Department of Sustainable Development and Construction within 10 days of such assignment; said assignment and written acceptance shall be forwarded to the City Secretary of the City of Dallas. Should **GRANTEE** fail to obtain prior written approval for assignment of this license or fail to provide the City of Dallas with the required written acceptance and a copy of the assignment, the Director of Department of Sustainable Development and Construction, or designee, may terminate this license.

SECTION 12. That the City Secretary is hereby authorized and directed to certify a copy of this ordinance for recordation in the Deed Record of Dallas County, Texas, which certified copy shall be delivered to the Director of Department of Sustainable Development and Construction, or designee. Upon receipt of the one-time license fee pursuant to Section 3 of this Ordinance, an acceptable certificate of insurance and the fee for publishing this ordinance which **GRANTEE** shall likewise pay, the Director of Department of Sustainable Development and Construction, or designee, shall deliver to **GRANTEE** the certified copy of this ordinance. The Director of Department of Sustainable Development and Construction, or designee, shall be the sole source for receiving certified copies of this ordinance for one year after its passage.

SECTION 13. That this ordinance shall take effect immediately from and after its passage and publication in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so ordained.

APPROVED AS TO FORM:
WARREN M. S. ERNST
City Attorney

DAVID COSSUM
Director of Department of Sustainable
Development and Construction

BY 
Assistant City Attorney

BY 
for Assistant Director

Passed _____.

FIELD NOTE DESCRIPTION
FOR A
LICENSE AREA AGREEMENT

EXHIBIT A

Being a 4,350 square foot (0.0999 acre) tract or parcel of land situated in the INDUSTRIAL PROPERTIES CORP SURVEY, ABSTRACT NO. 1773 and the C. G. COLE SURVEY, ABSTRACT NO. 271, Dallas County, Texas, being part of HI LINE DRIVE RIGHT-OF-WAY as dedicated by the Plat of FIFTEENTH INSTALLMENT OF THE TRINITY INDUSTRIAL DISTRICT, an Addition to the County of Dallas, Texas according to the plat recorded in Volume 24, Page 225, Map Records, Dallas County, Texas, annexed to the City of Dallas July 30, 1956 by City Ordinance No. 7001 and being more particularly described by metes and bounds as follows:

COMMENCING at a 3/8 inch steel rod found for the most northerly corner of Block 46/1003 of said Fifteenth Installment, etc., also being the intersection of the southeasterly right-of-way line of Oak Lawn Avenue with the southwesterly right-of-way line of Hi Line Drive;

THENCE South 60 degrees 04 minutes 27 seconds East with said southwesterly right-of-way line, 364.06 feet;

THENCE North 29 degrees 55 minutes 33 seconds East departing said right-of-way line, 51.74 feet to a pk nail set for the Place of Beginning for the herein described tract;

THENCE North 30 degrees 06 minutes 39 seconds East, 56.64 feet to a pk nail set;

THENCE South 59 degrees 58 minutes 06 seconds East, 76.85 feet to a pk nail set;

THENCE South 30 degrees 11 minutes 54 seconds West, 56.63 feet to a pk nail set;

THENCE North 59 degrees 58 minutes 29 seconds West, 76.76 feet to the PLACE OF BEGINNING and containing 0.0999 acres of land (4,350 square feet), more or less as surveyed by Moak Surveyors, Inc. during the month of July, 2015.

NOTE

THE BASIS OF BEARING FOR THE SUBJECT PROPERTY IS THE BURKE'S LINE FOR THE CITY OF DALLAS MONUMENTED AND RELATED TO THE RIGHT-OF-WAY LINE OF INDUSTRIAL BOULEVARD AND REFERENCED ON THE SURVEY FOR THE TRINITY RIVER CHANNEL ABANDONMENT, ROLL SKETCHES 8767 AND 8768 OF THE GENERAL LAND OFFICE, AUSTIN, TEXAS.

**LICENSE AREA AGREEMENT
PART OF**

**HI LINE DRIVE RIGHT-OF-WAY,
FIFTEENTH INSTALLMENT OF THE
TRINITY INDUSTRIAL DISTRICT,
CITY OF DALLAS, DALLAS COUNTY, TEXAS**


John W. Morgan, R.P.L.S. No. 5488

PAGE 1 OF 2

REVISED 07-27-2015

(FOR SPRG USE ONLY)

REVIEWED BY: JD

DATE: 7/30/2015

SPRG: 3363



Moak Surveyors, Inc.

LICENSED STATE AND REGISTERED
PROFESSIONAL LAND SURVEYORS

Texas • New Mexico • Arizona • Nevada

LICENSE NO. 10008600

1105 Cheek Sparger Road, Colleyville, Texas 76034

Metro 817-268-2211 • Fax 817-282-0401

www.moaksurveyors.com

Date 07-22-2015

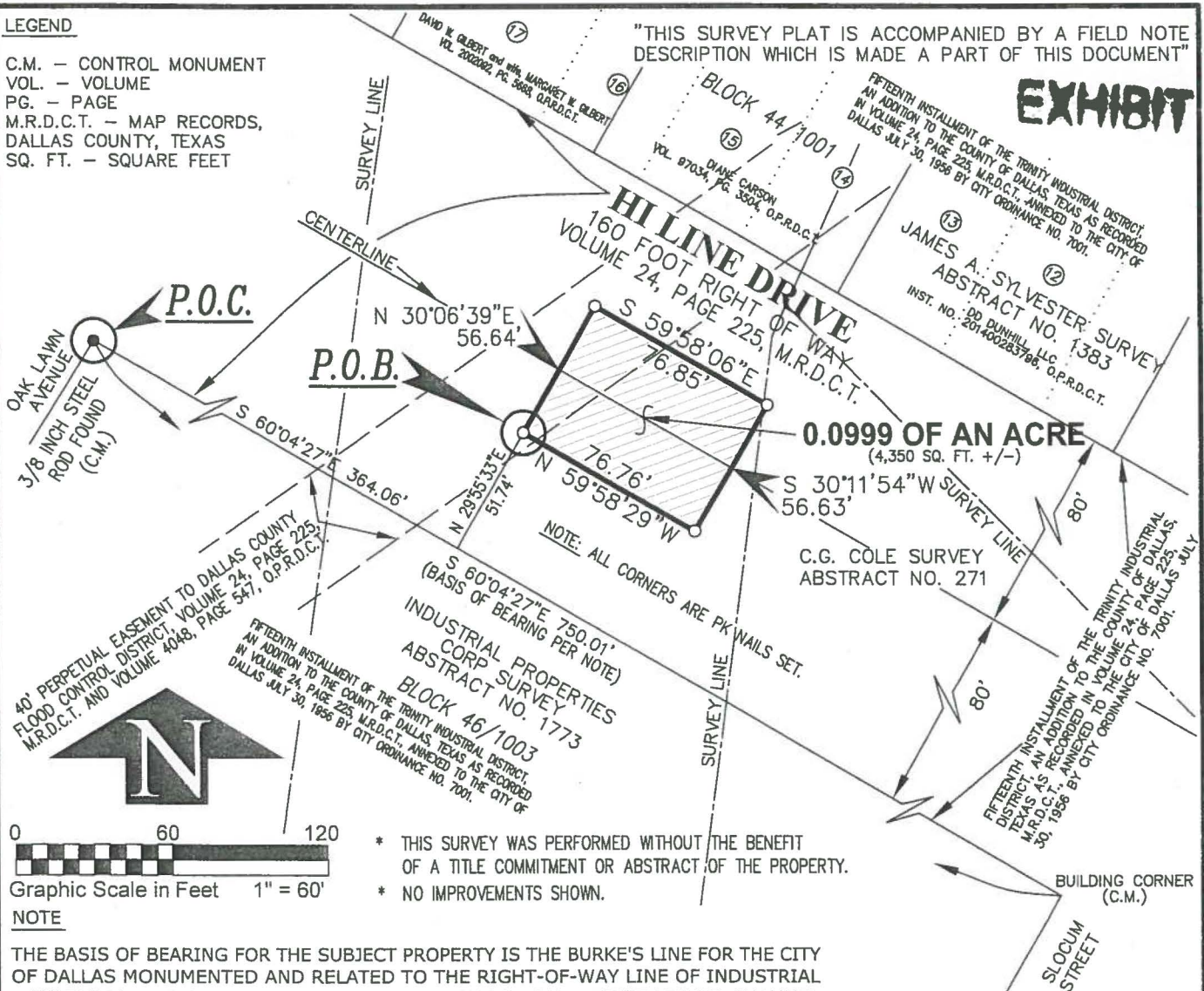
Job # 15-038

LEGEND

C.M. - CONTROL MONUMENT
VOL. - VOLUME
PG. - PAGE
M.R.D.C.T. - MAP RECORDS,
DALLAS COUNTY, TEXAS
SQ. FT. - SQUARE FEET

"THIS SURVEY PLAT IS ACCOMPANIED BY A FIELD NOTE
DESCRIPTION WHICH IS MADE A PART OF THIS DOCUMENT"

EXHIBIT A



LICENSE AREA AGREEMENT PART OF

HI LINE DRIVE RIGHT-OF-WAY, FIFTEENTH INSTALLMENT OF THE TRINITY INDUSTRIAL DISTRICT, CITY OF DALLAS, DALLAS COUNTY, TEXAS

John W. Morgan, R.P.L.S. No. 5488

PAGE 2 OF 2

REVISED 07-27-2015

Scale 1" = 60'

(FOR SPRG USE ONLY)

REVIEWED BY: JD

DATE: 7/30/2015

SPRG: 3363



COORDINATE FILE: 07-080

Moak Surveyors, Inc.

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www.moaksurveyors.com

Date 07-22-2015

Job # 15-038

**EXHIBIT B
COMMERCIAL ENTITY
ADDITIONAL LICENSE PROVISIONS**

That this license is granted subject to the following additional conditions, terms and reservations:

- (a) That at such time as this license is terminated or canceled for any reason whatsoever, **GRANTEE**, upon orders issued by the City acting through the Director of Sustainable Development and Construction, or designee, shall remove all installations, improvements and appurtenances owned by it situated in, under or attached to the licensed area, and shall restore the premises to its former condition in accordance with the requirements of the Director of Sustainable Development and Construction at the sole cost of **GRANTEE**. In the event, upon termination of this license, **GRANTEE** shall fail to remove its installations, improvements and appurtenances and to restore the licensed area in compliance with orders issued by City, or such work is not done to the satisfaction of the Director of Sustainable Development and Construction, then in either event the City shall have the right to do all work necessary to restore said area to its former condition or cause such work to be done, and to assess the cost of all such work against **GRANTEE**; in neither event shall the City of Dallas be liable to **GRANTEE** on account thereof.
- (b) It is further understood that if and when the City of Dallas, in the exercise of its discretion, shall determine that the grade of any street, sidewalk or parkway should be modified or changed, or that any other work should be done in connection with any public improvement which will affect the licensed area, and/or any of **GRANTEE's** installations and improvements thereon, any modifications or changes to **GRANTEE's** facilities in the licensed area or in construction or reconstruction of any public improvement attributable to **GRANTEE's** use of the licensed area and/or its installations and improvements thereon, shall be made at the sole expense of **GRANTEE** and to the satisfaction of the Director of Sustainable Development and Construction.
- (c) At such time as this license is granted, it is agreed, and a condition hereof, that **GRANTEE** shall procure and keep in full force and effect **Commercial General Liability Insurance** coverage issued by an insurance company authorized and approved by the State of Texas, acceptable to the City of Dallas and issued in the standard form approved by the Texas Department of Insurance. The insured provisions of this policy must name the City of Dallas as an additional insured protecting the City of Dallas against any and all claims for damages to persons or property as a result of or arising out of the use, operation and maintenance by **GRANTEE** of the licensed area and **GRANTEE's** installations, improvements, landscaping and equipment in connection therewith and located therein. The Commercial General Liability coverage must include, but not limited to, Premises/Operations, Independent Contractors and Contractual Liability with minimum combined bodily injury (including death) and property damage limits of not less than \$500,000 per occurrence and \$500,000 annual aggregate. This insurance shall also include coverage for underground, explosion and collapse hazards (i.e. not excluded). If this insurance is written on a claims-made form, coverage shall be continuous (by renewal or extended reporting period) for not less than

**EXHIBIT B
COMMERCIAL ENTITY
ADDITIONAL LICENSE PROVISIONS**

twelve (12) months following termination of this license and removal of the installations, improvements and appurtenances and restoration of the licensed area pursuant to paragraph (a) above. Coverage, including any renewals, shall contain the same retroactive date as the original policy applicable to this license. The City of Dallas reserves the right to review the insurance requirements set forth herein during the effective term of the license and to adjust insurance coverages and their limits when deemed necessary and prudent by the City of Dallas' Risk Management based upon changes in statutory law, court decisions, or the claims history of the industry as well as the City of Dallas.

1. **GRANTEE** agrees that with respect to the above required insurance, all insurance contracts and certificates of insurance will contain and state, in writing, that coverage shall not be canceled, nonrenewed or materially changed except after thirty (30) days written notice by certified mail to Department of Sustainable Development and Construction.
 2. **GRANTEE** shall carry said insurance at its expense and shall furnish the City of Dallas proof of such insurance. In the event said insurance should terminate during the licensing term hereof, or **GRANTEE** fails to furnish proof of insurance coverage in accordance with the specifications as required by this section, the Director of Sustainable Development and Construction, or designee, may terminate the license granted herein.
- (d) **GRANTEE** is prohibited from using the licensed area in any manner which violates Federal, State or local laws, regulations, rules and orders, regardless of when they become or became effective, including without limitation, those related to health, safety, noise, environmental protection, waste disposal and water and air quality, and shall provide satisfactory evidence of compliance upon the request of the City of Dallas. Should any discharge, leakage, spillage, emission or pollution of any type occur upon or from the licensed area due to **GRANTEE's** use and occupancy thereof, **GRANTEE**, at its expense, shall be obligated to clean up the licensed area to the satisfaction of the City of Dallas and any governmental body having jurisdiction thereover. The City of Dallas may, at its option, clean the licensed area. If the City of Dallas elects to do so, **GRANTEE** shall promptly pay to the City of Dallas the reasonable cost of such cleanup upon receipt of bills therefore. **GRANTEE** agrees that the indemnity provisions contained in paragraph (g) herein shall be fully applicable to the requirements of this paragraph, in event of **GRANTEE's** breach of this paragraph, or as a result of any such discharge, leakage, spillage, emission or pollution arising out of the **GRANTEE's** use of the licensed area.
- (e) This license is subject to all State laws, the provisions of the Charter of the City of Dallas as it now exists, or may hereafter be adopted or amended, and the ordinances of the City of Dallas now in effect or those which may hereafter be passed or adopted. The City of Dallas shall have the right to increase or decrease the compensation to be charged for the use contemplated by this grant in

**EXHIBIT B
COMMERCIAL ENTITY
ADDITIONAL LICENSE PROVISIONS**

accordance with the provisions of the Dallas City Code as it now exists, or as may hereafter be adopted or amended.

- (f) The Governing Body of the City of Dallas reserves the right, at any time without notice, to terminate and cancel this license, by resolution, upon a finding by the Governing Body that this license is inconsistent with the public use of the property or whenever the purpose or use of the license is likely to become a nuisance and all rights granted hereunder shall thereupon be considered fully terminated and canceled and the City of Dallas shall not be held liable by reason thereof. The decision of the Governing Body of the City in this matter shall be final and binding upon all parties insofar as the City's determination as to whether the **GRANTEE's** use of this license constitutes a nuisance or is inconsistent with the public use of the property.
- (g) As a condition hereof, **GRANTEE** agrees and is bound to defend, indemnify and hold the City of Dallas, its officers, agents and employees, harmless against any and all claims, lawsuits, judgments, costs and expenses for bodily injury (including death), property damage or other harm for which recovery of damages is sought, suffered by any person or persons, that may arise out of or be occasioned by the use, occupancy and maintenance of the licensed area or **GRANTEE's** installations and improvements within the licensed area, from any act or omission of any representative, agent, customer and/or employee of **GRANTEE**, or by **GRANTEE's** breach of any of the terms or provisions of this license, or by any negligent or strictly liable act or omission of **GRANTEE**, its officers, agents, employees or contractors in the use, occupancy and maintenance of **GRANTEE's** installations and improvements within the licensed area; except that the indemnity provided for in this paragraph shall not apply to any liability resulting from the sole negligence or fault of the City of Dallas, its officers, agents, employees or separate contractors, and in the event of joint and concurring negligence or fault of both the **GRANTEE** and the City of Dallas, responsibility and liability, if any, shall be apportioned comparatively in accordance with the laws of the State of Texas, without, however, waiving any governmental immunity available to the City of Dallas under Texas law and without waiving any defenses of the parties under Texas law. This obligation to indemnify and defend shall also include any claim for damage that any utility or communication company, whether publicly or privately owned, may sustain or receive by reason of **GRANTEE's** use of the licensed area or **GRANTEE's** improvements and equipment located thereon. In addition to the foregoing, **GRANTEE** covenants and agrees never to make a claim of any kind or character whatsoever against the City of Dallas for damage of any kind that it may suffer by reason of the installation, construction, reconstruction, operation or maintenance of any public improvement, utility or communication facility on the licensed area, whether presently in place or which may in the future be constructed or installed, including but not limited to, any water or wastewater mains or storm sewer facilities, regardless of whether such damage is due to

**EXHIBIT B
COMMERCIAL ENTITY
ADDITIONAL LICENSE PROVISIONS**

flooding, infiltration, backflow or seepage caused from the failure of any installation, natural causes, City's negligence, or from any other cause whatsoever.

- (h) This license is subject to any existing utilities or communication facilities, including drainage, presently located within the licensed area, owned and/or operated by the City of Dallas or any utility or communications company, public or private, and to any vested rights presently owned by an utility or communications company, public or private, for the use of the licensed area for facilities presently located within the boundaries of said licensed area. It is the intent of the foregoing that this permission herein is made expressly subject to the utilization of the licensed area for communication and utility purposes, both public and private, including drainage, over, under, through, across and along the licensed area. No buildings shall be constructed or placed upon, over or across the licensed area in such a manner as to interfere with the operation of any utilities and communication facilities. All and any communication company and utility, both public and private, shall have the right to remove and keep removed all or parts of any buildings which may in any way endanger or interfere with the construction, maintenance or efficiency of its respective systems within the licensed area. All communication companies and utilities, both public and private, shall have the full right to remove and keep removed all parts of any buildings, fences, trees, or other improvements or growths which in any way may endanger or interfere with the construction, maintenance and efficiency of its respective system and shall at all times have the full right of ingress and egress to or from and upon the licensed area for the purpose of constructing, relocating, inspecting, patrolling, maintaining and adding to or removing all or part of its respective systems without the necessity at any time of procuring the permission of anyone.

AGENDA ITEM # 36

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: January 27, 2016
COUNCIL DISTRICT(S): 6
DEPARTMENT: Sustainable Development and Construction
CMO: Ryan S. Evans, 671-9837
MAPSCO: 44G

SUBJECT

An ordinance abandoning a portion of Levee Street to AR Apartments, LLC, the abutting owner, containing approximately 2,052 square feet of land, located near the intersection of Turtle Creek Boulevard and Levee Street, and authorizing the quitclaim - Revenue: \$65,664, plus the \$20 ordinance publication fee

BACKGROUND

This item authorizes the abandonment of a portion of Levee Street to AR Apartments, LLC, the abutting owner. The area will be included with the property of the abutting owner for a multi-family development. The abandonment fee is based on an independent appraisal.

Notices were sent to 24 property owners located within 300 feet of the proposed abandonment area. There were two responses received in opposition to this request.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

Revenue: \$65,664, plus the \$20 ordinance publication fee

OWNER

AR Apartments, LLC

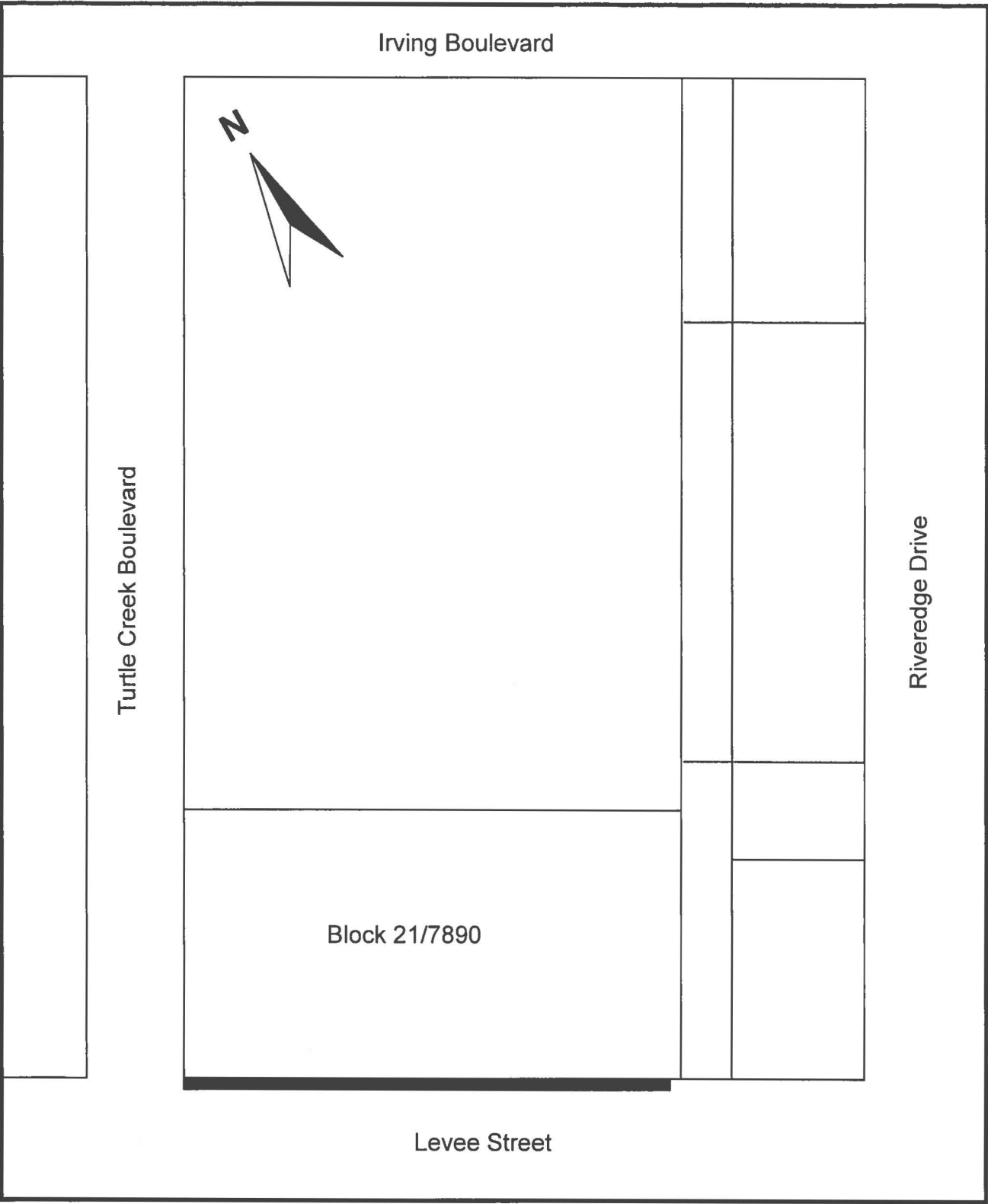
DCH 109 Design District, L.P.

Maple Multi-Family Development, LLC

Kenneth J Valach, President

MAP

Attached



Abandonment Area 

ORDINANCE NO. _____

An ordinance providing for the abandonment of a portion of Levee Street located adjacent to City Block 21/7890 in the City of Dallas and County of Dallas, Texas; providing for the quitclaim thereof to AR Apartments, LLC; providing for the terms and conditions of the abandonment and quitclaim made herein; providing for barricading; providing for the indemnification of the City of Dallas against damages arising out of the abandonment herein; providing for the consideration to be paid to the City of Dallas; providing for the payment of the publication fee; and providing an effective date for this ordinance.

ooo0ooo

WHEREAS, the City Council of the City of Dallas, acting pursuant to law and upon the request and petition of AR Apartments, LLC, a Delaware limited liability corporation, hereinafter referred to as **GRANTEE**, deems it advisable to abandon and quitclaim the hereinafter described tract of land to **GRANTEE**, and is of the opinion that, subject to the terms and conditions herein provided, said portion of Levee Street is not needed for public use, and same should be abandoned and quitclaimed to **GRANTEE**, as hereinafter stated; and

WHEREAS, the City Council of the City of Dallas is of the opinion that the best interest and welfare of the public will be served by abandoning and quitclaiming the same to **GRANTEE** for the consideration and subject to the terms and conditions hereinafter more fully set forth. **Now, Therefore,**

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That the tract of land described in Exhibit A, which is attached hereto and made a part hereof for all purposes, be and the same is abandoned, vacated and closed insofar as the right, title and interest of the public are concerned; subject, however, to the conditions hereinafter more fully set out.

SECTION 2. That for and in monetary consideration of the sum of **SIXTY FIVE THOUSAND SIX HUNDRED SIXTY FOUR AND NO/100 DOLLARS (\$65,664.00)** paid by **GRANTEE**, and the further consideration described in Sections 8, 9, 10 and 11, the

City of Dallas does by these presents **FOREVER QUITCLAIM** unto the said **GRANTEE**, subject to the conditions, reservations, and exceptions hereinafter made and with the restrictions and upon the covenants below stated, all of its right, title and interest in and to the certain tract of land hereinabove described in Exhibit A. **TO HAVE AND TO HOLD** all of such right, title and interest in and to the property and premises, subject aforesaid, together with all and singular the rights, privileges, hereditaments and appurtenances thereto in any manner belonging unto the said **GRANTEE** forever.

SECTION 3. That upon payment of the monetary consideration set forth in Section 2, **GRANTEE** accepts the terms, provisions, and conditions of this ordinance.

SECTION 4. That the Chief Financial Officer is authorized to deposit the sum paid by **GRANTEE** pursuant to Section 2 above in the General Fund 0001, Department DEV, Balance Sheet 0519 and Department of Sustainable Development and Construction-Real Estate Division shall be reimbursed for the cost of obtaining the legal description, appraisal and other administrative costs incurred. The reimbursement proceeds shall be deposited in General Fund 0001, Department DEV, Unit 1183, Object 5011 and any remaining proceeds shall be transferred to the General Capital Reserve Fund 0625, Department BMS, Unit 8888, Revenue Source 8416.

SECTION 5. That the abandonment and quitclaim provided for herein are made subject to all present zoning and deed restrictions, if the latter exist, and are subject to all existing easement rights of others, if any, whether apparent or non-apparent, aerial, surface, underground or otherwise, and are further subject to the conditions contained in Exhibit B, which is attached hereto and made a part hereof for all purposes.

SECTION 6. That the terms and conditions contained in this ordinance shall be binding upon **GRANTEE**, its successors and assigns.

SECTION 7. That the abandonment and quitclaim provided for herein shall extend only to the public right, title, easement and interest, and shall be construed to extend only to that interest the Governing Body of the City of Dallas may legally and lawfully abandon and vacate.

SECTION 8. That as a condition of this abandonment and as a part of the consideration for the quitclaim to **GRANTEE** herein, **GRANTEE**, its successors and assigns, agree to indemnify, defend, release and hold harmless the City of Dallas as to any claims for damages, fines, penalties, costs or expenses to persons or property that may arise out of, or be occasioned by or from: (i) the use and occupancy of the area described in Exhibit A by **GRANTEE**, its successors and assigns; (ii) the presence, generation, spillage, discharge, release, treatment or disposition of any Hazardous Substance on or affecting the area set out in Exhibit A; (iii) all corrective actions concerning any discovered Hazardous Substances on or affecting the area described in Exhibit A, which **GRANTEE**, its successors and assigns, agree to undertake and complete in accordance with applicable federal, state and local laws and regulations; and (iv) the abandonment, closing, vacation and quitclaim by the City of Dallas of the area set out in Exhibit A. **GRANTEE**, its successors and assigns, hereby agree to defend any and all suits, claims, or causes of action brought against the City of Dallas on account of same, and discharge any judgment or judgments that may be rendered against the City of Dallas in connection therewith. For purposes hereof, "Hazardous Substance" means the following: (a) any "hazardous substances" under the Comprehensive Environmental Response, Compensation, and Liability Act, 42 U.S.C. Section 9601 et seq., as amended; (b) any "hazardous substance" under the Texas Hazardous Substances Spill Prevention and Control Act, TEX. WATER CODE, Section 26.261 et seq., as amended; (c) petroleum or petroleum-based products (or any derivative or hazardous constituents thereof or additives thereto), including without limitation, fuel and lubricating oils; (d) any "hazardous chemicals" or "toxic chemicals" under the Occupational Safety and Health Act, 29 U.S.C. Section 651 et seq., as amended; (e) any "hazardous waste" under the Resource Conservation and Recovery Act, 42 U.S.C. Section 6901 et seq., as amended; and (f) any "chemical substance" under the Toxic Substance Control Act, 15 U.S.C. Section 2601 et seq., as amended. References to particular acts or codifications in this definition include all past and future amendments thereto, as well as applicable rules and regulations as now or hereafter promulgated thereunder.

SECTION 9. That as a condition of this abandonment and as a part of the consideration for the quitclaim made herein, **GRANTEE** shall:

- a) maintain 36' of pavement in accordance with Minimum Street Standards, and
- b) contact the Texas Excavation Safety System (Texas 811) to have facilities marked & located within affected easements before any excavations are started.

SECTION 10. That as a condition of this abandonment and as a part of the consideration for the quitclaim made herein, **GRANTEE** shall file a final replat of the adjoining properties prior to the issuance of any building permits affecting the tract of land abandoned and quitclaimed herein. This final replat shall be recorded by **GRANTEE** in the official real property records of the county in which the abandoned area is located after its approval by the City Plan Commission of the City of Dallas.

SECTION 11. That as a condition of this abandonment and as a part of the consideration for the quitclaim made herein, **GRANTEE** shall, immediately upon the passage of this ordinance, close, barricade and/or place signs in the area described in Exhibit A in accordance with detailed plans approved by the Director of Department of Sustainable Development and Construction. **GRANTEE's** responsibility for keeping the area described in Exhibit A closed, barricaded and/or the signs in place shall continue until the street improvements and intersection returns are removed by **GRANTEE**, its successors and assigns, to the satisfaction of the Director of Department of Sustainable Development and Construction.

SECTION 12. That the City Secretary is hereby authorized and directed to certify a copy of this ordinance for recordation in the official real property records of the county in which the abandonment area is located, which certified copy shall be delivered to the Director of Department of Sustainable Development and Construction, or designee.

Upon receipt of the monetary consideration set forth in Section 2, plus the fee for the publishing of this ordinance, which **GRANTEE** shall likewise pay the Director of Department of Sustainable Development and Construction, or designee: (i) shall deliver to **GRANTEE** a certified copy of this ordinance, and (ii) is authorized to and shall prepare and deliver a **QUITCLAIM DEED** with regard to the area abandoned herein, to **GRANTEE** hereunder, same to be executed by the City Manager on behalf of the City of Dallas, attested by the City Secretary and approved as to form by the City Attorney. The Director of Department of Sustainable Development and Construction, or designee, shall be the sole source for receiving certified copies of this ordinance for one year after its passage.

SECTION 13. That this ordinance shall take effect immediately from and after its passage and publication in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so ordained.

APPROVED AS TO FORM:
WARREN M. S. ERNST
City Attorney

DAVID COSSUM
Director of Department of Sustainable
Development and Construction

BY 
Assistant City Attorney

BY 
Assistant Director

Passed _____.

**STREET ABANDONMENT
PART OF LEVEE STREET
ADJACENT TO CITY OF DALLAS BLOCK 21/7890
CITY OF DALLAS, DALLAS COUNTY, TEXAS**

BEING A 2,052 SQUARE FOOT (0.0471 ACRES) TRACT OF LAND SITUATED IN THE AMARIAH HANNA SURVEY, ABSTRACT NO 564, CITY OF DALLAS, DALLAS COUNTY, TEXAS, ADJACENT TO CITY OF DALLAS BLOCK NUMBER 21/7890, AND BEING A PORTION OF LEVEE STREET, A 70.00 FOOT RIGHT-OF-WAY DEDICATED AND CREATED BY PLAT OF NINTH INSTALLMENT, TRINITY INDUSTRIAL DISTRICT, AN ADDITION TO THE CITY OF DALLAS, DALLAS COUNTY, TEXAS ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 15, PAGE 239, MAP RECORDS, DALLAS COUNTY, TEXAS; SAID 2,052 SQUARE FOOT TRACT OF LAND BEING MORE PARTICULARLY DESCRIBED BY METES AND BOUNDS AS FOLLOWS:

BEGINNING AT A 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "BURY" FOUND FOR THE POINT OF INTERSECTION OF THE SOUTHEASTERLY RIGHT-OF-WAY LINE OF TURTLE CREEK BOULEVARD - FORMERLY KNOWN AS HARVESTER STREET (A 90-FOOT WIDE PUBLIC RIGHT-OF-WAY) AS DEDICATED AND CREATED BY SAID NINTH INSTALLMENT, TRINITY INDUSTRIAL DISTRICT WITH THE NORTHEASTERLY RIGHT-OF-WAY LINE OF SAID LEVEE STREET, AND BEING THE MOST WESTERLY CORNER OF LOT 23, BLOCK 21/7890 OF SAID NINTH INSTALLMENT, TRINITY INDUSTRIAL DISTRICT SAME BEING THE MOST WESTERLY CORNER OF THAT CERTAIN TRACT OF LAND DESCRIBED IN THE SPECIAL WARRANTY DEED TO AR APARTMENTS, LLC., RECORDED IN INSTRUMENT NUMBER 201400301070, OFFICIAL PUBLIC RECORDS OF DALLAS COUNTY, TEXAS, (O.P.R.D.C.T.);

THENCE SOUTH 59°21'06" EAST ALONG THE COMMON LINE OF SAID LEVEE STREET AND SAID LOT 23, BLOCK 21/7890, A DISTANCE OF 413.00 FEET TO A 5/8-INCH IRON ROD WITH YELLOW PLASTIC CAP STAMPED "BURY" SET FOR CORNER;

THENCE, DEPARTING SAID COMMON LINE OVER AND ACROSS SAID LEVEE STREET RIGHT-OF-WAY, THE FOLLOWING THREE (3) COURSES AND DISTANCES:

- 1) SOUTH 30° 38' 54" WEST, A DISTANCE OF 5.00 FEET TO A 5/8-INCH IRON ROD WITH YELLOW PLASTIC CAP STAMPED "BURY" SET FOR CORNER;
- 2) NORTH 59° 21' 06" WEST, 5-FEET WESTERLY FROM AND PARALLEL TO SAID COMMON LINE OF LEVEE STREET AND LOT 23, BLOCK 21/7890, A DISTANCE OF 408.00 FEET TO A 5/8-INCH IRON ROD WITH YELLOW PLASTIC CAP STAMPED "BURY" SET FOR CORNER;
- 3) NORTH 14° 21' 19" WEST, A DISTANCE OF 7.07 FEET TO THE POINT OF BEGINNING;

CONTAINING A COMPUTED AREA OF 2,052 SQUARE FEET OR 0.0471 ACRES OF LAND.

(For SPRG use only)

Reviewed by: JD Date: 12/11/15 SPRG NO: 3127

EXHIBIT A

**STREET ABANDONMENT
PART OF LEVEE STREET
ADJACENT TO CITY OF DALLAS BLOCK 21/7890
CITY OF DALLAS, DALLAS COUNTY, TEXAS**

NOTES:

BEARINGS CALLED FOR HEREIN ARE REFERENCED TO THE TEXAS STATE PLANE COORDINATE SYSTEM, NORTH CENTRAL ZONE 4202 NORTH AMERICAN DATUM OF 1983, (2011).

A SKETCH WAS PREPARED ON EVEN DATE TO ACCOMPANY THIS DESCRIPTION.

(For SPRG use only)

Reviewed by: JD Date: 12/11/15 SPRG NO: 3127



TURTLE CREEK BOULEVARD

PER ORDINANCE NO. 19833
(F.K.A. HARVESTER STREET)
VOL. 15, PG. 239, M.R.D.C.T.
(A CALLED 90' WIDE PUBLIC R.O.W.)

POB

CIRF (CM)

CIRS

N 14°21'19" W
7.07'

30.0'

35.0'

LEVEE STREET
VOL. 15, PG. 239, M.R.D.C.T.
(A CALLED 70' WIDE PUBLIC R.O.W.)

N 59°21'06" W
408.00'

5.0'

S 30°38'54" W
5.00'

STREET ABANDONMENT
2,052 SQ. FT.
0.0471 ACRE

CALLLED 0.2893 ACRES
(TRACT 1)
LA ZONA VERDE LLC
INST. NO. 201400137817
O.P.R.D.C.T.

NOTES:
A FIELD NOTE DESCRIPTION WAS PREPARED ON EVEN DATE TO ACCOMPANY THIS SKETCH.
BEARINGS CALLED FOR HEREON ARE REFERENCED TO THE TEXAS STATE PLANE COORDINATE SYSTEM, NORTH CENTRAL ZONE 4202, NORTH AMERICAN DATUM OF 1983, (2011).

ENSERCH CORP
(NO DEED OF
RECORD FOUND)

FIRST REVISION BLOCK NO. 22 AND BLOCK NO. 25
INSTALLMENT NUMBER EIGHT
AND
INSTALLMENT NUMBER NINE TRINITY INDUSTRIAL DISTRICT
VOL. 16, PG. 331 M.R.D.C.T.

CALLLED 0.5780 ACRES
MK PARTNERS, LTD.
VOL. 99218, PG. 986,
D.R.D.C.T.

MICHELLE RAE KELLY AND
MAUREEN MURPHY
INST. NO. 201200081853,
O.P.R.D.C.T.

LUIS M. ALCALA ALCALA
DBA CUSTOM FURNITURE
VOL. 96014, PG. 3611,
D.R.D.C.T.

PARADOX PROPERTIES
INST. NO. 200600315571,
O.P.R.D.C.T.

CALLLED 0.0952 ACRES
PARADOX PROPERTIES, LP
INST. NO. 200600114378
O.P.R.D.C.T.

CALLLED 0.080 ACRES
DOLLANSTOWN, L.L.C.
INST. NO. 201400023358,
O.P.R.D.C.T.

CALLLED 0.3147 ACRES
LUIS ALCALA
VOL. 98253, PG. 1232,
D.R.D.C.T.

0 30 60
1" = 60'



LEGEND

CIRF/CIRS IRON ROD WITH YELLOW PLASTIC CAP STAMPED "BURY-INC"
FOUND/SET
INST. NO. INSTRUMENT, NUMBER
O.P.R.D.C.T. OFFICIAL PUBLIC RECORDS OF DALLAS COUNTY, TEXAS
M.R.D.C.T. MAP RECORDS OF DALLAS COUNTY, TEXAS
D.R.D.C.T. DEED RECORDS OF DALLAS COUNTY, TEXAS
F.K.A. FORMERLY KNOWN AS

R.O.W. RIGHT-OF-WAY
VOL., PG. VOLUME/PAGE
CENTERLINE
COMMON OWNERSHIP
POINT OF BEGINNING
CONTROLLING MONUMENT

BURY

5310 Harvest Hill Road, Suite 100
Dallas, Texas 75230
Tel. (972) 991-0011 Fax (972) 991-0278
TBPLS Registration Number 10107502
Copyright © 2015

(For SPRG use ONLY)

Reviewed By: JD
Date: 12/11/15
SPRG No.: 3127

DATE: 10/05/15

STREET ABANDONMENT
PART OF LEVEE STREET
ADJACENT TO CITY OF DALLAS
BLOCK NO. 21/7890
CITY OF DALLAS, DALLAS COUNTY, TEXAS



DAVID J. DE WEIRDT
REGISTERED PROFESSIONAL
LAND SURVEYOR NO. 5066

EXHIBIT B

ADDITIONAL ABANDONMENT PROVISIONS

That as a condition hereof, this abandonment is subject to any utilities or communication facilities, including without limitation water and wastewater lines, gas lines, and storm sewers, ("Facilities") presently located within the abandoned area described in Exhibit "A", owned and/or operated by the City of Dallas or any utility or communications company, public or private, ("Utility") and to the rights of any Utility for the use of the abandoned area for its Facilities. It is the intent of the foregoing to confirm and maintain and there is hereby reserved and excepted unto the City of Dallas, and not abandoned or conveyed hereunder, an easement (to which this abandonment is made expressly subject) over, upon, under, through, in, and across the abandoned area for each Utility for its respective Facilities located therein at the time of this abandonment, together with the right to make any subsequent alterations, additions, expansions, upgrades or modifications to such Facilities as may, from time to time be deemed necessary or convenient by the Utility owning and/or operating same. No buildings, structures (above or below ground) or trees shall be constructed or placed within the abandoned area without written consent of each affected Utility. Each Utility shall have the full right to remove and keep removed all or part of any buildings, fences, trees, or other improvements or growths which in any way may endanger or interfere with the construction, maintenance or efficiency of its respective Facilities lying within the abandoned area and shall at all times have the full right of ingress and egress to or from and upon the abandoned area for the purposes of reconstructing, removing, relocating, inspecting, patrolling, maintaining, expanding, upgrading, and/or adding to all or part of its Facilities without the necessity at any time of procuring the permission of anyone. The easement reserved hereunder and the conditions and restrictions to which this abandonment is subject shall remain for the benefit of the applicable Utility and/or operators of the Facilities until said Facilities are removed and relocated from the abandoned area. The relocation, removal or adjustment of any or all such Facilities, if made necessary by GRANTEE'S (whether one or more natural persons or legal entities) use of the abandonment area, shall be at the expense of GRANTEE herein, or GRANTEE'S successors and assigns. Should GRANTEE'S relocation or removal of the Facilities require the obtaining of new easements, the acquisition of same shall be at the expense of GRANTEE, GRANTEE'S successors and assigns. If any of the Facilities (or relocations thereof) are allowed to remain on any part of the abandoned area, the easements and buildings restrictions provided herein shall remain thereon. Upon removal or relocation of all of the Facilities, any easements reserved or created herein relating to such removed or relocated Facilities shall terminate, and any building restrictions herein created shall cease.

AGENDA ITEM # 37

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: January 27, 2016
COUNCIL DISTRICT(S): 2
DEPARTMENT: Sustainable Development and Construction
CMO: Ryan S. Evans, 671-9837
MAPSCO: 34Q

SUBJECT

An ordinance abandoning two sight easements to Fairfield Sadler LLC, the abutting owner, containing a total of approximately 226 square feet of land, located near the intersection of Inwood Road and Denton Drive, and authorizing the quitclaim - Revenue: \$5,400, plus the \$20 ordinance publication fee

BACKGROUND

This item authorizes the abandonment of two sight easements to Fairfield Sadler LLC, the abutting owner. The area will be included with the property of the abutting owner for a mixed-use development. The cost for this abandonment is the minimum processing fee pursuant to the Dallas City Code, therefore, no appraisal is required.

Notices were sent to six property owners located within 300 feet of the proposed abandonment area. There were no responses received in opposition to this request.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

Revenue: \$5,400, plus the \$20 ordinance publication fee

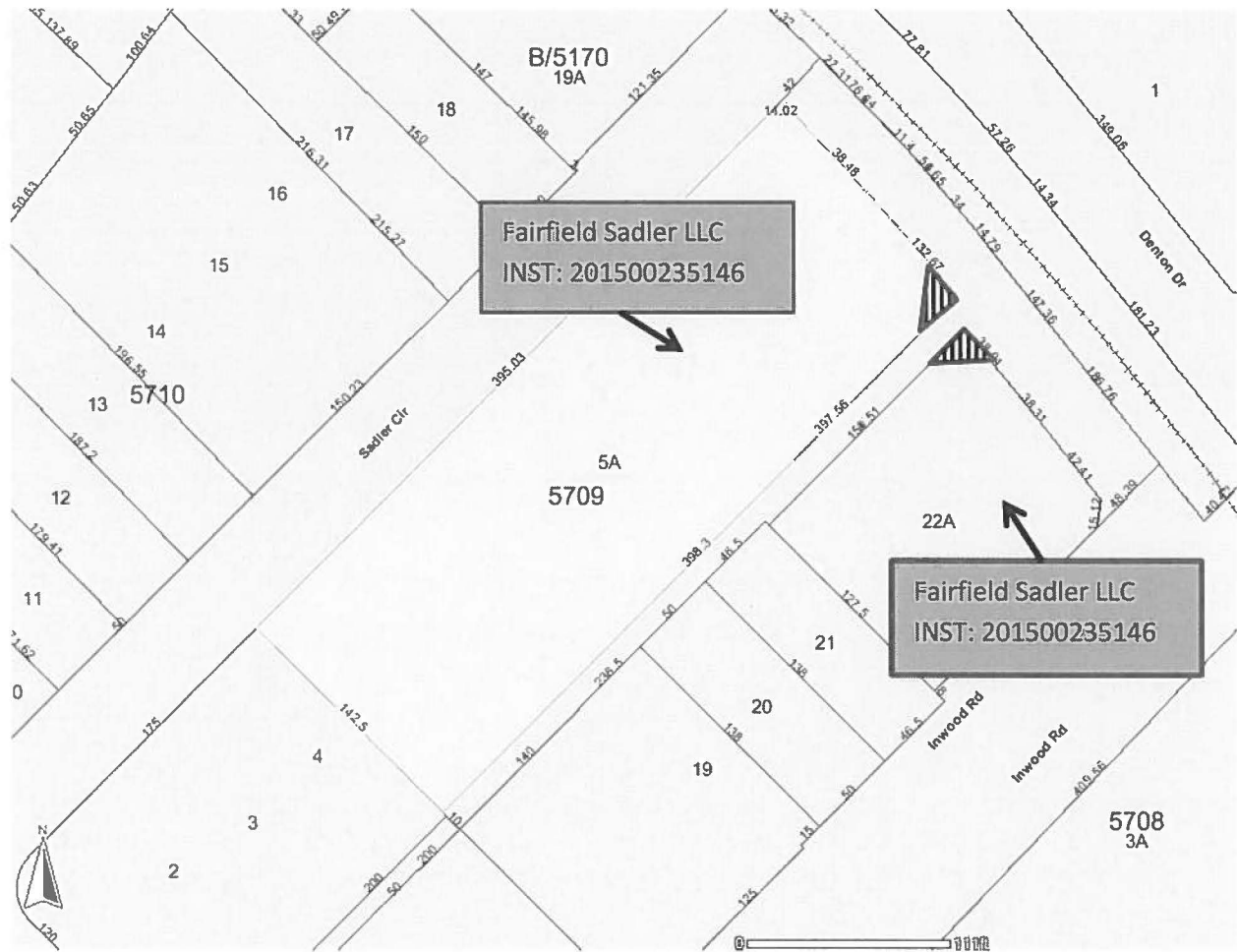
OWNER

Fairfield Sadler LLC

Christopher E. Hashioka, President and Chief Executive Officer

MAP

Attached



ABANDONMENT AREA: 

DEDICATION INSTRUMENT : **95244/2828**

LOG No.41634

BLK:A/5709

MAPSCO:34Q

APPLICANT: Fairfield Sadler LLC

ORDINANCE NO. _____

An ordinance providing for the abandonment of portions of two sight easements located in City Block A/5709 in the City of Dallas and County of Dallas, Texas; providing for the quitclaim thereof to Fairfield Sadler LLC; providing for the terms and conditions of the abandonment and quitclaim made herein; providing for barricading; providing for the indemnification of the City of Dallas against damages arising out of the abandonments herein; providing for the consideration to be paid to the City of Dallas; providing for the payment of the publication fee; and providing an effective date for this ordinance.

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WHEREAS, the City Council of the City of Dallas, acting pursuant to law and upon the request and petition of Fairfield Sadler LLC, a Delaware limited liability company, hereinafter referred to as **GRANTEE**, deems it advisable to abandon and quitclaim the hereinafter described tracts of land to **GRANTEE**, and is of the opinion that, subject to the terms and conditions herein provided, said portion of sight easements are not needed for public use, and same should be abandoned and quitclaimed to **GRANTEE**, as hereinafter stated; and

WHEREAS, the City Council of the City of Dallas is of the opinion that the best interest and welfare of the public will be served by abandoning and quitclaiming the same to **GRANTEE** for the consideration and subject to the terms and conditions hereinafter more fully set forth; **Now, Therefore,**

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That the tracts of land described in Exhibit A, which is attached hereto and made a part hereof for all purposes, be and the same is abandoned, vacated and closed insofar as the right, title and interest of the public are concerned; subject, however, to the conditions hereinafter more fully set out.

SECTION 2. That for and in monetary consideration of the sum of **FIVE THOUSAND FOUR HUNDRED AND NO/100 DOLLARS (\$5,400.00)** paid by **GRANTEE**, and the further consideration described in Sections 8, 9 and 10, the City of Dallas does by these

presents **FOREVER QUITCLAIM** unto the said **GRANTEE**, subject to the conditions, reservations, and exceptions hereinafter made and with the restrictions and upon the covenants below stated, all of its right, title and interest in and to the certain tracts of land hereinabove described in Exhibit A. **TO HAVE AND TO HOLD** all of such right, title and interest in and to the property and premises, subject aforesaid, together with all and singular the rights, privileges, hereditaments and appurtenances thereto in any manner belonging unto the said **GRANTEE** forever.

SECTION 3. That upon payment of the monetary consideration set forth in Section 2, **GRANTEE** accepts the terms, provisions, and conditions of this ordinance.

SECTION 4. That the Chief Financial Officer is authorized to deposit the sum paid by **GRANTEE** pursuant to Section 2 above in the General Fund 0001, Department DEV, Balance Sheet 0519 and Department of Sustainable Development and Construction-Real Estate Division shall be reimbursed for the cost of obtaining the legal description, appraisal and other administrative costs incurred. The reimbursement proceeds shall be deposited in General Fund 0001, Department DEV, Unit 1183, Object 5011 and any remaining proceeds shall be transferred to the General Capital Reserve Fund 0625, Department BMS, Unit 8888, Revenue Source 8416.

SECTION 5. That the abandonment and quitclaim provided for herein are made subject to all present zoning and deed restrictions, if the latter exist, and are subject to all existing easement rights of others, if any, whether apparent or non-apparent, aerial, surface, underground or otherwise, and are further subject to the conditions contained in Exhibit B, which is attached hereto and made a part hereof for all purposes.

SECTION 6. That the terms and conditions contained in this ordinance shall be binding upon **GRANTEE**, its successors and assigns.

SECTION 7. That the abandonment and quitclaim provided for herein shall extend only to the public right, title, easement and interest, and shall be construed to extend only to that interest the Governing Body of the City of Dallas may legally and lawfully abandon and vacate.

SECTION 8. That as a condition of this abandonment and as a part of the consideration for the quitclaim to **GRANTEE** herein, **GRANTEE**, its successors and assigns, agree to indemnify, defend, release and hold harmless the City of Dallas as to any and all claims for damages, fines, penalties, costs or expenses to persons or property that may arise out of, or be occasioned by or from: (i) the use and occupancy of the area described in Exhibit A by **GRANTEE**, its successors and assigns; (ii) the presence, generation, spillage, discharge, release, treatment or disposition of any Hazardous Substance on or affecting the area set out in Exhibit A; (iii) all corrective actions concerning any discovered Hazardous Substances on or affecting the area described in Exhibit A, which **GRANTEE**, its successors and assigns, agree to undertake and complete in accordance with applicable federal, state and local laws and regulations; and (iv) the abandonment, closing, vacation and quitclaim by the City of Dallas of the area set out in Exhibit A. **GRANTEE**, its successors and assigns, hereby agree to defend any and all suits, claims, or causes of action brought against the City of Dallas on account of same, and discharge any judgment or judgments that may be rendered against the City of Dallas in connection therewith. For purposes hereof, "Hazardous Substance" means the following: (a) any "hazardous substances" under the Comprehensive Environmental Response, Compensation, and Liability Act, 42 U.S.C. Section 9601 et seq., as amended; (b) any "hazardous substance" under the Texas Hazardous Substances Spill Prevention and Control Act, TEX. WATER CODE, Section 26.261 et seq., as amended; (c) petroleum or petroleum-based products (or any derivative or hazardous constituents thereof or additives thereto), including without limitation, fuel and lubricating oils; (d) any "hazardous chemicals" or "toxic chemicals" under the Occupational Safety and Health Act, 29 U.S.C. Section 651 et seq., as amended; (e) any "hazardous waste" under the Resource Conservation and Recovery Act, 42 U.S.C. Section 6901 et seq., as amended; and (f) any "chemical substance" under the Toxic Substance Control Act, 15 U.S.C. Section 2601 et seq., as amended. References to particular acts or codifications in this definition include all past and future amendments thereto, as well as applicable rules and regulations as now or hereafter promulgated thereunder.

SECTION 9. That as a condition of this abandonment and as a part of the consideration for the quitclaim made herein, **GRANTEE** shall file a final replat of the adjoining properties prior to the issuance of any building permits affecting the tracts of land abandoned and quitclaimed herein. This final replat shall be recorded by **GRANTEE** in the official real property records of the county in which the abandoned area is located after its approval by the City Plan Commission of the City of Dallas.

SECTION 10. That as a condition of this abandonment and as a part of the consideration for the quitclaim made herein, **GRANTEE** shall, immediately upon the passage of this ordinance, close, barricade and/or place signs in the area described in Exhibit A in accordance with detailed plans approved by the Director of Department of Sustainable Development and Construction. **GRANTEE's** responsibility for keeping the area described in Exhibit A closed, barricaded and/or the signs in place shall continue until the street improvements and intersection returns are removed by **GRANTEE**, its successors and assigns, to the satisfaction of the Director of Department of Sustainable Development and Construction.

SECTION 11. That the City Secretary is hereby authorized and directed to certify a copy of this ordinance for recordation in the official real property records of the county in which the abandonment area is located, which certified copy shall be delivered to the Director of Department of Sustainable Development and Construction, or designee. Upon receipt of the monetary consideration set forth in Section 2, plus the fee for the publishing of this ordinance, which **GRANTEE** shall likewise pay, the Director of Department of Sustainable Development and Construction, or designee: (i) shall deliver to **GRANTEE** a certified copy of this ordinance, and (ii) is authorized to and shall prepare and deliver a **QUITCLAIM DEED** with regard to the area abandoned herein, to **GRANTEE** hereunder, same to be executed by the City Manager on behalf of the City of Dallas, attested by the City Secretary and approved as to form by the City Attorney. The Director of Department of Sustainable Development and Construction, or designee, shall be the sole source for receiving certified copies of this ordinance for one year after its passage.

SECTION 12. That this ordinance shall take effect immediately from and after its passage and publication in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so ordained.

APPROVED AS TO FORM:

WARREN M. S. ERNST
City Attorney

DAVID COSSUM
Director of Department of Sustainable
Development and Construction

BY Brian N. Spivey II
Assistant City Attorney

BY Kathy Green
Assistant Director

Passed _____.

BEING a 112 square foot (0.003 acre) tract of land situated in the MILES BENNETT SURVEY, Abstract No. 52, City of Dallas, Dallas County, Texas and being part of Lot 5A in Block A/5709 of VIDEO POST AND TRANSFER ADDITION, an addition to the City of Dallas, Dallas County, Texas according to the Map thereof recorded in Volume 95244, Page 2828 of the Deed Records of Dallas County, Texas and all of a 15'x15' sight easement created by said plat and being more particularly described as follows:

BEGINNING at a 5/8" iron rod found at the easternmost corner of said Lot 5A, Block A/5709 and at the easternmost corner of said sight easement;

THENCE with the southeast line of said Lot 5A, Block A/5709, South 43°43'55" West, a distance of 15.00 feet to a point at the southernmost corner of said sight easement;

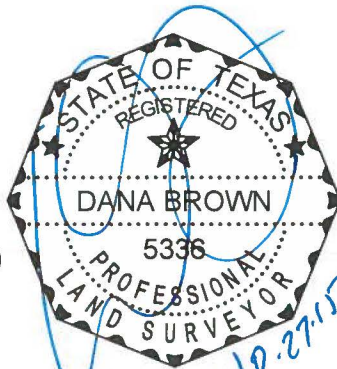
THENCE departing said southeast line of Lot 5A, Block A/5709 and with the west line of said sight easement, North 0°54'51" East, a distance of 22.01 feet to a point in the northeast line of said Lot 5A, Block A/5709, at the beginning of a non-tangent curve to the right having a central angle of 0°23'23", a radius of 2205.00 feet, a chord bearing and distance of South 41°54'13" East, 15.00 feet, from which a 1/2" iron rod found at the easternmost north corner of said Lot 5A, Block A/5709 bears along said northeast line of Lot 5A, Block A/5709 and along the northwesterly projection of said curve, an arc distance of 117.73 feet;

THENCE in a southeasterly direction with said northeast line and with said curve to the right, an arc distance of 15.00 feet to the **POINT OF BEGINNING** and containing 112 square feet or 0.003 acres of land.

(For SPRG use only)

Reviewed By: DBF
 Date: 12-22-2015
 SPRG NO: 3294

DANA BROWN
 REGISTERED PROFESSIONAL
 LAND SURVEYOR NO. 5336
 12750 MERIT DRIVE, SUITE 1000
 DALLAS, TEXAS 75251
 PH. 972-770-1300
 dana.brown@kimley-horn.com



SIGHT EASEMENT ABANDONMENT
 LOT 5A, BLOCK A/5709, VIDEO POST AND
 TRANSFER ADDITION
 MILES BENNETT SURVEY, ABSTRACT NO. 52
 CITY OF DALLAS
 DALLAS COUNTY, TEXAS

Kimley»Horn

12750 Merit Drive, Suite 1000
 Dallas, Texas 75251

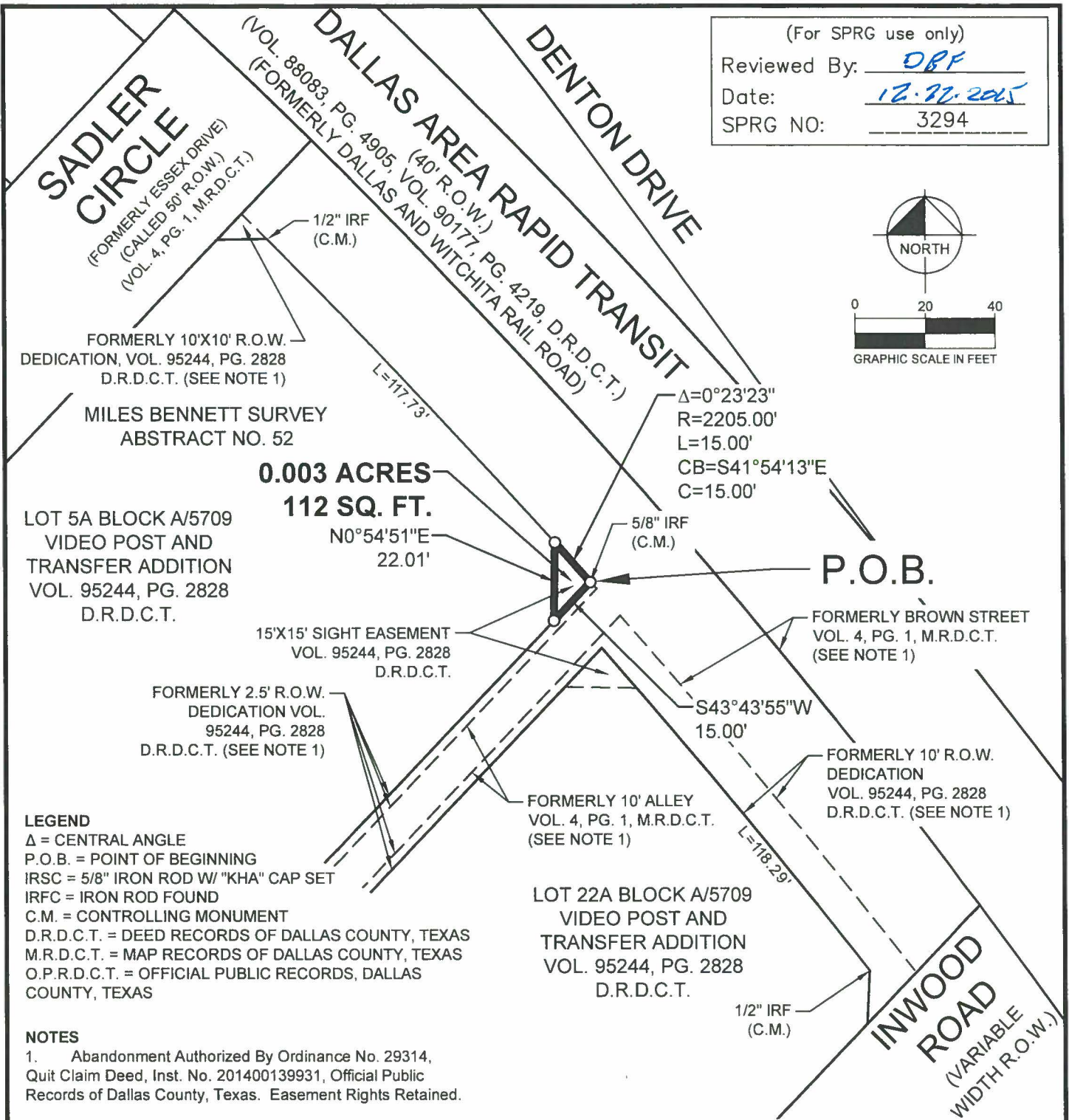
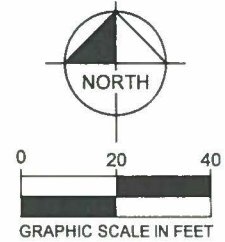
FIRM # 10115500

Tel No. (972) 770-1300
 Fax No. (972) 239-3820

Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
N/A	SRD	DAB	OCT. 2015	064483000	1 OF 2

(For SPRG use only)

Reviewed By: DBF
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FIRM # 10115500

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 Fax No. (972) 239-3820

Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
1" = 40'	SRD	DAB	OCT. 2015	064483000	2 OF 2

BEING a 114 square feet (0.003 acre) tract of land situated in the MILES BENNETT SURVEY, Abstract No. 52, City of Dallas, Dallas County, Texas and being part of Lot 22A in Block A/5709 of VIDEO POST AND TRANSFER ADDITION, an addition to the City of Dallas, Dallas County, Texas according to the Map thereof recorded in Volume 95244, Page 2828 of the Deed Records of Dallas County, Texas and all of a 15'x15' sight easement created by said plat and being more particularly described as follows:

BEGINNING at a 5/8" iron rod found at the northernmost corner of said Lot 22A, Block A/5709, the northernmost corner of said sight easement and at the beginning of a curve to the right having a central angle of 0°23'30", a radius of 2195.00 feet, a chord bearing and distance of South 41°06'00" East, 15.00 feet;

THENCE with the northeast line of said Lot 22A, Block A/5709, in a southeasterly direction and with said curve to the right, an arc distance of 15.00 feet to the easternmost corner of said sight easement, from which, a 1/2" iron rod found at the easterly most northeast corner of said Lot 22A, Block A/5709 bears along the southeasterly projection of said curve, an arc distance of 103.29 feet;

THENCE departing said northeast line and with the south line of said sight easement, North 89°07'22" West, a distance of 20.38 feet to a point in the northwest line of said Lot 22A, Block A/5709 at the westernmost corner of said sight easement;

THENCE with said northwest line, North 43°43'55" East, a distance of 15.21 feet to the **POINT OF BEGINNING** and containing 114 square feet or 0.003 acres of land.

Bearing system based on the Texas Coordinate System of 1983, North Central Zone (4202), North American Datum of 1983.

(For SPRG use only)

Reviewed By: DBF
Date: 12-22-2015
SPRG NO: 3359

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SIGHT EASEMENT ABANDONMENT
LOT 22A, BLOCK A/5709, VIDEO POST AND
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MILES BENNETT SURVEY, ABSTRACT NO. 52
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DALLAS COUNTY, TEXAS

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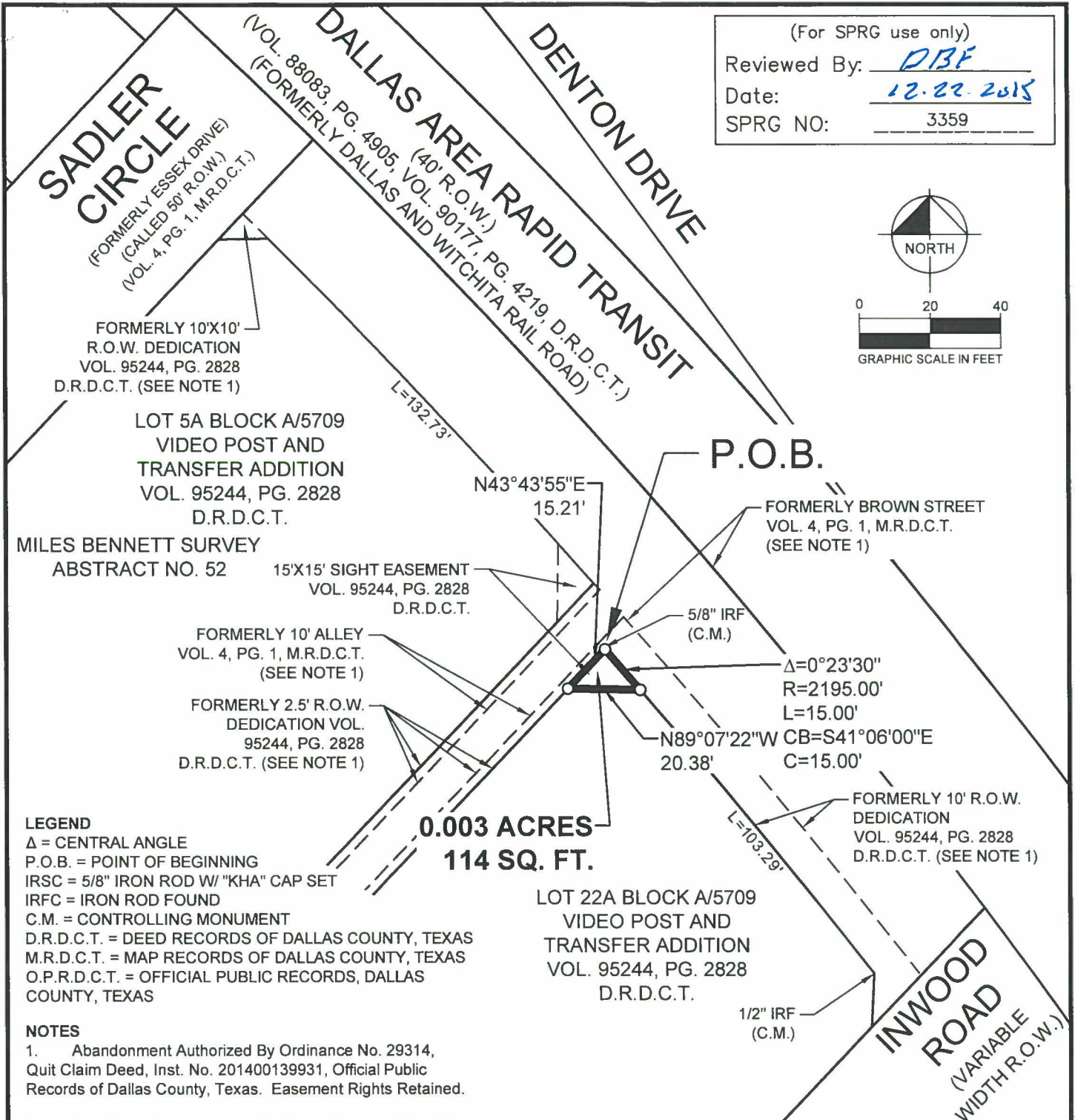
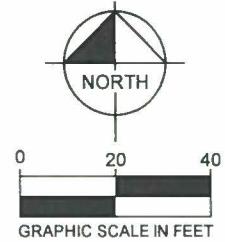
FIRM # 10115500

Tel No. (972) 770-1300
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Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
N/A	SRD	DAB	OCT. 2015	064483000	1 OF 2

(For SPRG use only)

Reviewed By: DBF
Date: 12.22.2015
SPRG NO: 3359



LEGEND

Δ = CENTRAL ANGLE
P.O.B. = POINT OF BEGINNING
IRSC = 5/8" IRON ROD W/ "KHA" CAP SET
IRFC = IRON ROD FOUND
C.M. = CONTROLLING MONUMENT
D.R.D.C.T. = DEED RECORDS OF DALLAS COUNTY, TEXAS
M.R.D.C.T. = MAP RECORDS OF DALLAS COUNTY, TEXAS
O.P.R.D.C.T. = OFFICIAL PUBLIC RECORDS, DALLAS COUNTY, TEXAS

NOTES

1. Abandonment Authorized By Ordinance No. 29314, Quit Claim Deed, Inst. No. 201400139931, Official Public Records of Dallas County, Texas. Easement Rights Retained.
2. Bearing system based on the Texas Coordinate System of 1983, North Central Zone (4202), North American Datum of 1983.

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SIGHT EASEMENT ABANDONMENT
LOT 22A, BLOCK A/5709, VIDEO POST AND
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MILES BENNETT SURVEY, ABSTRACT NO. 52
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Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
1" = 40'	SRD	DAB	OCT. 2015	064483000	2 OF 2

EXHIBIT B

ADDITIONAL ABANDONMENT PROVISIONS

That as a condition hereof, this abandonment is subject to any utilities or communication facilities, including without limitation water and wastewater lines, gas lines, and storm sewers, ("Facilities") presently located within the abandoned area described in Exhibit "A", owned and/or operated by the City of Dallas or any utility or communications company, public or private, ("Utility") and to the rights of any Utility for the use of the abandoned area for its Facilities. It is the intent of the foregoing to confirm and maintain and there is hereby reserved and excepted unto the City of Dallas, and not abandoned or conveyed hereunder, an easement (to which this abandonment is made expressly subject) over, upon, under, through, in, and across the abandoned area for each Utility for its respective Facilities located therein at the time of this abandonment, together with the right to make any subsequent alterations, additions, expansions, upgrades or modifications to such Facilities as may, from time to time be deemed necessary or convenient by the Utility owning and/or operating same. No buildings, structures (above or below ground) or trees shall be constructed or placed within the abandoned area without written consent of each affected Utility. Each Utility shall have the full right to remove and keep removed all or part of any buildings, fences, trees, or other improvements or growths which in any way may endanger or interfere with the construction, maintenance or efficiency of its respective Facilities lying within the abandoned area and shall at all times have the full right of ingress and egress to or from and upon the abandoned area for the purposes of reconstructing, removing, relocating, inspecting, patrolling, maintaining, expanding, upgrading, and/or adding to all or part of its Facilities without the necessity at any time of procuring the permission of anyone. The easement reserved hereunder and the conditions and restrictions to which this abandonment is subject shall remain for the benefit of the applicable Utility and/or operators of the Facilities until said Facilities are removed and relocated from the abandoned area. The relocation, removal or adjustment of any or all such Facilities, if made necessary by GRANTEE'S (whether one or more natural persons or legal entities) use of the abandonment area, shall be at the expense of GRANTEE herein, or GRANTEE'S successors and assigns. Should GRANTEE'S relocation or removal of the Facilities require the obtaining of new easements, the acquisition of same shall be at the expense of GRANTEE, GRANTEE'S successors and assigns. If any of the Facilities (or relocations thereof) are allowed to remain on any part of the abandoned area, the easements and buildings restrictions provided herein shall remain thereon. Upon removal or relocation of all of the Facilities, any easements reserved or created herein relating to such removed or relocated Facilities shall terminate, and any building restrictions herein created shall cease.

AGENDA ITEM # 39

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 4

DEPARTMENT: Sustainable Development and Construction
Public Works Department

CMO: Ryan S. Evans, 671-9837
Jill A. Jordan, P.E., 670-5299

MAPSCO: 55H

SUBJECT

Authorize acquisition, including the exercise of the right of eminent domain, if such becomes necessary, from Jaime Saucedah and Beth Saucedah, of approximately 13,650 square feet of land located near the intersection of Packard and Cadillac Streets for the Cadillac Heights Project Phase I - Not to exceed \$55,000 (\$50,000, plus closing costs and title expenses not to exceed \$5,000) - Financing: 2006 Bond Funds

BACKGROUND

This item authorizes the acquisition, including the exercise of the right of eminent domain, if such becomes necessary, from Jaime Saucedah and Beth Saucedah, of approximately 13,650 square feet of land for the Cadillac Heights Project Phase I. This property will be used for city service and maintenance facilities. The consideration is based on an independent appraisal.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized acquisition on December 8, 2003, by Resolution No. 03-3321.

Information about this item will be provided to the Economic Development Committee on January 19, 2016.

FISCAL INFORMATION

2006 Bond Funds - \$55,000 (\$50,000, plus closing costs and title expenses not to exceed \$5,000)

OWNERS

Jaime Saucedah

Beth Saucedah

MAP

Attached

CITY OF DALLAS PLAT BOOKS

XED SEPT. 25, 1951 ORD. NO. 525.6
 EY WM J ELLIOTT ABST. 4.48

ADDITION CADILLAC PLACE NO. 1

1-2-3-1-12-13-14
 BLOCKS 6641

SCALE 100 FT. EQUALS 1 INCH

SCHOOL DIST. DALLAS



January 27, 2016

A RESOLUTION DETERMINING UPON THE NECESSITY OF ACQUIRING REAL PROPERTY AND AUTHORIZING ITS APPROPRIATION AND/OR CONDEMNATION FOR PUBLIC USE.

DEFINITIONS: For the purposes of this resolution, the following definitions of terms shall apply:

"CITY": The City of Dallas

"PROPERTY": Approximately 13,650 square feet of land located in Dallas County and being the same property more particularly described in "Exhibit A", attached hereto and made a part hereof for all purposes.

"PROJECT": Cadillac Heights Phase I

"USE": City service and maintenance facilities

"PROPERTY INTEREST": Fee Simple

"OWNER": Jaime Saucedah and Beth Saucedah, provided, however, that the term "OWNER" as used in this resolution means all persons having an ownership interest, regardless of whether those persons are actually named herein.

"OFFER AMOUNT": \$50,000

"CLOSING COSTS AND TITLE EXPENSES": Not to exceed \$5,000

"AUTHORIZED AMOUNT": \$55,000 (\$50,000, plus closing costs and title expenses not to exceed \$5,000)

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That the USE of the PROPERTY for the PROJECT is a public use.

SECTION 2. That public necessity requires that the CITY acquire the PROPERTY INTEREST in the PROPERTY for the PROJECT.

January 27, 2016

SECTION 3. That for the purpose of acquiring the PROPERTY INTEREST in the PROPERTY, the Assistant Director of Sustainable Development and Construction, Real Estate Division, or such person as she may designate, is hereby authorized and directed to offer the OFFER AMOUNT as payment for the PROPERTY INTEREST in the PROPERTY.

SECTION 4. That in the event the OWNER accepts the OFFER AMOUNT, the Chief Financial Officer is authorized and directed to draw a warrant in favor of the OWNER, or the then current owner of record, or the title company closing the transaction described herein in the OFFER AMOUNT, CLOSING COSTS AND TITLE EXPENSES payable out of Land Acquisition in Cadillac Heights Funds, Fund No. BT11, Department PBW, Unit T825, Program No. PB06T825, Object 4210, Encumbrance No. CT-SUSVLT82572. The OFFER AMOUNT and the CLOSING COSTS AND TITLE EXPENSES together shall not exceed the AUTHORIZED AMOUNT.

SECTION 5. That the CITY is to have possession of the PROPERTY at closing; and the CITY will pay any title expenses and closing costs. In the event of condemnation, the CITY will pay court costs as may be assessed by the Special Commissioners or the court. Further, that litigation expenses determined by the City Attorney to be necessary are authorized for payment. All costs and expenses described in this section shall be paid from the previously described funds.

SECTION 6. That if the OWNER refuses to accept the OFFER AMOUNT, the CITY will appropriate the PROPERTY INTEREST in the PROPERTY for the PROJECT under the laws of eminent domain and the provisions of the Charter of the City of Dallas. In such case, the City Attorney is authorized and requested to file the necessary suit(s) and take the necessary action for the prompt acquisition of the PROPERTY INTEREST in the PROPERTY by condemnation or in any manner provided by law.

SECTION 7. That in the event it is subsequently determined that additional persons other than those named herein have an interest in the PROPERTY, the City Attorney is authorized and directed to join said parties as defendants in said condemnation suit(s).

SECTION 8. That to the extent the PROPERTY is being purchased wholly or partly with bond proceeds CITY has obtained an independent appraisal of the PROPERTY'S market value.

SECTION 9. That owner has been provided with a copy of the Landowner's Bill of Rights as required by Texas Property Code Section 21.0112.

January 27, 2016

SECTION 10. That in the event the Special Commissioners in Condemnation appointed by the Court return an award that is the same amount or less than the OFFER AMOUNT, the City Attorney is hereby authorized to settle the lawsuit for that amount and the Chief Financial Officer is hereby authorized to issue a check drawn on the previously described funds in an amount not to exceed the Commissioners' award made payable to the County Clerk of Dallas County, to be deposited into the registry of the Court, to enable the CITY to take possession of the PROPERTY without further action of the Dallas City Council.

SECTION 11. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

APPROVED AS TO FORM:
WARREN M. S. ERNST, City Attorney

BY 
Assistant City Attorney

EXHIBIT A

LOT ONE, BLOCK FOURTEEN, (14/6641) OF CADILLIAC PLACE, AN
ADDITION TO THE CITY OF DALLAS, TEXAS, ACCORDING TO THE MAP
THEREOF RECORDED IN VOLUME NO. 7, PAGE 459, MAP RECORDS OF
DALLAS, COUNTY, TEXAS.

AGENDA ITEM # 40

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2, 14

DEPARTMENT: Planning and Urban Design

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: 45 A B E F K L P & Q

SUBJECT

Authorize a professional services contract with MIG, Inc. for the preparation of an update to the City's Downtown Dallas 360 Plan adopted by City Council on April 13, 2011 - Not to exceed \$250,000 - Financing: Downtown Connection TIF District Funds

BACKGROUND

In 2011, City Council adopted the Downtown Dallas 360 Plan. The plan's adoption was the culmination of a partnership between the City of Dallas and Downtown Dallas Inc. and a one-and-a-half year long process which included two community forums, four stakeholder work sessions, numerous individual stakeholder meetings, six Technical Committee meetings, and a regularly maintained project web site. MIG, Inc. was selected by the City, through a request for proposals process in 2009, to complete the original Downtown Dallas 360 Plan.

The Downtown Dallas 360 Plan established a shared vision for achieving three overarching goals Downtown: (1) creating an exciting urban experience; (2) a balanced transportation system; and (3) an inclusive environment. The Plan primarily addressed seven districts within the freeway loop that forms the core central business district (CBD) area, established important relationships between the core and surrounding 9 square mile area of supporting districts, articulated actions to achieve the vision by focusing on transformative strategies and geographic focus areas within the core CBD area. The plan concluded with a matrix of 67 prioritized actions categorized as short term (1 to 2 years), medium term (3 to 5 years) and long term (5 to 10 years) to guide and focus public and private efforts, as well as "quick win" actions that provided immediate results and sustained downtown momentum.

BACKGROUND (Continued)

Since the plan's adoption in 2011, the core has transformed greatly. Downtown has experienced an increase in overall population, renovation is underway for the last remaining large vacant buildings in the core, announcement of high speed rail service to Houston, finalization of a portion of the D2 alignment, privatization of the Farmers Market, etc., all of which impact the downtown core and surrounding areas, emphasizing the need for coordination, connection and consensus around the next phase of Downtown Dallas' economic, social and environmental growth and development.

The City's Contracting Policy, A.D. 4-5, Section 9.3.5 provides for the ability to contract for consulting services with a specific firm without utilizing the City's procurement process, if there is only one consultant that can best provide the required service. MIG is best and uniquely qualified to perform the services, as they prepared the original Downtown Dallas 360 Plan and was awarded the original service contract through the City's RFP process.

The scope of the update includes:

1. Market Analysis, Economics, and Finance – Analysis of job growth (current, projected, and strategies), economic indicators, development strategies, and finance mechanisms.
2. Mobility - systematic look at opportunities, connections and transportation options that are needed to balance vehicular trips in the greater downtown area, provide connections to major destinations and corridors of activity and address how the City's core interface with the regional transportation system.
 - a. Regional System Integration
 - i. Define wants for regional transportation system, specific to Interstates 30-35-345 (TxDOT). This task will work with HNTB and TxDOT to coordinate plan processes, ultimately advancing scenarios best for Downtown.
 - ii. Review and analyze High Speed Rail potential with recommendations on how to best connect regional, local and all forms of transportation to the station.
 - iii. Lower Stemmons IH35 and The Canyon IH30: Analyze and recommend strategies for reconciling conflicting regional to local relationship between the local street system and the regional highway network with specific focus on: the "arrival experience" to Downtown, including improved east-west connections, MHH Bridge grade interface, Commerce cloverleaf
 - iv. Analyze modifications and decking opportunities for I-30 ramp.

BACKGROUND (Continued)

- b. Street Typologies
 - i. Update and refine Complete Streets (360 expanded geographic scope) and 360, including design standards and transportation performance metrics.
 - c. Circulation Framework
 - i. Update and refine 360's circulation framework to incorporate specific corridor modifications related to road diets, multi-modal integration (bike, streetcar, etc.), directional conversions, District connectors and signature streets.
 - d. Design Guidelines and/or Standards: Mobility and the Public Realm
 - i. Complete a Modern Street Design Manual based on Complete Streets refinement, upon adoption by City Council, tree and landscape standards.
 - e. Integration of Mobility section into neighborhood microplans to address specific needs.
3. Housing
- a. Build on Neighborhoods Plus; enhance 360 housing strategies with holistic neighborhood development, including housing, services, education, health and recreation.
 - b. Analyze and create across districts a strategy for sustainable and socially equitable mix of product and price point within the city's urban core, within emphasis on infill strategies for new construction and economics of providing and maintaining an affordable product.
4. Neighborhood Microplans
- a. Complete specific plans addressing neighborhood issues in all 15 (or more) districts such as: urban design, land use, walkability, retail, commercial office, entertainment, education, and innovation, at a tailored and varied scale according to neighborhood needs.
5. Implementation Strategy
- a. Tactical implementation plan with associated responsibilities, costs, and timelines; will also include bond priority recommendations and organizational structures for effective results.

BACKGROUND (Continued)

Downtown Dallas Inc. (DDI) has allocated \$250,000 to the update effort and has nearly completed Phase I – Outreach and Assessment portion of the update. Phase 1 included (1) inventory of concurrent planning efforts, identifying project that impact the areas of the 360 plan; (2) stakeholder and community engagement; (3) neighborhoods needs and assessment, a physical inventory of existing conditions, current projects, short and long term needs, and market conditions for each area; and (4) refinement and prioritization of scope and resources for further plan products. City is requesting the authorization of a professional services contract with MIG in the amount of \$250,000 to match DDI's contribution the plan's update.

ESTIMATED SCHEDULE OF PROJECT

Project Started June 2015
Project Completion June 2016

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On April 22, 2009, City Council authorized a contract with MIG, Inc. to develop a strategic plan for the downtown area, by Resolution No. 09-1109.

On April 13, 2011, City Council authorized the adoption of the Downtown Dallas 360 Plan, by Resolution No. 11-0996.

On December 10, 2015, the Downtown Connection TIF District board of directors reviewed and recommended approval of a professional services contract with MIG, Inc. for the preparation of an update to the City's Downtown Dallas 360 Plan in amount not to exceed \$250,000.

Council will be briefed by memorandum regarding this item.

On January 13, 2016, this item was deferred by Councilmember McGough.

FISCAL INFORMATION

\$250,000 – Downtown Connection TIF District Funds

<u>COUNCIL DISTRICT</u>	<u>AMOUNT</u>
2	\$125,000
14	\$125,000

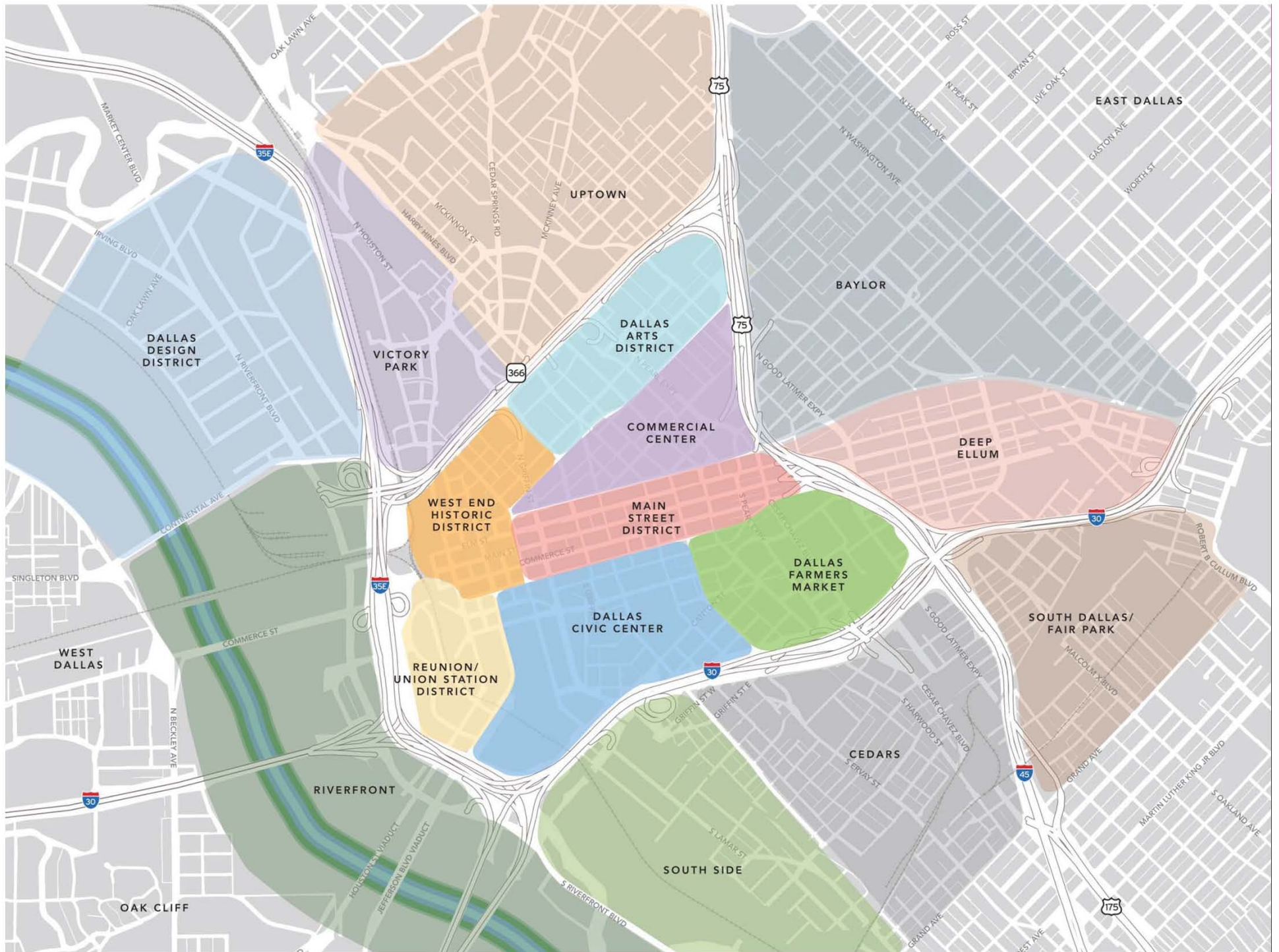
DEVELOPER

MIG, Inc.

Susan Goltsman, President
Carolyn Verheyen, Secretary
Daniel Iacofano, Treasurer

MAP

Attached.



January 27, 2016

WHEREAS, the City recognizes the importance of its role in local economic development initiatives and programs; and

WHEREAS, on June 8, 2005, City Council authorized the establishment of Tax Increment Financing Reinvestment Zone Number Eleven (Downtown Connection TIF District) and established a Board of Directors for the District to promote development or redevelopment in the Downtown Connection area by Ordinance No. 26020, as authorized by the Tax Increment Financing Act, Chapter 311 of the Texas Tax Code; and

WHEREAS, on August 29, 2005, City Council authorized the Project Plan and Reinvestment Zone Financing Plan for Tax Increment Financing Reinvestment Zone Number Eleven, (Downtown Connection TIF District); and authorized a participation agreement with Dallas County for the Downtown Connection TIF District by Ordinance No. 26096; and

WHEREAS, on April 22, 2009, City Council authorized a contract with MIG, Inc. to develop a strategic plan for the downtown area in an amount not to exceed \$515,000 by Resolution No. 09-1109; and

WHEREAS, on April 13, 2011, City Council authorized the adoption of the Downtown Dallas 360 Plan, by Resolution No. 11-0996; and

WHEREAS, since the Downtown Dallas 360 plan's adoption in 2011, the core has transformed greatly and has experienced an increase in overall population, renovation is underway for the last remaining large vacant buildings in the core, announcement of high speed rail service to Houston, finalization of a portion of the D2 alignment, privatization of the Farmers Market, etc., all of which impact the Downtown Connection TIF District and surrounding areas; and

WHEREAS, in furtherance of the Downtown Connection TIF District Project Plan and to ensure development priorities and implementation strategies are aligned with the City's vision for downtown, the City desires to enter into a services contract with MIG, Inc. to update the Downtown Dallas 360 plan to address changes in the downtown core of the past five years and address and/or update comprehensive issues such as mobility, urban design, housing, open space, Smart City, economics and land use.

NOW, THEREFORE,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

January 27, 2016

Section 1. That the City Manager, upon approval as to form by the City Attorney is hereby authorized to execute a professional services contract with MIG, Inc. in an amount not to exceed \$250,000.

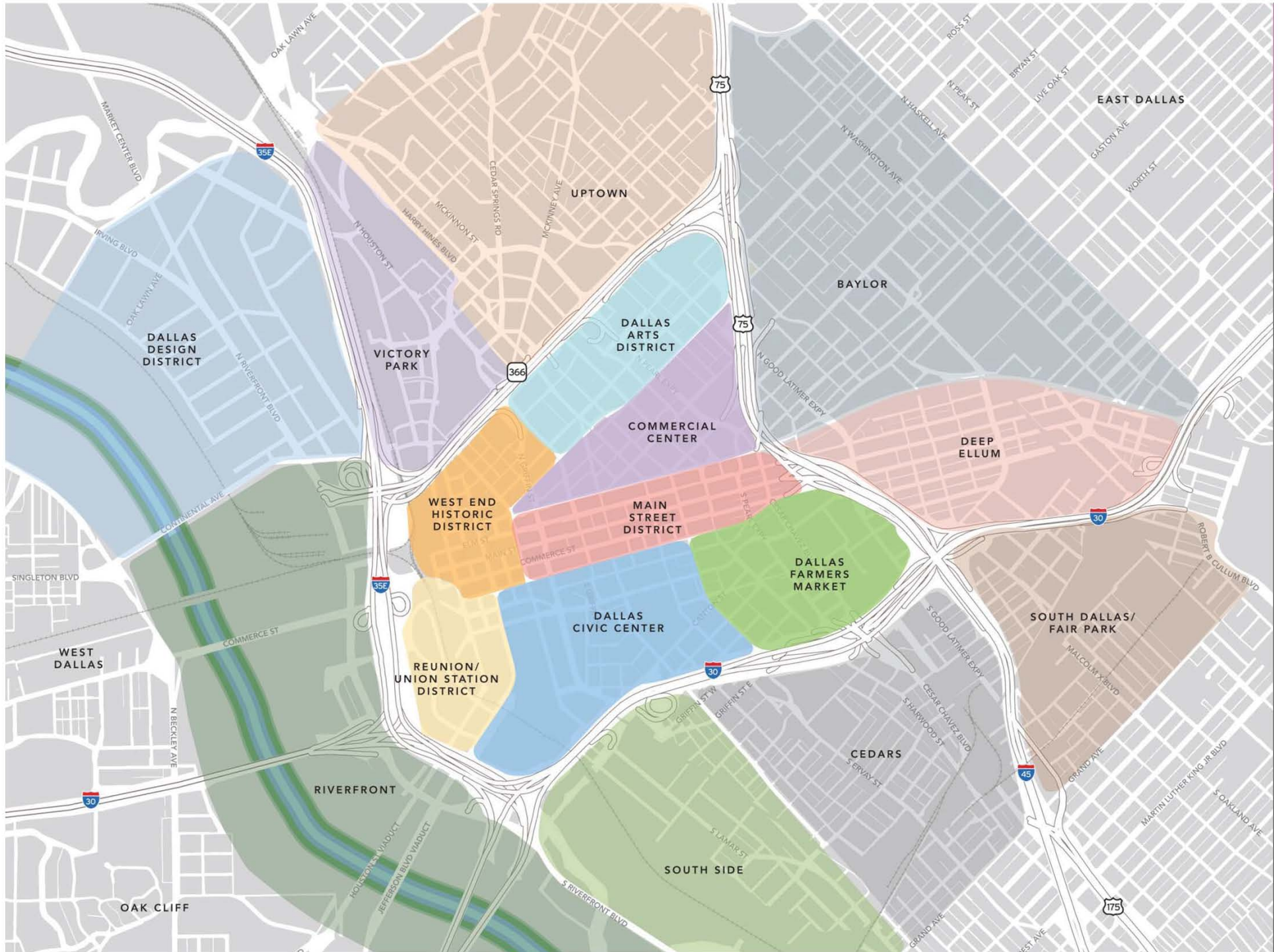
Section 2. That the professional services contract shall provide, among other provisions:

- a. MIG, Inc. shall complete an update to the Downtown Dallas 360 plan for the greater downtown area, as depicted on **Exhibit A**.
- b. That the contract's funds shall be dispersed in scheduled payments based on meeting certain required milestones and timelines for the plan's update, as set forth in the service contract.
- c. That the plan's update shall include a strategy for implementation, wherein specific actions are identified to accomplish the plan's goals, such as regulatory changes, strategic public and private investments, public/private financing strategies, etc.

Section 3. That the Chief Financial Officer is hereby authorized to encumber and disburse funds from Fund 0044, Department ECO, Unit W042, Object 3070, Activity DTTI, CT ECOW042A257, Vendor No. VS0000034342, in an amount not to exceed \$250,000.

Section 4. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

Exhibit A



KEY FOCUS AREA: E-Gov

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): N/A

DEPARTMENT: Public Works Department

CMO: Jill A. Jordan, P.E., 670-5299

MAPSCO: N/A

SUBJECT

An ordinance amending Chapter 43, Article VIII of the Dallas City Code, relating to improvements in public right-of-way by **(1)** amending registration requirements; **(2)** modifying the permit application requirements; **(3)** amending trench and backfill requirements; **(4)** providing additional signage requirements for vehicles and equipment in the right-of-way; **(5)** providing additional notification requirements; and **(6)** making conforming changes - Financing: No cost consideration to the City

BACKGROUND

Chapter 43 of the Dallas City Code currently provides conditions for conducting improvements and utility work in the public right-of-way. The proposed ordinance would amend Chapter 43 to include required notification to the city 24-hours before the start of any work, within 24-hours of any temporary pavements repairs, and within 24-hours of completion of permanent pavement repairs. Amendments will also require additional signage on the street or nearby notifying of temporary pavement repairs and provide additional trench backfill requirements.

PRIOR ACTION / REVIEW (COUNCIL, BOARDS, COMMISSIONS)

This item was briefed to Budget, Finance and Audit Committee on August 3, 2015.

Information about this item was provided to the Budget, Finance and Audit Committee on December 7, 2015.

This item was deferred at the request of Councilmember Phillip T. Kingston on December 9, 2015.

FISCAL INFORMATION

No cost consideration to the City.

ORDINANCE NO. _____

An ordinance amending Chapter 43, “Streets and Sidewalks,” of the Dallas City Code by amending Sections 43-137, 43-139, 43-140.2 and 43-141; amending the registration requirements; modifying the permit application requirements; providing additional trench backfill requirements; providing additional signage requirements; requiring additional notice for phases of construction; making conforming changes; providing a penalty not to exceed \$500; providing a savings clause; providing a severability clause; and providing an effective date.

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

SECTION 1. That Section 43-137, “Registration; Other Requirements,” of Article VIII, “Certain Uses of Public Right-of-Way,” of Chapter 43, “Streets and Sidewalks,” of the Dallas City Code is amended to read as follows:

“SEC. 43-137. REGISTRATION; OTHER REQUIREMENTS.

(a) Nothing in this section relieves any person [~~a public service provider~~] from obtaining a permit under this article to perform work in the public right-of-way.

(b) In order to protect the public health, safety, and welfare, a public service provider maintaining or operating existing facilities in the public right-of-way, and any other person working in the public right-of-way, must register with the director in accordance with the following requirements:

(1) The registration must be on a form furnished by the director and made in the name of the public service provider that owns the facilities or the person working in the public right-of-way.

(2) Registration expires March 1 of every other year after the calendar year in which the first registration occurs. If a registration is not renewed by the expiration date, the director shall furnish written notice to the public service provider or person that the registration has expired. If a [~~the~~] public service provider fails to renew registration within 30 calendar days

after the director gives notice of the expiration, the facilities of the public service provider will be deemed to have been legally abandoned.

(3) If information provided as part of the registration changes, the public service provider or person must inform the director in writing not more than 30 days after the date the change occurs.

(4) The public service provider or person shall also include the following with the registration:

(A) The name of the public service provider or person using the public right-of-way, including any business name, assumed name, or trade name the public service provider operates under or has operated under within the past five years.

(B) If the public service provider is a certificated telecommunications provider, the certificate number issued by the Texas Public Utility Commission.

(C) The ordinance number of any franchise or license issued by the city of Dallas that authorizes the public service provider or person to use the public right-of-way.

(D) The names, addresses, and telephone numbers of at least two persons who will be general, day-to-day contacts for the public service provider or person. At least one of the addresses must be within the Dallas/Fort Worth metropolitan area.

(E) The name and mailing address of the officer or agent designated as the person authorized to receive service of process on behalf of the public service provider or person.

(F) The name, address, and telephone number of any contractor or subcontractor, if known, who will be working in the public right-of-way on behalf of the public service provider or person.

(G) The names and telephone numbers of at least two persons serving as emergency contacts who can be reached by telephone 24 hours a day, seven days a week. The telephone numbers should be accessible without the city having to pay a long distance telephone or toll charge.

(H) Proof of existing insurance that complies with the following requirements:

(i) The minimum insurance coverage for a public service provider must be commercial general liability insurance, or any combination of general liability and umbrella/excess insurance, (including, but not limited to, premises operations, personal and advertising injury, products/completed operations, and independent contractors and contractual liability) with a minimum combined bodily injury (including death) and property damage limit of \$25,000,000 per occurrence, \$25,000,000 products/completed operations aggregate, and

\$25,000,000 general aggregate. The liability insurance policy must also include coverage for explosion, collapse, and underground hazards. The insurance coverage must be written by a company or companies approved to conduct business in the State of Texas. The city must be named as an additional insured on the policy by using endorsement CG 20 26 or broader.

(ii) The insurance filed by a public service provider or person working in the public right-of-way must also meet the same requirements as insurance filed by a permittee under Section 43-140(a)(3) through (a)(7). A public service provider or person registered under this section has the same duties, obligations, and liabilities as a permittee under Section 43-140(a)(3) through (a)(7), except that a public service provider or person registered under this section does not have to file separate proof of insurance every time it obtains a permit to perform work in the public right-of-way.

(iii) If the public service provider or person is an entity that has a tangible net worth ratio of 3 to 1 (assets to liabilities) with a minimum tangible net worth of at least \$100,000,000, proof of self-insurance sufficient to meet the coverage required in this subparagraph is sufficient to satisfy the insurance requirements of this subparagraph.

(5) The insurance requirements of Subsection (b)(4)(H) of this section do not apply to:

(A) construction or other activity performed by the city's own forces or by contractors hired by the city and working on city-owned facilities within the public right-of-way; or

(B) a public service provider or person operating facilities or performing construction pursuant to a valid existing franchise or license approved by the city council."

SECTION 2. That Subsection (c) of Section 43-139, "Permit Required; Exceptions; Conditions; Denial and Revocation," of Article VIII, "Certain Uses of Public Right-of-Way," of Chapter 43, "Streets and Sidewalks," of the Dallas City Code is amended to read as follows:

"(c) The following procedures and requirements govern the application for and issuance of a permit required under Subsection (a) of this section:

(1) A permit application must be made in writing on a form approved by the director. The application must be signed and submitted by the owner of the facility for which the permit is requested or, if the work does not involve a facility, by the owner of the improvement for which the permit is requested.

(2) Except in the case of a major project, a permit application must be submitted to the director not less than two business days before commencement of the proposed

construction unless emergency activity is required, in which case immediate notice, including the reasons for the emergency activity, must be given to the director.

(3) A permit application for a major project must be submitted enough time in advance of the commencement of the proposed construction to allow the director at least 30 business days for review. During this project submission review period, schedules, alternatives to cutting the street, utility assignments, special repair requirements, and all other questions will be resolved. Adjustments to time limits specified in the Pavement Cut and Repair Standards Manual may be granted by the director for major project work. The proposed construction on the project may commence upon issuance of the permit by the director.

(4) A permit application must include a statement by the applicant that the applicant has collected all available plans for existing city of Dallas underground facilities and other public and private utilities and has included those facilities and utilities in the applicant's design, showing no apparent conflict. The statement must also affirm that the applicant will perform field verifications as necessary during construction to locate all city and other existing underground facilities.

(5) A permit application for an above ground utility structure in or outside of a public right-of-way must include identification of appropriate locations for the structure that are consistent with the placement criteria set forth in the AGUS Placement Guidelines.

(6) The permit application on any project must include submittal of plans to the director. When required by the Texas Engineering Practice Act, as amended, the plans must be sealed by a professional engineer licensed to practice in the State of Texas. The plans must include the horizontal alignment of all proposed facilities in relation to all existing public and private facilities in plan view. The plans must clearly show the proposed locations of all above ground utility structures and include a detail view showing the height, width, and depth dimensions of each type of above ground utility structure (including any supporting pad) to be installed. If the project is a major project that is located within the central business district, crosses street intersections, or involves crossing proposed facilities over or under existing facilities, the plans must also include a representation of the vertical alignment of the facilities in profile view. Each sheet of the plans must have a note instructing the contractor to verify the location of underground utilities at least 100 feet in advance of all proposed utility crossings, and also at locations where the proposed facilities are shown to be running parallel to existing facilities within five feet. The plans must be half size (11" X 17") at a scale no smaller than 1" = 40' in plan view and 1" = 6' in profile view. Each project must be assigned a project number, which must appear on each sheet.

(7) A permit is required even if other authority has been granted by the director to make a pavement cut or excavation in a public right-of-way as part of a city construction project.

(8) The director shall state on the permit the activity for which the permit is issued and include any additional restrictions or requirements determined necessary by the director.

(9) The permittee has the exclusive responsibility to coordinate with other public service providers to protect all existing facilities in the public right-of-way in which the construction occurs.

(10) The permittee shall, as an express condition of the permit, comply in all respects with the requirements prescribed for the permitted activity in the Pavement Cut and Repair Standards Manual and with all other city ordinances and state or federal laws or regulations affecting the permitted activity.

(11) The director shall notify persons who ~~[public service providers that]~~ registered under Section 43-137 during the previous calendar year of pavement surfaces to be reconstructed or resurfaced by the city during the next calendar year.

(12) A person or public service provider planning construction within the public right-of-way shall notify the director by March 1 of each year of all then-known facility expansion or replacement projects planned for the next fiscal year that may require pavement cuts or excavations.

(13) The director may require any permittee to use trenchless technology or boring, instead of disturbing a public right-of-way surface, if it is:

- (A) in the best interest of the city;
- (B) technically, commercially, and economically feasible; and
- (C) not in violation of federal or state regulations or industry safety standards.

(14) Directional drilling or boring may not be used in the central business district, unless otherwise approved by the director as being in the best interest of the public health, safety, welfare, and convenience.

(15) In using trenchless technology or boring, whether or not required under Paragraph (13) of this subsection, the permittee must:

(A) obtain and have at the construction site recent plans from the city's water utilities department, and, where available, plans from owners of all other underground facilities, showing the horizontal and vertical placement of the underground facilities, if the permittee's proposed facilities will:

- (i) cross other existing facilities; or
- (ii) be located within five feet of existing facilities at any point;
- (B) locate all water main lines by potholing, if the permittee's proposed facilities will:

(i) cross other existing facilities; or

(ii) be located within five feet of existing facilities at any point;

and

(C) be able to locate the bore head at all times in accordance with the latest technologies and provide the location of the bore to the director upon request.

(16) The permittee shall maintain the construction area in a public right-of-way in a manner that avoids dust, other health hazards, and hazards to vehicular and pedestrian traffic until the public right-of-way is permanently repaired.

(17) When making a pavement cut or excavation, or placing spoils or excavated material in or along a public right-of-way, the permittee shall place barricades, warning signs, and warning lights at the location sufficient to warn the public of the hazard of the cut, excavation, spoils, or excavated material in compliance with the 1980 Edition of the Texas Manual on Uniform Traffic Control Devices, as amended, published by the Texas Department of Transportation.

(18) The director may require the permittee to share trench space to minimize the disruption of vehicular and pedestrian traffic or to provide space for needed city facility installations if such sharing is:

(A) technically, commercially, and economically feasible; and

(B) not in violation of state or federal regulations or industry safety standards.

(19) The permit application must include a traffic control plan that includes detailed drawings showing the proposed traffic controls for vehicular and pedestrian traffic for each phase of the proposed work in the public right-of-way.

SECTION 3. That Subsection (k) of Section 43-139, "Permit Required; Exceptions; Conditions; Denial and Revocation," of Article VIII, "Certain Uses of Public Right-of-Way," of Chapter 43, "Streets and Sidewalks," of the Dallas City Code is amended to read as follows:

"(k) Any variance from the requirements of this article must be approved in advance by the director. The director may grant a variance only if an extreme hardship exists and the public health, safety, welfare, and convenience is not adversely affected by granting the variance. The director may not approve any variance that would give a competitive advantage to one person ~~[public service provider]~~ over another person ~~[public service provider]~~ providing the same or similar service. The director may not grant a variance from the indemnity requirements of Section 43-140(d)."

SECTION 4. That Section 43-140.2, “Waiver of Bonding Requirements,” of Article VIII, “Certain Uses of Public Right-of-Way,” of Chapter 43, “Streets and Sidewalks,” of the Dallas City Code is amended to read as follows:

“SEC. 43-140.2. WAIVER OF BONDING REQUIREMENTS.

(a) A person registered under Section 43-137 ~~[public service provider]~~ may annually submit to the director a written request for a waiver from the requirement that it provide a performance bond, letter of credit, or cash deposit pursuant to Section 43-140.1.

(b) The waiver request must set forth in detail the basis for the request, including but not limited to:

(1) the person’s ~~[public service provider’s]~~ history of performance in completing its projects and complying with restoration obligations in the city’s rights-of-way; and

(2) documentation, in a form acceptable to the city, demonstrating that the person ~~[public service provider]~~ has unencumbered assets or reserves sufficient to cover the amount of the performance bond, letter of credit, or cash deposit that would otherwise be required under Section 43-140.1.

(c) Within 30 calendar days after receiving a written request for a waiver, the director may, for good cause shown, grant a waiver from the requirement that the person ~~[public service provider]~~ provide a performance bond, letter of credit, or cash deposit pursuant to Section 43-140.1. In making this decision, the director shall consider all of the following:

(1) The person’s ~~[public service provider’s]~~ record of performance in the city’s rights-of-way.

(2) The person’s ~~[public service provider’s]~~ record of compliance with this article.

(3) A showing of financial responsibility by the person ~~[public service provider]~~ sufficient to guarantee the full and faithful execution of the estimated work to be performed during the year in which the waiver is in effect.

(4) Any other factor relevant to a determination of the financial responsibility of the person ~~[public service provider]~~ and its ability to safely and fully perform permitted work.

(d) A waiver expires one year after being granted by the director, and the person ~~[public service provider]~~ must reapply for a waiver each year during which it will perform work in the city’s rights-of-way.

(e) Upon determining that a person ~~[public service provider]~~ is in violation of this article, the director may deny any request for a waiver and may terminate any existing waiver that had been granted under this section. A person ~~[public service provider]~~ whose waiver is terminated may not reapply for another waiver until two years have elapsed since the date of termination.

(f) If a waiver is denied or terminated by the director, the person ~~[public service provider]~~ shall immediately take all necessary steps to temporarily restore the right-of-way and then cease all work in the right-of-way until the person ~~[public service provider]~~ has provided a bond, letter of credit, or cash deposit that has been approved by the director.”

SECTION 5. That Subsection (f) “Tests,” of Section 43-141, “Miscellaneous Requirements for Street Excavation and Installations, Trench Safety, and Above Ground Utility Structures,” of Article VIII, “Certain Uses of Public Right-of-Way,” of Chapter 43, “Streets and Sidewalks,” of the Dallas City Code is amended to read as follows:

“(f) Tests.

(1) The permittee will be required to provide a certified construction materials testing lab, or use a testing method approved by the director, to perform the appropriate tests, at the permittee's expense, to ensure quality control for the backfill and pavement construction phases. Concrete strength test results must be submitted to the director for any placement greater than five cubic yards.

(2) Unless another method is approved by the director, tests must be made in accordance with the latest methods of the American Society of Testing and Materials. The results from tests for backfill compaction must be supplied to the city within three days of the backfill work completion and before pavement construction begins. The results from tests for pavement construction must be submitted within one week of completion of the project. Retesting after failure to pass the required tests will be at the expense of the permittee.

(3) Compaction testing is not required when a flowable ~~[type]~~ backfill material that complies with the *Pavement Cut and Repair Standards Manual*, as amended, is used ~~[and accepted]~~.

(4) If the materials used for the street repairs do not meet the minimum requirements of the *Pavement Cut and Repair Standards Manual*, they may be considered unacceptable and may be ordered to be removed and replaced at the permittee's expense. In cases where the repairs are unacceptable and the permittee refuses to make them acceptable, the work may be accomplished by the city, and all of the direct and indirect costs will be charged back to the permittee responsible for the work.

(5) The city at its expense may perform, or have performed, any material tests it deems necessary to verify conformance with the specifications set forth in Paragraph (6) of this subsection. If tests performed at the city's expense show cause for additional work or rework by the permittee, then further testing required to show conformance with the specifications will be at the expense of the permittee, including the cost of the original testing that showed the need for additional work or rework.

(6) Specifications for backfill compaction must meet the requirements contained in the *Pavement Cut and Repair Standards Manual*. Specifications for pavement testing must meet the requirements contained in the applicable provisions of the *Standard Specifications for Public Works Construction – North Central Texas* and the city's addendum thereto, as amended.”

SECTION 6. That Section 43-141, “Miscellaneous Requirements for Street Excavation and Installations, Trench Safety, and Above Ground Utility Structures,” of Article VIII, “Certain Uses of Public Right-of-Way,” of Chapter 43, “Streets and Sidewalks,” of the Dallas City Code is amended to add Subsection (h) to read as follows:

“(h) Signage.

(1) A copy of the approved permit must be displayed by the permittee at the worksite at all times during construction in the public right-of-way.

(2) Each vehicle and piece of equipment located in the permitted area of construction in the right-of-way must display a sign identifying the business name and primary contact information of the permittee. The sign letters and numbers must be legible and at least two inches in height.

(2) A copy of the approved traffic control plan required in Section 43-139 must be displayed at the permitted area of construction at all times when barriers are erected to divert or alter the flow of traffic.

(3) At least one sign labeled “Temporary Paving Repairs” must be displayed in accordance with the *Dallas Pavement Cut and Repair Standards Manual*, as amended, in any location that has temporary paving repairs. If temporary paving repairs exceed 50 feet in length, one “Temporary Paving Repairs” sign must be provided every 50 feet on the perimeter of the permitted area of public right-of-way under construction. Alternatively, a “Temporary Paving Repairs” sign may be written on the temporary paving repairs in accordance with this paragraph. The lettering of the written sign on the temporary paving repairs must be a minimum of three inches using only white paint. If temporary paving repairs exceed 40 feet in length, one painted “Temporary Paving Repairs” sign must be painted on the temporary paving repairs every 30 feet on the perimeter on the perimeter of the permitted area of public right-of-way under construction.”

SECTION 7. That Section 43-141, “Miscellaneous Requirements for Street Excavation and Installations, Trench Safety, and Above Ground Utility Structures,” of Article VIII, “Certain Uses of Public Right-Of-Way,” of Chapter 43, “Streets and Sidewalks,” of the Dallas City Code is amended to add Subsection (i) to read as follows:

“(i) Permittee notice requirements.

(1) After issuance of a permit under this article, the permittee shall provide written notice to the director:

(A) at least one business day before any material or equipment is placed in the permitted area or the commencement of any temporary construction;

(B) within one business day after completing the temporary construction; and

(C) at least one business day before any permanent construction begins.”

SECTION 8. That a person violating a provision of this ordinance, upon conviction, is punishable by a fine not to exceed \$500.

SECTION 9. That Chapter 43 of the Dallas City Code shall remain in full force and effect, save and except as amended by this ordinance.

SECTION 10. That the terms and provisions of this ordinance are severable and are governed by Section 1-4 of Chapter 1 of the Dallas City Code, as amended.

SECTION 11. That this ordinance shall take effect on April 1, 2016, and it is accordingly so ordained.

APPROVED AS TO FORM:

WARREN M.S. ERNST, City Attorney

By _____
Assistant City Attorney

Passed _____

AGENDA ITEM # 42

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 6

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 23 U

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for a Community service center, Child-care facility, and NO(A) Neighborhood Office District Uses on property zoned an NO(A) Neighborhood Office District and an MF-2(A) Multifamily District, at the southeast corner of Lombardy Lane and Geraldine Drive

Recommendation of Staff and CPC: Approval, subject to a development plan, landscape plan, and conditions

Z145-335(RB)

FILE NUMBER: Z145-335(RB)

DATE FILED: August 24, 2015

LOCATION: Southeast Corner of Lombardy Lane and Geraldine Drive

COUNCIL DISTRICT: 6

MAPSCO: 23 U

SIZE OF REQUEST: Approx. 1.578 Acres

CENSUS TRACT: 72.02

APPLICANT: Buckner Children and Family Services, Inc.

REPRESENTATIVES: Tommy Mann and Brad Williams

OWNERS: Miguel A. Trejo and Xochitl R. Trejo

REQUEST: An application for a Planned Development District for a Community service center, Child-care facility, and NO(A) Neighborhood Office District Uses on property zoned an NO(A) Neighborhood Office District and an MF-2(A) Multifamily District.

SUMMARY: The applicant proposes to remove the residential structures on a portion of the site, and develop the entire parcel with a community service center and child-care facility. A PDD is being requested for consideration of the following: 1) permit the community service center and child-care facility uses by right; 2) permit certain improvements to be located within a required front yard; 3) provide for an alternate parking requirement for a community service center; and, 4) permit an increase in structure height.

CPC RECOMMENDATION: Approval, subject to a development plan, landscape plan, and conditions.

STAFF RECOMMENDATION: Approval, subject to a development plan, landscape plan, and conditions.

GUIDING CRITERIA FOR RECOMMENDATION:

Staff recommends approval subject to the attached development plan, landscape plan, and conditions based upon:

1. *Performance impacts upon surrounding property* – As the proposed development will provide services for the adjacent area's residential uses, the attached recommended conditions will ensure no negative intrusion into this established residential area.
2. *Traffic impact* – A small increase in trip generation (considering approximately one acre is undeveloped and zoned for low scale office uses) is expected, however it is anticipated many of the residents utilizing the facility will arrive and depart by foot.
3. *Comprehensive Plan or Area Plan Conformance* – The request is in compliance with the designated Building Block for the area.
4. *Justification for a Planned Development District as opposed to straight zoning* – As the applicant wishes to establish a community serving use that will require rezoning for its delivery of community services (structure height, encroachments into a required setback not currently provided for in the development code), a PDD is required. As noted below, staff recommended design and landscape provisions will be required, in part, to address the increase in height and setback reductions.

Zoning History: There has been no recent zoning activity within the last five years in the immediate area that is relevant to this request.

Thoroughfare/Street

Existing & Proposed ROW

Lombardy Lane

Collector; 60' & 60' ROW

Geraldine Lane

Local; 50' ROW

Traffic: The Engineering Section of the Department of Sustainable Development and Construction has reviewed the requested PDD and determined that the proposed redevelopment will not significantly impact the street system.

STAFF ANALYSIS:

Comprehensive Plan: The request site is located in an area considered a Residential Neighborhood. This Building Block represents the life-blood of Dallas, the traditional neighborhood of single-family detached homes. Dallas has many neighborhoods that match this description, including Winnetka Heights, Preston Hollow, Lakewood and Wheatley Place. Single-family dwellings are the dominate land use in these areas. Some shops, restaurants or institutional land uses such as schools and religious centers that serve neighborhood residents may be located at the edges or at key intersections.

GOAL 1.2 Promote desired development.

Policy 1.2.1 Use Vision Building Blocks as a general guide for desired development patterns.

Land Use Compatibility: The request site consists of the westernmost one-third of Lombardy Lane's blockface, between Geraldine Lane and Lombardy Lane. Three single family structures are developed nearest to Geraldine Lane with the balance being undeveloped. The site possesses an approximate 14 foot of rise in elevation, from west to east.

A PDD is being requested for consideration of the following: 1) permit the community service center and child-care facility uses by right; 2) permit certain improvements to be located within a required front yard; 3) alternate off-street parking for a community service center; and, 4) permit an increase in structure height.

The area is dominated by low density residential uses (single family and multifamily structures). Calvary Hill Cemetery possesses approximately 40 acres to the northwest.

A planned development district is a marriage of uses and development standards that provides for flexibility of development that is sensitive to adjacent uses as well as preserving significant natural features. As there are not any natural features to address, staff has worked with the applicant to specifically address a well-designed, easily accessible community serving use. Additionally, a commitment to provide for significant permeable areas across the property has been worked through with the applicant.

As a result of this analysis, staff supports the request, subject to the attached plans and conditions.

Landscaping: The applicant has committed to establishing a site that possesses

significant planting areas across the site, inclusive of a courtyard (minimum of 1,500 square feet). As such, the attached landscape plan is the result of the collaborative effort between the applicant and the city arborist.

Design Standards: The main structure is very linear in design, and as it will serve as a community serving destination. In conjunction with an anticipated landscape vision, staff has recommended certain design criteria be incorporated, inclusive of façade treatments, prominent entryway, and as mentioned in the landscape section, a permeable area accessible to visitors to the property.

Off-Street Parking: The applicant has stated all uses, excluding the community service center will park per the use designations in the Dallas Development Code. The applicant has proposed an alternate off-street parking ratio for the community service center (1 space per 235 square feet in lieu of 1 space per 200 square feet). The requested parking demand study has determined that roughly 75 percent of attendees will arrive by foot as the adjacent residential area (single family and multifamily development) is within walking distance. Staff has reviewed the study and supports its analysis and methodology.

CPC ACTION

(December 17, 2015)

Motion: It was moved to recommend **approval** of a Planned Development District for a Community service center, Child-care facility, and NO(A) Neighborhood Office District Uses, subject to a revised development plan, revised landscape plan and revised conditions to include the following: 1) required off-street parking for community service center is one space for each 235 sq. ft. of floor area, 2) minimum 8 foot clearance of encroachment to the side yard, and 3) include court yard gate height to development and landscape plans on property zoned an NO(A) Neighborhood Office District and an MF-2(A) Multifamily District, at the southeast corner of Lombardy Lane and Geraldine Drive.

Maker: Anantasomboon

Second: Shidid

Result: Carried: 15 to 0

For: 15 - Anglin, Emmons, Houston, Davis, Shidid,
Anantasomboon, Abtahi, Haney, Jung,
Housewright, Schultz, Peadon, Murphy, Ridley,
Tarpley

Against: 0

Absent: 0

Vacancy: 0

Notices: Area: 500

Mailed: 72

Replies: For: 4

Against: 1

Speakers: For: Margaret McKissack, 700 N. Pearl St., Dallas, TX, 75201

Tommy Mann, 500 Winstead Bldg., Dallas, TX

Evan Beattie, 2808 Fairmount St., Dallas, TX, 75201

Roxan Staff, 2707 Northwest Hwy., Dallas, TX, 75220

For (Did not speak): Joe Carreon, 3150 Kendale Dr., Dallas, TX, 75220

Against: None

List of Officers and Directors

Buckner Children and Family Services, Inc..

Officers:

Albert L. Reyes, President
Charlie Wilson, Senior Vice President
Tony Lintelman, Treasurer
Jack David, Secretary

Directors:

Billy R. Allen
Barry Pryor
David C. Hennessee
Lynette Guy Ranton
Sue Courts
Scott McIlveene
Ellis Orozco
Carol C. Brian
Rodney Henry
Duke Presley
George S. Vorpahl
Rebeca L. Brokenbek
Lee E. Bush
Nell McCallum Morris
Cassandra Harris
Mary Barnes
J. Daniel Ellis
Ann Graves
Steve M. King
Henry G. Will
David E. Wulf
Watson Moore
Kay Struzick
Susan Sosebee

**CPC RECOMMENDED CONDITIONS FOR A PLANNED DEVELOPMENT
DISTRICT**

SEC. 51P ____ .101. LEGISLATIVE HISTORY.

PD _ was established by Ordinance No. __, passed by the Dallas City Council on _.

SEC. 51P ____ .102. PROPERTY LOCATION AND SIZE.

PD ____ is established on property generally located at the southeast corner of Lombardy Lane and Geraldine Drive. The size of PD ____ is approximately 1.578 acres.

SEC. 51P- ____ .103. DEFINITIONS AND INTERPRETATIONS.

(a) Unless otherwise stated, the definitions and interpretations in Chapter 51A apply to this article.

(b) Unless otherwise stated, all references to articles, divisions, or sections in this article are to articles, divisions, or sections in Chapter 51A.

(c) This district is considered to be a nonresidential zoning district.

SEC. 51P- ____ .104. EXHIBITS.

The following exhibits are incorporated into this article:

- (1) Exhibit __A: development plan.
- (2) Exhibit __B: landscape plan.

SEC. 51P- ____ .105. DEVELOPMENT PLAN.

Development and use of the Property must comply with the development plan (Exhibit A). If there is a conflict between the text of this article and the development plan, the text of this article controls.

SEC. 51P- ____ .106. MAIN USES PERMITTED.

(a) Except as provided in this section, the only main uses permitted are those main uses permitted in the NO(A) Neighborhood Office District, subject to the same conditions applicable in the NO(A) Neighborhood Office District, as set out in Chapter

51A. For example, a use permitted in the NO(A) Neighborhood Office District only by specific use permit (SUP) is permitted in this district only by SUP; a use subject to development impact review (DIR) in the NO(A) Neighborhood Office District is subject to DIR in this district, etc.

(b) The following uses are permitted by right:

- Child-care facility.
- Community service center.

SEC. 51P- _____.107. ACCESSORY USES.

As a general rule, an accessory use is permitted in any district in which the main use is permitted. Some specific accessory uses, however, due to their unique nature, are subject to additional regulations contained in Section 51A-4.217. For more information regarding accessory uses, consult Section 51A-4.217.

SEC. 51P- _____.108. YARD, LOT, AND SPACE REGULATIONS.

(Note: The yard, lot, and space regulations in this section must be read together with the yard, lot, and space regulations in Division 51A-4.400. If there is a conflict between this section and Division 51A-4.400, this section controls).

(a) In general. Except as provided in this section, the yard, lot, and space regulations for the NO(A) Neighborhood Office District apply.

(b) Front yard.

(1) Retaining walls a maximum of four feet in height may encroach into a required front yard. Railings, a maximum of four feet in height and a minimum of 50 percent open, when affixed to a retaining wall are not considered as part of the retaining wall height as provided in this subparagraph.

(2) The following may be located in the required front yard.

(A) railings for stairs, stoops, and porches with a maximum height of four feet, and

(B) patios and covered seating areas.

(3) Any canopy or awning in the required front yard must have at least a minimum clearance of eight feet from grade.

(c) Side yard.

(1) Minimum side yard is ten feet.

(2) Any awning or roof eave in the required side yard must have at least a minimum clearance of eight feet from grade.

(d) Floor area. For a child-care facility, maximum floor area is 4,000 square feet.

(e) Height.

(1) Except as provided in this subsection, maximum structure height for occupied floor area is 40 feet. Height is measured to the highest point of the structure.

(2) The following structures may project to a height not to exceed 12 feet above the maximum height in this section.

- (A) Elevator penthouse or bulkhead.
 - (B) Mechanical equipment room.
 - (C) Cooling tower.
 - (D) Tank designed to hold liquids.
 - (E) Ornamental cupola or dome.
 - (F) Skylights.
 - (G) Clerestory.
 - (H) Visual screens which surround roof mounted mechanical equipment.
 - (I) Chimney and vent stacks.
 - (J) Parapet wall, limited to a height of four feet.
- (3) Maximum height for light standards is 16 feet, measured to the top of the fixture.

SEC. 51P- _____.109.

OFF-STREET PARKING AND LOADING.

(a) In general. Except as provided in this section, consult the use regulations in Division 51A-4.200 for the specific off-street parking and loading requirements for each use.

(b) Community service center. One space for each 235 square feet of floor area.

SEC. 51P- _____.110. ENVIRONMENTAL PERFORMANCE STANDARDS.

See Article VI.

SEC. 51P- _____.111. LANDSCAPING.

(a) In general. Except as provided in this section, landscaping must be provided in accordance with Article X.

(b) Community service center and child-care facility. Landscaping must be provided as shown on the landscape plan (Exhibit ____B). If there is a conflict between the text of this article and the landscape plan, the text of this article controls.

(c) Maintenance. Plant materials must be maintained in a healthy, growing condition.

SEC. 51P- _____.112. SIGNS.

Signs must comply with the provisions for non-business zoning districts in Article VII.

SEC. 51P- _____.113. DESIGN STANDARDS FOR A COMMUNITY SERVICE CENTER AND CHILD-CARE FACILITY.

(a) Building facades.

(1) A building façade facing a public street right-of-way must contain a minimum of 20 percent fenestration.

(2) Each façade must have one or more of the following changes:

(A) A minimum of two changes of color, texture, or material, either diagonally, horizontally, or vertically, at intervals of not less than 10 feet and not more than 30 feet.

(B) Changes in plane with a depth of at least 12 inches, either diagonally, horizontally, or vertically, for each 50 feet of street facing facade.

(b) Building entrances. A minimum of one pedestrian entryway on Lombardy Lane and one pedestrian entryway on Geraldine Drive must be a visually prominent entrance. An example of a visually prominent entrance is an archway, canopy affixed to the façade, or building materials for the pedestrian entryway that are visually constructed differently than those materials utilized on each respective façade.

(c) Pedestrian amenities. A minimum of two of each of the following pedestrian amenities must be provided along Lombardy Lane and a minimum of one of each of the following must be provided along Geraldine Drive:

- (1) benches,
- (2) trash receptacles, and
- (3) bicycle racks.

(d) Courtyard. A courtyard with a minimum of 1,500 square feet of land area must be provided in the location shown on the development plan.

(e) Sidewalks. Sidewalks must be constructed to provide for an unobstructed sidewalk width of six feet along Lombardy Lane and Geraldine Drive.

SEC. 51P- _____.114. FENCES.

A person shall not erect or maintain a fence in a required front yard more than four feet above grade.

SEC. 51P- _____.115. ADDITIONAL PROVISIONS.

(a) The Property must be properly maintained in a state of good repair and neat appearance.

(b) Development and use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the city.

SEC. 51P- _____.116. COMPLIANCE WITH CONDITIONS.

(a) All paved areas, permanent drives, streets, and drainage structures, if any, must be constructed in accordance with standard city specifications, and completed to the satisfaction of the director of public works and transportation.

(b) The building official shall not issue a building permit to authorize work, or a certificate of occupancy to authorize the operation of a use, in this district until there has been full compliance with this article, the Dallas Development Code, the construction codes, and all other ordinances, rules, and regulations of the city.

DEVELOPMENT PLAN
 Buckner International - Buckman Lake
 Dallas, Texas

June 17, 2014
 Preliminary Buckman Lake Development Plan
 Buckman Lake, TX
 Draft for EIR, R/W



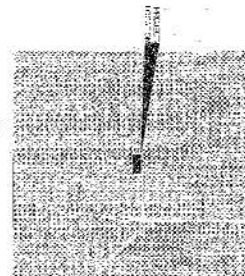
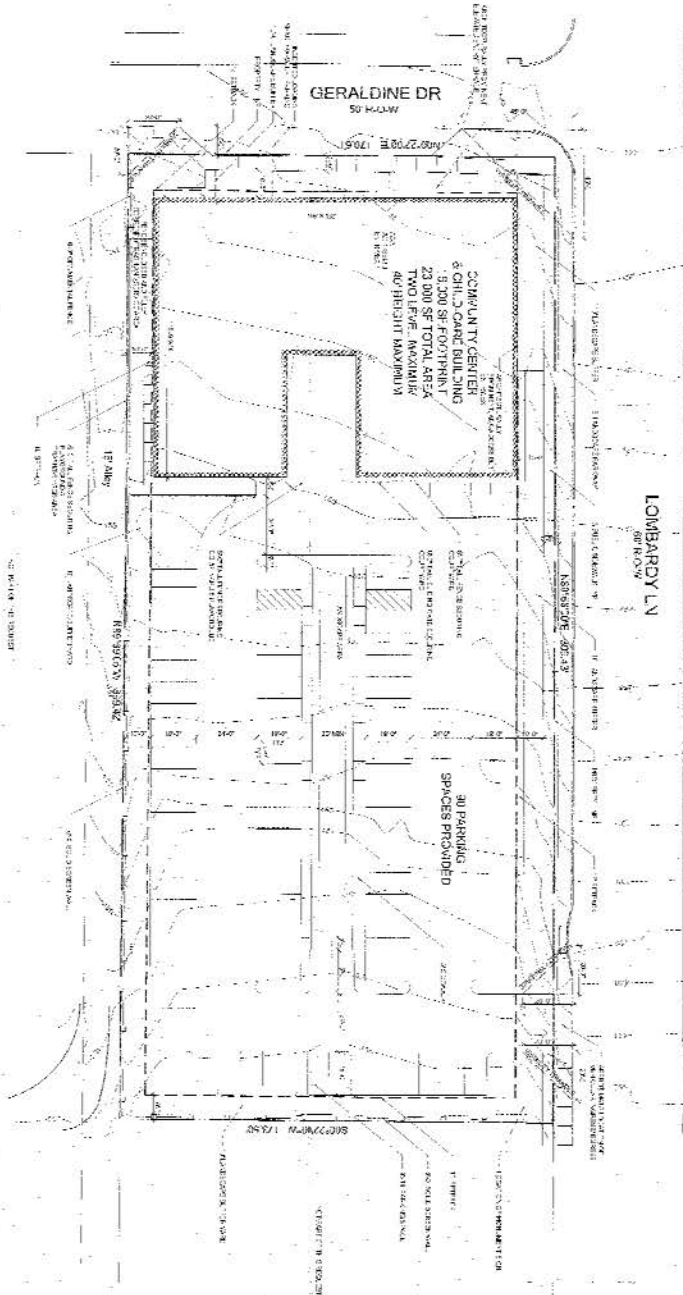
0 20 40 80 Feet



2500 PINEHURST BLVD., SUITE 300
 DALLAS, TEXAS 75227 214-221-3466

3800 W. WYATT BLVD., SUITE 100
 FORT WORTH, TEXAS 76107 817-330-1200

Z145-335



VAR. LOT 4 SPACE DATA

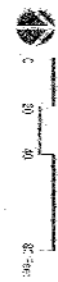
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LOT AREA	12,340
FAA	10,000
MAXIMUM	61
MINIMUM	2
MAXIMUM	10
MINIMUM	10

PROJECTIONS

PLAN AREA	30,000
LOT AREA	30,000
FAA	10,000
MAXIMUM	61
MINIMUM	2
MAXIMUM	10
MINIMUM	10

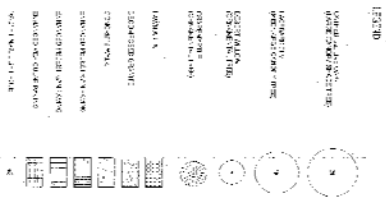
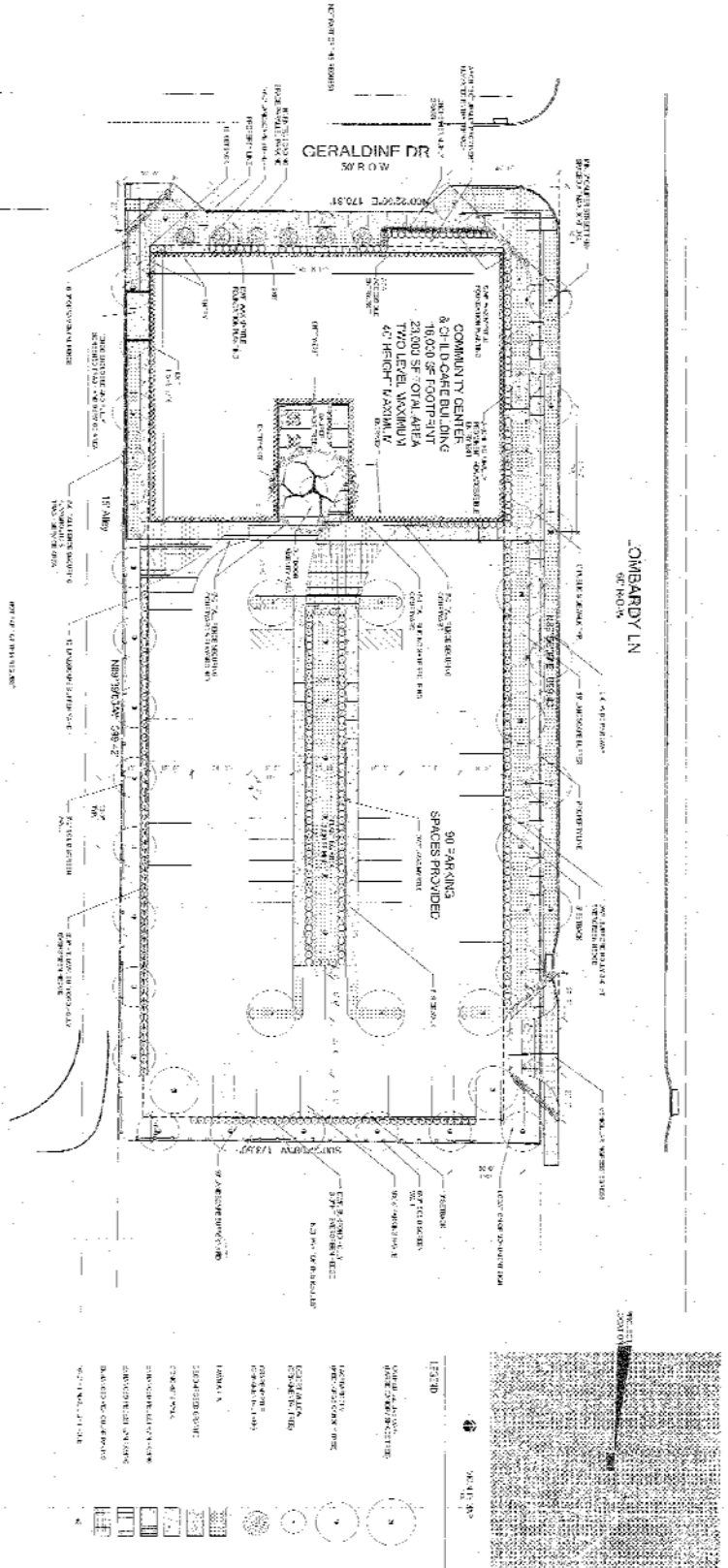
LANDSCAPE PLAN
 Building located on Barton Lake
 Date: 08/08

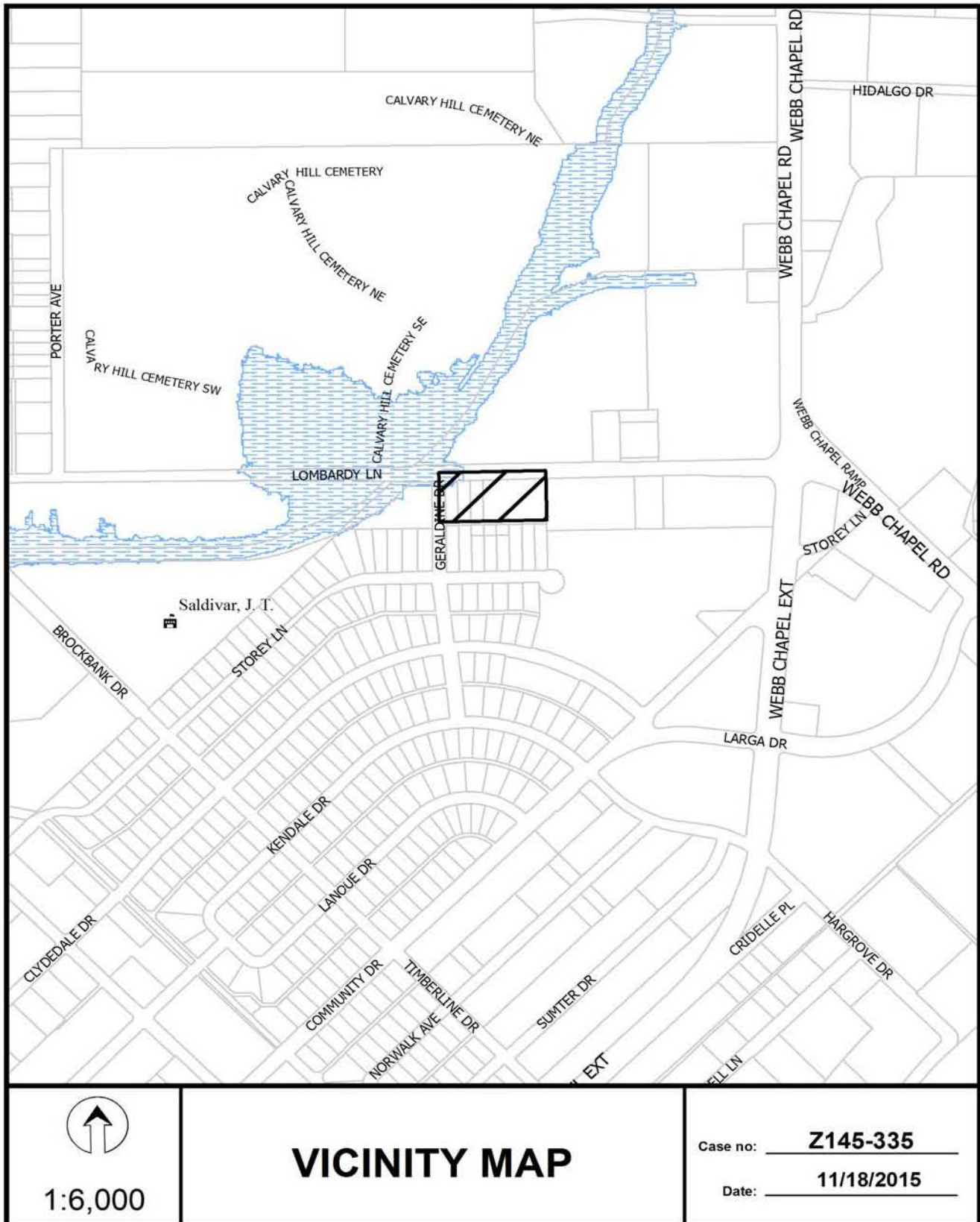
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 2008
 11/11/2008

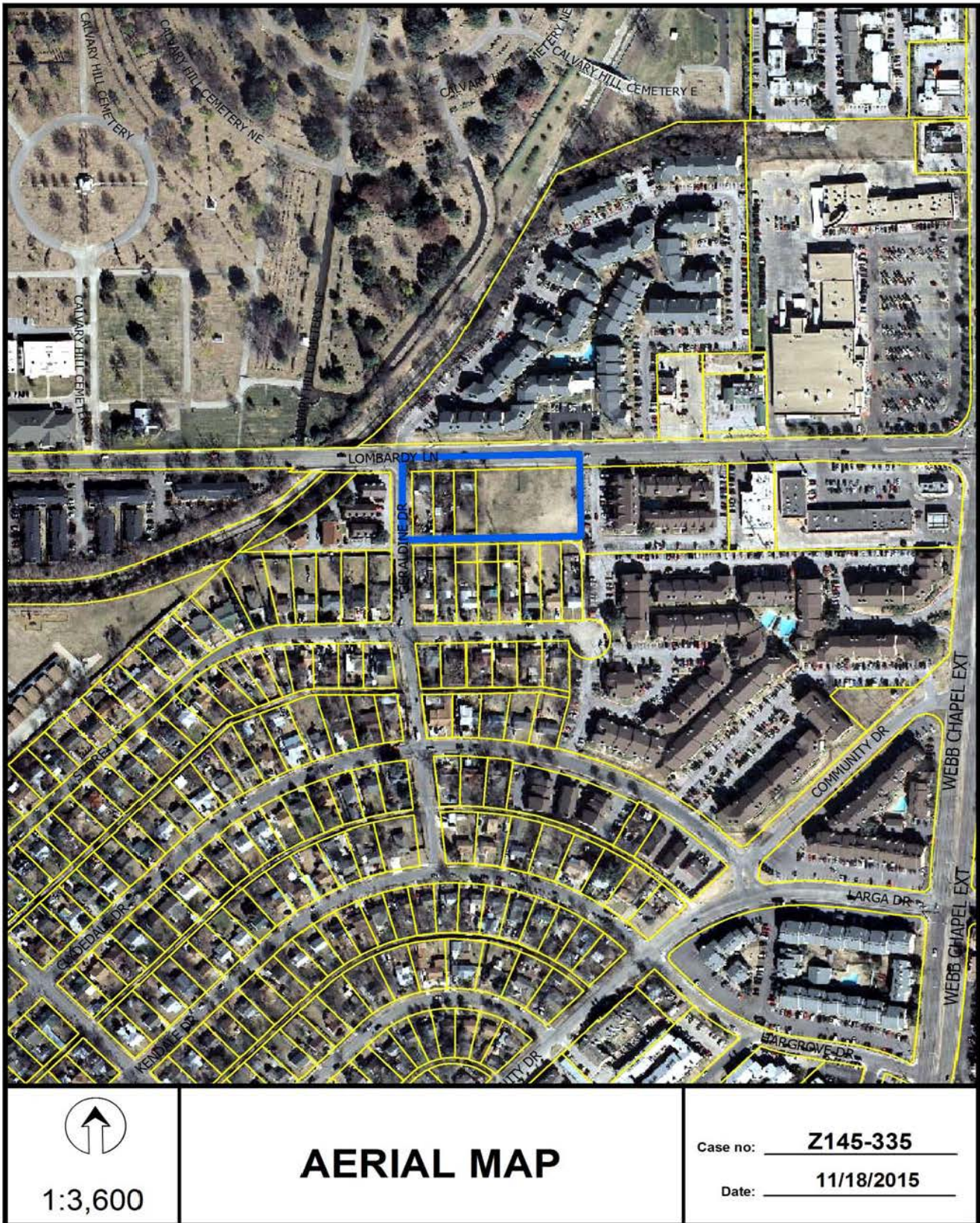


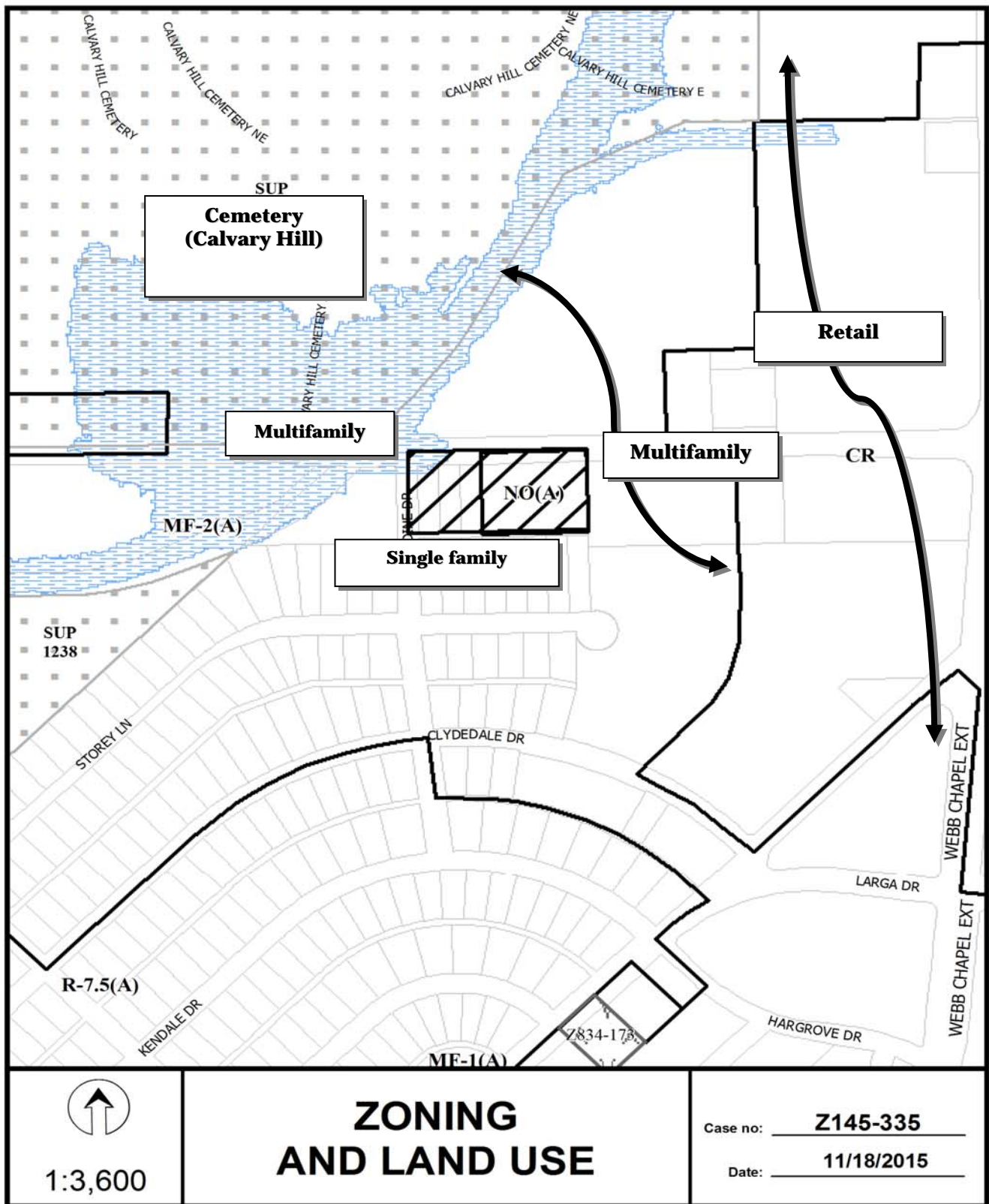
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Z 145-335

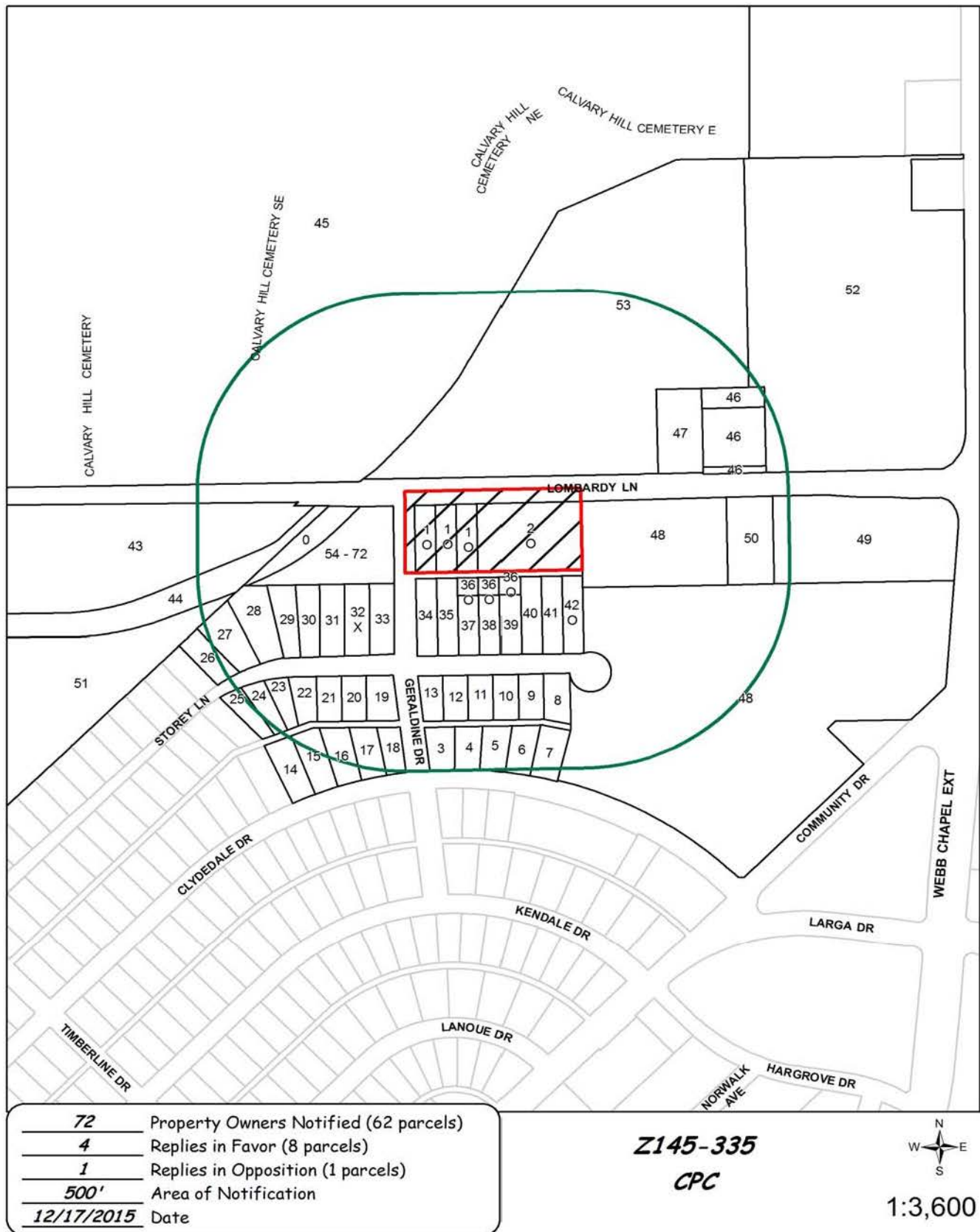








CPC RESPONSES



12/16/2015

Reply List of Property Owners***Z145-335******72 Property Owners Notified******4 Property Owners in Favor******1 Property Owners Opposed***

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
O	1	3310 LOMBARDY LN	TREJO MIGUEL A &
O	2	3314 LOMBARDY LN	TREJO MIGUEL A & XOCHITL
	3	3303 CLYDEDALE DR	ARROYO SILVIA
	4	3307 CLYDEDALE DR	JAIMES IGNACIO & GENOVEVA
	5	3313 CLYDEDALE DR	PEREA ALFONSO & KARIA
	6	3317 CLYDEDALE DR	KRL MANAGEMENT LLC
	7	3323 CLYDEDALE DR	RAMIREZ RUBEN C
	8	3326 STOREY LN	ESPARZA JESUS & MARIA
	9	3320 STOREY LN	ZAINOS RUBEN &
	10	3316 STOREY LN	ALFARO CRECENCIO &
	11	3310 STOREY LN	BENAVIDES CANDIDA C
	12	3306 STOREY LN	SALAZAR ROSA IRENE
	13	3302 STOREY LN	MARTINEZ JOSE &
	14	3207 CLYDEDALE DR	PEREZ JOSE G & MARIA E
	15	3213 CLYDEDALE DR	CARRERA AUDON & HORTENCIA
	16	3217 CLYDEDALE DR	AGUINAGA ANNA MARIE &
	17	3221 CLYDEDALE DR	FLORES FELIPE &
	18	3225 CLYDEDALE DR	RAMIREZ JOSE F
	19	3238 STOREY LN	VENTURA EUGENIA
	20	3234 STOREY LN	PICHARDO JOSE A
	21	3230 STOREY LN	MALDONADO GUILLERMINA
	22	3226 STOREY LN	DURAN ARTURO E
	23	3222 STOREY LN	VELOZ CLAUDIO &
	24	3218 STOREY LN	GUTIERREZ IGNACIO &
	25	3212 STOREY LN	SANCHEZ MELVIN F
	26	3213 STOREY LN	ROQUE FRANCISCO &

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	27	3219 STOREY LN	LEON ALBERTO E &
	28	3223 STOREY LN	NUNEZ JOSE & LEOBIGILDA M
	29	3227 STOREY LN	DANIELS DELALUZ MARIA
	30	3231 STOREY LN	MARTINEZ MANUEL
	31	3235 STOREY LN	GONZALEZ JUAN ANTONIO
X	32	3239 STOREY LN	LOVO MARIA A
	33	3243 STOREY LN	MURILLO ENRIQUE &
	34	3303 STOREY LN	DAVALOS JOSE GAUDALUPE &
	35	3307 STOREY LN	ALEJO ANUAR
O	36	3311 STOREY LN	TREJO MIGUEL & XOCHITL
	37	3311 STOREY LN	HERNANDEZ JUAN M &
	38	3315 STOREY LN	VELAZQUEZ PEDRO & JUANA
	39	3319 STOREY LN	RODRIGUEZ PEDRO & SANDRA
	40	3323 STOREY LN	GARCIA LUIS B & MARIA
	41	3327 STOREY LN	GAMEZ FLORENTINO &
O	42	3331 STOREY LN	TREJO XOCHITL R
	43	3130 LOMBARDY LN	GR CANYON CREEK LP
	44	3130 LOMBARDY LN	PARKS JOHN A JR
	45	3235 LOMBARDY LN	CALVARY HILL CEMETERY
	46	3383 LOMBARDY LN	JUAREZ MARIO
	47	3373 LOMBARDY LN	TORRES ANTONIO
	48	3350 LOMBARDY LN	VREC PECAN LP
	49	3400 LOMBARDY LN	PECAN PLAZA LTD
	50	3380 LOMBARDY LN	ROADE PROPERTIES LTD
	51	9510 BROCKBANK DR	Dallas ISD
	52	9727 WEBB CHAPEL RD	WEBBS CHAPEL DEV GRP LLC
	53	3353 LOMBARDY LN	VREC BAYOU LP
	54	3240 LOMBARDY LN	BURNETT JOE W
	55	3240 LOMBARDY LN	ARELLANO BERNARDO ET AL
	56	3240 LOMBARDY LN	TRAN TU & CAM LY
	57	3240 LOMBARDY LN	SOTO FILIBERTO &

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
58	3240	LOMBARDY LN	ANZORA NOE O
59	3240	LOMBARDY LN	LY DIEP & CAM KHA LY
60	3240	LOMBARDY LN	LY VAN &
61	3240	LOMBARDY LN	VU LINH MY
62	3240	LOMBARDY LN	MAZARIEGOS WALTER &
63	3240	LOMBARDY LN	LY DIEP & CAM LY
64	3240	LOMBARDY LN	VILLEDA FRANCISCO J ORTIZ
65	3240	LOMBARDY LN	GOMEZ CECILIO & MARIA C
66	3240	LOMBARDY LN	GOMEZ CECILIO
67	3240	LOMBARDY LN	HARKLEROAD DONNA L
68	3240	LOMBARDY LN	TREJO XOCHITL
69	3240	LOMBARDY LN	PELLECER GENSSER
70	3240	LOMBARDY LN	DURAN ARTURO H
71	3240	LOMBARDY LN	MARTINEZ ENRIQUE VASQUEZ
72	3240	LOMBARDY LN	DURAN ARTURO H &

AGENDA ITEM # 43

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 6

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 22 H

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a D-1 Liquor Control Overlay and an ordinance granting a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the southeast corner of Royal Lane and North Stemmons Freeway

Recommendation of Staff and CPC: Approval of a D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and conditions
Z145-344(OTH)

FILE NUMBER: Z145-344(OTH)

DATE FILED: September 1, 2015

LOCATION: Southeast corner of Royal Lane and North Stemmons Freeway

COUNCIL DISTRICT: 6

MAPSCO: 22H

SIZE OF REQUEST: Approx. 0.4695 acres

CENSUS TRACT: 99.00

APPLICANT/ OWNER: HSY INC.

REPRESENTATIVE: Santos Martinez, Masterplan

REQUEST: An application for a D-1 Liquor Control Overlay and a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay.

SUMMARY: The applicant proposes to sell alcoholic beverages for off-premise consumption in conjunction with a general merchandise or food store 3,500 square foot or less. The applicant proposes to demolish the existing structure and to build a new 2,400 square foot building on the property.

CPC RECOMMENDATION: Approval of the D-1 Overlay; and approval of a Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and conditions.

STAFF RECOMMENDATION: Approval of the D-1 Overlay; and approval of a Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and conditions.

GUIDING CRITERIA FOR RECOMMENDATION:

The following factors are listed in Chapter 51A of the Dallas Development Code to guide the determination as to whether or not an SUP shall be granted. Staff has listed its findings based upon each component below:

1. *Compatibility with surrounding uses and community facilities* – The sale of alcoholic beverages in conjunction with other retail sales will not affect compatibility with surrounded uses.
2. *Contribution to, enhancement, or promoting the welfare of the area of request and adjacent properties* – The proposed use neither contributes to nor deters from the welfare of the area.
3. *Not a detriment to the public health, safety, or general welfare* – The proposed use does not compromise the public health, safety, or general welfare of the public.
4. *Conforms in all other respects to all applicable zoning regulations and standards* – The request complies with all zoning regulations and standards. No variances or special exceptions have been requested.

Zoning History: There have been three zoning changes requested in the area within the last five years.

1. **Z101-198** On August 10, 2011, the City Council approved a D-1 Liquor Control Overlay and Specific Use Permit No. 1853 for the sale of alcoholic beverages in conjunction with a general merchandise or food store use 3,500 square feet or less on property located at the northeast corner of Royal Lane and I-35/N. Stemmons Freeway.

Thoroughfares/Streets:

Thoroughfare/Street	Type	Existing ROW
Royal Lane	Principal Arterial	100 Feet
Stemmons Freeway	Freeway	Variable

Traffic: The Engineering Section of the Department of Sustainable Development and Construction has reviewed the request and determined that it will not impact the surrounding street system.

STAFF ANALYSIS:

Comprehensive Plan: The Comprehensive Plan does not make a specific land use recommendation related to the request; however, the *forwardDallas!* Vision Illustration is comprised of a series of Building Blocks that shows general land use patterns. They are generalized patterns without well-defined boundaries that indicate where certain types and densities of development might logically occur. The comprehensive plan classifies this area as Business Center or Corridor Building Block.

The Business Center or Corridor Building Block represents major employment or shopping destinations outside of Downtown. Business Centers are usually at major intersections or along highways or major arterials and rely heavily on car access. These areas typically include high-rise office towers and low- to mid-rise residential buildings for condos or apartments and may include multifamily housing. Land uses are typically separated from one another by parking lots, freeways or arterial roads. Streets in these areas emphasize efficient movement of vehicles. Bold lighting and linear landscaping features such as esplanades and tree-lined boulevards can all work to distinguish and identify these areas. Public spaces may be at key entry points and central locations. Gateway landscaping, monuments and other devices will provide visibility from the freeway and guide visitors to destinations. Public transit may play a role in these areas and help create some pockets of transit oriented development. Business Centers and Corridors provide important concentrations of employment within Dallas that compete with suburban areas.

Surrounding Land Uses:

	Zoning	Land Use
Site	RR	Vacant structure
North	RR with a D and D-1, SUP No. 1863	Retail, SUP for the sale of alcohol
East	PD No. 498	Retail, vehicle service center
South	IR	Warehouse
West	CR and IR	Freeway and retail uses

Land Use Compatibility:

The 0.4695 acre site is currently developed with a one-story, vacant building with a drive through. The building was formerly used for a restaurant with drive-in or drive through service. The applicant's project includes the demolition of the existing building,

construction of a new 2,400 square foot structure for a general merchandise store 3,500 square feet or less and a motor vehicle fueling station, which are both allowed by right. The motor vehicle fueling station will have six fuel pumps. The applicant is requesting the SUP for the sale of alcoholic beverages in conjunction with the general merchandise of food store, which requires a Specific Use Permit.

The request site is adjacent to a retail store to the east, and a warehouse to the south. Surrounding uses are motor vehicle fueling stations, motor vehicle related uses, restaurants and other retail related uses.

The general provisions for a Specific Use Permit in Section 51A-4.219 of the Dallas Development Code specifically state: (1) The SUP provides a means for developing certain uses in a manner in which the specific use will be consistent with the character of the neighborhood; (2) Each SUP application must be evaluated as to its probable effect on the adjacent property and the community welfare and may be approved or denied as the findings indicate appropriate; (3) The city council shall not grant an SUP for a use except upon a finding that the use will: (A) complement or be compatible with the surrounding uses and community facilities; (B) contribute to, enhance, or promote the welfare of the area of request and adjacent properties; (C) not be detrimental to the public health, safety, or general welfare; and (D) conform in all other respects to all applicable zoning regulations and standards. The request does not appear to have an adverse impact on the surrounding zoning and land uses.

Staff has reviewed the applicant's request and supports the Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to conditions.

Development Standards:

<u>DISTRICT</u>	<u>SETBACKS</u>		Density	Height	Lot Coverage	Special Standards	PRIMARY Uses
	Front	Side/Rear					
RR Regional retail	15'	20' adjacent to residential OTHER: No Min.	1.5 FAR overall 0.5 office	70' 5 stories	80%	Proximity Slope U-form setback Visual Intrusion	Retail & personal service, office

Parking:

The Dallas Development Code requires off-street parking to be provided for a general merchandise use at one space for each 200 square feet of floor area, and two spaces for a motor vehicle fueling station. Therefore, the parking requirements for the proposed 2,400 square foot general merchandise use are 12 spaces; and two spaces for the motor vehicle fueling station. The total required parking spaces is 14. The attached site plan depicts a total of 14 parking spaces.

Landscaping:

The proposed development is required to provide landscaping in accordance with Article X of the Dallas Development Code. Some of the mandatory requirements are site and street trees.

Police Records:

The following list is the police records for the last five years for the previous use of the property, an office use. The property was previously utilized as an office [Texas Car Title Loans and Pay Day Services], and as a restaurant without drive-through before the office use. Staff found no records of the restaurant name.

Police Records
November 23, 2010- November 23, 2015

Offense	Date	UCR_ Offense	Address	Premise
ASSAULT M/A	8/28/2009	ASSAULT	02450 ROYAL LN	910 - PUBLIC STREET
ABANDON PROPERTY	10/10/2009	FOUND	02450 ROYAL LN	PUBLIC STREET
CRIMINAL MISCHIEF	11/29/2009	VANDALISM & CRIMINAL MISCHIEF	02450 ROYAL LN	134 - CASH CHECK
SUSPICIOUS PERSON	12/30/2013	Not Coded	02450 ROYAL LN	TITLE LOANS

CPC ACTION – December 3, 2015

Motion: It was moved to recommend **approval** of a D-1 Liquor Control Overlay and **approval** of a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and revised conditions on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the southeast corner of Royal Lane and North Stemmons Freeway.

Maker: Houston
Second: Abtahi
Result: Carried: 14 to 0

For: 14 - Anglin, Emmons, Houston, Davis, Shidid,
Abtahi, Haney, Jung, Housewright, Schultz,
Peadon, Murphy, Ridley, Tarpley

Against: 0
Absent: 1 - Anantasomboon
Vacancy: 0

Notices: Area: 200 Mailed: 9
Replies: For: 1 Against: 0
Speakers: None

Z145-344(OTH)

LIST OF PARTNERS

HSY INC.

Kim Ok Yang

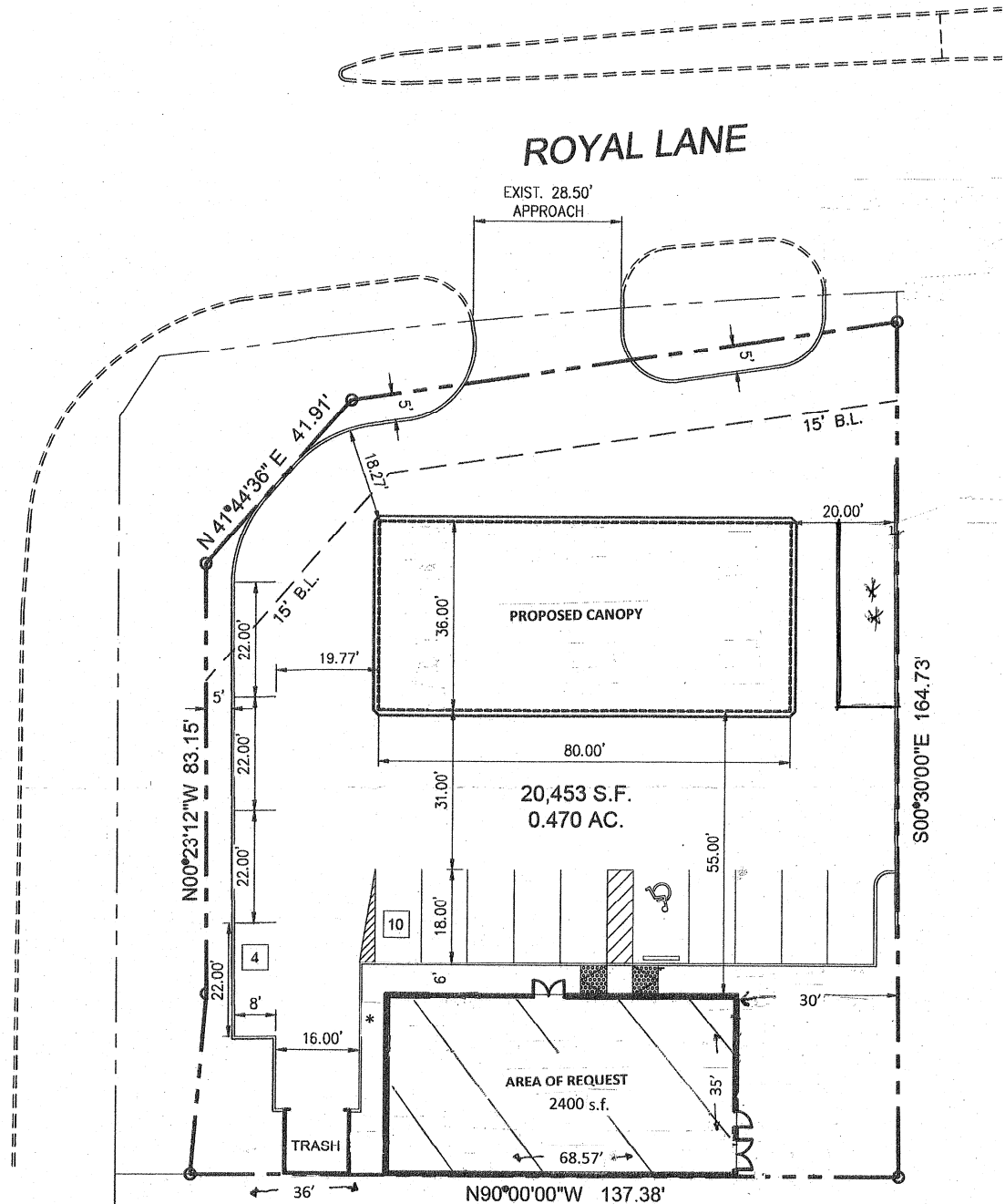
President

Proposed SUP Conditions

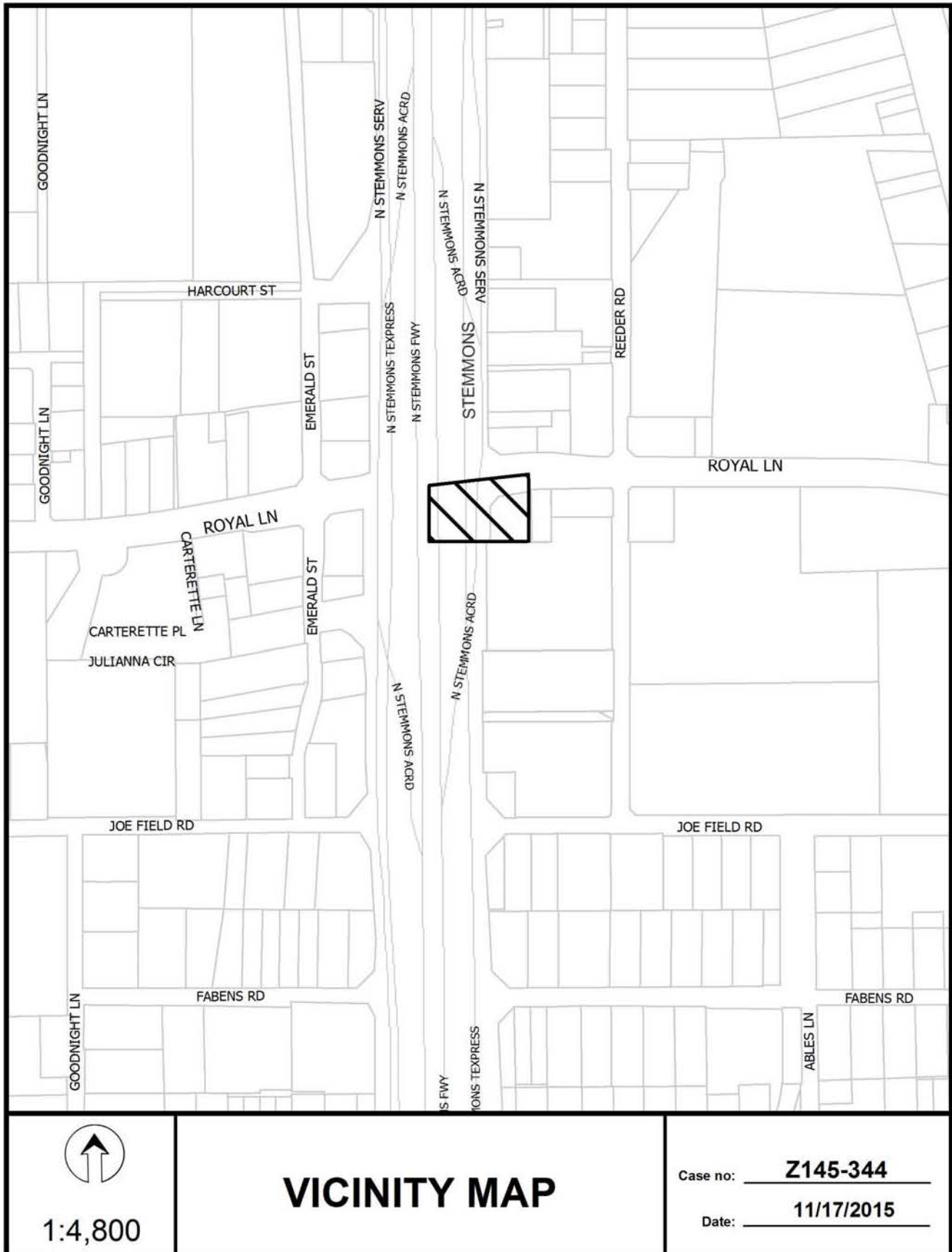
1. USE: The only use authorized by this specific use permit is the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less.
2. SITE PLAN: Use and development of the Property must comply with the attached site plan.
3. TIME LIMIT: This specific use permit expires on_____, (two-year period from the passage of this ordinance, but is eligible for automatic renewals for additional five-year periods, pursuant to Section 51A-4.219 of Chapter 51A of the Dallas City Code, as amended. For automatic renewal to occur, the Property owner must file a complete application for automatic renewal with the director before the expiration of the current period. Failure to timely file a complete application will render this specific use permit ineligible for automatic renewal. (Note: The Code currently provides that applications for automatic renewal must be filed after the 180th but before the 120th day before the expiration of the current specific use permit period. The Property owner is responsible for checking the Code for possible revisions to this provision. The deadline for applications for automatic renewal is strictly enforced).
4. MAINTENANCE: The entire Property must be properly maintained in a state of good repair and neat appearance.
5. GENERAL REQUIREMENTS: Use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the City of Dallas

Proposed Site Plan

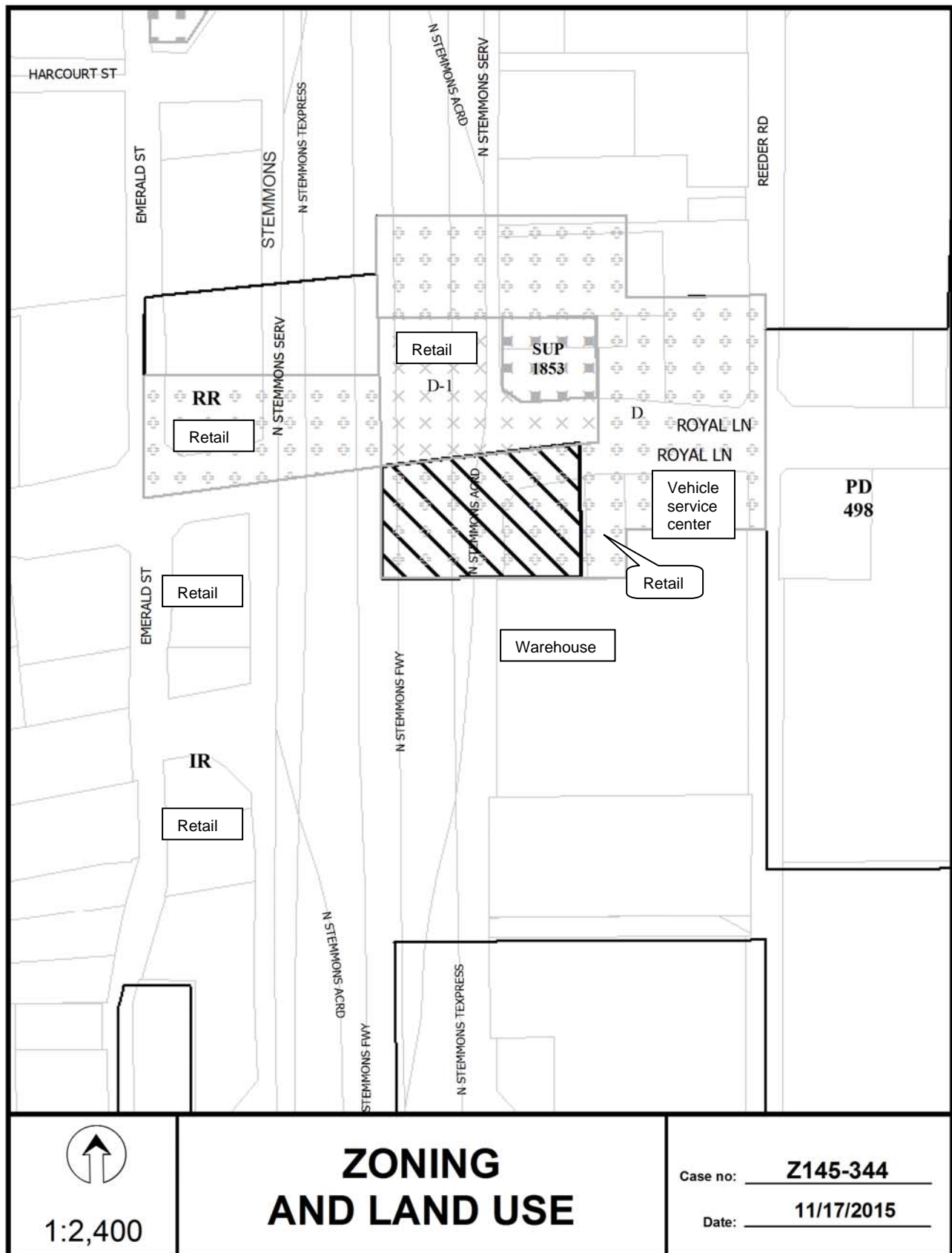
I.H. 35 STEMMONS FREEWAY
(FRONTAGE ROAD)

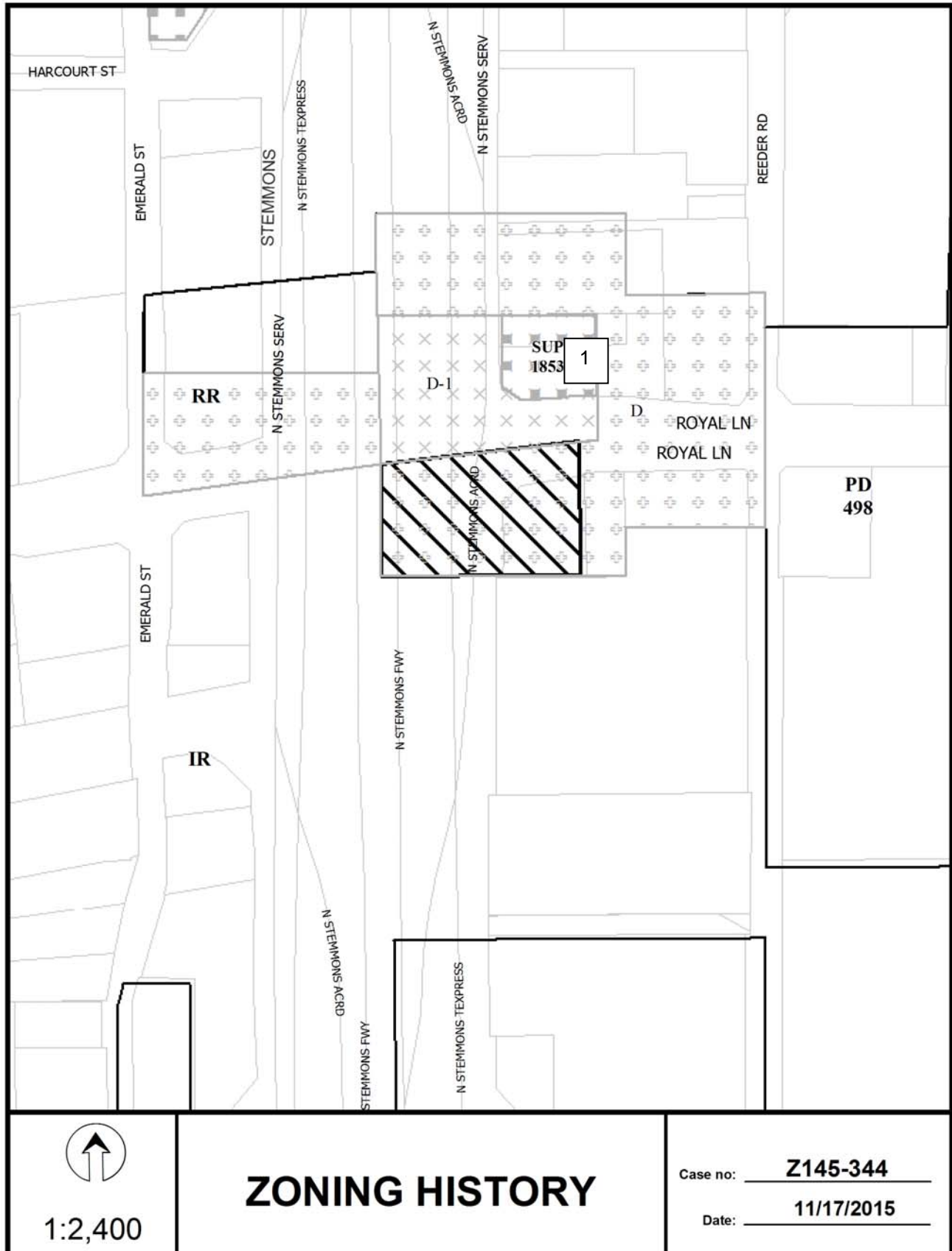


Z145-344(OTH)

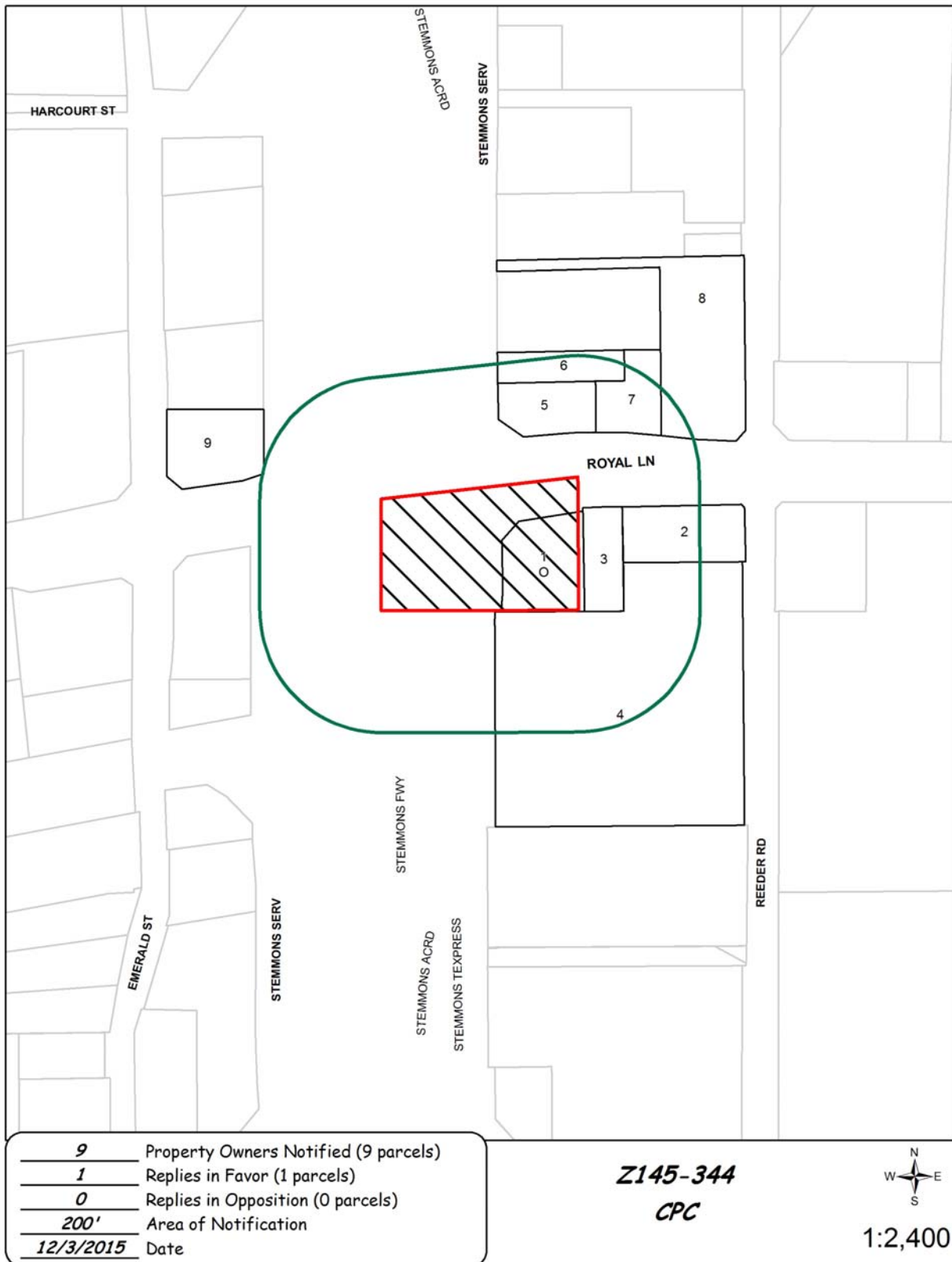








CPC RESPONSES



12/02/2015

Reply List of Property Owners

Z145-344

9 Property Owners Notified 1 Property Owners in Favor 0 Property Owners Opposed

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
O	1 2450	ROYAL LN	HSY INC
	2 2464	ROYAL LN	KLACHIAN GARY &
	3 2454	ROYAL LN	V V S N HOLDING LLC
	4 11359	REEDER RD	DIANA MOORE FOUNDATION
	5 11404	N STEMMONS FWY	STATEWIDE STATIONS INC
	6 11404	N STEMMONS FWY	QUIK WAY RETAIL AS II LTD
	7 2465	ROYAL LN	C2 GLASS DISTRIBUTORS INC
	8 2475	ROYAL LN	MCDONALDS USA LLC
	9 2405	ROYAL LN	GREEK DEVELOPMENT LLC

AGENDA ITEM # 44

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 6

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 22 K

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting an IM Industrial Manufacturing District on property zoned an IR Industrial Research District, on the north line of X Street, west of Tantor Road

Recommendation of Staff and CPC: Approval

Z145-365(OTH)

FILE NUMBER: Z145-365(OTH)

DATE FILED: September 29, 2015

LOCATION: On the north line of X Street, west of Tantor Road

COUNCIL DISTRICT: 6

MAPSCO: 22K

SIZE OF REQUEST: Approx. 0.92 acres

CENSUS TRACT: 99.00

APPLICANT / OWNER: Rockbrook Realty Ltd.

REPRESENTATIVE: Michael R. Coker Company

REQUEST: An application for an IM Industrial Manufacturing District on property zoned an IR Industrial Research District.

SUMMARY: The applicant indicates that they propose to utilize the property for uses permitted in the IM District. While the applicant has not indicated specifically how the property will be utilized, he has stated that it could be combined with the property to the east to create a larger building site.

CPC RECOMMENDATION: Approval

STAFF RECOMMENDATION: Approval

GUIDING CRITERIA FOR STAFF RECOMMENDATION:

Staff recommends approval based upon:

1. *Performance impacts upon surrounding property* – The proposed zoning district will be consistent with the zoning to the east and north. There are nonconforming residential uses to the west. These uses would not get residential adjacency protection offered by the Development Code since the property is zoned IR Industrial Research.
2. *Traffic impact* – Certain industrial uses could have an impact upon the surrounding street system.
3. *Comprehensive Plan or Area Plan Conformance* – The forwardDallas! Comprehensive Plan identifies the area as an Industrial Area Building Block. While the subject site is not located directly on a freight rail line or on a major street, it is in proximity to these access points.

Zoning History: There has been one zoning request in the area within the last five years.

1. **Z145-352** Staff is processing an automatic renewal for a Specific Use Permit No. 1811 for a metal salvage facility on property located at 11221 Tantor Road.

Thoroughfares/Streets:

Thoroughfare/Street	Type	Existing ROW
Tantor Road	Local	60 feet
X Street	Local	60 feet

STAFF ANALYSIS:

Comprehensive Plan:

The forwardDallas! Comprehensive Plan was adopted by the City Council in June 2006. The forwardDallas! Comprehensive Plan outlines several goals and policies which can serve as a framework for assisting in evaluating the applicant's request. The Plan classifies the area as an Industrial Area Building Block.

Industrial Areas, which offer important employment opportunities, occupy large areas of land and usually are near major roads and heavy rail lines. Evolving technology and the need for freight movement through Dallas to the rest of the country and internationally

means this sector can offer good opportunities for jobs. Logistics and warehousing, a growing industry with strong potential for upward mobility of skilled workers, would thrive in such areas. Examples include Southport and the Agile Port, parts of West Dallas along I-30, and the Stemmons industrial area. These areas include a mix of low- and medium-density industrial buildings and industrial yards and have large surface parking for cars and trucks. Industrial Areas rely on quality road access and may be linked to rail for freight purposes. Street lanes are wide and intersections are large. Transit, sidewalks and other pedestrian improvements are limited.

Surrounding Land Uses:

	Zoning	Land Use
Site	IR	Undeveloped
North	IM	Metal salvage facility
East	IM	Outside storage
South	IR	Outside storage, undeveloped
West	IR	Residential, non-conforming use

Land Use Compatibility:

The request site is approximately 0.92 acres of land and is currently undeveloped. The existing zoning of the property is IR Industrial Research District.

The property is surrounded by industrial uses to the north; industrial to the east; undeveloped and industrial to the south; nonconforming single family and industrial to the west. The property has been zoned industrial since at least 1988.

The differences in the development standards for the IR and IM districts are listed below. In terms of land uses, IM permits more industrial uses by a Specific Use Permit and industrial (outside) not potentially incompatible uses (i.e. concrete batch plant) and organic compost recycling facility by right.

Development Standards:

DISTRICT	Setbacks		Density	Height	Lot Coverage	Special Standards	Primary Uses
	Front	Side/Rear					
Existing							
IR Industrial research	15'	30' adjacent to residential OTHER: No Min.	2.0 FAR overall 0.75 office/ retail 0.5 retail	200' 15 stories	80%	Proximity Slope Visual Intrusion	Industrial, wholesale distribution & storage, supporting office & retail
Proposed							
IM Industrial manufacturing	15' 0' on minor	30' adjacent to residential OTHER: No Min.	2.0 FAR overall 0.75 office/ retail 0.5 retail	110' 8 stories	80%	Proximity Slope Visual Intrusion	Industrial (inside / outside) not potentially incompatible, metal salvage facility, outside salvage and reclamation by SUP

Parking:

Pursuant to the Dallas Development Code, off-street and loading required parking must be provided in accordance with Division 51A-4.200 for the specific off-street parking and loading requirements for each use.

Landscaping:

All landscaping for the proposed development must comply with the requirements of Article X.

CPC ACTION:
December 3, 2015

Motion: It was moved to recommend **approval** of an IM Industrial Manufacturing District on property zoned an IR Industrial Research District, on the north line of X Street, west of Tantor Road.

Maker: Emmons
Second: Shidid
Result: Carried: 14 to 0

For: 14 - Anglin, Emmons, Houston, Davis, Shidid,
Abtahi, Haney, Jung, Housewright, Schultz,
Peadon, Murphy, Ridley, Tarpley

Against: 0
Absent: 1 - Anantasomboon
Vacancy: 0

Notices: Area: 200 Mailed: 14
Replies: For: 0 Against: 0

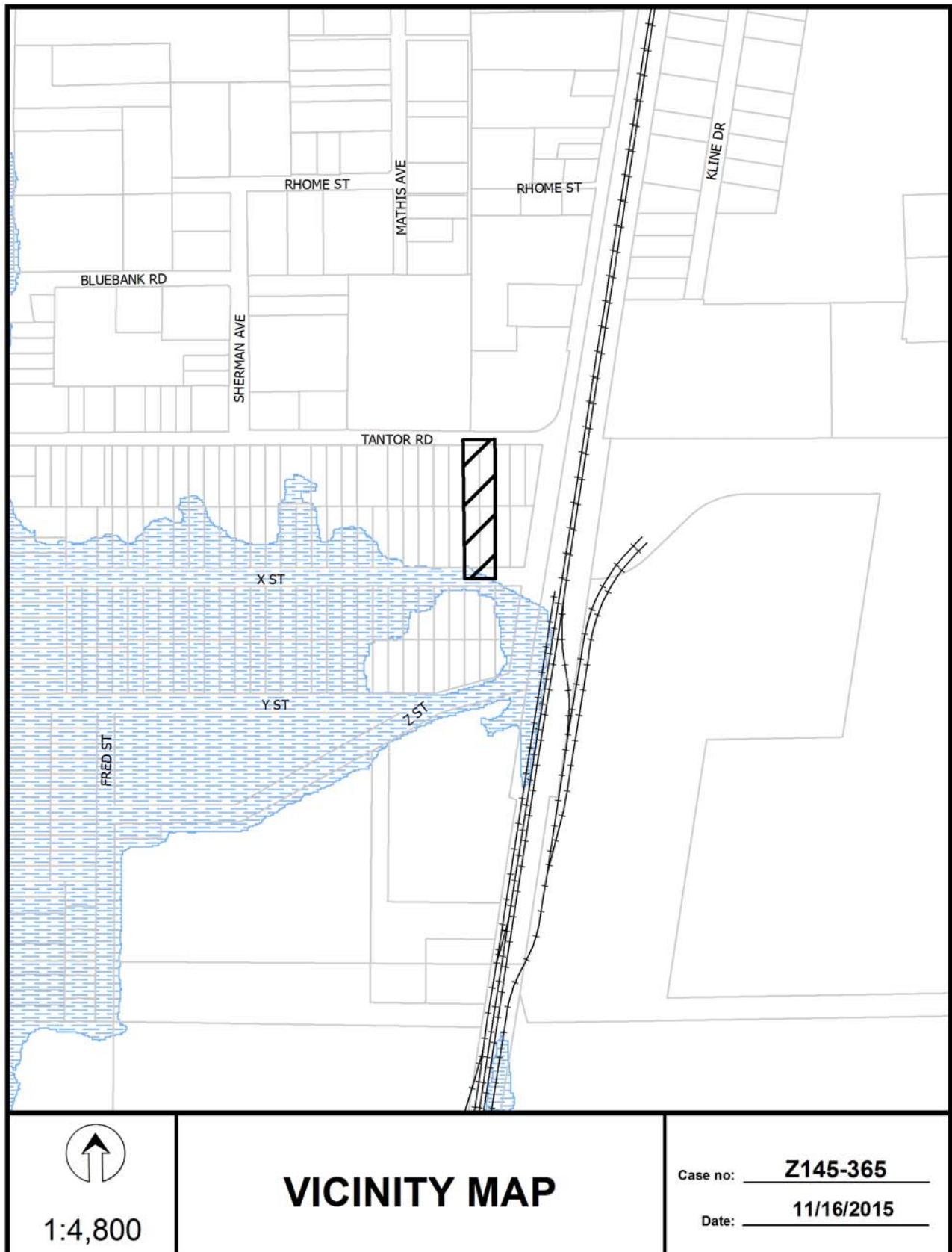
Speakers: For: Michael Coker, 3111 Canton St., Dallas, TX 75226
Against: None

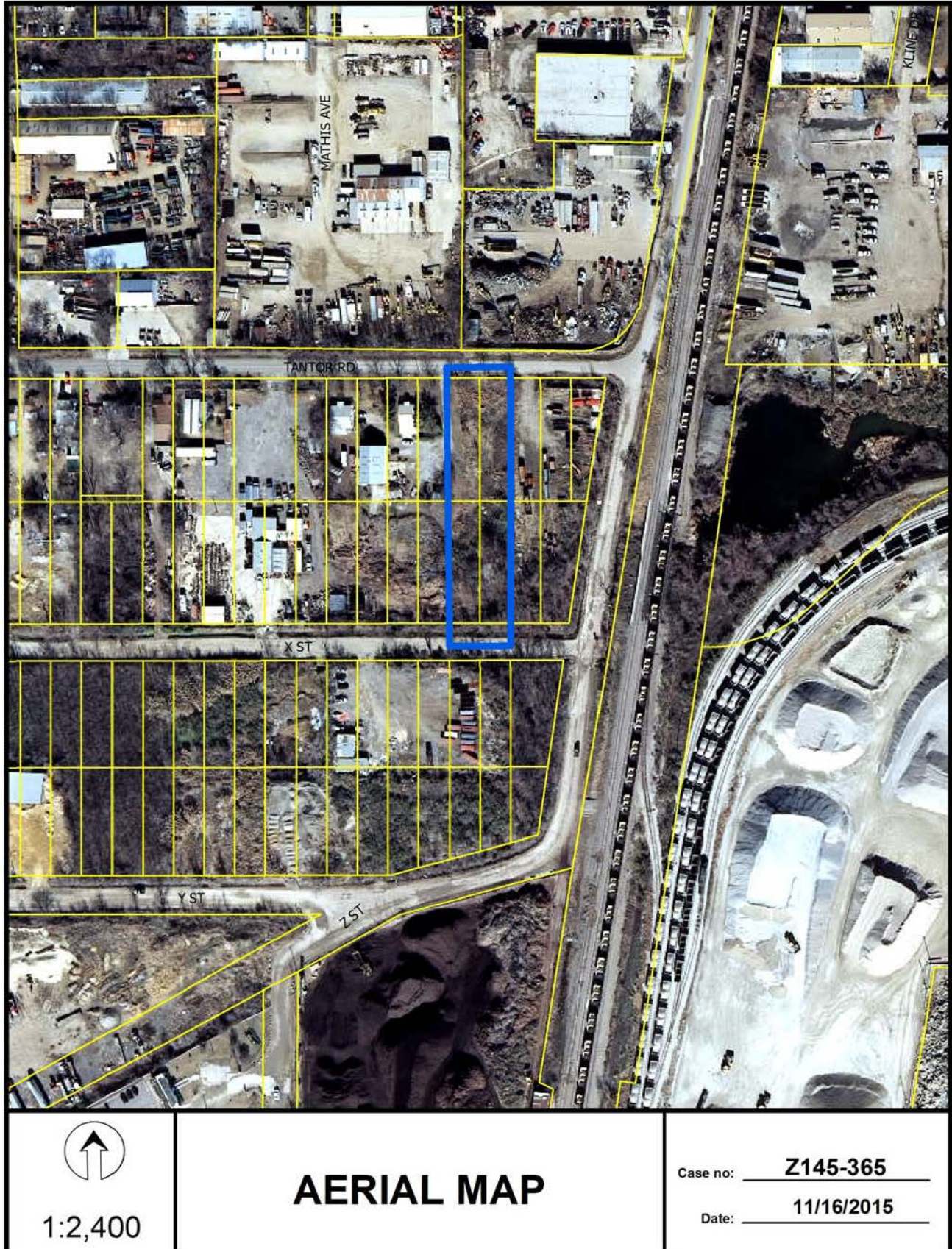
Z145-365(OTH)

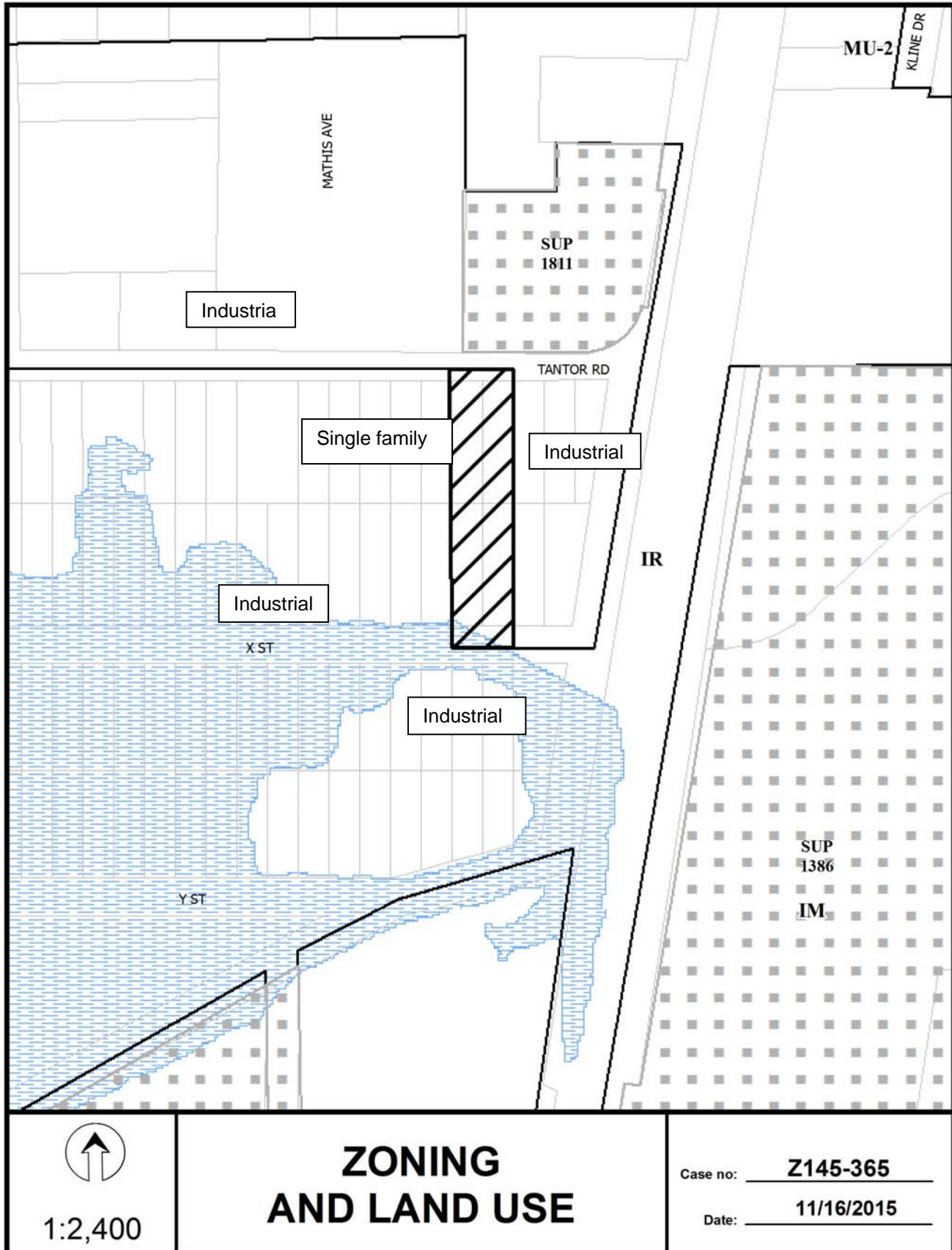
Partners

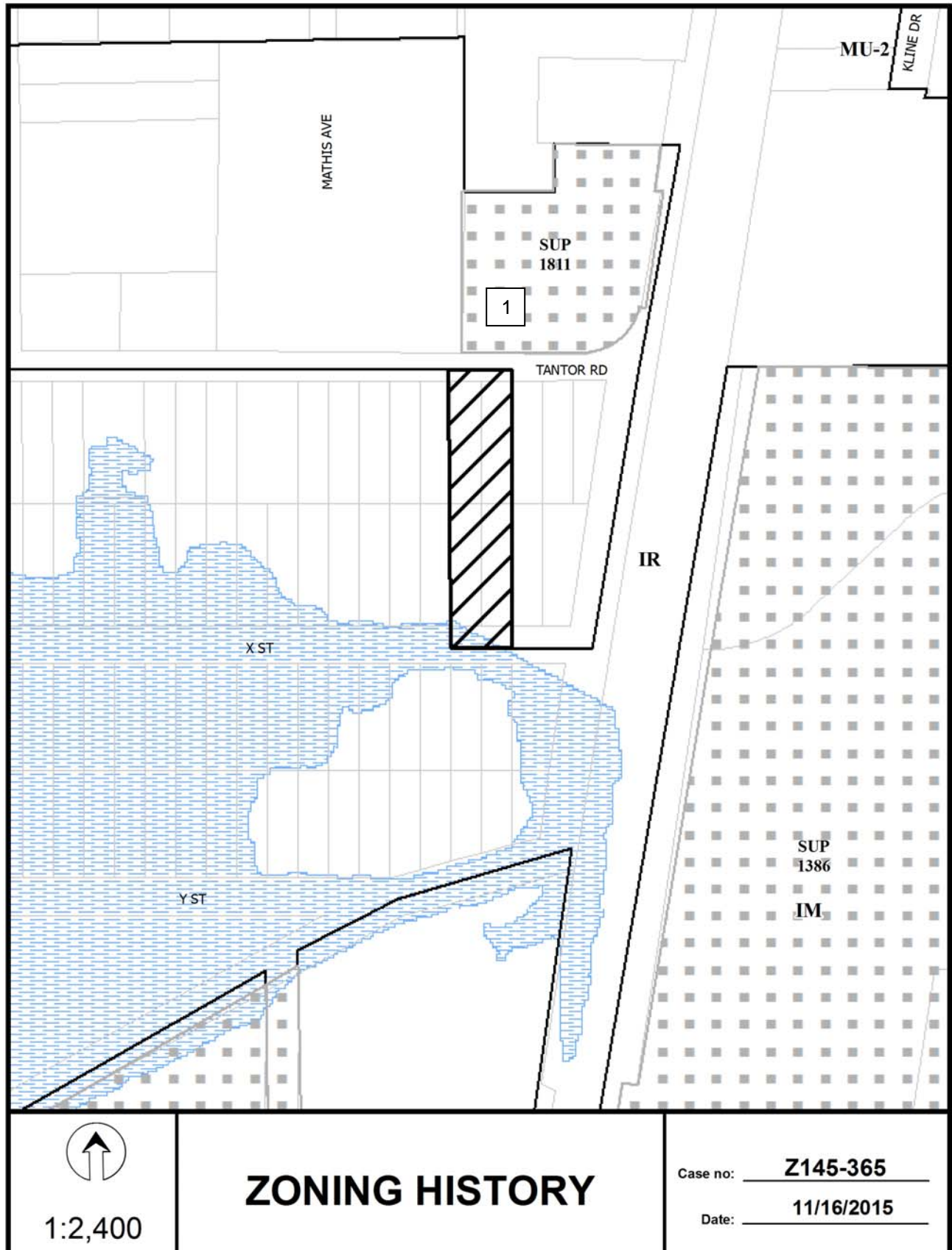
Rockbrook Realty Ltd.

Donald F. Finn, General Manager

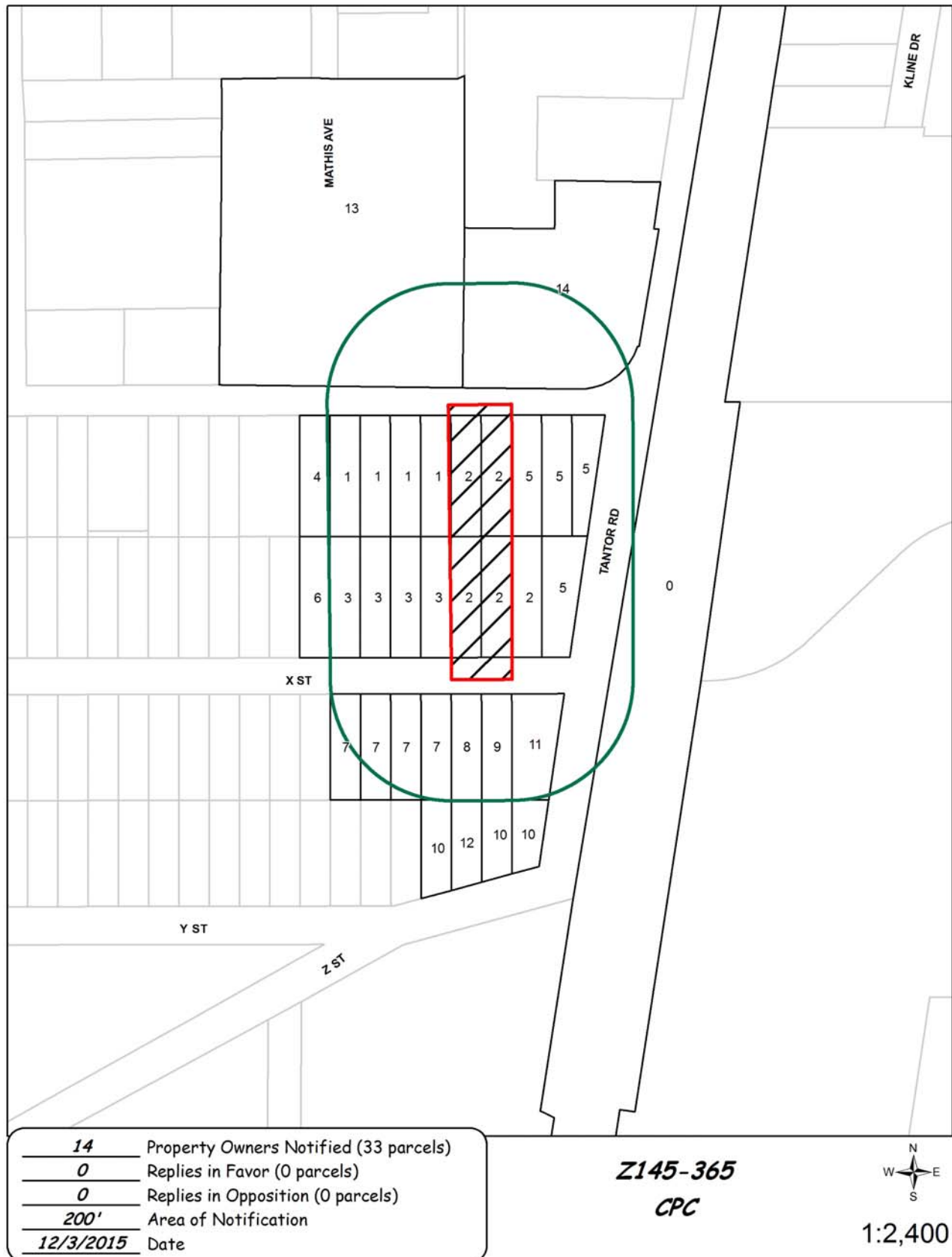








CPC RESPONSES



12/02/2015

Reply List of Property Owners

Z145-365

***14 Property Owners Notified
Owners Opposed***

0 Property Owners in Favor

0 Property

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	1	1720 TANTOR RD	SMITH DONALD M
	2	1728 TANTOR RD	ROCKBROOK REALTY LTD
	3	1755 X ST	THOMPSON THOMAS P & BELLA A
	4	1718 TANTOR RD	THOMPSON THOMAS P
	5	1732 TANTOR RD	ROCKBROOK REALTY LTD PS
	6	1739 X ST	THOMPSON THOMAS P & BELLA
	7	1742 X ST	HERNANDEZ JOE & ESTER
	8	1758 X ST	KWON YOONGHEE
	9	1762 X ST	TERRY ISABELLE
	10	1765 Y ST	TERRY R H
	11	1768 X ST	TERRY ROY K
	12	1761 Y ST	TERRY ROY H
	13	11301 MATHIS ST	LACY MATHIS LTD
	14	11221 TANTOR RD	GREEN ROOF RECYCLING LLC

AGENDA ITEM # 45

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 3

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 62 H

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for an Open-enrollment charter school on property zoned an IR Industrial/Research District at the northwest corner of Dresser Way and South Walton Walker Boulevard

Recommendation of Staff and CPC: Approval for a five-year period with eligibility for automatic renewals for additional five-year periods, subject to a site/landscape plan, traffic management plan and conditions
Z156-106(RB)

FILE NUMBER: Z156-106(RB)

DATE FILED: October 9, 2015

LOCATION: Northwest Corner of Dresser Way and South Walton Walker Boulevard

COUNCIL DISTRICT: 3

MAPSCO: 62 H

SIZE OF REQUEST: Approx. 3.193 Acres

CENSUS TRACT: 165.20

APPLICANT: Advantage Academy

REPRESENTATIVE: Rob Baldwin

OWNER: John and Laura Warren Trust

REQUEST: An application for a Specific Use Permit for an Open-enrollment charter school on property zoned an IR Industrial/Research District.

SUMMARY: The applicant proposes to utilize the existing improvements for a 24 classroom (12 elementary and 12 middle school/junior high) open-enrollment charter school.

CPC RECOMMENDATION: **Approval** for a five-year period with eligibility for automatic renewals for additional five-year periods, subject to a site/landscape plan, traffic management plan and conditions.

STAFF RECOMMENDATION: **Approval** for a five-year period with eligibility for automatic renewals for additional five-year periods, subject to a site/landscape plan, traffic management plan and conditions.

GUIDING CRITERIA FOR RECOMMENATION:

The following factors are listed in Chapter 51A of the Dallas Development Code to guide the determination as to whether or not an SUP shall be granted. Staff has listed its findings based upon each component below:

1. *Compatibility with surrounding uses and community facilities* – The request being located along a thoroughfare and local street that serves an office use, will have minimal impact on adjacent uses in the immediate area as assessed by the traffic management plan.
2. *Contribution to, enhancement, or promoting the welfare of the area of request and adjacent properties* – The underlying zoning permits the requested institutional use by SUP. The applicant's request will not require an expansion of floor area, nor will it affect accessibility to adjacent properties. Lastly, the existing trees as noted on the attached site and landscape plan will be retained, thus continuing to soften the physical improvements.
3. *Not a detriment to the public health, safety, or general welfare* – The school will utilize existing improvements on the property, with proposed outdoor play areas situated away from travelways and buffered from impacting adjacent uses.
4. *Conforms in all other respects to all applicable zoning regulations and standards* – All improvements must comply with the building code and will require a final inspection to ensure compliance with the school's state charter as well as the attached conditions and plan.

Zoning History: There has been no zoning activity in the immediate area within the past five years.

Traffic: The Engineering Section of the Department of Sustainable Development and Construction has reviewed the request and traffic management plan and determined that it has not significantly impacted the surrounding roadway system. Furthermore, staff's recommended conditions will require scheduled updates of the TMP to ensure all queuing for unloading/loading is contained on the property.

<u>Thoroughfare/Street</u>	<u>Designation; Existing & Proposed ROW</u>
South Walton Walker	Principal Arterial; 107' & 107' ROW
Dresser Way	Local; 60' ROW

STAFF ANALYSIS:

Comprehensive Plan: The site is located in an Industrial Area, which offers important employment opportunities, occupy large areas of land and usually are near major roads and heavy rail lines. Evolving technology and the need for freight movement through Dallas to the rest of the country and internationally means this sector can offer good opportunities for jobs. Logistics and warehousing, a growing industry with strong potential for upward mobility of skilled workers, would thrive in such areas. Examples include Southport and the Agile Port, parts of West Dallas along I-30, and the Stemmons industrial area. These areas include a mix of low- and medium-density industrial buildings and industrial yards and have large surface parking for cars and trucks. Industrial Areas rely on quality road access and may be linked to rail for freight purposes. Street lanes are wide and intersections are large. Transit, sidewalks and other pedestrian improvements are limited.

Land Use Compatibility: The request site is developed with a two-story office building with surface parking around its perimeter as well as a concentration of spaces situated between back-of-curb and the building façade along the Dresser Way frontage, with two ingress/egress points situated along this street frontage as well.. Additionally, a traffic management plan has been submitted to address unloading/loading operations for a 24 classroom campus (12 elementary/12 junior high/middle school) with a maximum enrollment of 528 students. As a means of better understanding the orientation and scope of this request, the applicant has operated an open-enrollment charter school one-half mile to the northwest (4009, 4010 Joseph Hardin Drive; grades K-8; 28 classrooms) since 1999. The applicant's representative has indicated there will be no interaction between both campuses.

Surrounding land use is predominately nonresidential in nature (office, financial institution, vehicle display, sales, and service), with mini-warehouse (SUP No. 1529) and medium density multifamily uses to the northeast/east across South Walton Walker Boulevard.

The general provisions for a Specific Use Permit in Section 51A-4.219 of the Dallas Development Code specifically state: (1) The SUP provides a means for developing certain uses in a manner in which the specific use will be consistent with the character of the neighborhood; (2) Each SUP application must be evaluated as to its probable effect on the adjacent property and the community welfare and may be approved or denied as the findings indicate appropriate; (3) The city council shall not grant an SUP for a use except upon a finding that the use will: (A) complement or be compatible with the surrounding uses and community facilities; (B) contribute to, enhance, or promote the welfare of the area of request and adjacent properties; (C) not be detrimental to the public health, safety, or general welfare; and (D) conform in all other respects to all applicable zoning regulations and standards. The regulations in this chapter have been established in accordance with a comprehensive plan for the purpose of promoting the

health, safety, morals, and general welfare of the city.

The applicant has worked with staff to ensure secured outdoor play areas are provided. As noted on the attached plan, these areas are at the perimeter of the property and will not impact adjacent uses. The submitted traffic management plan and Traffic Management Plan circulation exhibit (see attached) will ensure all unloading and loading operations are accommodated on-site, thus ensuring no queuing occurs in the public right-of-way.

As a result of this analysis, staff is supportive of the request, subject to the attached site/landscape plan, traffic management plan, and conditions.

Landscaping. The site possesses mature plantings across the site as well as foundation planting areas typically found around older garden style office buildings. As the applicant will utilize existing improvements, there will be no requirement for additional landscaping. As these plantings provide for a softening of the site (surface parking areas and building), staff's intent is to capture as a requirement for the proposed school, thus the landscape component noted on the attached plan. This also establishes a baseline for any future need to comply with tree mitigation on the property.

It should be noted that any expansion/revision for the proposed school or redevelopment of the site (for an open-enrollment charter school) will require an amendment to the plan to address whatever redevelopment/expansion is requested. If the property were to be redeveloped with a use allowed by right under existing IR District entitlements, landscaping will be required per Article X.

CPC ACTION

(December 3, 2015)

Motion: It was moved to recommend **approval** of a Specific Use Permit for an Open-enrollment charter school for a five-year period with eligibility for automatic renewals for additional five-year periods, subject to a site/landscape plan, traffic management plan and conditions on property zoned an IR Industrial/Research District at the northwest corner of Dresser Way and South Walton Walker Boulevard.

Maker: Houston
Second: Abtahi
Result: Carried: 14 to 0

For: 14 - Anglin, Emmons, Houston, Davis, Shidid,
Abtahi, Haney, Jung, Housewright, Schultz,
Peadon, Murphy, Ridley, Tarpley

Against: 0
Absent: 1 - Anantasomboon
Vacancy: 0

Notices: Area: 300 Mailed: 11
Replies: For: 1 Against: 0

Speakers: None



618 W. Wheatland Rd. ★ Duncanville, TX 75116 ★ Phone: 214.276.5800 ★ www.advantageisd.org

**Advantage Academy
List of Officers:**

Angela R. McDonald
President, Superintendent/CEO

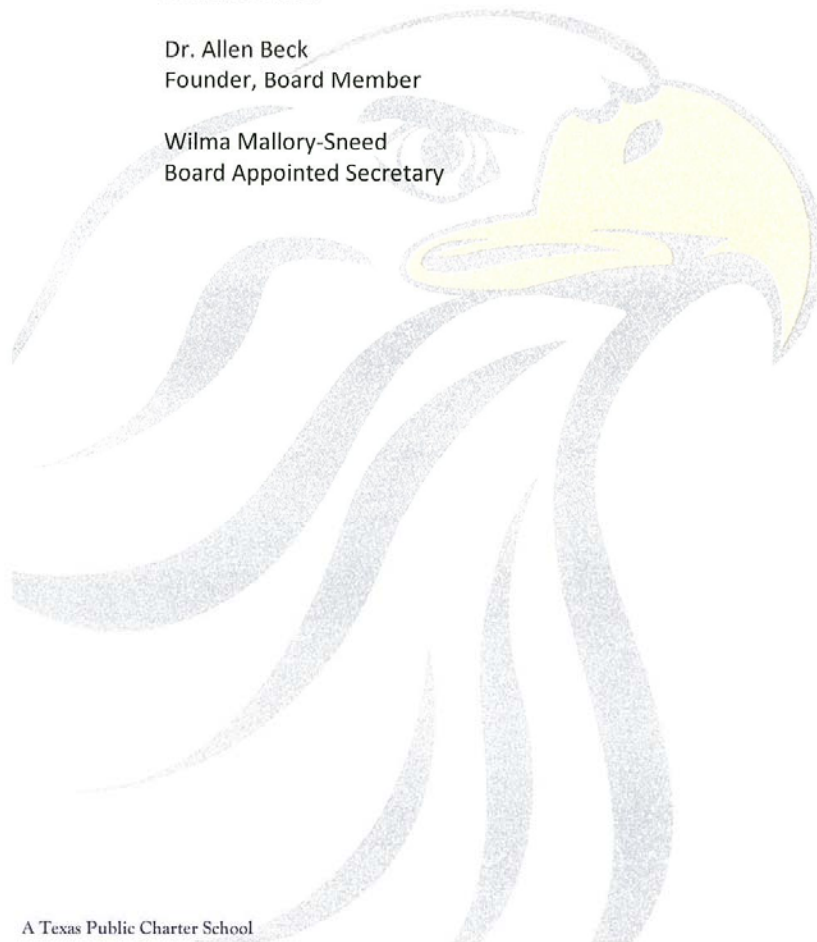
Gerald (Jerry) Zielke
Treasurer

Robert A. Nickell Jr.
Board Member

Kristina Blake
Board Member

Dr. Allen Beck
Founder, Board Member

Wilma Mallory-Sneed
Board Appointed Secretary



Z156-106

**CONDITIONS FOR A SPECIFIC USE PERMIT
FOR AN OPEN ENROLLMENT CHARTER SCHOOL**

1. USE: The only use authorized by this specific use permit is an open-enrollment charter school.
2. SITE PLAN: Use and development of the Property must comply with the attached site and landscape plan.
3. TIME LIMIT: This specific use permit expires on (five-years from the passage of the ordinance), but is eligible for automatic renewal for additional five-year periods pursuant to Section 51A-4.219 of Chapter 51A of the Dallas City Code, as amended. For automatic renewal to occur, the Property owner must file a complete application for automatic renewal with the director before the expiration of the current period. Failure to timely file a complete application will render this specific use permit ineligible for automatic renewal. (Note: The Code currently provides that applications for automatic renewal must be filed after the 180th but before the 120th day before the expiration of the current specific use permit period. The Property owner is responsible for checking the Code for possible revisions to this provision. The deadline for applications for automatic renewal is strictly enforced.)
4. LANDSCAPING. Landscaping must be provided as shown on the attached site and landscape plan.
5. CLASSROOMS: Maximum number of classrooms is 24.
6. HOURS OF OPERATION: The open-enrollment charter school may only operate between 6:00 a.m. and 9:00 p.m., Monday through Saturday.
7. INGRESS/EGRESS: Ingress and egress must be provided in the location shown on the attached site plan. No other ingress or egress is permitted.
8. OFF-STREET PARKING: Parking must be located as shown on the attached site and landscape plan.
9. TRAFFIC MANAGEMENT PLAN:
 - A. In general. Operation of the open-enrollment charter school must comply with the attached traffic management plan.
 - B. Queuing. Queuing is only permitted inside the Property. Student drop-off and pick-up are not permitted within city rights-of-way.
 - C. Traffic Management Plan.
 - i. The Property owner or operator shall prepare a traffic study evaluating the sufficiency of the traffic management plan. The initial traffic study must be submitted to the director by November 1, 2017. After the initial traffic study, the

Property owner or operator shall submit updates of the traffic study to the director by November 1 of every odd year thereafter (2019, 2021, etc).

ii. The traffic study must be in writing, performed by a licensed engineer, based on a minimum of four samples taken on different school days at different drop-off and pick-up times over a two-week period, and must contain an analysis of the following:

- a. ingress and egress points;
- b. queue lengths;
- c. number and location of personnel assisting with loading and unloading of students;
- d. drop-off and pick-up locations;
- e. drop-off and pick-up hours for each grade level;
- f. hours for each grade level; and
- g. circulation.

iii. Within 30 days after submission of a traffic study, the director shall determine if the current traffic management plan is sufficient.

a. If the director determines that the current traffic management plan is sufficient, the director shall notify the applicant in writing.

b. If the director determines that the current traffic management plan results in traffic hazards or traffic congestion, the director shall require the Property owner to submit an amended traffic management plan. If the Property owner fails to submit an amended traffic management plan within 30 days, the director shall notify the city plan commission.

D. Amendment process.

(1) A traffic management plan may be amended using minor plan amendment fee and public hearing process in Section 51A-1.105(k)(3) of Chapter 51A of the Dallas City Code, as amended.

(2) The city plan commission shall authorize changes in a traffic management plan if the proposed amendments improve queuing or traffic circulation; eliminate traffic hazards; or decrease traffic congestion.

10. MAINTENANCE: The Property must be properly maintained in a state of good repair and neat appearance.

11. GENERAL REQUIREMENTS: Use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the City of Dallas.



Z156-106(RB)

Technical Memorandum

To: Mr. Robert Baldwin, AICP — Baldwin Associates, Inc.
CC: Advantage Academy
From: David Nevarez, P.E. — DeShazo Group, Inc.
Date: October 8, 2015
Re: Traffic Management Plan for Advantage Academy at 4221 S. Walton Walker Blvd. in Dallas, Texas
DeShazo Project Number 15142

INTRODUCTION

DeShazo Group, Inc. (DeShazo) is an engineering consulting firm based in Dallas, Texas providing licensed engineers skilled in the field of traffic/transportation engineering. The services of DeShazo were retained by Baldwin Associates, LLC ("the client") on behalf of Advantage Academy to provide a requisite traffic management plan (TMP) for an expansion of their "North Duncanville" campus in Dallas, Texas.

Advantage Academy is an open-enrollment charter school serving students from pre-kindergarten to eighth grade. The "North Duncanville" campus currently operates in two separate city blocks. Students in Pre-Kindergarten through Grade 5 attend classes at 4010 Joseph Hardin Drive. A separate building across the street, located at 4009 Joseph Hardin Drive serves grades 6, 7 and 8. The current school enrollment is 507 students, albeit classrooms are not occupied at full enrollment capacity. DeShazo previously prepared an original TMP on September 30, 2009 and an updated review of current conditions on February 4, 2015. A copy of these reports is provided as reference in the **Appendix**. Also, **Exhibit 1** presents a site location map with all the properties associated with the North Duncanville Campus.

The proposed expansion of the school campus plans to redistribute existing classrooms and relocate 3rd through 8th grade to a third building located at 4221 S. Walton Walker Blvd. The subject building will hold 24 classrooms and a capacity of 528 students.

The proposed school property is zoned IR (Industrial research) and is regulated by Chapter 51A of the Dallas Development Code. City of Dallas approval is required to gain entitlements for the proposed expansion. As part of the approval process, submittal of a TMP is required as a record of the preferred strategies to be used by the school to ensure overall traffic safety and efficiency. This TMP is intended to assess existing and anticipated traffic conditions during the school's peak periods on the basis of satisfying these objectives. By consent of the TMP submittal, the school is agreeing to the strategies for which the school will be held self-accountable until and unless the City of Dallas deems further measures are appropriate.

Advantage Academy
Traffic Management Plan
Page 1

TRAFFIC MANAGEMENT PLAN

A Traffic Management Plan (TMP) is important to safely achieve an optimum level of traffic flow and circulation during peak traffic periods associated with student drop-off and pick-up. Properly managing the vehicular traffic generated during the critical periods inherently improves the safety and efficiency of all modes of travel and also minimizes the operational impact on the public street system. The TMP should not be considered a comprehensive set of instructions to ensure adequate safety; however, it should be used as a tool to facilitate a safer and more efficient environment.

The analysis summarized below utilizes the proposed school site plan to evaluate aspects such as passenger loading/unloading and vehicle queuing (i.e. stacking) that occur at the school in order to accommodate the observed peak demands within the site. A concerted effort and full participation by the school administration, staff, students, and parents are encouraged to provide and maintain safe and efficient traffic operations. [NOTE: In this report the term "parent" refers to any parent, family member, legal guardian, or other individual who is involved in the pick-up or drop-off of one or more students at the school.]

School Operational Characteristics

Table 1 summarizes the known operational characteristics for Advantage Academy assumed in this analysis:

Table 1. School Operational Characteristics

	Existing Conditions (4009/4010 Joseph Hardin Dr.) <i>students [classrooms]</i>	Proposed Capacity (4221 S. Walton Walker Blvd.) <i>students [classrooms]</i>
Student Enrollment:		
➤ Pre-Kindergarten	33 [1]	--
➤ Kindergarten	57 [3]	--
➤ 1 st Grade	44 [3]	--
➤ 2 nd Grade	65 [3]	--
➤ 3 rd Grade	65 [3]	88 [4]
➤ 4 th Grade	52 [3]	88 [4]
➤ 5 th Grade	49 [3]	88 [4]
➤ 6 th Grade	59 [3]	88 [4]
➤ 7 th Grade	50 [3]	88 [4]
➤ 8 th Grade	33 [2]	88 [4]
Total:	507 [27]	528 [24]
Daily Start/End Schedule:		
➤ Grades PK – 2 nd :	8:00 AM – 3:15 PM	TBD
➤ Grades 3 rd – 5 th :	8:00 AM – 3:30 PM	
➤ Grades 6 th – 8 th :	8:00 AM – 3:45 PM	
Student population not part of normal pick-up (e.g., after-school, walking, etc.):	<10%	<10%

NOTE #1: School facilities on 4009 and 4010 Joseph Hardin Dr are to remain in operation with a smaller student population. These facilities are part of the scope of this study but their respective traffic management plan is included in the Appendix.

NOTE #2: To the highest degree practical, accounts of "existing conditions" presented in this report are based upon actual on-site observations conducted by DeShazo during typical school day(s) conditions on Thursday, September 24, 2015 and from personal interviews of school representatives. The analysis and recommendations for "proposed capacity" conditions are based upon evaluations of "existing conditions" and supplemented by DeShazo's professional judgment and experience. "Proposed capacity" conditions are intended to reflect the anticipated day-to-day conditions at full occupancy.

NOTE #3: Occasional functions or other events may be held at the school, which generate traffic outside of traditional drop-off and pick-up periods. While some of the measures presented in this report may apply in such cases, traffic characteristics other than those directly associated with primary drop-off and pick-up periods are not the subject of this analysis.

Site Access and Circulation

The proposed school property has frontage on both S. Walton Walker Blvd. and Dresser Way. There are two driveways on Dresser Way but no direct vehicular access to S. Walton Walker Blvd. Each driveway is over 30 feet wide and can accommodate both inbound and outbound traffic. However, access should be limited to only one driveway during the school peak hours of traffic. Two plans were prepared to accommodate the traffic operations based on student population. **Exhibit 2** presents recommendations that can support up to 184 students per dismissal time—an anticipated initial condition for the proposed site. **Exhibit 3** presents a different route around the building to accommodate traffic that exceeds the 184 student limit but can accommodate up to 328 students per dismissal time. In both scenarios, vehicles enter the campus to unload students within the site during the morning drop-off period. In the afternoon, vehicles also enter the school site via the westernmost driveway and directly proceed to form a queue towards the loading/unloading area along the prescribed route. Once in queue, traffic operates in double lanes with the opportunity to exit upon reaching the loading/unloading zone. Based upon actual observations of existing traffic operations at the existing sites, parents have no problem exiting sequentially upon leaving the loading area. Exiting traffic drives towards the egress driveway along the designated route.

Passenger Loading and Vehicular Queue

During the afternoon pick-up period, the proposed school site will implement a managed “carpool” system. Parents will arrive to the site with identification name tags that pairs them with the corresponding student. During the pick-up period, the names of students are on display through the vehicle’s windshield while parents circulate through the prescribed route. School staff is positioned at strategic locations to relay the sequence of parent arrival back to the loading area. In the meantime, students are prepped for pick-up as parents approach their corresponding loading area. Several vehicles are loaded simultaneously with the assistance of other school staff stationed at the loading area. Only one single loading zone in front of the main building entrance will serve pick-up operations. In general, the site provides ample queuing space with vehicles forming two rows. Once loaded, vehicles are cleared by school staff to carefully exit the site along the designated route.

School observations consistently indicate that maximum queues occur during the afternoon peak period when students are being picked-up—the morning period is typically not a significant traffic issues since drop-off activities are more temporally distributed and occur much more quickly than student pick-up. DeShazo empirically quantified the peak number of parent-vehicles on site during the afternoon pick-up period based upon field observations commissioned during typical school-day conditions (on Thursday, September 24, 2015). The vehicle accumulation count includes all vehicles in queue. Assuming that the number of vehicles generated during the afternoon pick-up period is directly proportional to the number of students enrolled, the peak queue for the future conditions at full occupancy can be estimated. The projected peak number of vehicles during each dismissal time is summarized in **Table 2**.

Table 2. Peak Vehicles In Queue during Afternoon Pick-Up Period

Group	Dismissal Time	Existing Max Queue (observed)	Queue Rate (observed)
Grades K, 1, 2	3:15 PM	40 cars 166 students	0.24 cars/student
Grades 3, 4, 5	3:30 PM	21 cars 166 students	0.13 cars/student*
Grades 6, 7, 8	3:45 PM	35 cars 142 students	0.25 cars/student

Table 2 presents actual queue lengths of up to *0.25 vehicles per student*. Also, a lower rate for grades 3 through 5 represents an unidentified number of students (but less than 10% of the student population) who are picked up at the loading zone for grades 6 through 8 at a later dismissal period (3:45 PM).

Recommendations

The following recommendations are provided by DeShazo to the Advantage Academy Charter School for the management of vehicular traffic generated by the school during peak traffic conditions. [NOTE: Generally, traffic delays and congestion that occurs during the afternoon pick-up period is notably greater than the traffic generated during the morning drop-off period due to the timing and concentration characteristics. In most instances, achieving efficiency during the afternoon period is most critical, while the morning traffic operations require nominal active management. Therefore, the recommendations provided herein pertain specifically to the afternoon period operations.]

1. DeShazo recommends implementation of the traffic circulation plan for the proposed school facility as depicted in **Exhibit 2** during initial conditions with a maximum of 184 students per dismissal time. The recommendations presented in **Exhibit 3** pertain to traffic conditions where the number of students dismissed at a time exceeds this figure. The traffic management plan presented in **Exhibit 3**, however, also limits the dismissal period to no more than 328 students at a time. These recommendations are based upon a review of the traffic operations at the existing campus and the anticipated needs of traffic during peak conditions at the proposed site. The plans were designed to optimize the on-site vehicular circulation and retention of queued vehicles in a manner that promotes safety and operational efficiency.
 - The recommended plan presented in **Exhibit 2** provides 1,060 linear feet of on-site vehicular queuing (i.e. storage for up to 45 vehicles @ approximately 23.5 feet per vehicle). This capacity is expected to accommodate the projected demand of up to 184 students. Therefore, it is recommended that a staggering time remains in place with dismissals limited to 184 students and separated by at least 15 minutes.
 - In the event that number of dismissed students exceeds the 184-student limit, a different plan should take effect as presented in **Exhibit 3**. This plan provides 1,940 linear feet of on-site vehicular queuing (i.e. storage for up to 82 vehicles @ approximately 23.5 feet per vehicle). This capacity is expected to accommodate the projected demand of up to 328 students. Therefore, it is recommended that a staggering time remains in place with dismissals limited to 328 students and separated by at least 15 minutes.

NOTE: Studies of student pick-up operations consistently show that vehicular traffic typically clears after 10 minutes following the student dismissal time. Although parents of subsequent dismissal groups are expected to arrive while the previous queue is still in progress, previous queues will have cleared in time for the next one to start lining up.
2. The plan also includes a recommended configuration of temporary traffic control devices (such as traffic cones, etc.) that shall be installed on a daily basis when typical traffic conditions are expected. An appropriate number of school staff should be assigned to fulfill the duties of student supervision, traffic control, and other related duties as generally depicted on the plan.
3. Staff directing traffic at the intersecting point of two queue lanes (and other areas, where appropriate) should, in lieu of simple hand gestures, procure and use reversible hand-paddle signs with the messages (and symbols) for STOP and for SLOW (i.e., proceed slowly). Optional additional equipment used by staff may include whistles (for audible warnings) and flashlights (for visual warnings) in order to better-gain the attention of motorists.

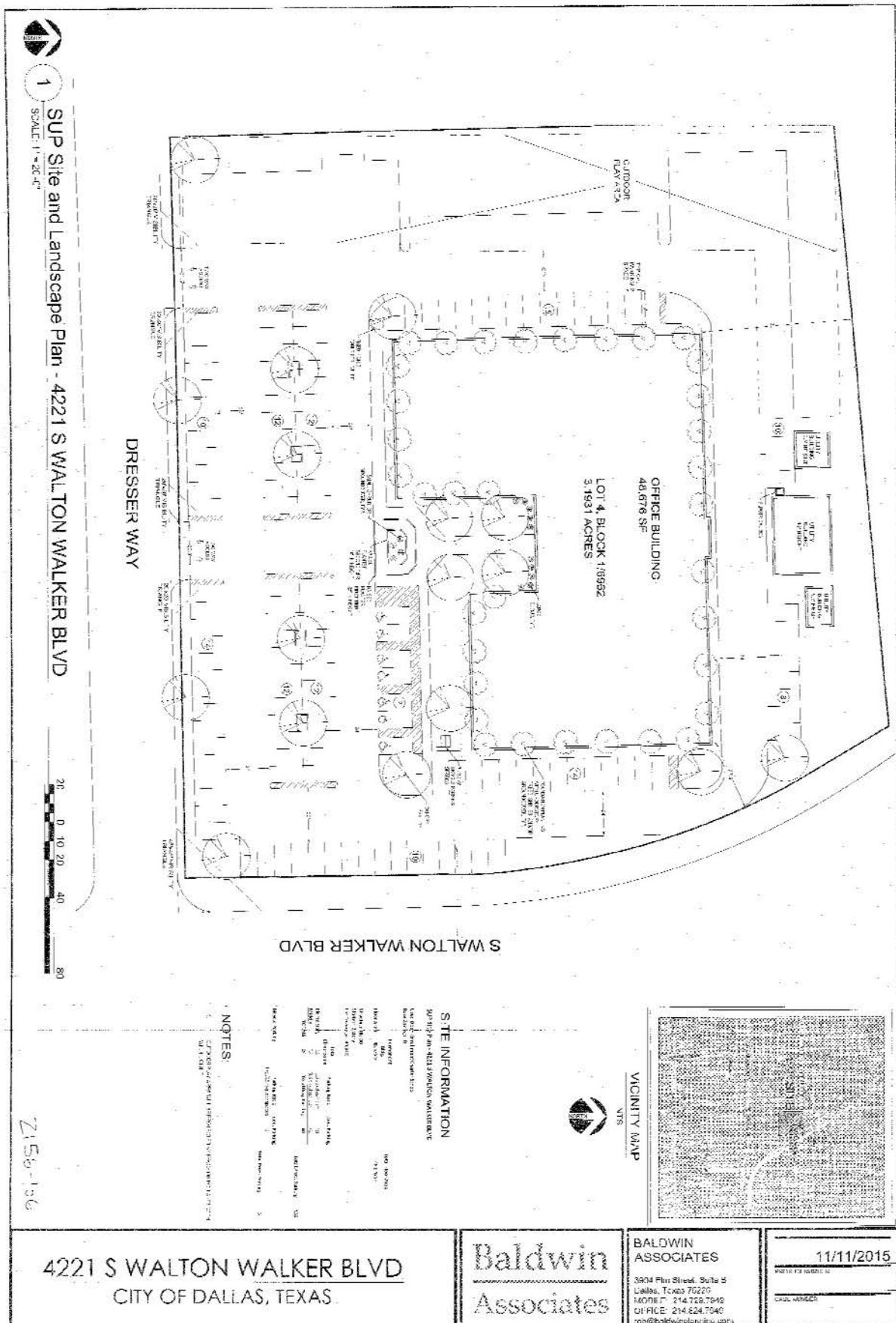
The full cooperation of all school staff members, students, and parents is crucial for the success of any traffic management plan. Proper training of school staff on duties and expectations pertaining to the plan is recommended. Sufficient communications at the beginning of each school term (and otherwise, as needed) with students and parents on their duties and expectations is also recommended.

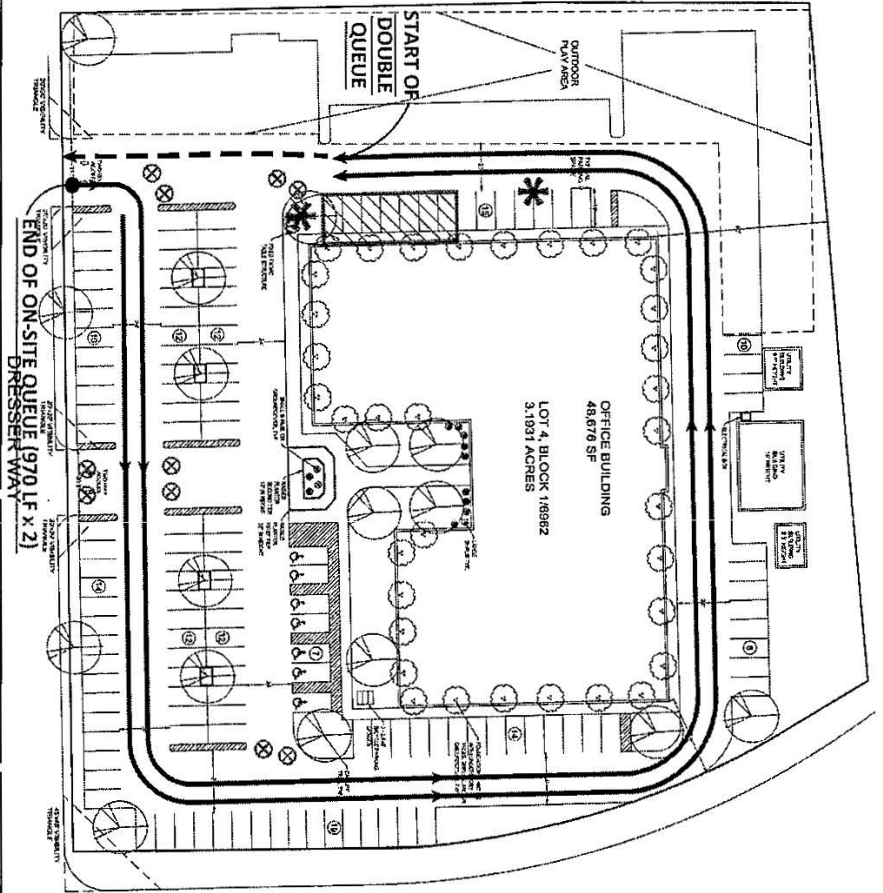
Passenger loading and unloading within public right-of-way should be avoided at all. To the extent possible, all queuing and parking should be accommodated within the school site boundaries. For circumstances where this cannot be avoided, the school should coordinate with City staff responsible for traffic operations in the area to investigate appropriate mitigation measures. Also, to minimize liabilities, no person(s) other than deputized officers of the law should engage or attempt to influence traffic operations in public right-of-way to minimize liabilities.

SUMMARY

Observations of the existing traffic management and a cursory review of carpool procedures indicate that current operations are optimal and should remain in practice in the future. This TMP is to be used by Advantage Academy to provide safe and efficient transportation of students, staff, and faculty to and from the site. The Plan was developed with the intent of optimizing safety and efficiency and the goal of accommodating vehicular traffic generated by the school at peak traffic periods within the site. The details of the TMP shall be reviewed by the school on a regular basis to confirm its effectiveness.

END OF MEMO





SCALE:



Queuing Summary

Student Group	Dismissal Times & Number of Students	Vehicle Traffic Demand
Grades 3-8th	Dismissals will be limited to 328 students with pick up periods separated by at least 15 minutes.	Provided 1,940 LF (82 cars)
Students		Required 1,927 LF (82 cars)
		Surplus 13 LF (0 cars)

*Vehicular queue calculated at 23.5 feet/passenger car based on field observations.

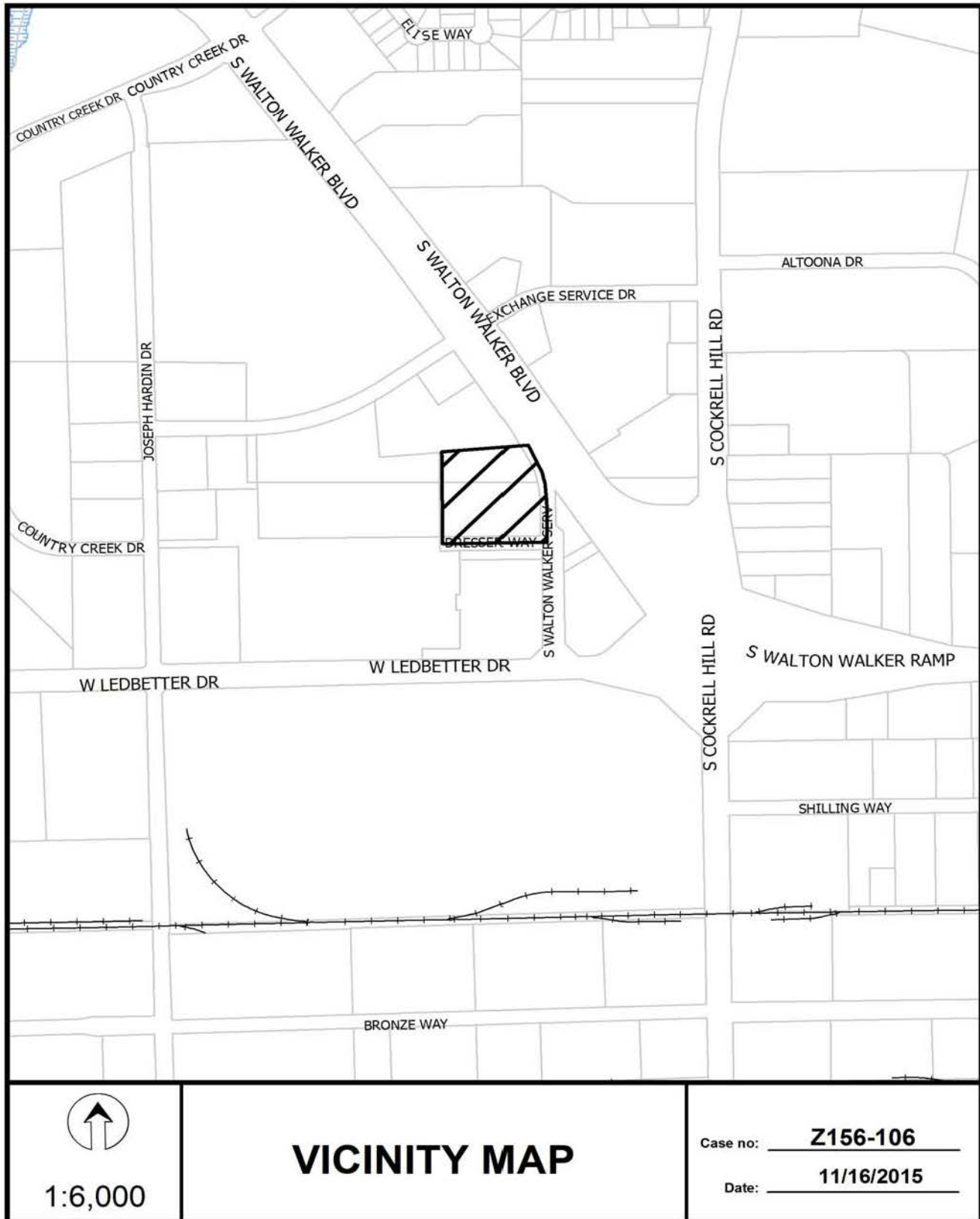
- Legend**
- * - School Staff
 - - Loading Area
 - - Queue Capacity
 - - Outbound Route
 - ⊗ - Traffic Cones

The purpose of this Traffic Management Plan (TMP) is to evaluate traffic operations that promote safety and efficient vehicle circulation. This TMP was developed to prevent queuing of drop-off/pick-up related vehicles within the city rights-of-way. The school administration should adhere to this TMP. Any deficiency due to spillover of queuing into undesignated areas of the city rights-of-way, including roadway travel lanes, should be corrected by the school immediately.

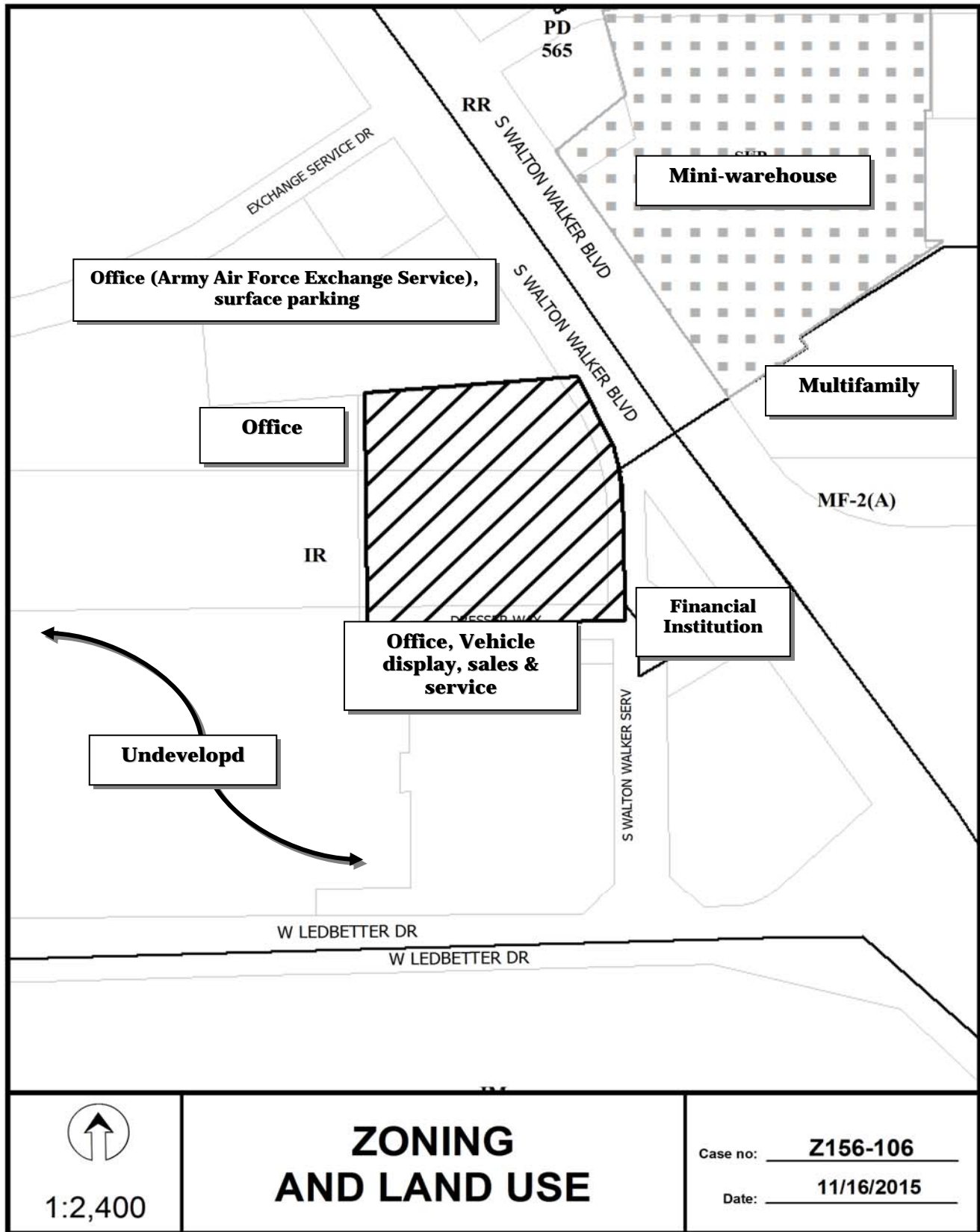
I, David Newarez, P.E. #106200, certify that the results of the queuing analysis—upon complete enforcement of this Traffic Management Plan—indicate that no queuing of vehicles will extend into City of Dallas rights-of-way as a result of internal queuing constraints during the study peak hours of school operation.

EXHIBIT 3
Traffic Management Plan (Building Capacity, 528 Students)
Advantage Academy - North Duncanville Campus
4221 S Walton Walker Blvd, Dallas, Texas.

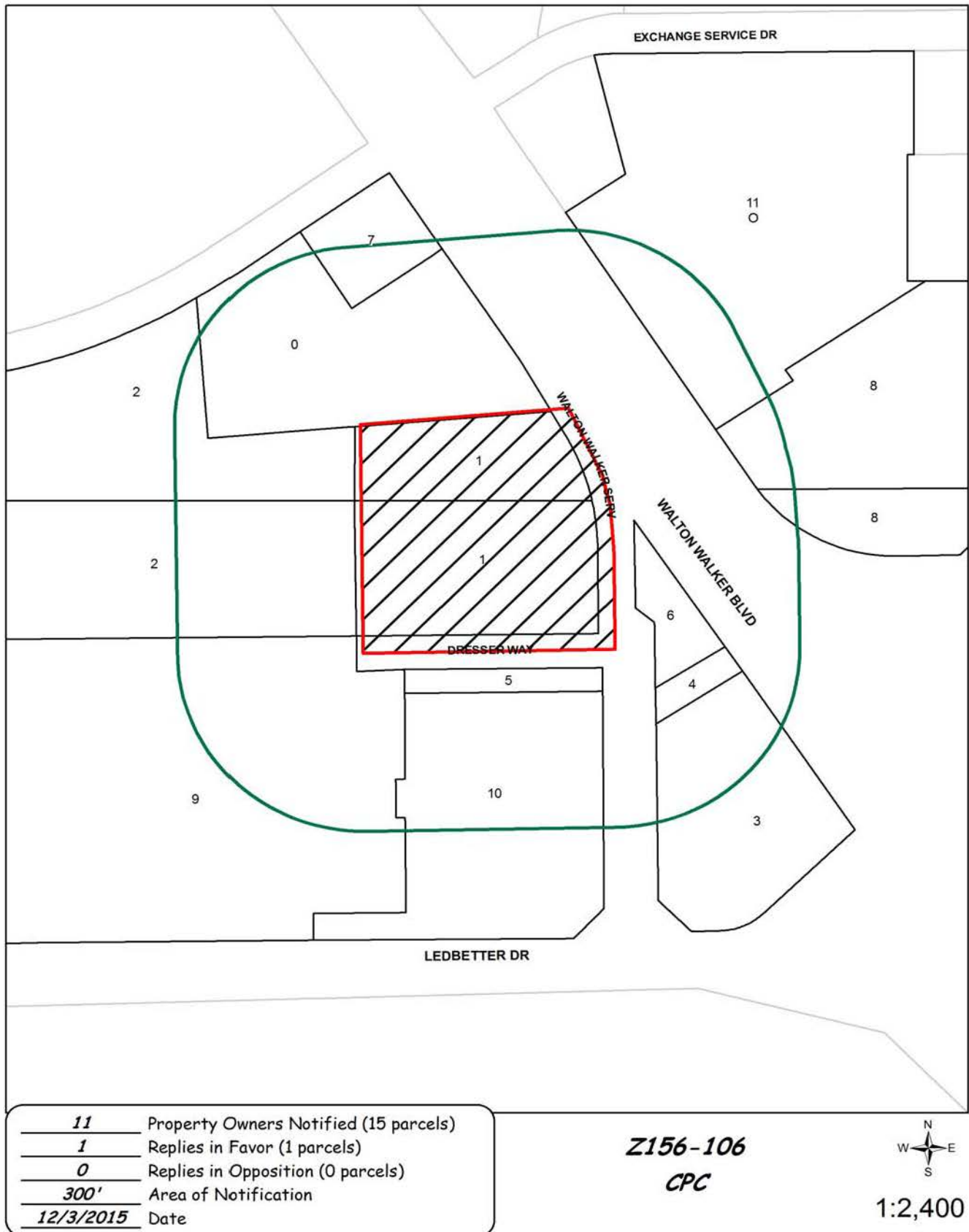
Deshazo Group, Inc.
Texas Registered Engineering Firm F-3199
400 S. Houston St. Suite 330
Dallas, Texas 75202
(214) 748-6740







CPC RESPONSES



12/02/2015

Reply List of Property Owners***Z156-106******11 Property Owners Notified******1 Property Owners in Favor******0 Property Owners Opposed***

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	1	4221 WALTON WALKER BLVD	WARREN JOHN & LAURA TRUST
	2	4542 EXCHANGE SERVICE DR	ARMY & AIR FORCE
	3	4201 WALTON WALKER BLVD	LAVENDER HILL PPITES LLLP
	4	4233 WALTON WALKER BLVD	GIFFORD HILL EMPLOYEES CU
	5	4445 W LEDBETTER DR	UCCELLO IMMOBILIEN GMBH
	6	4233 S WALTON WALKER BLVD	VALDEZ RICARDO & MATILDE
	7	4015 WALTON WALKER BLVD	ARMY & AIRFORCE EXCHANGE
	8	4101 S COCKRELL HILL RD	RIDGEWOOD TIMBER INC
	9	4601 W LEDBETTER DR	WALLACE H DALTON
	10	4445 W LEDBETTER DR	YOUNG AMERICA INSURANCE COMPANY
O	11	4116 S WALTON WALKER BLVD	SANDLIAN COLBY B & GENEVIEVE B REV TRUST

AGENDA ITEM # 46

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 6

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 22 F

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for IR Industrial/Research District and a library, art gallery or museum uses on property zoned an IR Industrial/Research District on the southeast corner of Royal Lane and Luna Road

Recommendation of Staff and CPC: Approval, subject to a development plan and conditions

Z156-108(WE)

FILE NUMBER: Z156-108(WE) **DATE FILED:** October 12, 2015

LOCATION: Southeast corner of Royal Lane and Luna Road

COUNCIL DISTRICT: 6 **MAPSCO:** 22F

SIZE OF REQUEST: Approx. 7.187 acres **CENSUS TRACT:** 99.00

APPLICANT / OWNER: Henry Morris, III

REPRESENTATIVE: John D. Blacker

REQUEST: An application for a Planned Development District for IR Industrial/Research District and a library, art gallery or museum uses on property zoned an IR Industrial/Research District.

SUMMARY: The purpose of this request is to develop a 30,900-square-foot museum. New construction will be approximately 14,000 square feet and approximately 16,900 square feet of the existing campus will be renovated to allow for exhibit space for the museum. The museum will provide for an additional lecture hall and lobby space. A library, art gallery or museum is not a permitted use in an IR Industrial/Research District. The Building Official has determined that the museum cannot be permitted as an accessory use to the office (it exceeds the maximum five percent permitted for accessory uses). Therefore, the applicant has submitted this request to permit this use with a Planned Development District.

CPC RECOMMENDATION: Approval, subject to a development plan and conditions.

STAFF RECOMMENDATION: Approval, subject to a development plan and conditions.

GUIDING CRITERIA FOR RECOMMENDATION:

Staff recommends approval based upon:

1. *Performance impacts upon surrounding property* – A library, art gallery or museum is not envisioned as an intended use in an IR District. It is a less intense use than those permitted in the IR Industrial/Research District. Furthermore, permitting this lesser intense use will not degrade the integrity of the surrounding IR District as it is limited within the constraints shown on the development plan. The expansion of the campus to include a museum will not have a negative impact on the surrounding uses.
2. *Traffic impact* – The Engineering Section of the Department of Sustainable Development and Construction has determined that the request will not have a negative impact on the street system.
3. *Comprehensive Plan or Area Plan Conformance* – The *forwardDallas! Comprehensive Plan* shows that the request site is located in an Industrial area Building Block. The request is generally consistent with this building block.
4. *Justification for PD Planned Development District Zoning as opposed to a straight zoning district* – The proposed Planned Development District will permit an additional use that is currently not permitted within the district. The PDD conditions will comply with all IR regulations, except for planting street trees along Luna Road. The applicant will comply with Article X requirements for the remaining portion of the site.

Zoning History: There have not been any zoning cases in the area over the past five years.

Thoroughfares/Streets:

Thoroughfare/Street	Type	Existing ROW	Proposed ROW
Royal Lane	Principal Arterial	100 ft.	100 ft.
Luna Road	Principal Arterial	76 ft.	107 ft.

Traffic: The Engineering Section of the Department Sustainable Development and Construction has reviewed the request and determined that it will not have a detrimental impact on the surrounding street system.

COMPREHENSIVE PLAN: The *forwardDallas! Comprehensive Plan* was adopted by the City Council in June 2006. The *forwardDallas! Comprehensive Plan* outlines several goals and policies which can serve as a framework for assisting in evaluating the applicant's request. The Plan identifies the request site as being within an Industrial Area Building Block.

Industrial Areas, which offer important employment opportunities, occupy large areas of land and usually are near major roads and heavy rail lines. Evolving technology and the need for freight movement through Dallas to the rest of the country and internationally means this sector can offer good opportunities for jobs. Logistics and warehousing, a growing industry with strong potential for upward mobility of skilled workers, would thrive in such areas. Examples include Southport and the Agile Port, parts of West Dallas along I-30, and the Stemmons industrial area. These areas include a mix of low- and medium-density industrial buildings and industrial yards and have large surface parking for cars and trucks. Industrial Areas rely on quality road access and may be linked to rail for freight purposes. Street lanes are wide and intersections are large. Transit, sidewalks and other pedestrian improvements are limited.

Land Use:

	Zoning	Land Use
Site	IR	Office, Warehouse
North	City of Farmers Branch	Office
South	IR	Landscaping Company
East	IR	Retail
West	IR w/ floodplain	Golf Course

Land Use Compatibility: The request site is currently developed with a one-story, 52,005-square-foot office building. The applicant proposes to renovate approximately 16,900 square feet of floor area within an existing structure to allow for an exhibit space for a museum. This renovated space is located within the northeastern portion of the existing building. In addition, the site will be developed with a new 14,000-square-foot museum that will consist of a new atrium space, a 30-foot diameter dome planetarium, an additional lecture hall, and lobby space.

The surrounding uses consist of various retail and commercial and business uses. The City of Farmers Branch is located north of the site, across Royal Lane. A property west of the site, across Luna Road is a public golf course.

Development Standards:

<u>DISTRICT</u>	<u>SETBACKS</u>		Density	Height	Lot Coverage	Special Standards	PRIMARY Uses
	Front	Side/Rear					
IR - existing Industrial research	15'	30' adjacent to residential OTHER: No Min.	2.0 FAR overall 0.75 office/ retail 0.5 retail	200' 15 stories	80%	Proximity Slope Visual Intrusion	Industrial, wholesale distribution & storage, supporting office & retail
IR - Proposal Industrial research	15'	30' adjacent to residential OTHER: No Min.	2.0 FAR overall 0.75 office/ retail 0.5 retail	200' 15 stories	80%	Proximity Slope Visual Intrusion	Industrial, wholesale distribution & storage, supporting office & retail, Library, art gallery or museum

Landscaping: Landscaping of any development will be in accordance with Article X, with the exception of the requirement for street trees along Luna Road frontage. The

applicant would have to setback farther on the site (removing excess parking) to provide the trees due to the utility easement. No alternative has been provided.

Parking: The parking requirement for a library, art gallery or museum is one space per 500 square feet of floor area. The number of off-street parking spaces that is required for the 14,000 square foot library, art gallery or museum is 28 spaces. As for a warehouse use, the off-street parking requirement is one space per 1,000 square feet of floor area up to 20,000 square feet. The existing 13,525 square foot warehouse requires 14 spaces. The applicant will provide approximately 177 parking spaces on site to accommodate the new and existing developments.

CPC Action (December 17, 2015)

Motion: It was moved to recommend **approval** of a Planned Development District for IR Industrial/Research District and a library, art gallery or museum uses, subject to a development plan and revised conditions to include a maximum 30,900 sq. ft. of floor area on property zoned an IR Industrial/Research District on the southeast corner of Royal Lane and Luna Road.

Maker: Anantasomboon

Second: Shidid

Result: Carried: 15 to 0

For: 15 - Anglin, Emmons, Houston, Davis*, Shidid,
Anantasomboon, Abtahi, Haney, Jung,
Housewright, Schultz*, Peadon*, Murphy,
Ridley, Tarpley

Against: 0

Absent: 0

Vacancy: 0

*out of the room, shown voting in favor

Notices: Area: 500

Mailed: 29

Replies: For: 1

Against: 0

Speakers: None

CPC PROPOSED PDD CONDITIONS

SEC. P-____.101. LEGISLATIVE HISTORY.

PD was established by Ordinance No. _____, passed by the Dallas City Council on _____.

SEC.P-____.102. PROPERTY LOCATION AND SIZE.

PD ____ is established on property located on the southeast corner of Luna Road and Royal Lane. The size of PD ____ is approximately 7.187 acres.

SEC. P-____.103 DEFINITIONS AND INTERPRETATIONS.

(a) Unless otherwise stated, the definitions in Chapter 51A apply to this article.

(b) Unless otherwise stated, all references to articles, divisions, or sections in this article are to articles, divisions, or sections in Chapter 51A.

(C) This district is considered to be a non-residential zoning district.

SEC. P-____.104 EXHIBITS.

The following exhibit is incorporated into this division:

Exhibit ____A: development plan.

SEC. P-____.105. DEVELOPMENT PLAN

Development and use of the Property must comply with the development plan (Exhibit ____). In the event of a conflict between the provisions of this article and the development plan, the provisions of this article control.

SEC. P-____.106. MAIN USES PERMITTED.

(a) Except as provided in this subsection, the only main uses permitted are those main uses permitted in the IR Industrial Research zoning district, subject to the same conditions applicable in the IR Industrial Research District, as set out in Chapter 51A. For example, a use permitted in the IR Industrial Research Zoning District only by specific use permit (SUP) is permitted in this district only by SUP; a use subject to development impact review (DIR) in the IR Industrial Research zoning district is subject to DIR in this district, etc.

(b) Library, art gallery or museum is permitted by right

SEC. P-____.107. ACCESSORY USES.

As a general rule, an accessory use is permitted in any district in which the main use is permitted. Some specific accessory uses, however, due to their unique nature, are subject to additional regulations in Section 51A-4.217. For more information regarding accessory uses, consult Section 51A-4.217.

The following accessory uses are not permitted in this district:

- Accessory community center (private)
- Accessory pathological waste incinerator
- Home occupation
- Private stable

In this district, an SUP may be required for the following accessory uses:

- Accessory medical / infectious waste incinerator [*See Section 51A-4.217 (3.1).*]

SEC.P-____.108. YARD, LOT, AND SPACE REGULATIONS.

(Note: The yard, lot, and space regulations in this section must be read together with the yard, lot and space regulations in Part I of this article. If there is a conflict between this section and Part I of this article, this section controls.)

(a) In general. Except as provided in this section, the yard, lot, and space regulations for the IR Industrial Research District apply.

(b) Height. For a Library, art gallery or museum, maximum height is 45 feet.

(c) Floor area. For a Library, art gallery or museum, maximum floor area is 30,900 square feet.

SEC. P-____.109. OFF-STREET PARKING AND LOADING.

In general. Except as provided in this section, consult the use regulations in Division 51A-4.200 for the specific off-street parking and loading requirements for each use.

SEC. P-____.110. ENVIRONMENTAL PERFORMANCE STANDARDS.

See Article VI.

SEC. P-____.111. LANDSCAPING.

(a) Landscaping must be provided in accordance with Article X.

(b) Street trees will not be provided along the Luna Road frontage.

- (c) All plant materials must be maintained in a healthy, growing condition.

SEC. P-____.112. SIGNS.

(a) In general. Except as provided in this section, signs must comply with the provisions for non-business zoning districts in Article VII.

SEC. P-____.113. ADDITIONAL PROVISIONS.

(a) In general.

(1) The Property must be properly maintained in a state of good repair and neat appearance.

(2) Development and use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the city.

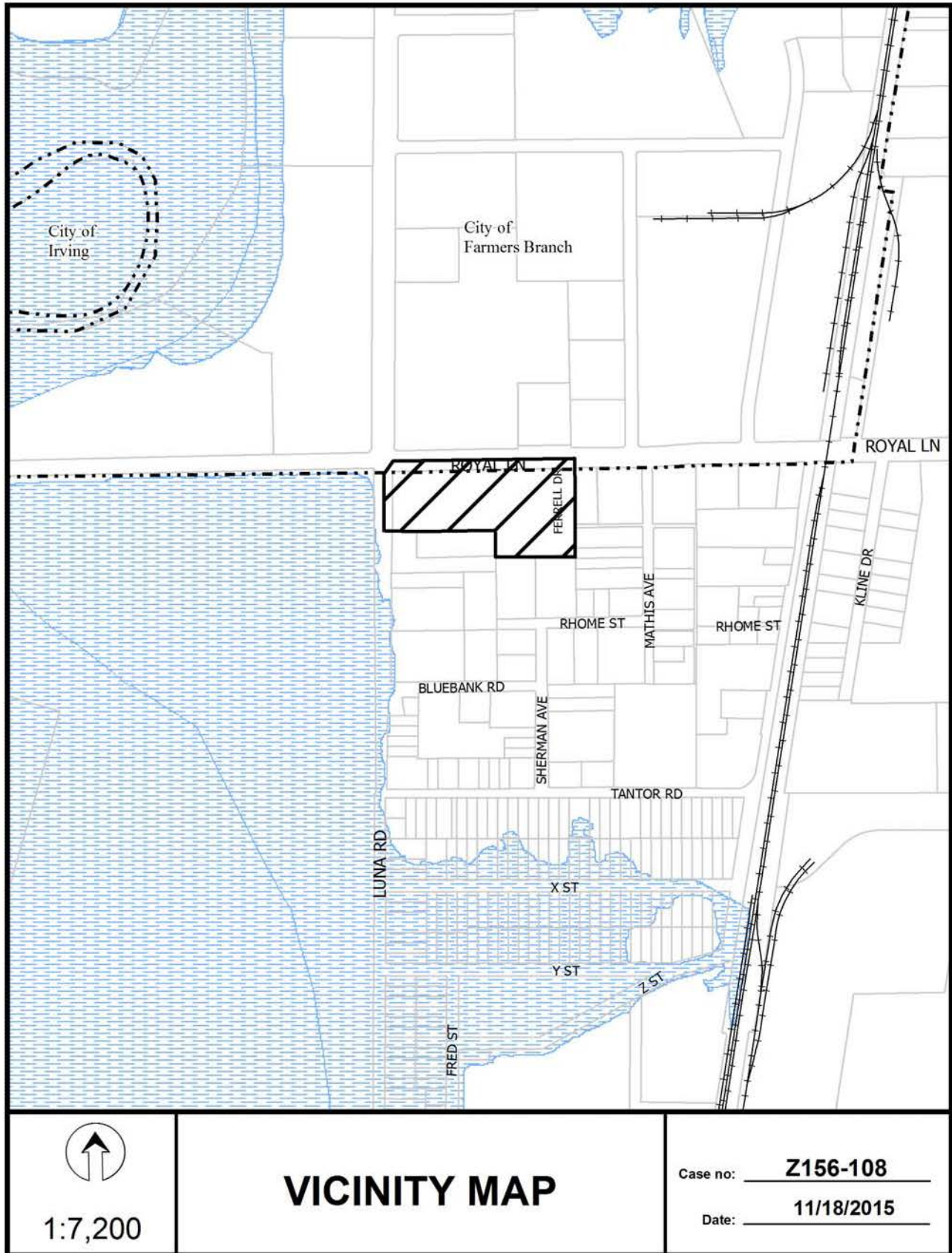
(3) Development and use of the Property must comply with this article.

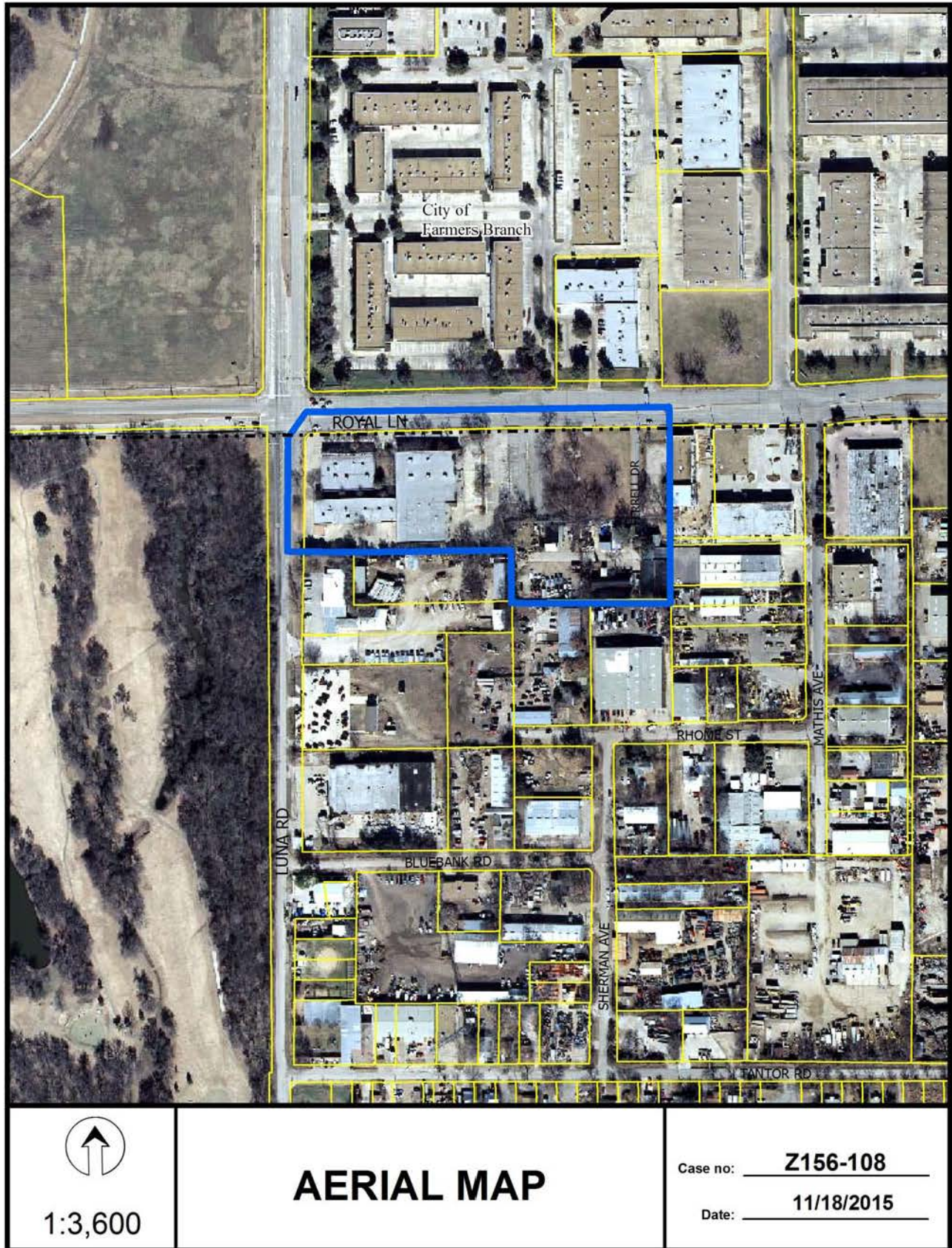
SEC. P _____.114. COMPLIANCE WITH CONDITIONS.

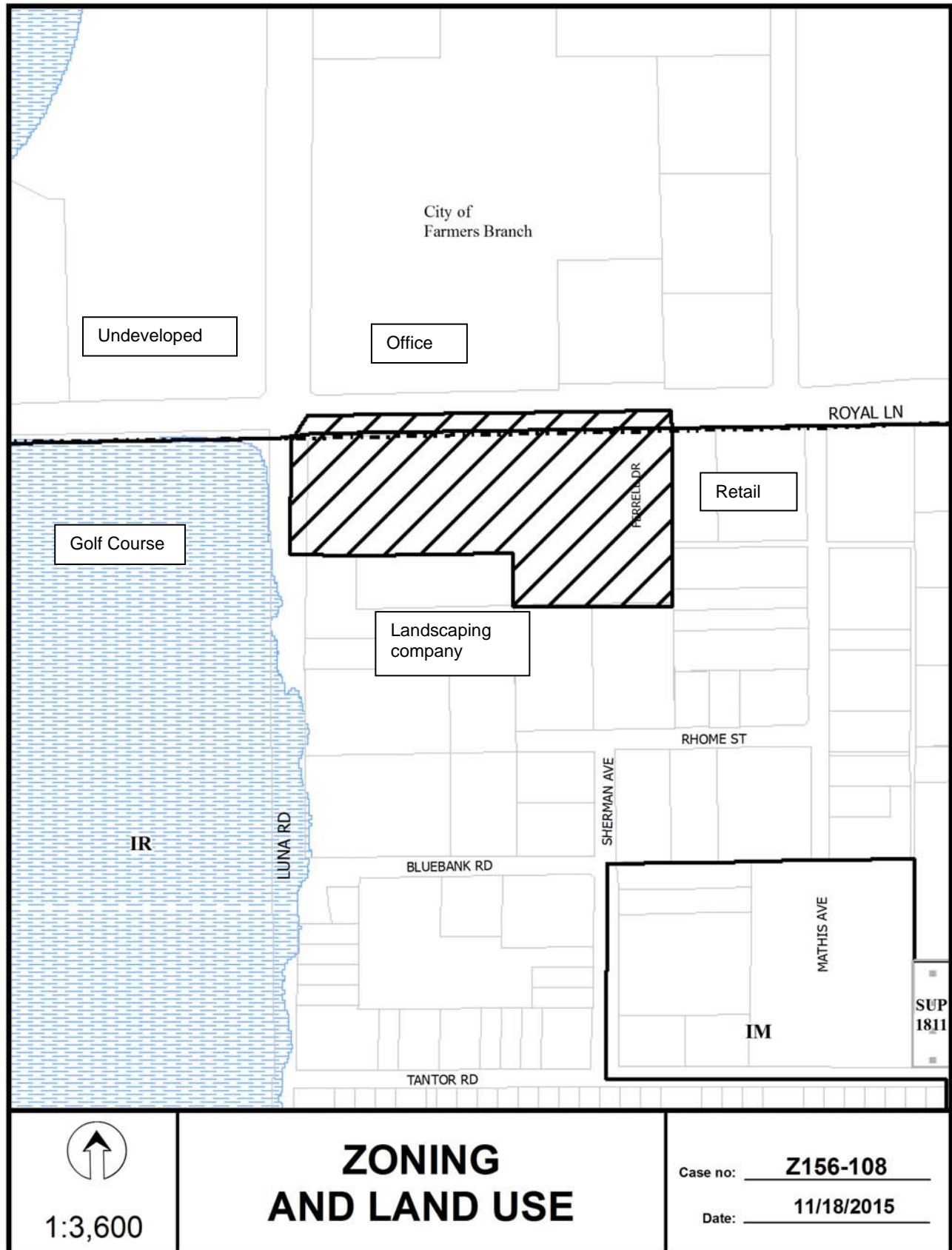
(a) All paved areas, permanent drives, streets, and drainage structures, if any, must be constructed in accordance with standard city specifications, and completed to the satisfaction of the city.

(b) The building official shall not issue a building permit or a certificate of occupancy for a use in this planned development district until there has been full compliance with this division, the Dallas Development Code, the construction codes, and all other ordinances, rules, and regulations of the city.

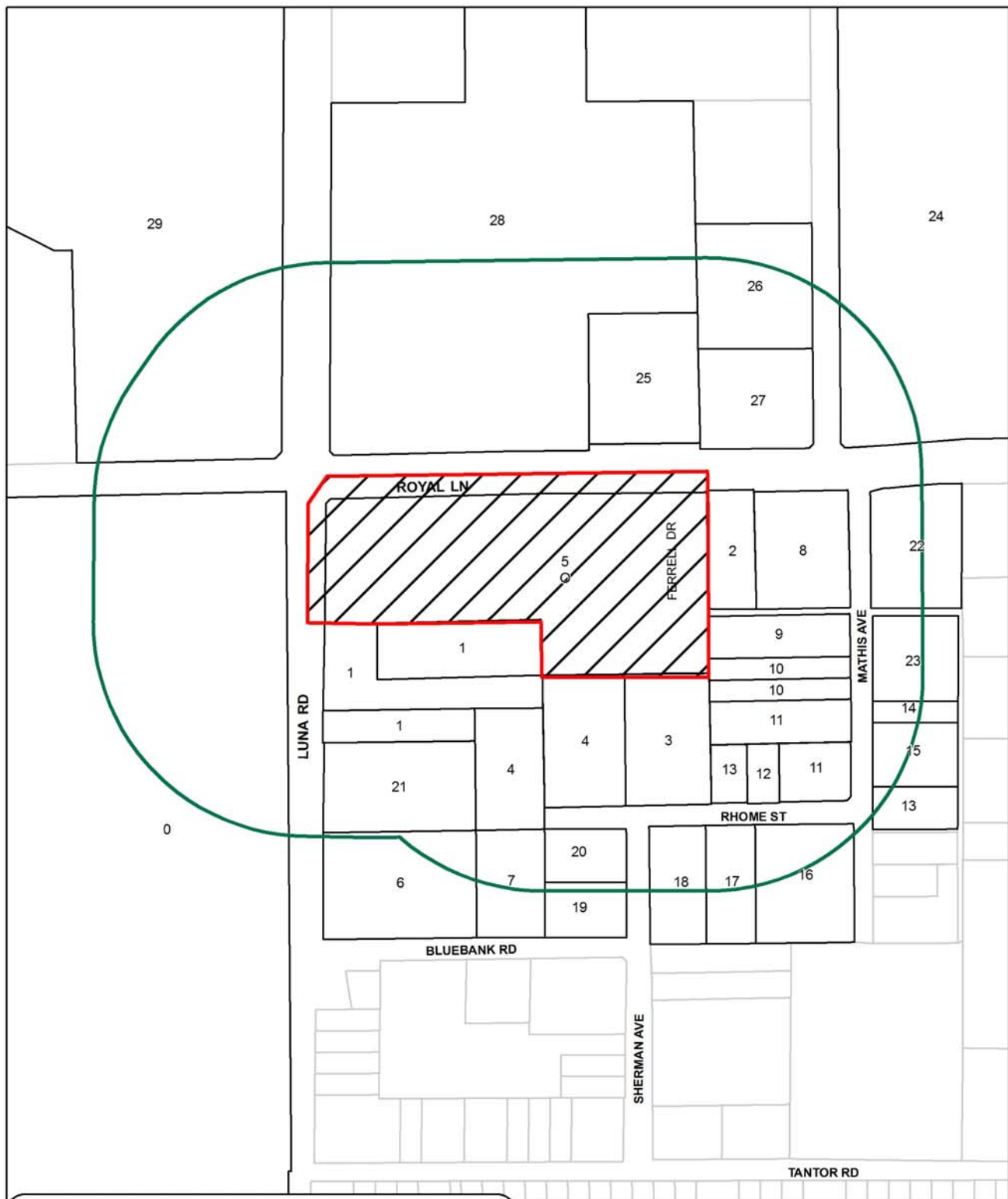








CPC RESPONSES



<u>29</u>	Property Owners Notified (36 parcels)
<u>1</u>	Replies in Favor (1 parcels)
<u>0</u>	Replies in Opposition (0 parcels)
<u>500'</u>	Area of Notification
<u>12/17/2015</u>	Date

Z156-108
CPC



1:3,600

Notification List of Property Owners

Z156-108

29 Property Owners Notified 1 Property Owners in Favor 0 Property Owners Opposed

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	1 11350	LUNA RD	M & D JACOBSON PPTIES LTD
	2 1878	ROYAL LN	GSMHJ REALTY INC
	3 1715	RHOME ST	ASAM INVESTMENTS LLC
	4 1645	RHOME ST	PROTON PROPERTIES LLC
O	5 1830	ROYAL LN	INSTITUTE FOR CREATION
	6 11330	LUNA RD	LUNA ROAD PS THE
	7 1617	BLUEBANK RD	DESHAZER MIKE & LINDA
	8 1894	ROYAL LN	OSEE PROPERTIES LLC
	9 11353	MATHIS ST	HOWCO REALTY LLC
	10 11343	MATHIS ST	ANDERSON PAVING INC
	11 11339	MATHIS ST	ANDERSON PAVING INC
	12 1743	RHOME ST	MCGILL RICHARD D
	13 1737	RHOME ST	CIRCLE G A TEXAS
	14 11340	MATHIS ST	NISSI DEVELOPMENT GROUP INC
	15 11332	MATHIS ST	NISSI DEVELOPMENT GROUP INC
	16 1746	RHOME ST	ALLIANCE SERVICES INC
	17 1736	RHOME ST	RHOME INV PPTIES INC
	18 11324	SHERMAN AVE	ROMERO JOSE ADALBERTO & ANGELA I
	19 11307	SHERMAN AVE	GOODALL MICHAEL A ET AL
	20 11325	SHERMAN AVE	DESHAZER MICHAEL S &
	21 11336	LUNA RD	PROTON ENTERPRISES LLC
	22 1908	ROYAL LN	KIM AND RYOU PROPERTY LLC
	23 11356	MATHIS ST	R & C ANDERSON INVESTMENT
	24 1901	ROYAL LN	AMB PROPERTY II LP
	25 1805	ROYAL LN	PCCP DALLAS ACQUISITIONS I
	26 11419	FERRELL DR	RIVERBEND DFW INDUSTRIAL

Z156-108(WE)

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	27 11431	FERRELL DR	DHS REAL ESTATE GROUP LLC
	28 1801	ROYAL LN	PAR CAPITAL WESTWOOD LLC
	29 1715	ROYAL LN	WATERS EDGE APARTMENTS PHASE II LLC

AGENDA ITEM # 47

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 46 E

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting an NS(A) Neighborhood Service District and a resolution accepting deed restrictions volunteered by the applicant on property zoned an NO(A) Neighborhood Office District, on the southwest side of North Peak Street, southeast of Gaston Avenue

Recommendation of Staff: Approval

Recommendation of CPC: Approval with deed restrictions volunteered by the applicant

Z156-111(OTH)

FILE NUMBER: Z156-111(OTH)

DATE FILED: October 12, 2015

LOCATION: Southwest side of North Peak Street, southeast of Gaston Avenue

COUNCIL DISTRICT: 2

MAPSCO: 46E

SIZE OF REQUEST: Approx. 0.3749 acres

CENSUS TRACT: 15.02

APPLICANT: Spanish House LLC

OWNER: 4 Golden Properties, LLC

REPRESENTATIVE: Rob Baldwin

REQUEST: An application for an NS(A) Neighborhood Service District with deed restrictions volunteered by the applicant on property zoned an NO(A) Neighborhood Office District.

SUMMARY: The applicant proposes to utilize the property for offices and to teach Spanish classes to adults [Spanish House], which is classified as a personal service use. However, all uses within the NS(A) Neighborhood Service District would be permitted with the exception of those prohibited with the volunteered deed restrictions.

CPC RECOMMENDATION: Approval with deed restrictions volunteered by the applicant.

STAFF RECOMMENDATION: Approval

GUIDING CRITERIA FOR STAFF RECOMMENDATION:

Staff recommends approval based upon:

1. *Performance impacts upon surrounding property* – The proposed zoning district is a good transition between the commercial property to the north, west and east, and the residential to the east and south.
2. *Traffic impact* – The proposed zoning will not have a negative impact on the existing street system.
3. *Comprehensive Plan or Area Plan Conformance* – The forwardDallas! Comprehensive Plan identifies the area as an Urban Corridor Building Block. A form district has not been recommended due to the size of the parcel.

Thoroughfares/Streets:

Thoroughfare/Street	Type	Existing ROW
North Peak Street	Principal Arterial	60 feet

Traffic:

The Engineering Section of the Department of Sustainable Development and Construction reviewed the proposed zoning and determined it will not have a negative impact on the existing street system.

STAFF ANALYSIS:

Comprehensive Plan:

The forwardDallas! Comprehensive Plan was adopted by the City Council in June 2006. The forwardDallas! Comprehensive Plan outlines several goals and policies which can serve as a framework for assisting in evaluating the applicant's request. The Plan classifies the area as an Urban Corridor Building Block.

Urban corridor areas will offer a wide range of housing options including single-family detached dwellings, condos and townhomes. The forward Dallas project plans to develop various shops and offices in these areas so that the residents can get all necessary services and job opportunities within the walking distance. Also, various parks, pathways, pedestrian oriented landscaping, transit stops and road will be developed.

The proposed zoning change complies with the intent of the Urban Corridor areas established in the forwardDallas! Comprehensive Plan.

Surrounding Land Uses:

	Zoning	Land Use
Site	NO(A)	Unoccupied structure
North	PD No. 98, PD No. 298 & H/65	Retail, multifamily
East	PD No. 98 & H/65	Single family
South	PD No. 98 & H/65	Single family
West	PD No. 298	Single Family, retail

Land Use Compatibility:

The request site is approximately 0.3749 acres of land and is currently developed with a 5,000 square foot unoccupied structure. The existing zoning of the property is NO(A) Neighborhood Office District. The building will be reduced to 4,000 square feet to provide more parking spaces. The structure will include a maximum of five rooms for individual or group Spanish classes. Operations for the Spanish classes will be from 8:00 a.m. – 9:00 p.m., Monday thru Friday and 8:00 a.m. to 3:00 p.m. on Saturday. The office use will be from 8:00 a.m. to 5:00 p.m. Monday thru Friday.

The property is surrounded by retail and multifamily uses to the north, multifamily and single family to the east; single family to the south; and retail and single family to the west.

Development Standards:

DISTRICT	Setbacks		Density	Height	Lot Coverage	Special Standards	Primary Uses
	Front	Side/Rear					
Existing							
NO(A) Neighborhood Office	15'	20' adjacent to residential OTHER: No Min.	0.5 FAR	30' 2 stories	50%	Proximity Slope Visual Intrusion	Office
Proposed							
NS(A) Neighborhood service	15'	20' adjacent to residential OTHER: No Min.	0.5 FAR	30' 2 stories	40%		Retail & personal service, office

Parking:

Pursuant to the Dallas Development Code, off-street and loading required parking must be provided in accordance with Division 51A-4.200 for the specific off-street parking and loading requirements for each use. The existing building will be reduced in size to accommodate required parking.

Z156-111(OTH)

Landscaping:

All landscaping must comply with the requirements of Article X.

CPC ACTION:
December 3, 2015

Motion: It was moved to recommend **approval** of an NS(A) Neighborhood Service District with deed restrictions volunteered by the applicant for the following main uses to be prohibited: 1) General merchandise or food store 3,500 sq. ft. or less, 2) Dry Cleaning or laundry store, 3) Motor vehicle fueling station, and 4) Tower or antenna for cellular communication on property zoned an NO(A) Neighborhood Office District, on the southwest side of North Peak Street, southeast of Gaston Avenue.

Maker: Emmons
Second: Abtahi
Result: Carried: 14 to 0

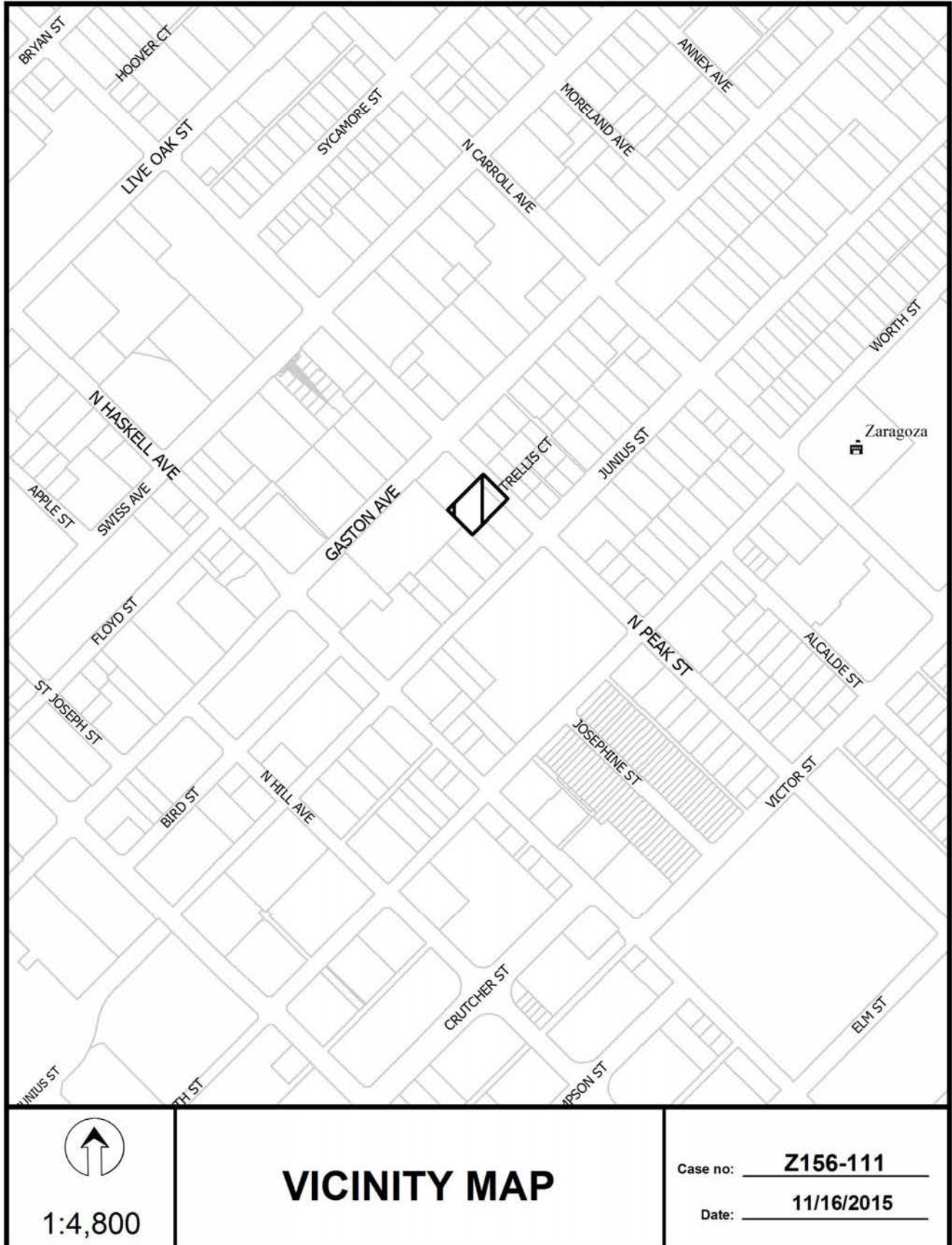
For: 14 - Anglin, Emmons, Houston, Davis, Shidid, Abtahi,
Haney, Jung, Housewright*, Schultz, Peadon,
Murphy, Ridley, Tarpley

Against: 0
Absent: 1 - Anantasomboon
Vacancy: 0

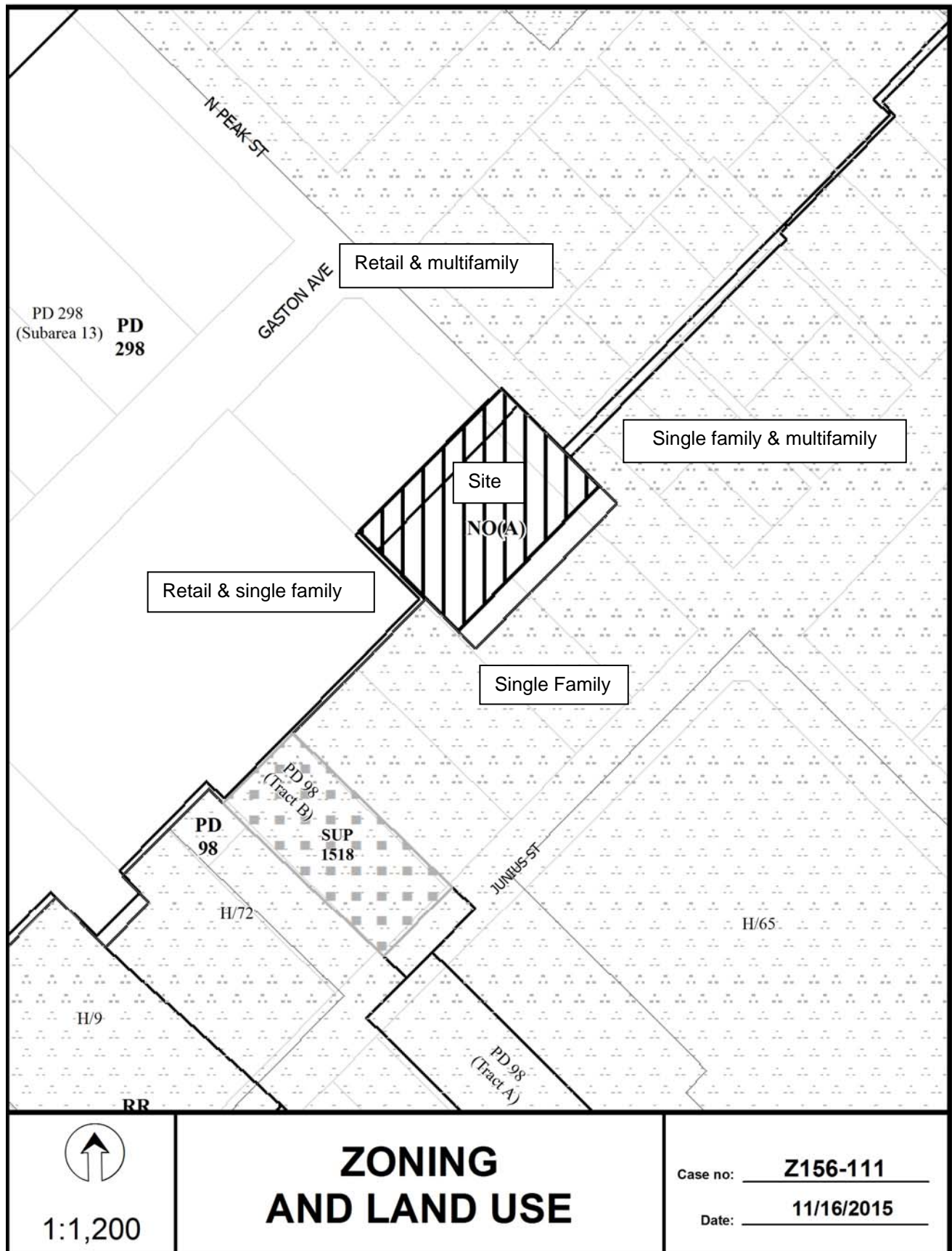
*out of the room, shown voting in favor

Notices: Area: 200 Mailed: 20
Replies: For: 0 Against: 1

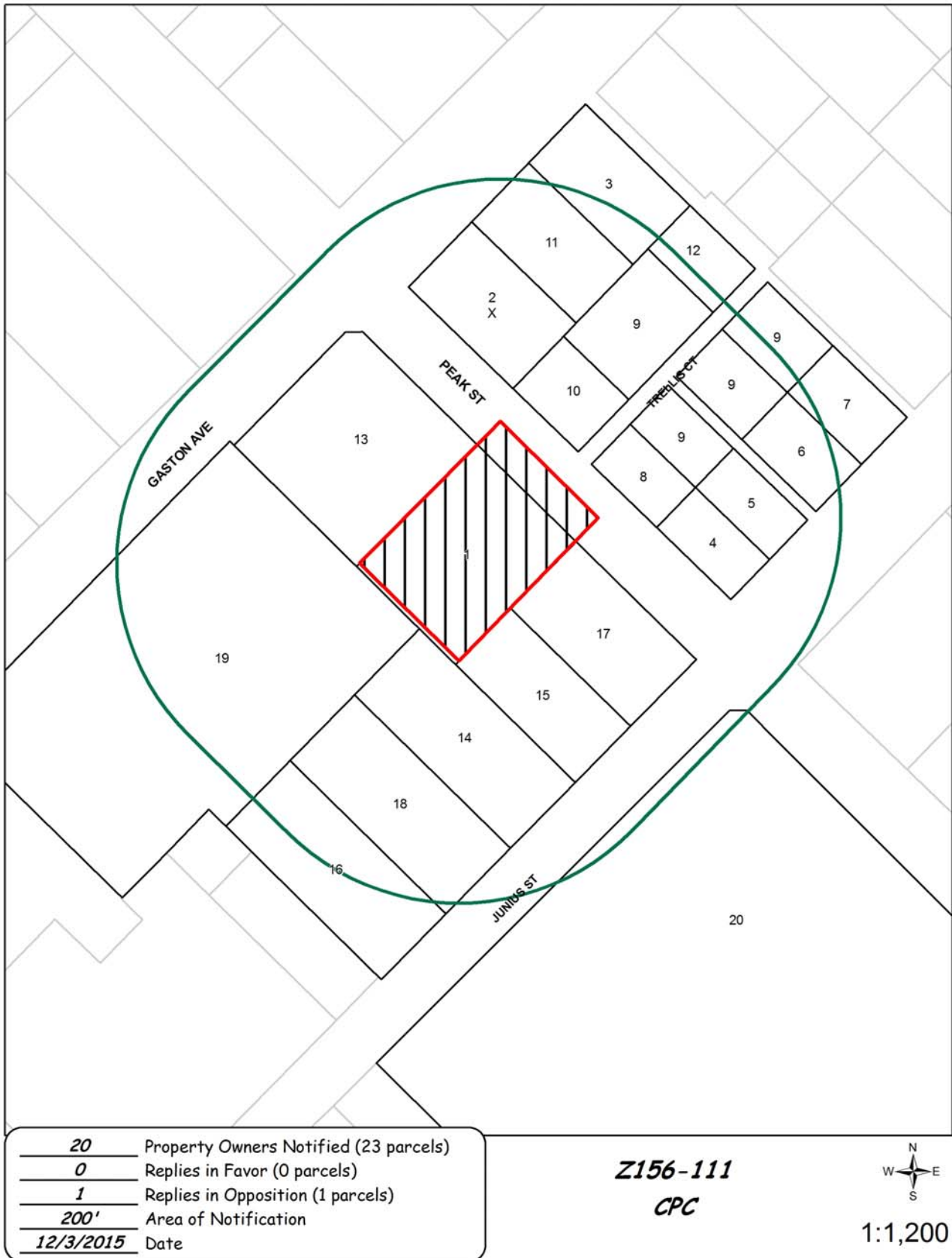
Speakers: For: Rob Baldwin, 3904 Elm St., Dallas, TX, 75226
Against: None







CPC RESPONSES



12/02/2015

Reply List of Property Owners
Z156-111

20 Property Owners Notified 0 Property Owners in Favor 1 Property Owners Opposed

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	1 801	N PEAK ST	4 GOLDEN PROPERTIES LLC
X	2 4302	GASTON AVE	RENDE VINCENT
	3 4310	GASTON AVE	CLAYTON SIMS HOLDINGS LLC
	4 4303	JUNIUS ST	DUKE WALKER &
	5 4307	JUNIUS ST	PEREZ EMMANUEL
	6 4311	JUNIUS ST	WRIGHT SHARON S
	7 4313	JUNIUS ST	DULANEY DESERIE A
	8 4302	TRELLIS CT	BELLIEU MICAH
	9 4306	TRELLIS CT	BROUNOFF ZELMAN
	10 802	N PEAK ST	DEJULIAN DEYSI MEDELIA
	11 4306	GASTON AVE	BUCHANAN JO ANN
	12 4315	TRELLIS CT	CHEVALIER LEE
	13 4210	GASTON AVE	FRIS CHKN LLC % CAJUN OPERATING CO
	14 4209	JUNIUS ST	HENLEY HUDSON PPTIES PORFOLIO I LLC
	15 4215	JUNIUS ST	EPPERSON MARVA Y
	16 4125	JUNIUS ST	PENFOLD CRAIG
	17 4219	JUNIUS ST	BAXAVANIS NICHOLAS &
	18 4203	JUNIUS ST	COMBE GEORGE
	19 4120	GASTON AVE	ALDI TX LLC
	20 629	N PEAK ST	EAST DALLAS CHRISTIAN

AGENDA ITEM # 48

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 5

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 59 L

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting an amendment and an expansion of Planned Development District No. 805 on property zoned Planned Development District No. 805 with a D-1 Liquor Control Overlay and a CR Community Retail District with a D Liquor Control Overlay, generally on the northeast corner of Lake June Road and North Masters Drive

Recommendation of Staff and CPC: Approval, subject to a revised development plan and conditions

Z156-113(SM)

FILE NUMBER: Z156-113(SM)

DATE FILED: October 14, 2015

LOCATION: Northeast corner of Lake June Road and North Masters Drive

COUNCIL DISTRICT: 5

MAPSCO: 59-L

SIZE OF REQUEST: Approx. 16.4 acres

CENSUS TRACT: 119.00

APPLICANT/OWNER: NEC Lake June & Masters, LP

REPRESENTATIVE: Karl A. Crawley, Masterplan Consultants

REQUEST: An application for an amendment and an expansion of Planned Development District No. 805 on property zoned Planned Development District No. 805 with a D-1 Liquor Control Overlay and a CR Community Retail District with a D Liquor Control Overlay.

SUMMARY: The applicant proposes to modify the sign regulations to allow three additional detached signs in the interior of the property, to expand the PDD to a recently acquired corner and to add two new monument signs to the expansion. The property is developed with existing retail, restaurant, and personal service uses in a series of one-story buildings that have existed since 1985. A drive through financial institution was constructed along Lake June Road in 2011. The shopping center was renovated over the past 15 years. The application proposes no significant changes to the existing development.

CPC RECOMMENDATION: **Approval**, subject to a revised development plan and conditions.

STAFF RECOMMENDATION: **Approval**, subject to a revised development plan and conditions.

GUIDING CRITERIA FOR STAFF RECOMMENDATION:

Staff recommends approval based upon:

1. *Performance impacts upon surrounding property* – The proposed modifications to the sign regulations and PDD expansion are not foreseen to cause negative impacts upon the surrounding property. The request to add signs that are not normally allowed in Article VII specifies the location of these additional signs to approximately 370 to 420 feet from the nearest eastbound traffic on Lake June Road and are not visible from North Masters drive because of intervening buildings. Therefore, the placement of the signs will not impact the surrounding properties but rather provides orientation for patrons internal to the area of request.
2. *Traffic impact* – The proposed changes to the development standards will not have an impact on traffic.
3. *Comprehensive Plan or Area Plan Conformance* – The *forwardDallas! Comprehensive Plan* shows that the request site is located in a Multi-Modal Corridor Building Block. This request is consistent with the building block.
4. *Justification for PD Planned Development District Zoning as opposed to a straight zoning district* – The modifications to the sign regulations require an amendment to the current planned development district.

BACKGROUND INFORMATION:

- The shopping center was constructed in 1985 and 1986 according to permit records.
- On March 25, 2009, PDD No. 805 was approved by City Council.
- In 2009, several interior and exterior renovation permits were completed.
- On November 1, 2011, a new drive through financial institution was constructed within the PDD.

Zoning History: There have been four recent zoning changes in the area within the last five years.

1. **Z112-239:** On August 22, 2012, City Council approved an application for a D-1 Liquor Control Overlay and Specific Use Permit No. 1987 for a two-year period for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned a CR-D Community Retail with a D Liquor Control Overlay.

2. **Z112-245:** On September 26, 2012, City Council approved application for a D-1 Liquor Control Overlay and Specific Use Permit No. 1983 for a two-year period with eligibility for automatic renewals for five-year periods for the sale of alcoholic beverages in conjunction with a general merchandise or food store use greater than 3,500 square feet on property zoned a CR-D Community Retail District with a D Liquor Control Overlay.
3. **Z134-256:** Specific Use Permit No. 1983 was issued a renewal until September 26, 2019.
4. **Z145-252:** An automatic renewal is pending for Specific Use Permit No. 1809 was from the previous renewal until August 25, 2015.

Thoroughfares/Streets:

Thoroughfare/Street	Type	Existing Dimension
Lake June Road	Principal Arterial	Minimum-6 lanes-Divided, 100' ROW
Masters Drive	Principal Arterial	Minimum-6 lanes-Divided, 100' ROW

STAFF ANALYSIS:

Comprehensive Plan:

The *forwardDallas!* Comprehensive Plan designates the area of request in the Multi-modal corridor.

This Building Block should encourage the redevelopment of aging auto-oriented single family neighborhoods. These areas offer dense mixed use and then transition to multi-family and single family housing at the edge. These corridors should diminish quickly in scale, density, and intensity away from the corridor, respecting existing single family neighborhoods while maintaining a strong focus on transit-orientation and access.

Surrounding Land Uses:

	Zoning	Land Use
Site	PDD 805	Retail, restaurant, and personal services
North	CR and TH-3(A)	Townhouses
East	R-7.5(A)	Single family

South	CR	Retail, restaurant, and personal services
West	CR	Child-care facility, retail, restaurant, & medical office

Land Use:

The applicant proposes two amendments with the request: (1) to allow an additional three detached signs, not to exceed 30 feet above grade and 200 square feet in effective area and situated towards the interior of the property; and (2) to allow two additional monument signs on the parcel that is requested to be added to the existing PDD. Because the first request limits the new signs approximately 300 feet north of the Lake June Road right-of-way and is recessed within the existing building facades, these signs primarily advertise to visitors of the shopping center, rather than motorists on the adjoining streets.

Since the land uses are not proposed to change, the request to allow an additional three detached signs, not to exceed 30 feet above grade and 200 square feet in effective area and situated towards the interior of the property is compatible with surrounding land uses. Additionally, the request to add signs that are not normally allowed in Article VII specifies the location of these additional signs to approximately 370 to 420 feet from the nearest eastbound traffic on Lake June Road and are not visible from North Masters drive because of intervening buildings. Therefore, the placement of the signs will not impact the surrounding properties but rather provides orientation for patrons internal to the area of request.

The second request is to allow the shopping center expansion an additional two monument signs, each limited to five feet high and ten feet wide. Business zoning district sign regulations limit detached signs to one per frontage on a lot, require detached signs to be spaced 200 feet apart, and may not exceed 200 square feet in effective area.¹ Due to the expansive area of the planned development district, requiring the locations to be shown on the development plan, and limiting the effective area to 50 square feet (or 5 feet in height and 10 feet in width), staff recommends approval for this request.

Landscaping:

Landscaping must be provided in accordance with Article X of the Dallas Development Code.

¹ Reference subparagraphs b and c in Section 51A-7.304, the Dallas Development Code.

CPC Action – December 17, 2015:

Motion: It was moved to recommend **approval** of an amendment and expansion of Planned Development District No. 805, subject to a revised development plan and revised conditions to include 1) maximum effective area of 5ft. in height and 10ft. in width on property zoned Planned Development District No. 805 with a D-1 Liquor Control Overlay and a CR Community Retail District with a D Liquor Control Overlay, generally at the northeast corner of Lake June Road and North Masters Drive.

Maker: Shidid
Second: Murphy
Result: Carried: 15 to 0

For: 15 - Anglin, Emmons, Houston, Davis, Shidid,
Anantasomboon, Abtahi, Haney, Jung,
Housewright, Schultz, Peadon, Murphy, Ridley,
Tarpley

Against: 0
Absent: 0
Vacancy: 0

Notices: Area: 500 Mailed: 177
Replies: For: 0 Against: 0

Speakers: For: Karl Crawley, 900 Jackson St., Dallas, TX, 75202
Against: None

LIST OF OFFICERS

Owners:

NEC Lake June & Masters, L.P., a Delaware limited partnership
Syd Hurley, President

Paso Vista GP, LLC., a Texas corporation; general Partner

Sydney J. Hurley IV, Executive Manger

PVP Lake June Investment Partners III, L.P., a Delaware limited partnership
VPC GP, Inc., a Texas corporation its general partner
S.J. Hurley, IV, President

JP MORGAN CHASE BANK, NA

Jamie Dimon, Chairman and CEO
Ashley Bacon, Chief Risk Officer
Stephen M. Cutler, General Counsel
John L. Donnelly, Head of Human Resources
Mary Callahan Erdoes, Asset Management CEO
Marianna Lake, Chief Financial Officer
Douglas B. Petno, Commercial Banking CEO
Daniel E. Pinto, Corporate and Investment Bank CEO
Gordon A Smith, Consumer and Community Banking CEO
Matthew E Zames, Chief Operating Officer
Joseph M Evangelisti, Corporate Communications
Anthony J Horan, Secretary
Mark W O'Donovan, Controller
James R Vallone, General Auditor
Sarah M Youngwood, Investor Relations

PROPOSED CONDITIONS

ARTICLE 805.

PD 805.

SEC. 51P-805.101. LEGISLATIVE HISTORY.

PD 805 was established by Ordinance No. 27515, passed by the Dallas City Council on March 25, 2009. (Ord. 27515)

SEC. 51P-805.102. PROPERTY LOCATION AND SIZE.

PD 805 is established on property located at the northeast corner of Masters Drive and Lake June Road. The size of PD 805 is approximately ~~14.86~~ 16.4 acres. (Ord. 27515)

SEC. 51P-805.103. DEFINITIONS AND INTERPRETATIONS.

(a) Unless otherwise stated, the definitions and interpretations in Chapter 51A apply to this article.

(b) Unless otherwise stated, all references to articles, divisions, or sections in this article are to articles, divisions, or sections in Chapter 51A. In this article:

(1) **MASSAGE ESTABLISHMENT** means any building, room, place, or establishment, other than a regularly licensed hospital, where manipulated massage or manipulated exercises are practiced upon the human body by anyone not a duly licensed physician or chiropractor whether with or without the use of mechanical, therapeutic, or bathing devices, and includes Turkish bathhouses. This term does not include, however, duly licensed beauty parlors or barbershops or a place wherein registered physical therapists treat only patients recommended by a licensed physician and operated only under such physician's direction. **MASSAGE** means any process consisting of kneading, rubbing, or otherwise manipulating the skin of the body of a human being, either with the hand or by means of electrical instruments or apparatus, or other special apparatus, but does not include massage by duly licensed physicians and chiropractors, and registered physical therapists who treat only patients recommended by a licensed physician and who operate only under such physician's direction, nor massage of the face practiced by beauty parlors or barbershops duly licensed under the penal code of the state.

(2) **TATTOO OR BODY PIERCING STUDIO** means a business in which tattooing or body piercing is performed. **TATTOOING** means the practice of producing an indelible mark or figure on the human body by scarring or inserting a pigment under the skin using needles, scalpels, or other related equipment. **BODY PIERCING** means piercing of body parts, other than ears, to allow the insertion of jewelry.

(c) This district is considered to be a nonresidential zoning district. (Ord. 27515)

SEC. 51P-805.104. EXHIBITS.

The following exhibits are incorporated into this article:

- (1) Exhibit 805A: development plan.
- (2) Exhibit 805B: sign plans. (Ord. 27515)

SEC. 51P-805.105. DEVELOPMENT PLAN.

Development and use of the Property must comply with the development plan (Exhibit 805A). If there is a conflict between the text of this article and the development plan, the text of this article controls. (Ord. 27515)

SEC. 51P-805.106. MAIN USES PERMITTED.

(a) Except as provided in this section, the only main uses permitted are those main uses permitted in the CR Community Retail District, subject to the same conditions applicable in the CR Community Retail District, as set out in Chapter 51A. For example, a use permitted in the CR Community Retail District only by specific use permit (SUP) is permitted in this district only by SUP; a use subject to development impact review (DIR) in the CR Community Retail District is subject to DIR in this district; etc.

(b) The following uses are not permitted:

- Cemetery or mausoleum.
- College dormitory, fraternity or sorority house.
- Convent or monastery.
- Hospital.
- Hotel or motel.
- Massage establishment.
- Swap or buy shop.
- Tattoo or body piercing studio.

(Ord. 27515)

SEC. 51P-805.107. ACCESSORY USES.

(a) As a general rule, an accessory use is permitted in any district in which the main use is permitted. Some specific accessory uses, however, due to their unique nature, are subject to additional regulations in Section 51A-4.217. For more information regarding accessory uses, consult Section 51A-4.217.

(b) The following accessory uses are not permitted:

- Private stable.

(c) The following accessory use is permitted by SUP only:

- Accessory helistop.

(Ord. 27515)

SEC. 51P-805.108. YARD, LOT, AND SPACE REGULATIONS.

(Note: The yard, lot, and space regulations in this section must be read together with the yard, lot, and space regulations in Division 51A-4.400. If there is a conflict between this section and Division 51A-4.400, this section controls.)

The yard, lot, and space regulations for the CR Community Retail District, apply. (Ord. 27515)

SEC. 51P-805.109. OFF-STREET PARKING AND LOADING.

(a) Consult the use regulations in Division 51A-4.200 for the specific off-street parking and loading requirements for each use.

(b) For purposes of calculating the off-street parking and loading requirements, the Property is considered one lot.

(c) Ingress from and egress to Old Jamestown Avenue is prohibited. (Ord. 27515)

SEC. 51P-805.110. ENVIRONMENTAL PERFORMANCE STANDARDS.

See Article VI. (Ord. 27515)

SEC. 51P-805.111. LANDSCAPING.

(a) Landscaping must be provided in accordance with Article X.

(b) Plant materials must be maintained in a healthy, growing condition. (Ord. 27515)

SEC. 51P-805.112. SIGNS.

(a) Except as provided in this section, signs must comply with the provisions for business zoning districts in Article VII.

(b) The only detached premise signs allowed are as follows:

(1) One detached single-tenant premise sign is permitted toward the east end of Lake June Road, in the location shown on the development plan, and must comply with the dimensions and effective area shown on the sign plans (Exhibit 805B).

(2) Two detached multi-tenant premise signs are permitted, one near the entrance from Masters Drive and one toward the west end of Lake June Road, in the locations shown on the development plan, and must comply with the dimensions and effective areas shown on the sign plans (Exhibit 805B).

(3) **Four** ~~Two~~ detached monument premise signs are permitted in the locations shown on the development plan.

(A) Two of the detached monument signs and must comply with the dimensions and effective areas shown on the sign plans (Exhibit 805B).

(B) The remaining detached monument signs may not exceed five feet in height or 10 feet in width.

(4) Three detached premise signs, each with a maximum height of 30 feet above grade and 200 square feet of effective area, must be located in the areas shown on the development plan.

(c) For purposes of sign regulation, the Property is considered one lot. (Ord. Nos. 27515; 27677)

SEC. 51P-805.113. ADDITIONAL PROVISIONS.

(a) The Property must be properly maintained in a state of good repair and neat appearance.

(b) Development and use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the city. (Ord. 27515)

SEC. 51P-805.114. COMPLIANCE WITH CONDITIONS.

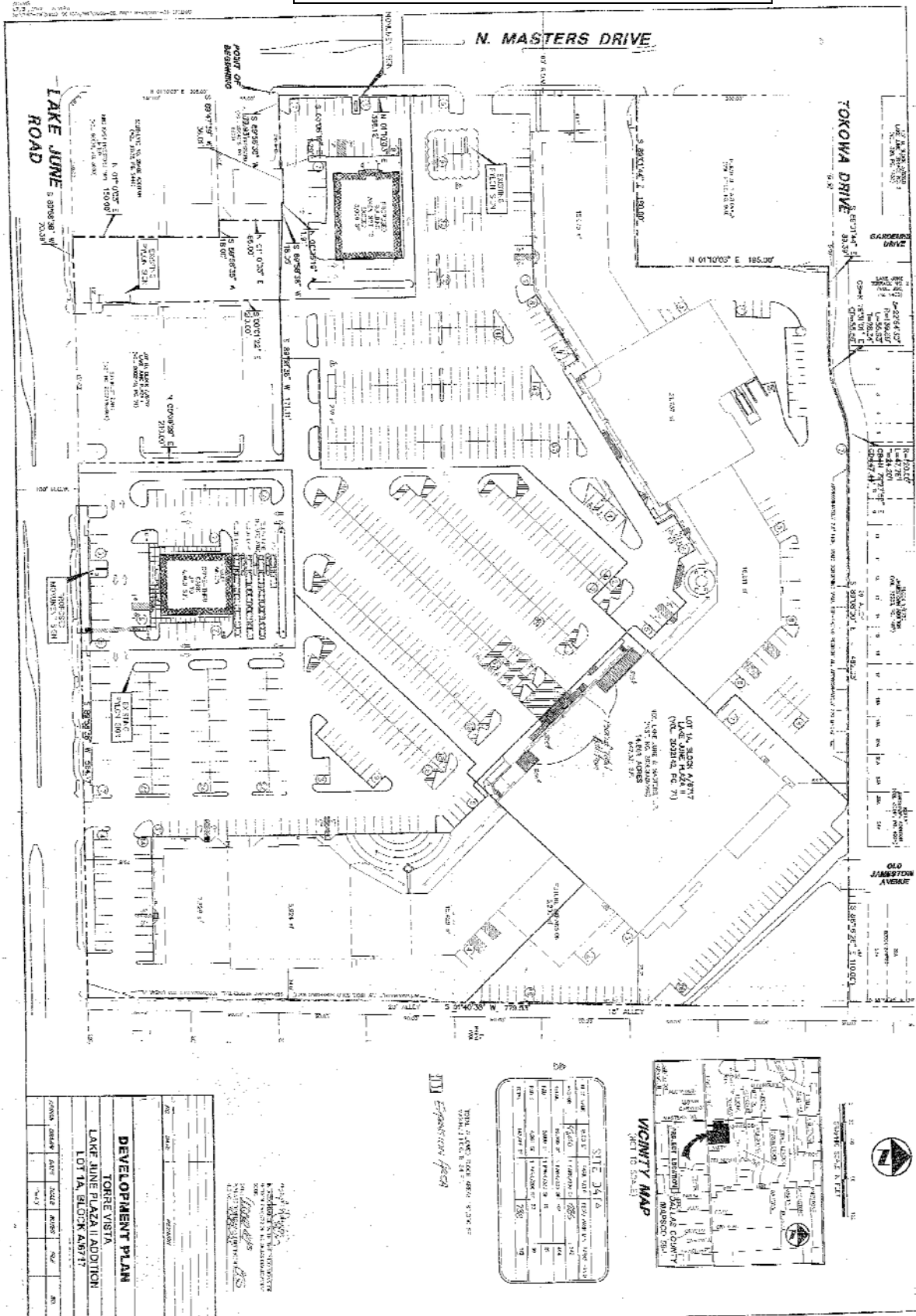
(a) All paved areas, permanent drives, streets, and drainage structures, if any, must be constructed in accordance with standard city specifications, and completed to the satisfaction of the director of public works and transportation.

(b) The building official shall not issue a building permit to authorize work, or a certificate of occupancy to authorize the operation of a use in this district, until there has been full compliance with this article, the Dallas Development Code, the construction codes, and all other ordinances, rules, and regulations of the city. (Ord. 27515)

SEC. 51P-805.115. ZONING MAP.

PD 805 is located on Zoning Map No. L-11. (Ord. 27515)

EXISTING DEVELOPMENT PLAN



Scale: 1" = 40'



SITE 3410

NO.	DATE	DESCRIPTION	BY	CHKD.
1	10/10/10	PRELIMINARY	...	...
2	11/10/10	REVISED	...	...
3	12/10/10	REVISED	...	...
4	01/11/11	REVISED	...	...
5	02/11/11	REVISED	...	...
6	03/11/11	REVISED	...	...
7	04/11/11	REVISED	...	...
8	05/11/11	REVISED	...	...
9	06/11/11	REVISED	...	...
10	07/11/11	REVISED	...	...
11	08/11/11	REVISED	...	...
12	09/11/11	REVISED	...	...

EXISTING DEVELOPMENT PLAN
 PREPARED BY: [Signature]
 DATE: 10/10/10

DEVELOPMENT PLAN

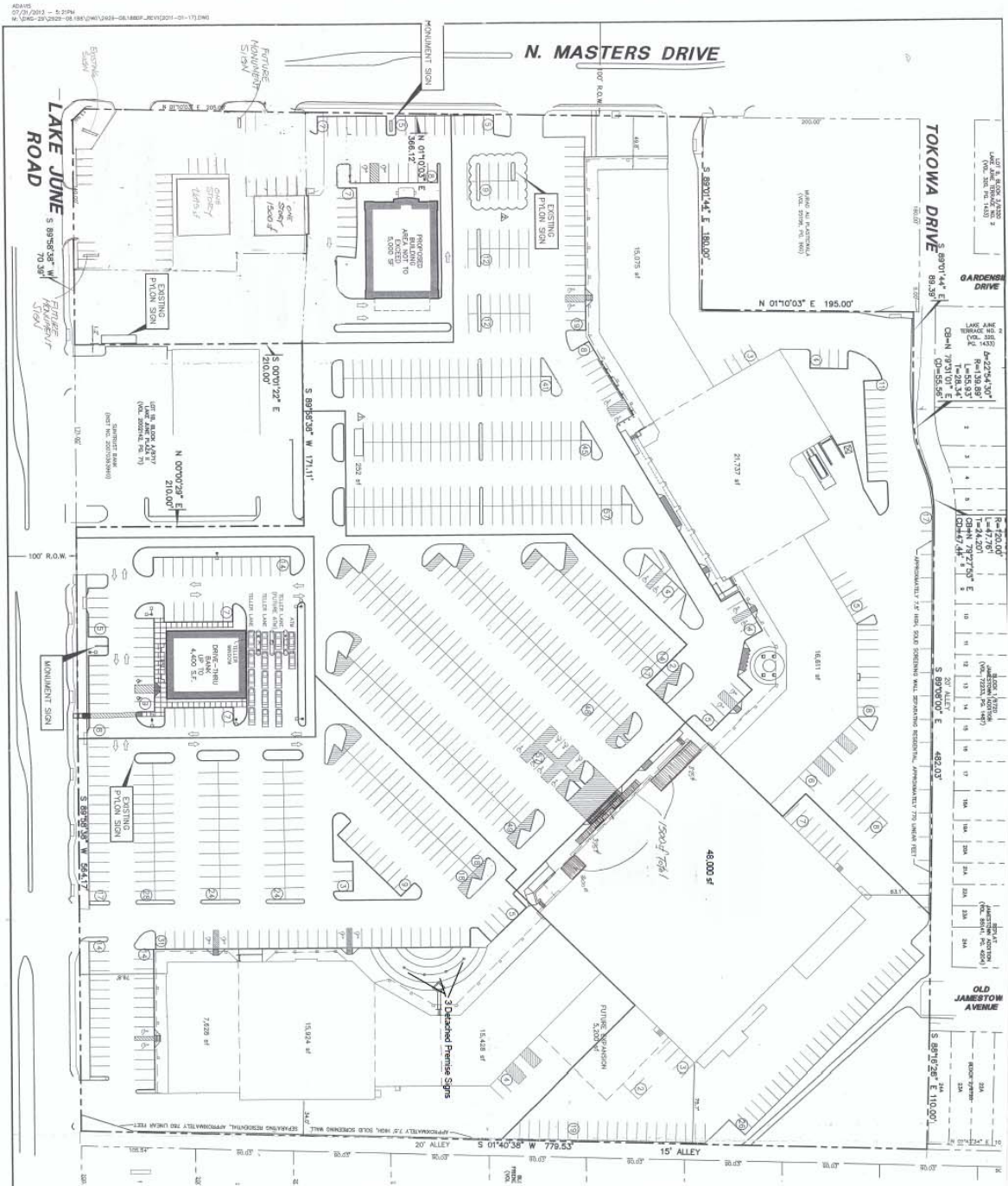
TORRE VISTA

LAKE JONE PLAZA II ADDITION

LOT 1A, BLOCK 4671

NO.	DATE	DESCRIPTION	BY	CHKD.
1	10/10/10	PRELIMINARY	...	...
2	11/10/10	REVISED	...	...
3	12/10/10	REVISED	...	...
4	01/11/11	REVISED	...	...
5	02/11/11	REVISED	...	...
6	03/11/11	REVISED	...	...
7	04/11/11	REVISED	...	...
8	05/11/11	REVISED	...	...
9	06/11/11	REVISED	...	...
10	07/11/11	REVISED	...	...
11	08/11/11	REVISED	...	...
12	09/11/11	REVISED	...	...

PROPOSED DEVELOPMENT PLAN

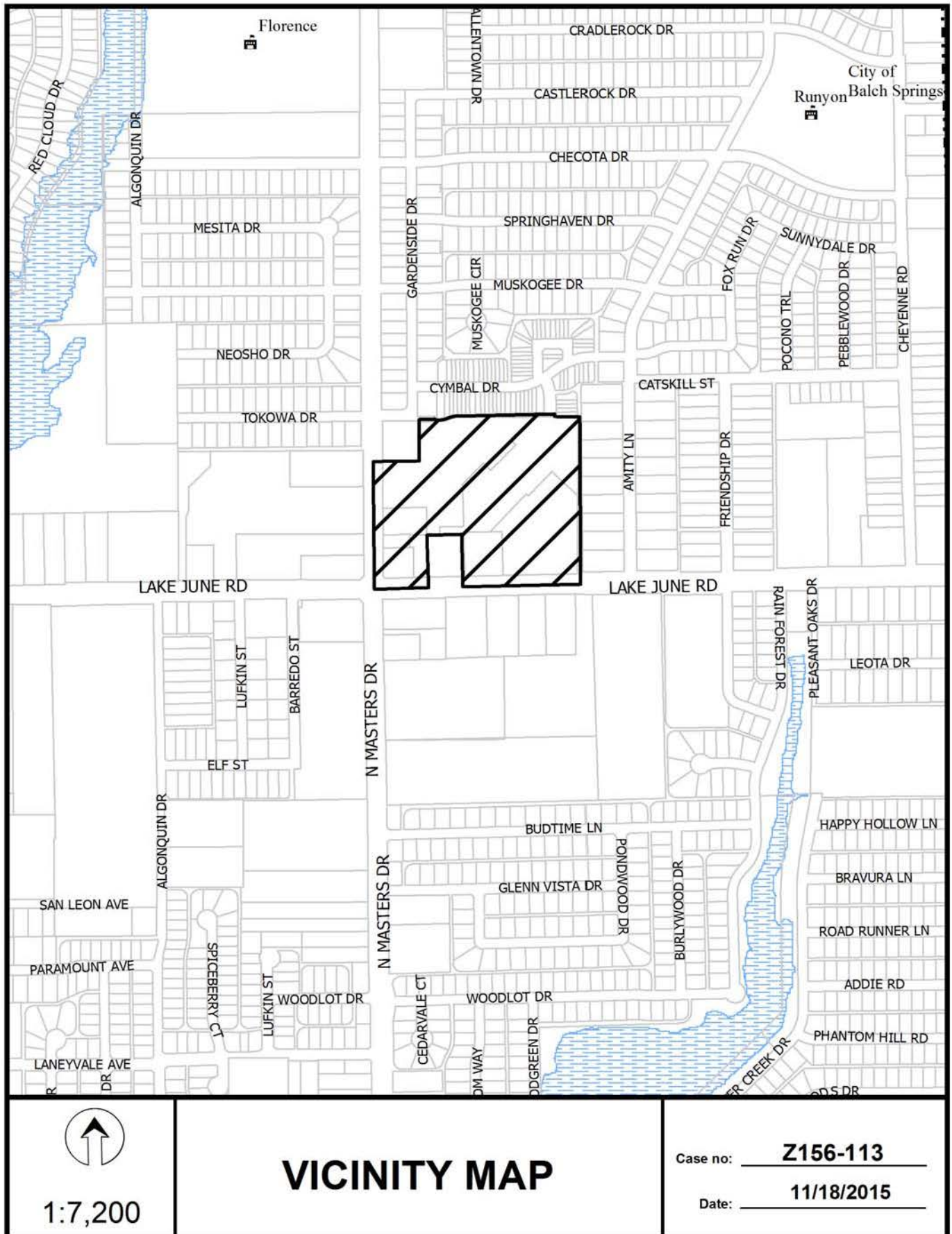


SITE DATA

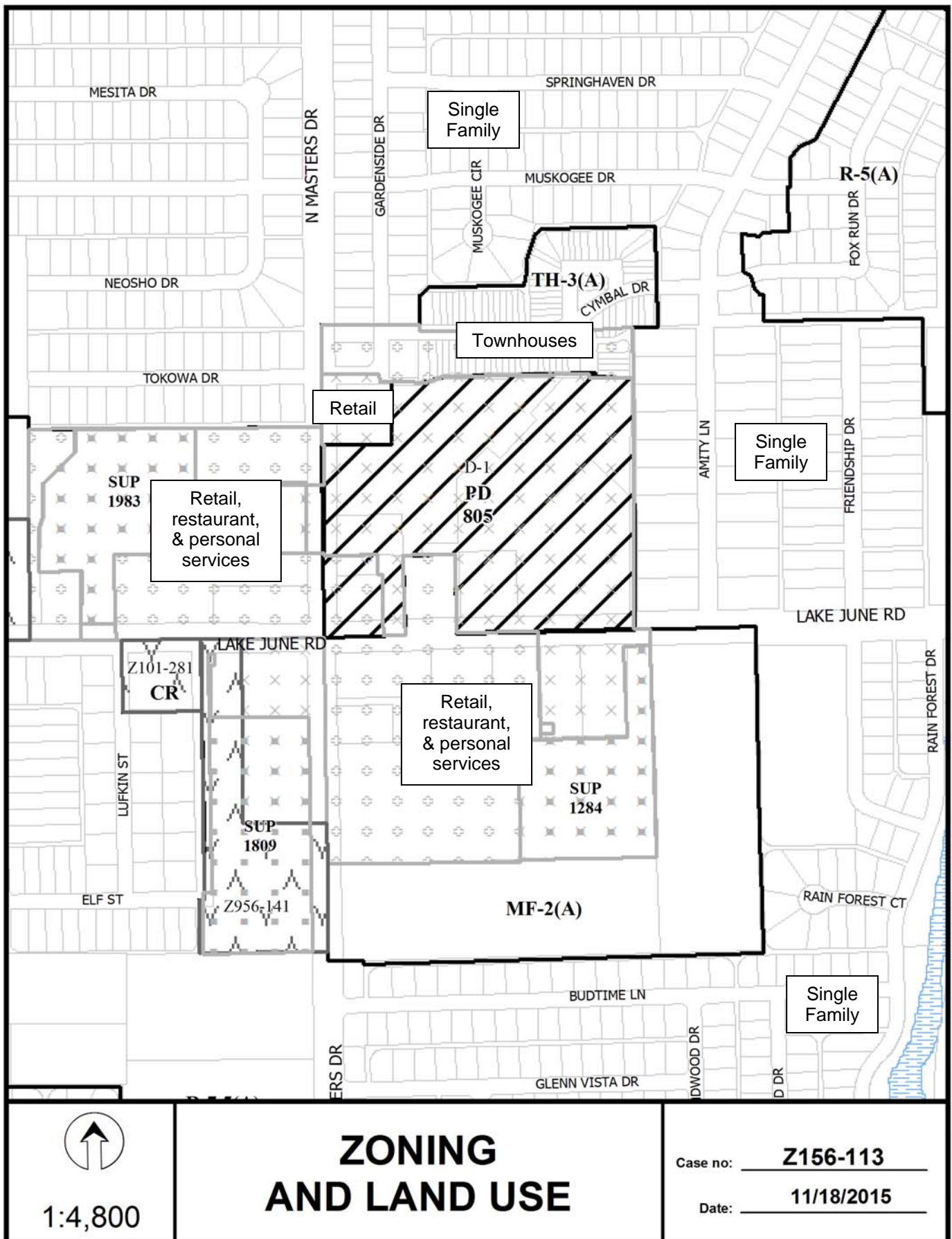
Land Area: 16.4 acres
 Zoning: PD 805
 Floor Area: 160,000 sq ft
 Uses: CR District restrictions
 See PD conditions
 Development Standards: CR District
 Height: One-story, 30 feet
 Parking: Per SIA
 Provided: 843 spaces
 Signs: Per PD conditions

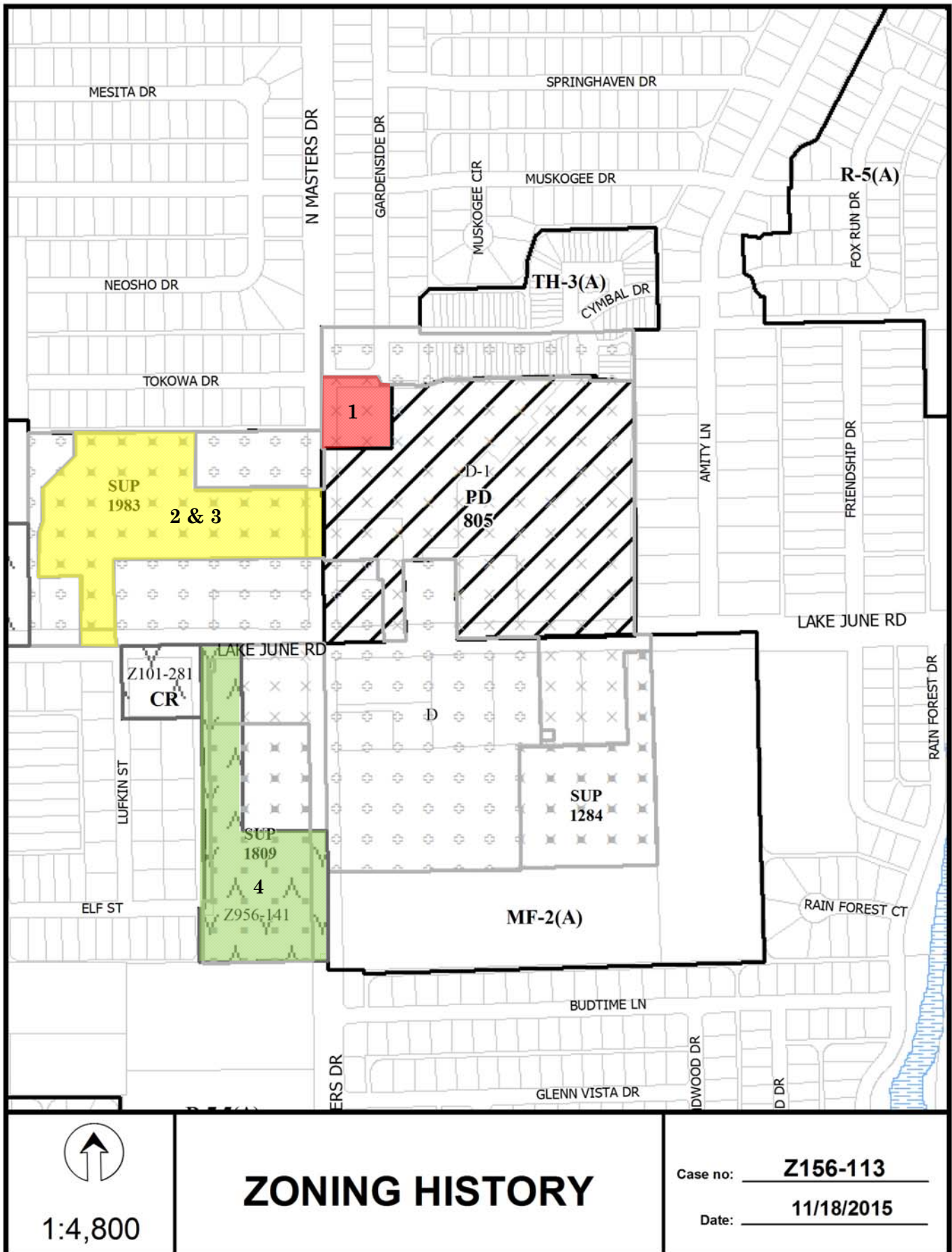
Z156-113

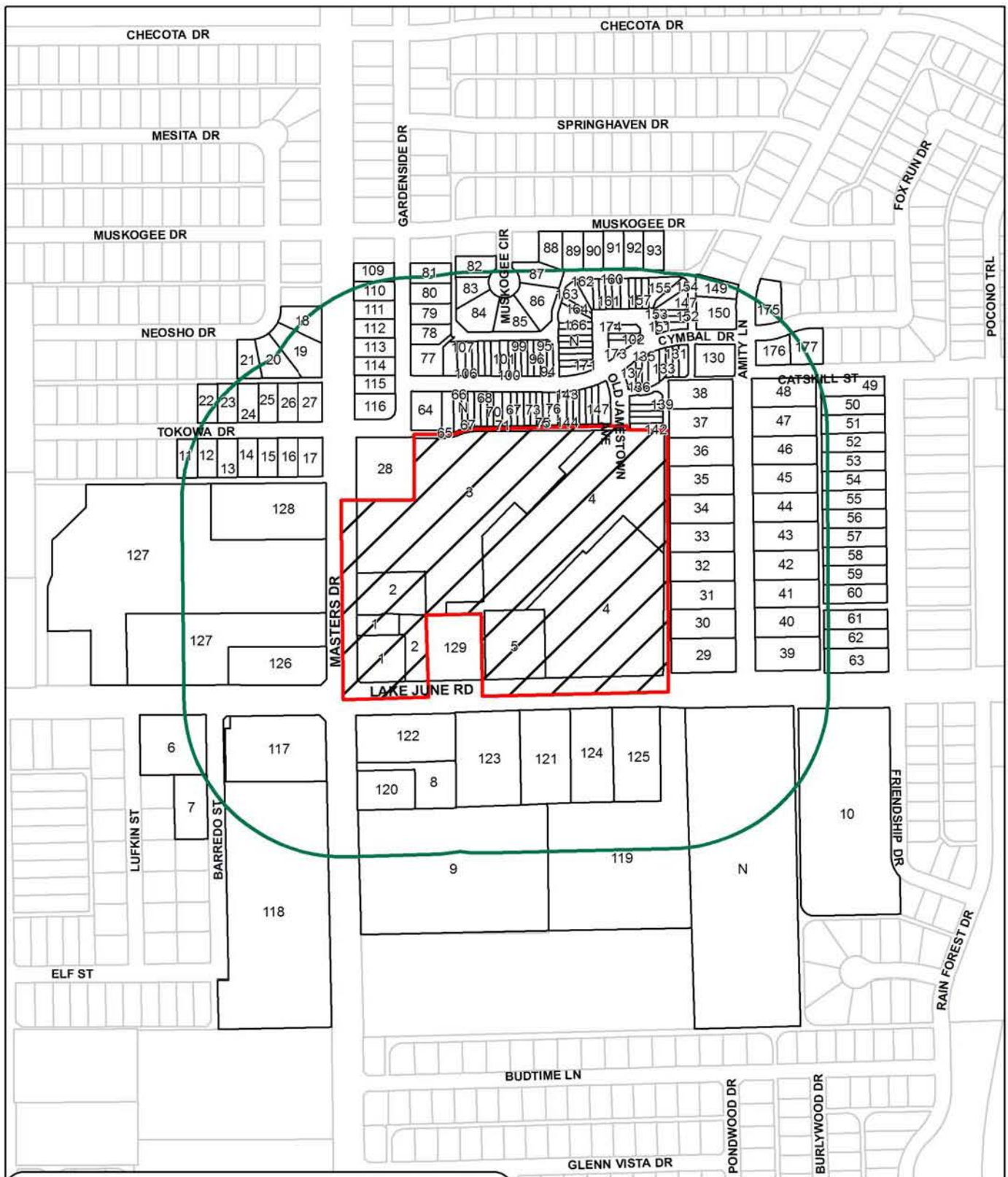
DEVELOPMENT PLAN				
TORRE VISTA				
LAKE JUNE PLAZA II ADDITION				
DATE	DATE	DATE	DATE	DATE
1-1-10	1-1-10	1-1-10	1-1-10	1-1-10











<u>177</u>	Property Owners Notified (193 parcels)
<u>0</u>	Replies in Favor (0 parcels)
<u>0</u>	Replies in Opposition (0 parcels)
<u>500'</u>	Area of Notification
<u>12/17/2015</u>	Date

Z156-113
CPC



1:4,800

12/16/2015

Reply List of Property Owners***Z156-113******177 Property Owners Notified******0 Property Owners in Favor******0 Property Owners Opposed***

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
1	1318	N MASTERS DR	PVP LAKE JUNE INVESTMENT PARTNERS III LLC
2	3	LAKE JUNE RD	NEC LAKE JUNE & MASTERS L
3	10325	LAKE JUNE RD	NEC LAKE JUNE & MASTERS L
4	10325	LAKE JUNE RD	NEC LAKE JUNE & MASTERS L
5	10321	LAKE JUNE RD	JPMORGAN CHASE BANK NA
6	10106	LAKE JUNE RD	FIREBRAND PPTIES LP
7	1233	BARREDO ST	HARDIN GENEVIEVE P
8	1200	N MASTERS DR	AUTOZONE INC
9	1220	N MASTERS DR	ELM RIDGE APARTMENTS LP
10	10602	LAKE JUNE RD	FIRST BAPTIST CHURCH OF
11	10104	TOKOWA DR	BURNS KIM TR
12	10108	TOKOWA DR	CHAVEZ PEGGY
13	10112	TOKOWA DR	MURILLO CIPRIANDO
14	10116	TOKOWA DR	CARTER MAE L
15	10120	TOKOWA DR	MA ST PARTNERS 7
16	10124	TOKOWA DR	CARDENAS LAURA
17	10128	TOKOWA DR	LOPEZ FERMIN & SELENE
18	10112	NEOSHO DR	ROJAS JUAN CARLOS & ELENA
19	10108	NEOSHO DR	HARRIS LEE FRANCES
20	10104	NEOSHO DR	VILLALOBOS OSIRIS ANGELICA &
21	10058	NEOSHO DR	ULMER MICHAEL & MILLY
22	10107	TOKOWA DR	PEDRAZA ALEJANDRO
23	10111	TOKOWA DR	TREJO ANTONIO AGUILAR &
24	10115	TOKOWA DR	TRAYLOR SAMUEL D &
25	10119	TOKOWA DR	MCELROY VICKIE J
26	10123	TOKOWA DR	MORENO VICTORIANA

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
27	10127	TOKOWA DR	FERMAN ORTIZ &
28	1346	N MASTERS DR	PLASTICWALA MURAD ALI
29	1307	AMITY LN	WALLACE MITCHELL W &
30	1311	AMITY LN	ENGLISH SHEILA A
31	1319	AMITY LN	LOPEZ MA JOSEFINA
32	1327	AMITY LN	ALARCON DAVID
33	1333	AMITY LN	LOPEZ VERONICA
34	1341	AMITY LN	JARAMILLO ELIAQUIN
35	1347	AMITY LN	SHOFNER TOMMY JOE JR
36	1355	AMITY LN	LOPEZ MA JOSEFINA
37	1363	AMITY LN	CORTES VICTOR ALFONSO & ROSARIO GUADALUPE
38	1369	AMITY LN	TADEOLOPEZ JORGE JR
39	1304	AMITY LN	LEFEVERS TAMMY SUE
40	1310	AMITY LN	CALVO AVELINO & JULIETA
41	1318	AMITY LN	ALVARADO RICARDO & AIDA
42	1326	AMITY LN	ROBISON BEVERLY ANN
43	1334	AMITY LN	BENITEZ ARELI &
44	1340	AMITY LN	WEBB VALERIE I
45	1348	AMITY LN	DAVIS HARVEY EUGENE &
46	1354	AMITY LN	A FILIBERTO BALTAZAR
47	1362	AMITY LN	HAND KEVIN C
48	1368	AMITY LN	GARCIA MICHAEL &
49	1371	FRIENDSHIP DR	TAVARES JUAN CARLOS JR
50	1367	FRIENDSHIP DR	SLOAN ANTHONY WAYNE
51	1361	FRIENDSHIP DR	RUIZ BENITO & NANCY A
52	1357	FRIENDSHIP DR	SLOAN LISA GAYLE
53	1351	FRIENDSHIP DR	CALDERON ALBERTO &
54	1347	FRIENDSHIP DR	MARTINEZ ROBERTO &
55	1341	FRIENDSHIP DR	ROSAS BENITO N
56	1337	FRIENDSHIP DR	CEDILLO SEVERO &

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
57	1335	FRIENDSHIP DR	HERNANDEZ AARON
58	1329	FRIENDSHIP DR	KENNEDY SUE E
59	1323	FRIENDSHIP DR	SPICER SUE ELLEN
60	1321	FRIENDSHIP DR	MARTINEZ RAMIRO
61	1311	FRIENDSHIP DR	ELCA INVESTMENTS LLC
62	1307	FRIENDSHIP DR	GIADOLOR WILLIAM
63	1301	FRIENDSHIP DR	MARTINEZ JULIO & JUANA
64	10306	CYMBAL DR	ZAMORA JOSE LUIS
65	10308	CYMBAL DR	ROMAN JOSE S
66	10312	CYMBAL DR	PEREZ MARIA
67	10320	CYMBAL DR	FROSSARD T E JR
68	10328	CYMBAL DR	HYDEN DAVID C &
69	10332	CYMBAL DR	GARCIA MARIA DEL SOCORRO &
70	10336	CYMBAL DR	ANDRADE RODOLFO
71	10340	CYMBAL DR	PEREZ SANJUANA GUADALUPE
72	10348	CYMBAL DR	RODRIGUEZ CRISTINA
73	10356	CYMBAL DR	LUCIO JESUS OSCAR & CLAUDIA CATALINA
74	10360	CYMBAL DR	BARBEE WILLIAM
75	10364	CYMBAL DR	TI LONG TERM HOLDINGS LLC
76	10368	CYMBAL DR	CHAVARRIA LESLIE RUBI DE LA MORA
77	1420	GARDENSIDE DR	HERNANDEZ FELIPE GARAY &
78	1424	GARDENSIDE DR	DELIRA JOSE R
79	1430	GARDENSIDE DR	SHAW BRUCE E & PEARLINE W
80	1434	GARDENSIDE DR	FINCHER BURMA ESTATE OF
81	1440	GARDENSIDE DR	RODRIGUEZ LUIS ALBERTO
82	10324	MUSKOGEE CIR	WAFFER EVERINE EST OF
83	10328	MUSKOGEE CIR	GOMEZ MIRIAM
84	10332	MUSKOGEE CIR	YOUNGER YENKA Y
85	10336	MUSKOGEE CIR	GASTON BETTY JOE
86	10340	MUSKOGEE CIR	GUZMAN JOSE A P &
87	10344	MUSKOGEE CIR	DAVIS JENNIFER

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
88	10356	MUSKOGEE DR	LEYVA JOSE RUBEN
89	10362	MUSKOGEE DR	MILES LUSTER L & GAY O
90	10366	MUSKOGEE DR	EASTER AVAN SR & MELBA J
91	10404	MUSKOGEE DR	WILLIAMS YETTA L
92	10410	MUSKOGEE DR	GRANT LATASHA
93	10414	MUSKOGEE DR	HICKMAN BERNICE E
94	10367	CYMBAL DR	BARRERA JESUS &
95	10363	CYMBAL DR	MORA MARIA DEL CARMEN &
96	10359	CYMBAL DR	LEWIS ANTHONY W SR
97	10355	CYMBAL DR	FLORES MARIBEL
98	10351	CYMBAL DR	VELAZQUEZ FERNANDO &
99	10347	CYMBAL DR	RODRIGUEZ ABEL R &
100	10343	CYMBAL DR	REBEL PPTY INVESTMENTS LLC
101	10339	CYMBAL DR	BARRERA JESUS & MARIA
102	10335	CYMBAL DR	HERRMANN ROGER
103	10331	CYMBAL DR	FUENTES ADRIANA JARAMILLO
104	10327	CYMBAL DR	REIS ANGELA
105	10323	CYMBAL DR	GARY DEMETIRE
106	10319	CYMBAL DR	AGUILAR GEANA LEE GONZALEZ
107	10315	CYMBAL DR	VELASQUEZ PATRICIA
108	10309	CYMBAL DR	NAVEJAS VICKEY
109	1441	GARDENSIDE DR	SANCHEZTORRES LUIS CARLOS &
110	1435	GARDENSIDE DR	MCALLISTER RICKY C
111	1431	GARDENSIDE DR	COOPER JOSEPH C
112	1425	GARDENSIDE DR	NELSON LARRY
113	1421	GARDENSIDE DR	SLEDGE JOAN C
114	1415	GARDENSIDE DR	HERNANDEZ JESUS R & EVA
115	1409	GARDENSIDE DR	BENAVIDES PATRICIO & EVA
116	1403	GARDENSIDE DR	SNEED JEROME E
117	10152	LAKE JUNE RD	WALGREEN CO
118	1227	N MASTERS DR	AMERICA CAN!

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
119	10406	LAKE JUNE RD	SANDLIAN COLBY B &
120	1268	N MASTERS DR	TORRES ATILANO
121	10304	LAKE JUNE RD	RETAIL BUILDINGS INC
122	10208	LAKE JUNE RD	7 ELEVEN INC
123	10218	LAKE JUNE RD	AUTOZONE INC
124	10320	LAKE JUNE RD	LAKE JUNE INVESTMENT PARTNERS II LP
125	10320	LAKE JUNE RD	MCDONALDS REAL ESTATE COMPANY
126	10155	LAKE JUNE RD	KING KASH INVESTORS 1991
127	10121	LAKE JUNE RD	PINFIN PROPERTIES LP
128	1339	N MASTERS DR	SUNRISE ENTERPRISES INC
129	10315	LAKE JUNE RD	OREILY AUTO ENTERPRISES LLC
130	10440	CYMBAL DR	GRANADOS ERIBERTO CRISTIAN GARCIA
131	10430	CYMBAL DR	BROOKS JOCIEL
132	10422	CYMBAL DR	GARCIA SANTIAGO
133	10418	CYMBAL DR	BARRERA JESUS & MARIA ELENA
134	10414	CYMBAL DR	FREENEY CLIFFORD
135	10410	CYMBAL DR	C & C RESIDENTIAL PPTIES INC
136	10406	CYMBAL DR	COULSON ALLAN & ADRIANNA TR
137	10402	CYMBAL DR	REYNA SAMUEL
138	1366	OLD JAMESTOWN AVE YAN WENHAO &	
139	1362	OLD JAMESTOWN AVE HADNOT TROY DION	
140	1358	OLD JAMESTOWN AVE HENDRICKS BARRY L &	
141	1354	OLD JAMESTOWN AVE WILLIAMS BENTON NEAL	
142	1350	OLD JAMESTOWN AVESMITH SHIRLEY J	
143	10372	CYMBAL DR	BELTRAN BENITA
144	10376	CYMBAL DR	VILLALOBOS FRANCISCO J
145	10380	CYMBAL DR	HUME MARY JO &
146	10384	CYMBAL DR	CONTRERAS EDGAR
147	10388	CYMBAL DR	MATTHEWS EDWARD L
148	10396	CYMBAL DR	CONDE FLORENCIO R
149	1425	AMITY LN	MECCA APRIL INC

12/16/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
150	1415	AMITY LN	INVESTALL INC
151	1493	OLD JAMESTOWN CT	CARO ROSA L
152	1489	OLD JAMESTOWN CT	DCA OF TEXAS INC
153	1485	OLD JAMESTOWN CT	ANGUIANO LETICIA P
154	1481	OLD JAMESTOWN CT	TEXAS ACCEPTANCE INC
155	1477	OLD JAMESTOWN CT	HERNANDEZ RUBEN
156	1473	OLD JAMESTOWN CT	VELASQUEZ ERIKA
157	1469	OLD JAMESTOWN CT	JONES STEVE A
158	1465	OLD JAMESTOWN CT	JOHNSON JOHNNIE MAE
159	1461	OLD JAMESTOWN CT	JACKSON EARNEST J &
160	1457	OLD JAMESTOWN CT	SAUNDERS WAYNE
161	1453	OLD JAMESTOWN CT	HERNANDEZ RUBEN
162	1441	OLD JAMESTOWN CT	LOMMEL ARTHUR
163	1437	OLD JAMESTOWN CT	ESTERS JESSE
164	1435	OLD JAMESTOWN CT	RUIZ ROBERTO &
165	1433	OLD JAMESTOWN CT	TORRE MARIA ISABEL HERNANDEZ DE LA
166	1429	OLD JAMESTOWN CT	ROJASOCAMPO ROMANI
167	1425	OLD JAMESTOWN CT	CRUZ RUBEN HERNANDEZ SANTA
168	1417	OLD JAMESTOWN CT	EJIGU HAILU
169	1413	OLD JAMESTOWN CT	HERNANDEZ FRANCISCO
170	1409	OLD JAMESTOWN CT	RAOFFPUR DAVID
171	1405	OLD JAMESTOWN CT	SMITH PHYLLIS LYNELLE
172	1401	OLD JAMESTOWN CT	HERNANDEZ RUBEN
173	1406	OLD JAMESTOWN CT	HERNANDEZ RUBEN SANTA CRUZ
174	1418	OLD JAMESTOWN CT	ARECHAR MARIA D
175	10503	CYMBAL DR	CASTILLO J ROBERTO &
176	10504	CYMBAL DR	RUIZ REYNALDO
177	10508	CYMBAL DR	GARRETT CHERMINE L

AGENDA ITEM # 49

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 7

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 48 E

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a D-1 Liquor Control Overlay and an ordinance granting a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the south side of Samuell Boulevard, east of North Jim Miller Road

Recommendation of Staff: Approval of a D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and conditions

Recommendation of CPC: Approval of a D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period, subject to a site plan and conditions

Z156-114(OTH)

FILE NUMBER: Z156-114(OTH)

DATE FILED: October 15, 2015

LOCATION: South side of Samuell Boulevard, east of North Jim Miller Road.

COUNCIL DISTRICT: 7

MAPSCO: 48-E

SIZE OF REQUEST: Approx. 0.5554 acres

CENSUS TRACT: 122.07

APPLICANT: Mansoor Ali

OWNER: Chung Joe Ink, Tr.

REPRESENTATIVE: Victor Castro, Plans and Permits Solutions

REQUEST: An application for a D-1 Liquor Control Overlay and a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less on property zoned an RR Regional Retail District with a D Liquor Control Overlay.

SUMMARY: The applicant proposes to sell alcohol in conjunction with a general merchandise or food store [Food Plus] in the existing building facing Samuell Boulevard.

CPC RECOMMENDATION: Approval of the D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period, subject to a site plan and conditions.

STAFF RECOMMENDATION: Approval of a D-1 Liquor Control Overlay; and approval of a Specific Use Permit for a two-year period with eligibility for automatic renewals for additional five-year periods, subject to a site plan and conditions.

GUIDING CRITERIA FOR STAFF RECOMMENDATION:

The following factors are listed in Chapter 51A of the Dallas Development Code to guide the determination as to whether or not an SUP shall be granted. Staff has listed its findings based upon each component below:

1. *Compatibility with surrounding uses and community facilities* – The sale of alcoholic beverages in conjunction with the proposed general merchandise or food store will not negatively impact compatibility with the surrounding land uses.
2. *Contribution to, enhancement, or promoting the welfare of the area of request and adjacent properties* – The sale alcoholic beverages in conjunction with the proposed general merchandise or food store neither contributes to nor deters the welfare of adjacent properties.
3. *Not a detriment to the public health, safety, or general welfare* – The sale of alcoholic beverages is not anticipated to be a detriment to the public health, safety or general welfare of the public.
4. *Conforms in all other respects to all applicable zoning regulations and standards* – Based on information depicted on the site plan, the proposed use complies with all applicable zoning regulations and standards of the City of Dallas. No variances or special exceptions are requested.

Zoning History: There have been two recent zoning requests in the area within the last five years.

1. **Z134-352** On December 11, 2013, the City Council approved the renewal of Specific use Permit No. 1907 for the sale of alcoholic beverages in conjunction with a general merchandise or food store greater than 3,500 square feet located on the southeast corner of North Jim Miller and Samuell Road.
2. **Z123-302** On May 13, 2015, the City Council approved the renewal of Specific use Permit No. 1907 for the sale of alcoholic beverages in conjunction with a general merchandise or food store greater than 3,500 square feet located on the southeast corner of North Jim Miller and Samuell Road.

Thoroughfares/Streets:

Thoroughfares/Street	Type	Existing ROW
Samuell Boulevard	Principle Arterial	120 ft.
Jim Miller Road	Minor Arterial	100 ft.

Traffic:

The Engineering Section of the Department of Sustainable Development and Construction reviewed the request and determined that it will not negatively impact the surrounding roadway system.

STAFF ANALYSIS:**Comprehensive Plan:**

The *fowardDallas! Comprehensive Plan* was adopted by the City Council in June 2006. The *fowardDallas! Comprehensive Plan* outlines several goals and policies which can serve as a framework for assisting in evaluating the applicant's request. The Plan identifies the request site as being on a Business Center Corridor.

The Business Center or Corridor Building Block represents major employment or shopping destinations outside of Downtown. Business Centers are usually at major intersections or along highways or major arterials and rely heavily on car access. These areas typically include high-rise office towers and low- to mid-rise residential buildings for condos or apartments and may include multifamily housing. Land uses are typically separated from one another by parking lots, freeways or arterial roads. Streets in these areas emphasize efficient movement of vehicles. Bold lighting and linear landscaping features such as esplanades and tree-lined boulevards can all work to distinguish and identify these areas. Public spaces may be at key entry points and central locations. Gateway landscaping, monuments and other devices will provide visibility from the freeway and guide visitors to destinations. Public transit may play a role in these areas and help create some pockets of transit oriented development. Business Centers and Corridors provide important concentrations of employment within Dallas that compete with suburban areas.

Surrounding Land Uses:

	Zoning	Land Use
Site	RR, D	Retail and personal service use
North	RR, D-1	Office, retail, restaurants
East	RR, D	Retail uses
South	RR, D, MF-2(A), R-7.5(A)	Retail uses, residential uses
West	RR, D, SUP 1907	Retail uses, SUP for sale of alcohol (CVS)

Land Use Compatibility:

The approximately 0.5554 acre site is zoned an RR Regional Retail District with a D Liquor Control Overlay and is developed with a multi-tenant 5,625 square foot building with an unoccupied suite, a retail use and a personal service use.

Surrounding uses primarily consist of retail, office, restaurants to the north, east, and west; to the south, a car wash and residential.

The general provisions for a Specific Use Permit in Section 51A-4.219 of the Dallas Development Code specifically state: (1) The SUP provides a means for developing certain uses in a manner in which the specific use will be consistent with the character of the neighborhood; (2) Each SUP application must be evaluated as to its probable effect on the adjacent property and the community welfare and may be approved or denied as the findings indicate appropriate; (3) The city council shall not grant an SUP for a use except upon a finding that the use will: (A) complement or be compatible with the surrounding uses and community facilities; (B) contribute to, enhance, or promote the welfare of the area of request and adjacent properties; (C) not be detrimental to the public health, safety, or general welfare; and (D) conform in all other respects to all applicable zoning regulations and standards. The regulations in this chapter have been established in accordance with a comprehensive plan for the purpose of promoting the health, safety, morals, and general welfare of the city.

The proposed general merchandise or food store must with Chapter 12B (Convenience Store) requirements. The purpose of regulating convenience stores is to protect the health, safety, and welfare of the citizens of the City of Dallas by reducing the occurrence of crime, preventing the escalation of crime, and increasing the successful prosecution of crime that occurs in convenience stores in the city. A business owner of a convenience store can get the CS license only after the store is opened. It can be registered before opening the store; however, the inspection and approval of the CS cannot be made until after the store is opened. In addition, conditions and time periods for the use provide an opportunity for continued evaluation of the site. The short time period will also require that the request be re-evaluated to ensure the use is compatible in this location and that all conditions are being met.

Development Standards:

DISTRICT	SETBACKS		Density FAR	Height	Lot Coverage	Special Standards	Primary Uses
	Front	Side/Rear					
Existing							
RR Regional retail	15'	20' adjacent to residential OTHER: No Min.	1.5 FAR overall 0.5 office	70' 5 stories	80%	Proximity Slope U-form setback Visual Intrusion	Retail & personal service, office

Parking:

The Dallas Development Code requires off-street parking to be provided for a general merchandise or food store use at one space for each 200 square feet of floor area. The proposed use requires 8 parking spaces. Parking must be provided as shown on the

Z156-114(OTH)

site plan.

Landscaping:

Landscaping is required in accordance with Article X of the Dallas Development Code. However, the applicant's request will not trigger any Article X requirements as no new construction is proposed on the site.

**Proposed SUP Conditions
Z156-114(OTH)**

1. USE: The only use authorized by this specific use permit is the sale of alcoholic beverages for off-premise consumption as part of the operation of a general merchandise or food store 3,500 square feet or less.
2. SITE PLAN: Use and development of the Property must comply with the attached site plan.
3. TIME LIMIT: This specific use permit expires on _____ (two years).
5. MAINTENANCE: The Property must be properly maintained in a state of good repair and neat appearance.
6. GENERAL REQUIREMENTS: Use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the City of Dallas.

Z156-114(OTH)

CPC ACTION – December 17, 2015

Z156-114(OTH)

Planner: Olga Torres Holyoak

Motion: It was moved to recommend **approval** of a D-1 Liquor Control Overlay and **approval** a Specific Use Permit for the sale of alcoholic beverages in conjunction with a general merchandise or food store 3,500 square feet or less for a two-year period, subject to a site plan and conditions on property zoned an RR Regional Retail District with a D Liquor Control Overlay on the south side of Samuell Boulevard, east of North Jim Miller Road.

Maker: Abtahi
Second: Houston
Result: Carried: 15 to 0

For: 15 - Anglin, Emmons, Houston, Davis, Shidid,
Anantasomboon, Abtahi, Haney, Jung,
Housewright, Schultz, Peadon, Murphy,
Ridley*, Tarpley

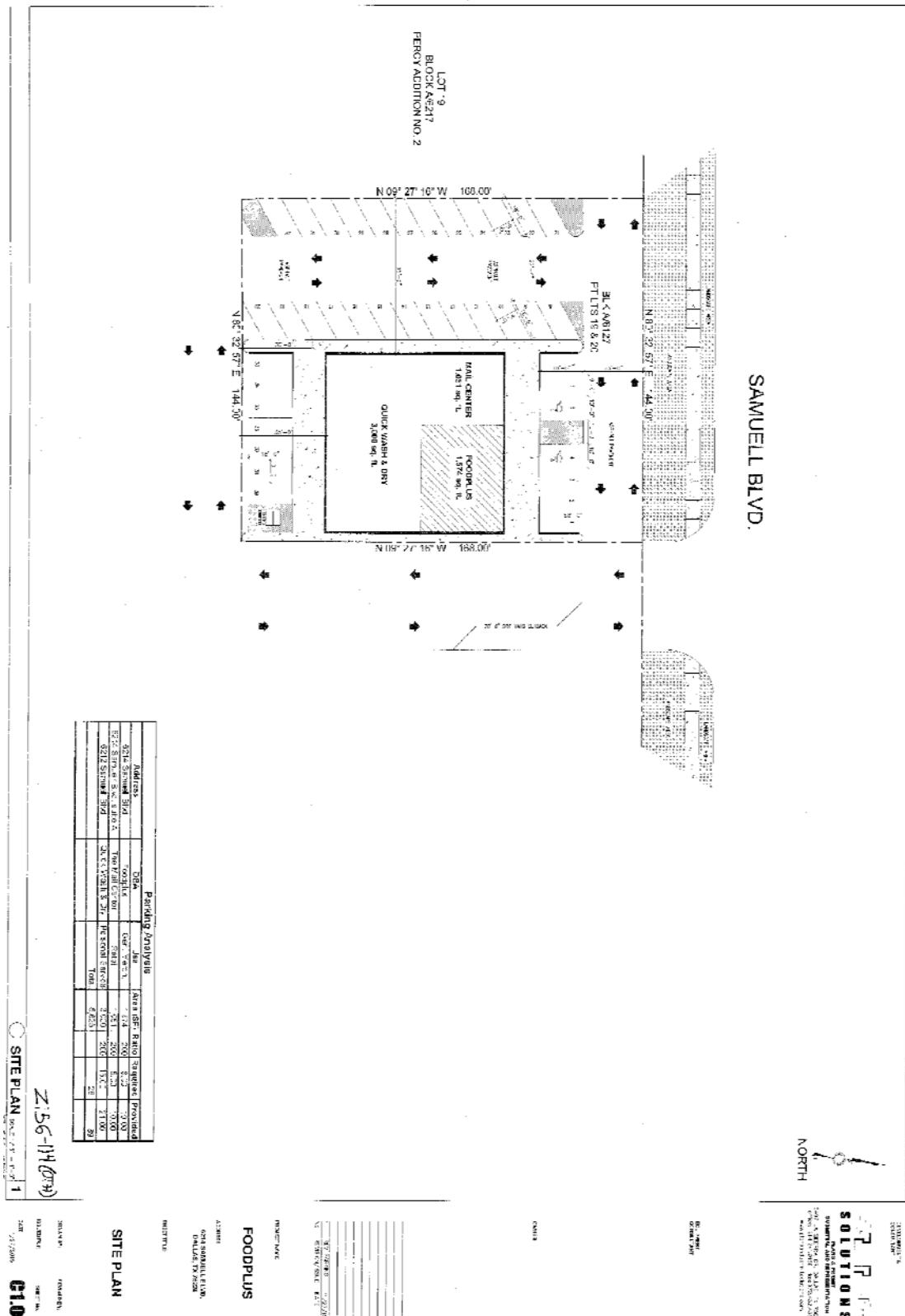
Against: 0
Absent: 0
Vacancy: 0

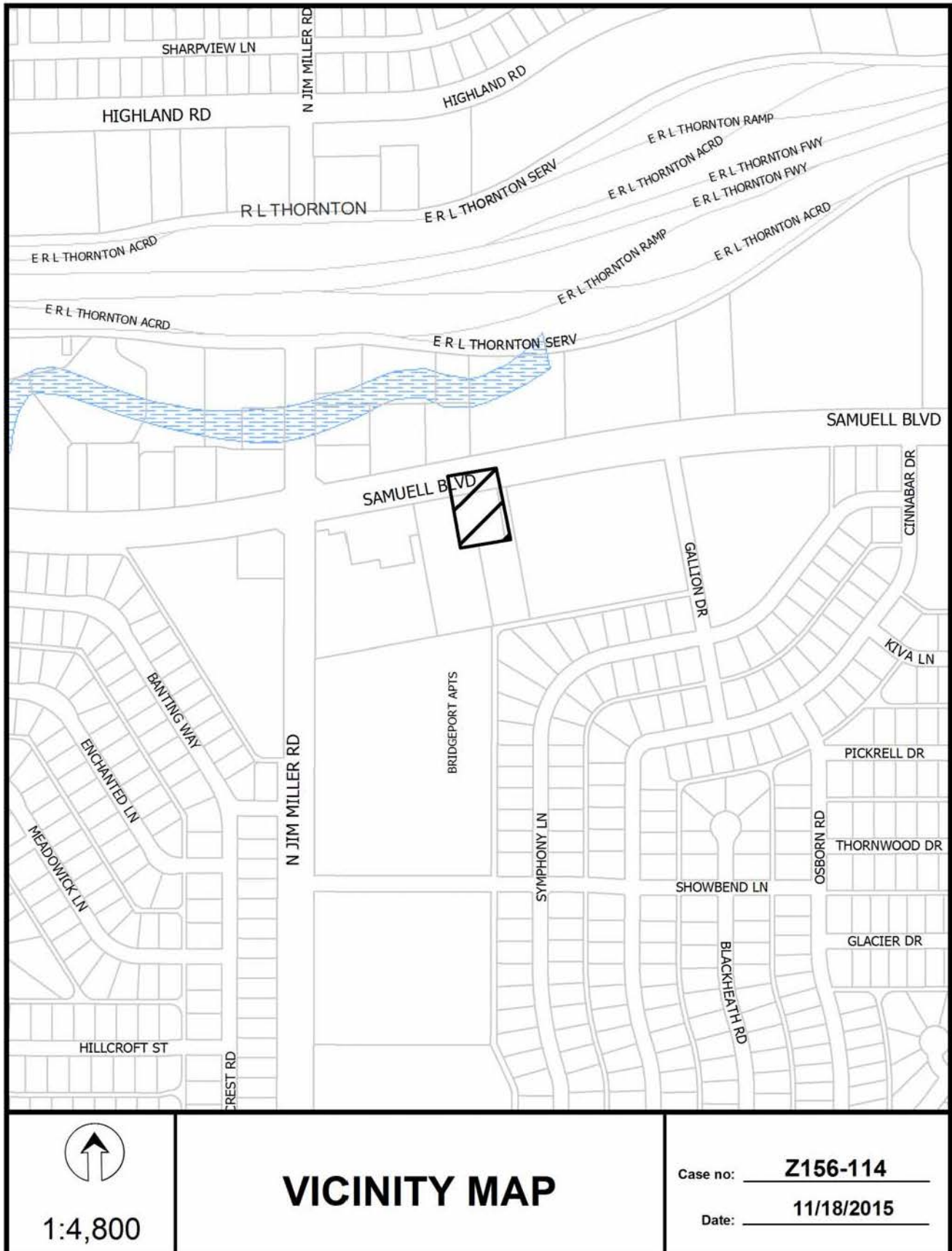
*out of the room, shown voting in favor

Notices:	Area: 200	Mailed: 9
Replies:	For: 1	Against: 0

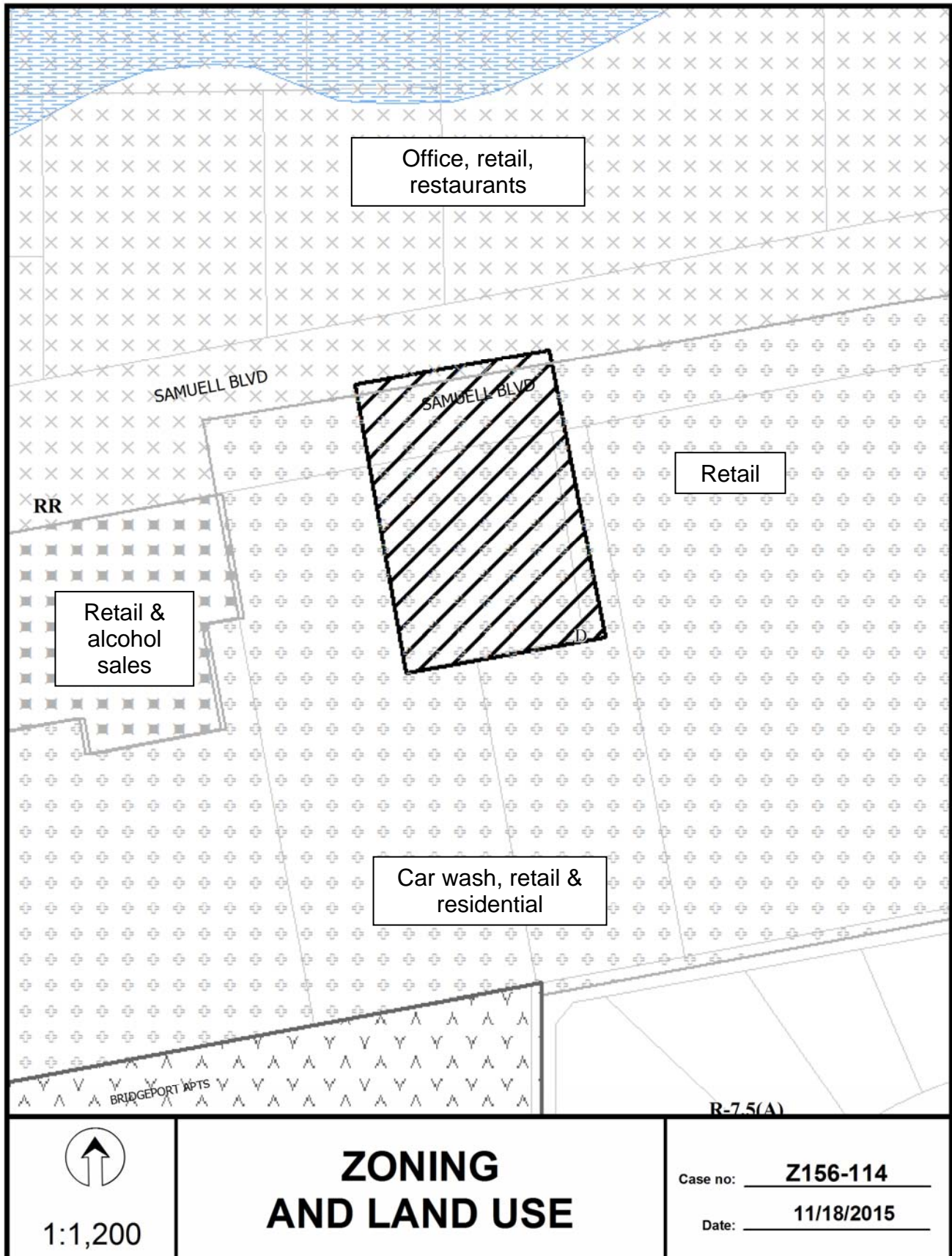
Speakers: None

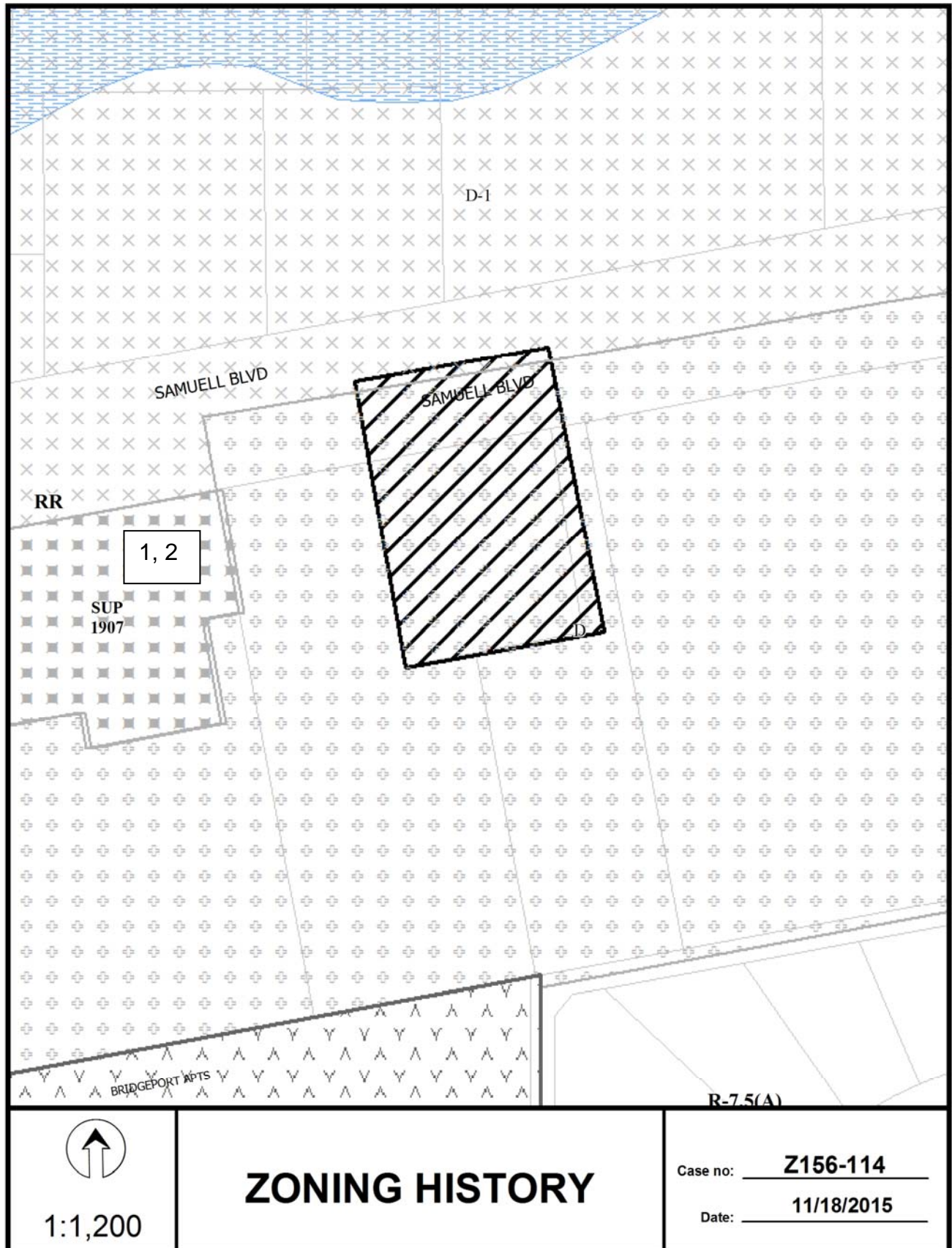
PROPOSED SITE PLAN











CPC RESPONSES



12/16/2015

Reply List of Property Owners

Z156-114

***9 Property Owners Notified
Owners Opposed***

1 Property Owners in Favor

0 Property

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
O	1	6000 SAMUELL BLVD	CHUNG JOE INKI TR &
	2	6024 SAMUELL BLVD	DRY WAYNE F
	3	6118 SAMUELL BLVD	BALLAS VICTOR
	4	6885 SAMUELL BLVD	FIREBRAND PROPERTIES LP
	5	6155 SAMUELL BLVD	SAMUELL RETAIL LTD
	6	6300 SAMUELL BLVD	SAM VILLAGE LTD PS
	7	6004 SAMUELL BLVD	PARKER MICHAEL
	8	5470 N JIM MILLER RD	CFIC LLC
	9	8344 E R L THORNTON FWY	OZINUS THORNTON LLC

AGENDA ITEM # 50

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 14

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 36 E

SUBJECT

A public hearing to receive comments regarding an application for a Planned Development District for certain CS Commercial Service District Uses, a Specific Use Permit for a Mini-warehouse, and termination of deed restrictions on property zoned a CS Commercial Service District on the north line of East University Boulevard, east of North Central Expressway

Recommendation of Staff: Denial

Recommendation of CPC: Denial without prejudice

Z134-319(RB)

FILE NUMBER: Z134-319(RB)

DATE FILED: August 19, 2014

LOCATION: North Line of East University Boulevard, East of North Central Expressway

COUNCIL DISTRICT: 14

MAPSCO: 36E

SIZE OF REQUEST: Approx. 32,190 Sq. Ft. **CENSUS TRACT:** 79.13

APPLICANT: OP Acquisitions, LLC

REPRESENTATIVE: Rob Baldwin

OWNERS: East Campus Holdings, LP; City of Dallas

REQUEST: An application for a Planned Development District for certain CS Commercial Service District Uses, a Specific Use Permit for a Mini-warehouse, and termination of deed restrictions on property zoned a CS Commercial Service District.

SUMMARY: The applicant proposes to redevelop the property with a mini-warehouse use (by Specific Use Permit) and 2,400 square feet of retail uses. The site consists of a commercial structure and surface parking area within the site's eastern half. All improvements will be removed, providing for a six story mini-warehouse use encompassing approximately 129,000 square feet of floor area. Additionally, the applicant will be terminating the existing deed restrictions that limit permitted uses (see attached deed restriction). A PDD is being requested to: 1) increase floor area, structure height, and number of stories; 2) reduce parking for a mini-warehouse; and, 3) enhance attached signage. While not specifically requested, staff is of the opinion a PDD also will consider a reduction in 'adequate' off-street parking. On October 28, 2015, the City Council remanded this request back to the City Plan Commission with the consideration of permitting the mini-warehouse use by SUP as opposed to by right.

CPC RECOMMENDATION: Denial without prejudice

STAFF RECOMMENDATION: Denial

GUIDING CRITERIA FOR RECOMMENDATION:

Staff recommends denial of the request, based upon:

1. *Performance impacts upon surrounding property* – While the proposed development standards are compatible with the built environment, the intensity of the proposed use, and a lack of adequate off-street parking could impact adjacent development.
2. *Traffic impact* – No increase in trip generations is expected, which currently are consistent with those generated by the surrounding nonresidential development.
3. *Comprehensive Plan or Area Plan Conformance* – The request is in compliance with the designated Building Block for the area, however staff envisions transit-oriented development for this area and would expect a residential component as part of any rezoning request.
4. *Justification for a Planned Development District as opposed to straight zoning* – As the applicant wishes to establish development rights over that currently permitted, a PDD was presented for consideration.

Zoning History: There has been no recent zoning activity in the immediate area relevant to the request within the past five years.

Thoroughfare

University Boulevard

Existing & Proposed ROW

Collector; 60' & 60' ROW

Traffic: The Engineering Section of the Building Inspection Division of the Department of Sustainable Development and Construction has reviewed the requested development vision in conjunction with the required Traffic Impact Analysis and determined that the proposed development will not significantly impact the street system.

STAFF ANALYSIS:

Comprehensive Plan: The Urban Mixed-Use Building Block incorporates a vibrant mix of residential and employment uses at a lower density than the Downtown Building Block. These areas are typically near Downtown, along the Trinity River or near major transit centers. Examples include Uptown, the City Place/West Village area, Stemmons Design District, Cedars and Deep Ellum. Urban Mixed-Use Building Blocks provide residents with a vibrant blend of opportunities to live, work, shop and play within a closely defined area. Buildings range from high-rise residential or mid-rise commercial towers to townhomes and small corner shops. Good access to transit is a critical

element. Similar to Downtown, the Urban Mixed-Use Building Blocks offer employment and housing options and are important economic growth areas for businesses. People on foot or bike can enjoy interesting storefronts at ground level with benches, public art, on-street parking and wide sidewalks, creating an appealing streetscape. Large parking areas and other auto-oriented land uses are typically located at the edges.

GOAL 1.2 Promote desired development.

Policy 1.2.1 Use Vision Building Blocks as a general guide for desired development patterns.

Land Use Compatibility:

On October 28, 2015, the City Council remanded this request back to the City Plan Commission with the consideration of permitting the mini-warehouse use by SUP (the City Plan Commission and City Council previously considered the mini-warehouse use by right). The predominant development pattern in the immediate area consists of nonresidential uses, heavily influenced by office structures and structured parking to serve their requirements, with retail uses concentrated to the west/northwest, and eastward (approximately 530 feet) along the Greenville Avenue alignment. An outside industrial use (University Park) is situated along the south line of University Boulevard. The DART right-of-way (Red Line) abuts the site's western property line, with various office and retail uses fronting parcels between the DART right-of-way and the northbound service road of North Central Expressway (travel lanes are depressed through this section), both north and south of University Boulevard. With respect to DART's presence in the immediate area, both the Lovers Lane Station as well as the Mockingbird Station (Park and Ride) serves this area; approximately 880 and 2,900 feet, respectively.

As the current zoning permits the mini-warehouse use by right, the applicant's intent is to utilize this CS District zoning as a base for certain permitted uses (i.e., prohibiting some uses allowed either by right or by SUP) along with the intended mini-warehouse development while requesting an increase in development rights (see table, below). While the use is not of concern to staff, it was strongly encouraged to come back with a mixed use incorporating a component of multifamily dwellings. As the site abuts office and structured parking to the north and east, it would be envisioned the units would be oriented towards unobstructed views west/northwest/south. As the site is constrained by its platted lot, off-street parking could be a concern; however, the site's proximity to mass transit would allow for consideration of reduced parking requirements for residential as part of a mixed use project. With respect to structural design, it would be anticipated at grade podium parking would be incorporated into the building's design.

The applicant is proposing the minimum amount of parking for the use when compared

to existing use categories with similar floor area (office, retail; see Off-street parking section, below). As such, the proposed redevelopment is not 'taxing' when considering land area required for parking and maneuvering. As the site is somewhat restricted in size, the majority of redevelopment options per the attached list of permitted uses would tend to be one-story, again being somewhat impacted by off-street parking requirements.

At this point, the applicant has submitted revised conditions and a development/site plan (for mini-warehouse use) that captures the intent of what was discussed during the previously referenced City Council public hearing. In consideration of the applicant's current, staff would recommend the following additional provisions for the mini-warehouse use (in addition to a residential component as provided above):

- 1) in addition to required parking for the floor area devoted to rental units, require an additional off-street parking space for a caretaker's quarters (whether anticipated now or in the future);*
- 2) consider an increase in required parking to adequately serve the proposed development;*

In consideration of the portion of the request to permit a mini-warehouse by SUP, the general provisions for a Specific Use Permit in Section 51A-4.219 of the Dallas Development Code specifically state: (1) The SUP provides a means for developing certain uses in a manner in which the specific use will be consistent with the character of the neighborhood; (2) Each SUP application must be evaluated as to its probable effect on the adjacent property and the community welfare and may be approved or denied as the findings indicate appropriate; (3) The city council shall not grant an SUP for a use except upon a finding that the use will: (A) complement or be compatible with the surrounding uses and community facilities; (B) contribute to, enhance, or promote the welfare of the area of request and adjacent properties; (C) not be detrimental to the public health, safety, or general welfare; and (D) conform in all other respects to all applicable zoning regulations and standards. The regulations in this chapter have been established in accordance with a comprehensive plan for the purpose of promoting the health, safety, morals, and general welfare of the city.

Other than the two items noted above, the applicant has provided for certain provisions previously addressed by staff [1) prohibit outside display and storage of rental vehicles and other materials, and 2) require access to storage units from an internal corridor]. As such, the request complies with the general provisions for an SUP.

With respect to the part of the request to terminate existing deed restrictions, staff has no objections to this. As the applicant has established specific uses, those overlapping uses (in comparison to those restricted in the existing deed restrictions) are not being requested, thus staff supports the termination of deed restrictions.

In summary of this analysis, staff cannot support the applicant's request as currently

presented. It should be stated that lack of support is not related to the requested increase in development rights nor the more restrictive provisions related to the mini-warehouse use provided within the attached SUP conditions, but rather a lack of a development that is more of a transit oriented development, with or without a mini-warehouse component. The attached PDD and SUP conditions as presented by the applicant have been reviewed and provided should the City Council proceed with support for the request.

Landscaping: The applicant will be complying with landscape requirements as outlined in Article X (street trees, site trees, two design standards) during the building permit process.

Off-Street Parking: As noted above, any redevelopment of the site will largely be impacted by an ability to provide parking on site. For purposes of this section, consideration of special parking (remote, shared) is not being considered.

By definition, the mini-warehouse use permits a maximum of 500 square feet of floor area per rental unit; required off-street parking is a minimum of six spaces, what is being proposed. Based on the applicant's development plan, 258 rental units could be available (applicant has not established a maximum number of rental units). As noted above, staff is concerned as to adequate parking being provided.

By comparison, a 129,000 square foot office would require 387 parking spaces; an 8,000 square foot restaurant would require 80 spaces; or, an 8,000 square foot furniture store would require 16 spaces.

The majority of mini-warehouse projects are developed in a horizontal pattern; i.e., one and two story structures across a parcel, with parking spaces interspersed throughout the development and located in close proximity to the rental units.

As a truly vertical product, one such does exist and was approved (also as a PDD) and may provide the City Council with a comparison in assessing this request. This particular development provides for 79,740 square feet of floor area within a four story structure, providing parking at one space for each 4,200 square feet of floor area.

Signage: The applicant is governed by sign provisions for a business zoning district. With respect to attached signage, the applicant is requesting an effective area of 25 percent for both the western and southern façade.

PROVISION	CS (existing)	PROPOSED PDD
F/S/R SETBACKS	15'/0'/0'	NO REVISION
DENSITY	N/A	N/A
HEIGHT	45 FEET	80 FEET
STORIES	THREE	SIX
FLOOR AREA	24,176 SF (RESTRICTED OFFICE/LODGING/ RETAIL TO 16,117 SF)	126,538 SF/MINI-WHSE; 2,400 SF/RETAIL
LOT COVERAGE	80%	N/A
OFF-STREET PARKING	PER USE-DALLAS DEVELOPMENT CODE	6 SPACES/MINI-WHSE (NO PARKING FOR CARETAKERS QTRS); CODE REQ. FOR RETAIL
LANDSCAPE	ARTICLE X	N/A
SIGNS	BUSINESS ZONING DISTRICT	ATTACHED SIGNS-≤25% OF AREA OF WEST AND SOUTHERN, EACH

CPC ACTION

On November 19, 2015, the City Plan Commission held this request under advisement until December 17, 2015.

December 17, 2015

Motion: It was moved to recommend **denial without prejudice** for a Planned Development District for certain CS Commercial Service District Uses, a Specific Use Permit for a Mini-warehouse, and the termination of deed restrictions on property zoned a CS Commercial Service District on the north line of East University Boulevard, east of North Central Expressway.

Maker: Ridley
Second: Schultz
Result: Carried: 14 to 0

For: 14 - Anglin, Emmons*, Houston, Davis, Shidid,
Anantasomboon, Abtahi, Haney, Jung, Schultz,
Peadon, Murphy, Ridley, Tarpley

Against: 0
Absent: 0
Vacancy: 0
Conflict: 1 - Housewright

*out of the room, shown voting in favor

Notices: Area: 500 Mailed: 29
Replies: For: 2 Against: 1

Speakers: For: Rob Baldwin, 3904Elm St., Dallas, TX, 75226
Against: None

LIST OF OFFICERS

Applicant:

OP Acquisitions, LLC

Clay Likover, officer

Owner:

East Campus Holdings, LP

Seth Bame, officer

APPLICANT REQUESTED/STAFF RECOMMENDED CONDITIONS

“ARTICLE ____.

PD ____.

SEC. 51P-____.101. LEGISLATIVE HISTORY.

PD ____ was established by Ordinance No._____, passed by the Dallas City Council on _____.

SEC. 51P- ____ .102. PROPERTY LOCATION AND SIZE.

PD _____ is established on property located on the north line of University Boulevard, east of Central Expressway. The size of PD _____ is approximately 0.74 acres.

SEC. 51P- ____ .103. DEFINITIONS AND INTERPRETATIONS.

(a) Unless otherwise stated, the definitions and interpretations in Chapter 51A apply to this article.

(b) Unless otherwise stated, all references to articles, divisions, or sections in this article are to articles, divisions, or sections in Chapter 51A.

(c) This district is considered to be a nonresidential zoning district.

SEC. 51P- ____ .104. EXHIBITS.

The following exhibit is incorporated into this article: Exhibit ____A: development plan and mini-warehouse site plan.

SEC. 51P- ____ .105. DEVELOPMENT PLAN.

(a) Except as provided in this section, development and use of the Property must comply with the development plan and mini-warehouse site plan. (Exhibit ____A). If there is a conflict between the text of this article and the development plan, the text of this article controls.

(b) For a mini-warehouse, the development plan and mini-warehouse site plan will serve as the required site plan to fulfil, in part, the requirements for a Specific Use Permit site plan.

SEC. 51P- ____ .106. MAIN USES PERMITTED.

The following uses are the only main uses permitted:

(1) Agricultural uses.

- Crop production.

(2) Commercial and business service uses.

- Building repair and maintenance shop.
- Catering service.
- Custom business services.
- Electronics service center.
- Medical or scientific laboratory. *[SUP]*
- Tool or equipment rental.

(3) Industrial uses.

- Gas drilling and production. *[SUP]*
- Industrial (inside) for light manufacturing, limited to a bakery.
- Temporary concrete or asphalt batching plant. *[By special authorization of the building official.]*

(4) Institutional and community service uses.

- Adult day care facility.
- Cemetery or mausoleum. *[SUP]*
- Child-care facility.
- Church.
- College, university, or seminary.
- Community service center. *[SUP]*
- Convent or monastery.
- Hospital. *[SUP]*
- Public school.
- Open enrollment charter school. *[SUP]*
- Private school. *[SUP]*

(5) Lodging uses.

- Hotel or motel. *[See Section 51A-4.205(1)(B).]*
- Lodging or boarding house. *[SUP]*
- Overnight general purpose shelter.

(6) Miscellaneous uses.

- Attached non-premise sign. *[SUP]*
- Carnival or circus (temporary). *[By special authorization of the building official.]*
- Temporary construction or sales office.

(7) Office uses.

- Alternative financial establishment. *[SUP]*
- Financial institution without drive-in window.
- Financial institution with drive-in window.
- Medical clinic or ambulatory surgical center.
- Office.

(8) Recreation uses.

- Country club with private membership.
- Private recreation center, club, or area.
- Public park, playground, or golf course.

(9) Residential uses.

- College dormitory, fraternity, or sorority house.

(10) Retail and personal service uses.

- Alcoholic beverage establishments. *[See Section 51A-4.210(b)(4).]*
- Ambulance service.
- Animal shelter or clinic without outside runs.
- Auto service center.
- Business school.
- Car wash.
- Commercial amusement (inside). *[SUP may be required. See Section 51A-4.210(b)(7)(B)]*
- Commercial amusement (outside). *[SUP]*
- Commercial parking lot or garage.
- Convenience store with drive-through. *[SUP]*
- Dry cleaning or laundry store.
- Furniture store.
- General merchandise or food store 3,500 square feet or less.
- General merchandise or food store greater than 3,500 square feet.
- General merchandise or food store 100,000 square feet or more. *[SUP]*
- Home improvement center, lumber, brick, or building materials sales yard.
- Household equipment and appliance repair.
- Liquor store.
- Mortuary, funeral home, or commercial wedding chapel.
- Motor vehicle fuelling station.
- Nursery, garden shop, or plant sales.

- Pawn shop.
- Personal service uses.
- Restaurant without drive-in or drive-through service.
- Restaurant with drive-in or drive-through service.
- Swap or buy shop. *[SUP]*
- Temporary retail use.
- Theater.

(11) Transportation uses.

- Transit passenger shelter.
- Transit passenger station or transfer center.

(12) Utility and public service uses.

- Commercial radio or television transmitting station.
- Electrical substation.
- Local utilities.
- Police or fire station.
- Post office.
- Radio, television, or microwave tower.
- Tower/antenna for cellular communication.
- Utility or government installation other than listed.

(13) Wholesale, distribution, and storage uses.

- Mini-warehouse. *[SUP and only in conjunction with a minimum of 2,400 square of retail and personal service uses.]*
- Office showroom/warehouse.
- Recycling buy-back center. *[SUP or RAR may be required. See Section 51A-4.213(11).]*
- Recycling collection center. *[SUP or RAR may be required. See Section 51A-4.213(11.1).]*
- Recycling drop-off container. *[SUP required if the requirements of Section 51A-4.213(11.2)(E) are not satisfied.]*
- Recycling drop-off for special occasion collection. *[SUP required if the requirements of Section 51A-4.213(11.3)(E) are not satisfied.]*

SEC. 51P-____.107.

ACCESSORY USES.

As a general rule, an accessory use is permitted in any district in which the main use is permitted. Some specific accessory uses, however, due to their unique nature, are subject to additional regulations in Section 51A-4.217. For more information regarding accessory uses, consult Section 51A-4.217.

SEC. 51P-____.108.

YARD, LOT, AND SPACE REGULATIONS.

(Note: The yard, lot, and space regulations in this section must be read together with the yard, lot, and space regulations in Division 51A-4.400. If there is a conflict between this section and Division 51A-4.400, this section controls.)

(a) Except as provided in this section, the yard, lot, and space regulations for the CS Commercial Service District apply.

(b) Floor area. Maximum floor area is 128,938 square feet.

(c) Height. Maximum structure height is 80 feet.

(d) Stories. Maximum number of stories above grade is six.

SEC. 51P- _____.109.

OFF-STREET PARKING AND LOADING.

(a) Except as provided in this section, consult the use regulations in Division 51A-4.200 for the specific off-street parking and loading requirements for each use.

Applicant requested:

(b) Mini-warehouse. A minimum of six off-street parking spaces must be provided.
--

Staff recommended:

(b) For a mini-warehouse, one space for each 4,200 square feet of floor area is required, plus one additional space for a caretaker's quarters.

SEC. 51P- _____.110.

ENVIRONMENTAL PERFORMANCE STANDARDS.

See Article VI.

SEC. 51P- _____.111.

LANDSCAPING.

(a) Landscaping must be provided in accordance with Article X.

(b) Plant materials must be maintained in a healthy, growing condition.

SEC. 51P- _____.112.

SIGNS.

(a) Except as provided in this section, signs must comply with the provisions for business zoning districts in Article VII.

(b) Attached signs.

(1) The maximum effective area for the west façade is 25 percent of this façade area.

(2) The maximum effective area for the south façade is 25 percent of this façade area.

SEC. 51P- _____.113. ADDITIONAL PROVISIONS.

(a) The Property must be properly maintained in a state of good repair and neat appearance.

(b) Development and use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the city.

SEC. 51P- _____.114. COMPLIANCE WITH CONDITIONS.

(a) All paved areas, permanent drives, streets, and drainage structures, if any, must be constructed in accordance with standard city specifications, and completed to the satisfaction of the city.

(b) The building official shall not issue a building permit to authorize work, or a certificate of occupancy to authorize the operation of a use, until there has been full compliance with this article, the Dallas Development Code, the construction codes, and all other ordinances, rules, and regulations of the city.

**APPLICANT REQUESTED
SPECIFIC USE PERMIT FOR A MINI-WAREHOUSE**

1. USE. The only use authorized by this specific use permit is a mini-warehouse.
2. SITE PLAN: Use and development of the property must comply with the attached development plan and mini-warehouse site plan.
3. TIME LIMIT: This specific use permit expires on 15 YEARS, but is eligible for automatic renewal for an additional 15-year period pursuant to Section 51A-4.219 of Chapter 51A of the Dallas City Code, as amended. For automatic renewal to occur, the Property owner must file a complete application for automatic renewal with the director before the expiration of the current period. Failure to timely file a complete application will render this specific use permit ineligible for automatic renewal. (Note: The Code currently provides that applications for automatic renewal must be filed after the 180th but before the 120th day before the expiration of the current specific use permit period. The Property owner is responsible for checking the Code for possible revisions to this provision. The deadline for applications for automatic renewal is strictly enforced.)
4. ACCESS TO STORAGE UNITS: All storage units must be accessed from an internal corridor.
5. FLOOR AREA: The maximum floor area is 126,538 square feet.
6. RENTAL AND OUTSIDE DISPLAY: Rental and outside display of vehicles and trailers normally associated with moving goods and personal possessions from one location to another are prohibited.
7. MAINTENANCE: The Property must be properly maintained in a state of good repair and neat appearance.
8. GENERAL REQUIREMENTS: Use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the City of Dallas.

941995

DEED RESTRICTIONS *WJS*

THE STATE OF TEXAS)
) KNOW ALL PERSONS BY THESE PRESENTS:
 COUNTY OF DALLAS)

I.

The undersigned, **DEMAC CORP., a Texas corporation** (the "Owner"), is the owner of the property located in the City of Dallas, Dallas County, Texas described in Exhibit A attached hereto and made a part hereof (the "Property"), such Property being more commonly known as 5531 East University Boulevard, Dallas, Texas and being the same property conveyed to Owner by Warranty Deed dated April 10, 1980 from Mary Alice Dealey and recorded in Volume 80113, Page 1871 of the Deed Records of Dallas County, Texas. *WJS*

II.

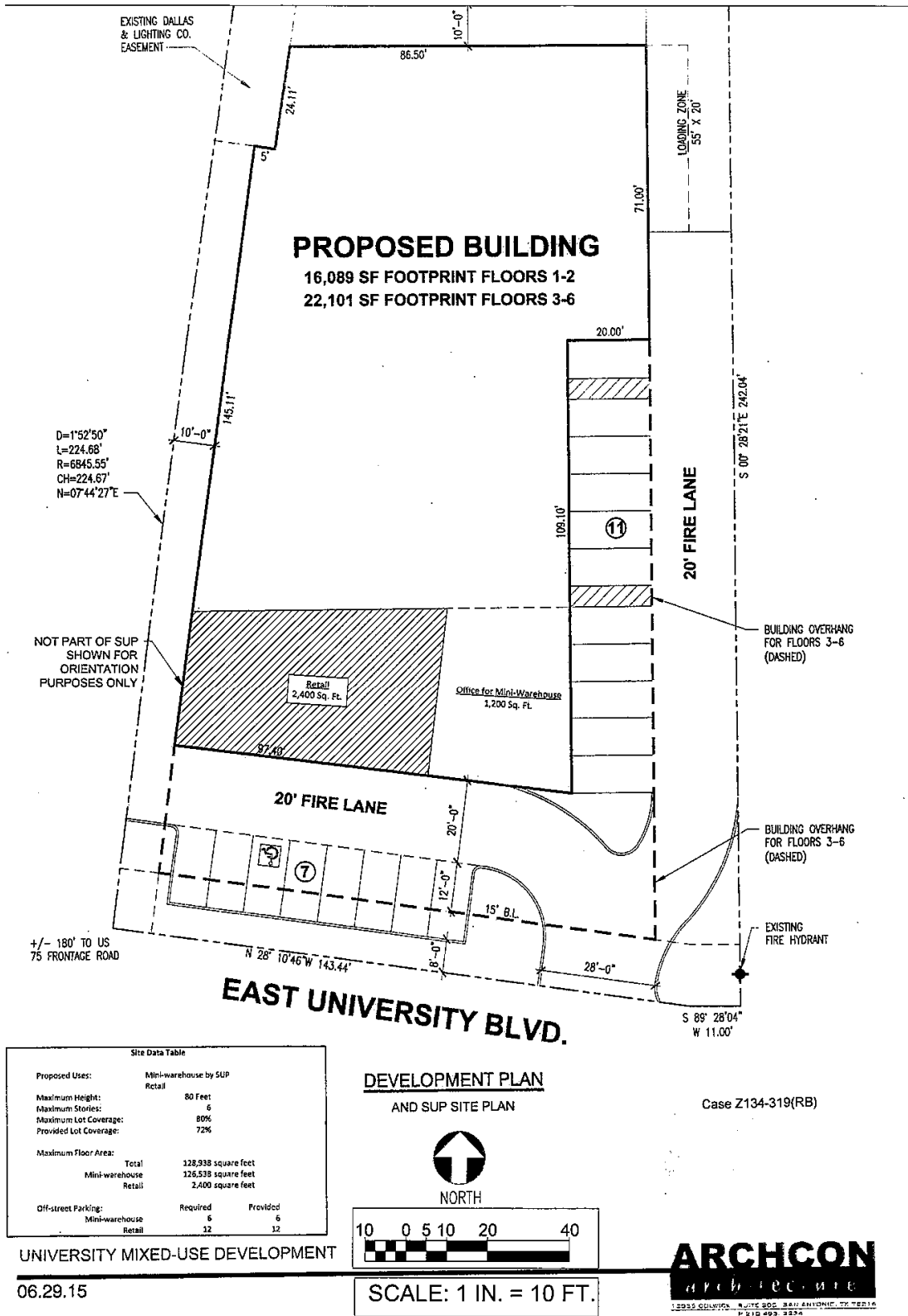
The Owner does hereby impress all of the Property with the following deed restriction (the "Restriction"), to wit:

The following shall be the only uses permitted on the Property:

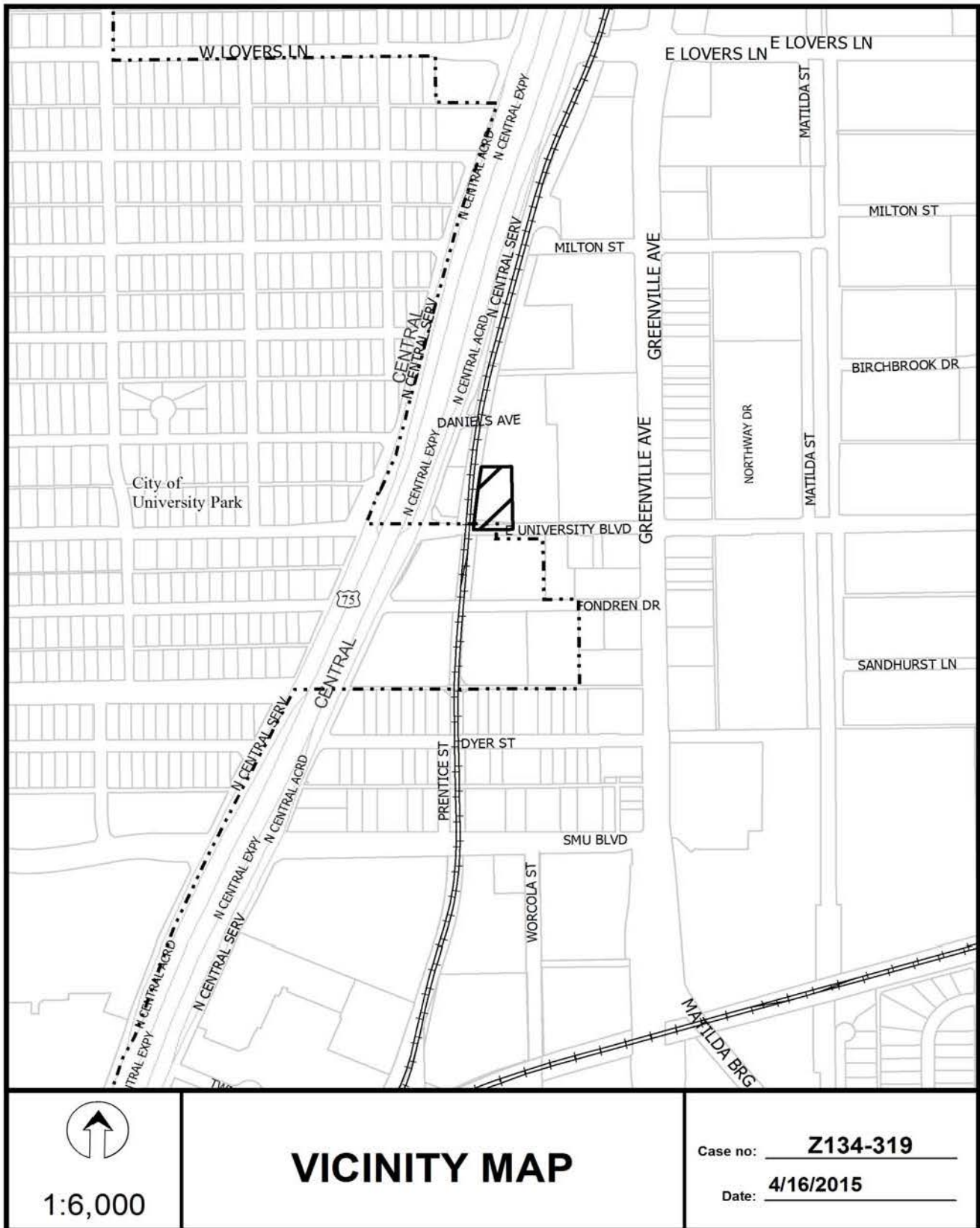
- 1) Industrial (Inside) for Light Manufacturing Uses limited to the operation of a Commercial Bakery and uses customarily incidental to a Commercial Bakery. For purposes hereof, a "Commercial Bakery" is defined as an industrial facility where the baking and processing of baked goods takes place wholly within an enclosed building.
- 2) Office Showroom/Warehouse Uses.
- 3) Those uses permitted in the CR Community Retail District.

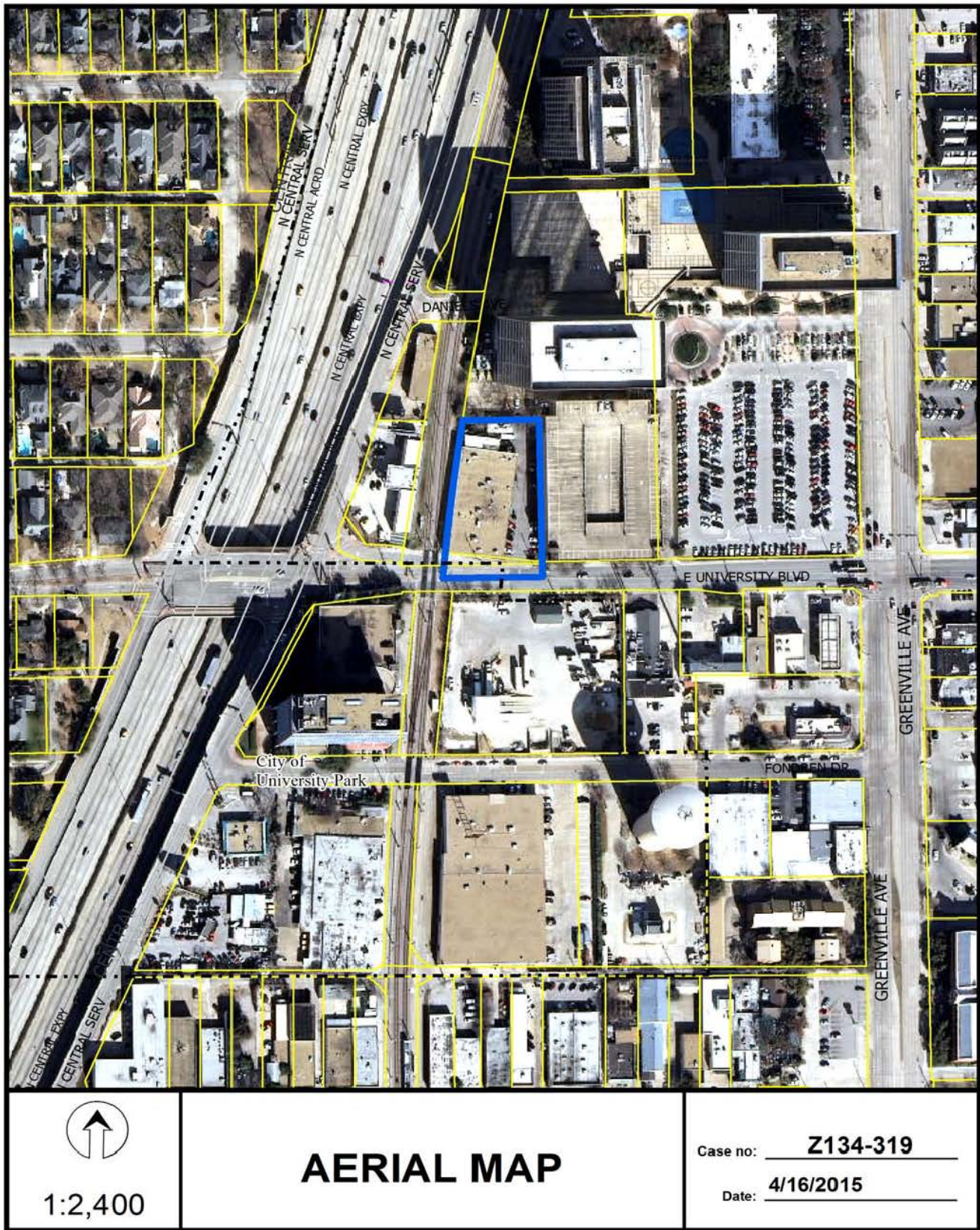
III.

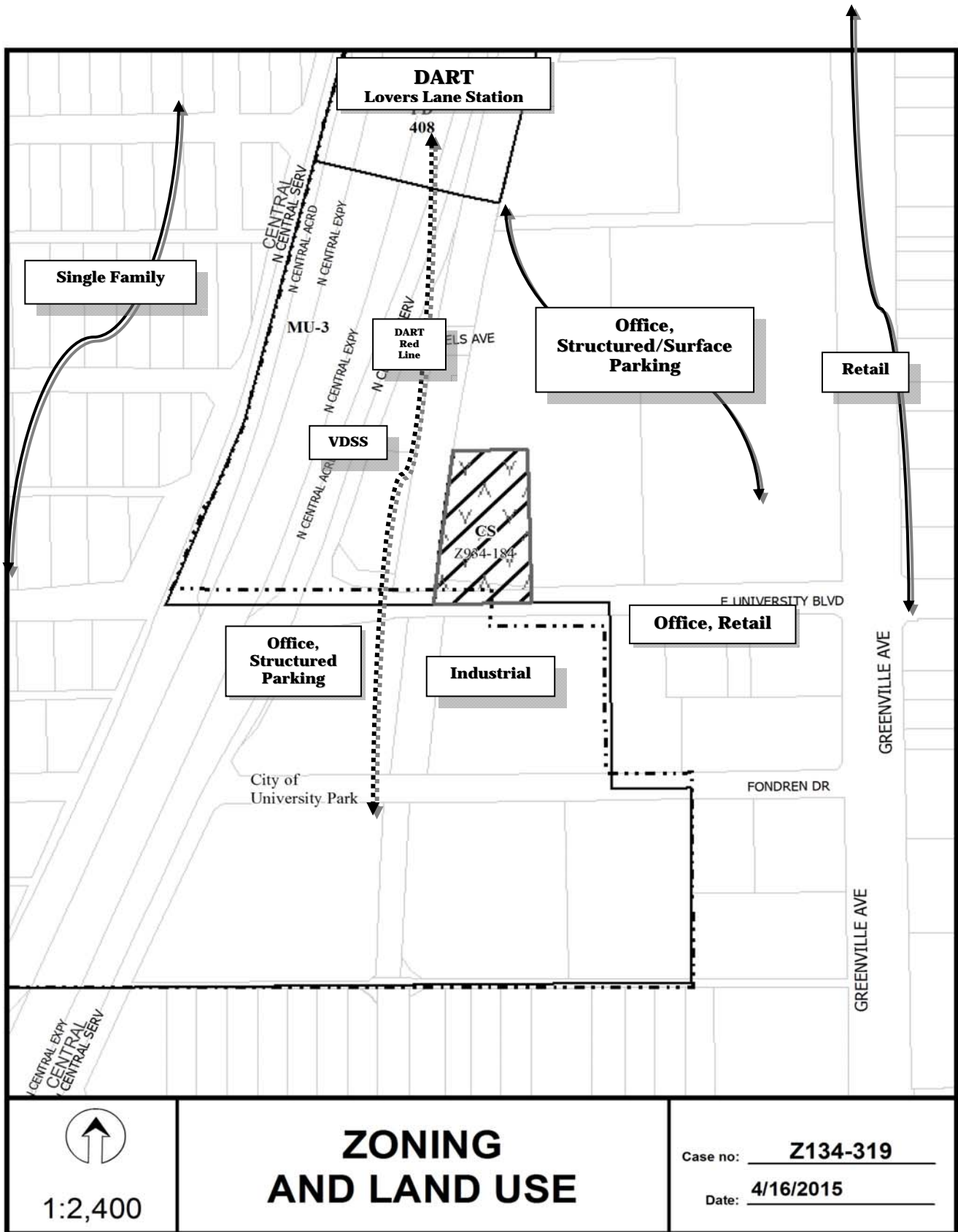
The Restriction shall continue in full force and effect for a period of twenty (20) years from the date of execution, and shall automatically be extended for additional periods of ten (10) years unless amended or terminated in the manner specified in this document.



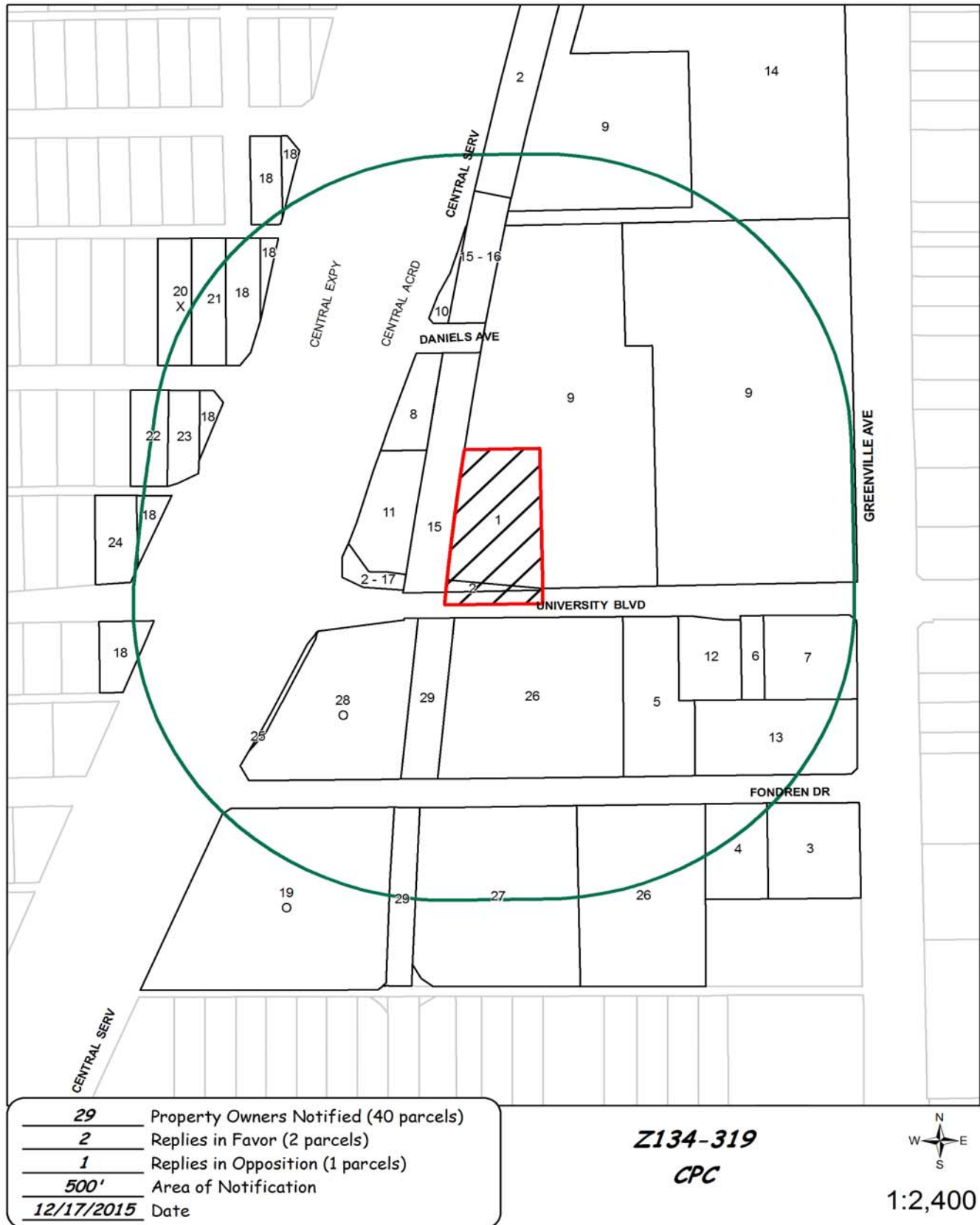
Proposed Development Plan and Mini-warehouse Site Plan







CPC RESPONSES



01/04/2016

Reply List of Property Owners***Z134-319******29 Property Owners Notified******2 Property Owners in Favor******1 Property Owners Opposed***

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	1	5531 E UNIVERSITY BLVD	EAST CAMPUS HOLDINGS LP
	2	5603 MILTON ST	DALLAS AREA RAPID TRANSIT
	3	4645 GREENVILLE AVE	SAYAH INVESTMENTS LP
	4	5620 FONDREN DR	DDW PROPERTIES LLC
	5	5614 E UNIVERSITY BLVD	FIFTY SIX FOURTEEN
	6	5626 E UNIVERSITY BLVD	CAMPBELL RANDY &
	7	4747 GREENVILLE AVE	ZAKI FAMILY PARTNERSHIP LP THE
	8	6530 N CENTRAL EXPY	6530 N CENTRAL LLC
	9	4925 GREENVILLE AVE	LOCH ENERGY SQUARE LP
	10	6600 N CENTRAL EXPY	UNIVERSITY SIGN PARTNERSHIP
	11	5515 E UNIVERSITY BLVD	SAAD BASEL R
	12	5622 E UNIVERSITY BLVD	HOPPER JAMES T & KATHLEEN
	13	4703 GREENVILLE AVE	EDWARD T FARRIS CO LTD
	14	5646 MILTON ST	MEADOWS BUILDING LLC
	15	555 2ND AVE	DART
	16	555 2ND AVE	DART
	17	403 REUNION BLVD	DALLAS AREA RAPID TRANSIT
	18	2801 UNIVERSITY BLVD	UNIVERSITY PARK CITY OF
O	19	6400 N CENTRAL EXPY	MUSTANG MOCKINGBIRD PROPERTIES
X	20	2708 DANIEL AVE	FARR MARGARET JEAN
	21	2706 DANIEL AVE	DUNSTON JAYSON & HEATHER
	22	2709 DANIEL AVE	KAPLAN CHERYL H & ARTHUR
	23	2705 DANIEL AVE	UNIVERSITY PARK CITY OF
	24	2804 UNIVERSITY BLVD	CAHILL JOHN E
	25	6400 N CENTRAL EXPY	DALLAS AREA RAPID TRANSIT
	26	2525 UNIVERSITY BLVD	UNIVERSITY PARK CITY OF

Z134-319(RB)

01/04/2016

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	27	2651 FONDREN DR	JARRELL CREDIT EQUIVALENT
O	28	6440 N CENTRAL EXPY	TURLEY WINDLE & SHIRLEY A
	29	2600 FONDREN DR	DALLAS AREA RAPID TRANSIT

AGENDA ITEM # 51

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 45 Q

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for a flea market on property zoned Subdistrict 2 of Planned Development District No. 357, the Farmers Market Special Purpose District on the south corner of St. Louis Street and South Harwood Street

Recommendation of Staff: Approval for a three-year period with eligibility for automatic renewals for additional three-year periods, subject to a site plan and conditions

Recommendation of CPC: Approval for a ten-year period with eligibility for automatic renewals for additional ten-year periods, subject to a site plan and conditions

Z145-361(WE)

ACM: Ryan S. Evans

FILE NUMBER: Z145-361(WE) **DATE FILED:** September 24, 2015**LOCATION:** South corner of St. Louis Street and South Harwood Street**COUNCIL DISTRICT:** 2 **MAPSCO:** 45Q**SIZE OF REQUEST:** Approx. 7,840.8 sq. ft. **CENSUS TRACT:** 204.00

APPLICANT: Rubio's Sweet Harvest**OWNER:** Dallas DSL PPTY Management LP**REPRESENTATIVE:** Audra Buckley, Permitted Development**REQUEST:** An application for a Specific Use Permit for a flea market on property zoned Subdistrict 2 of Planned Development District No. 357, the Farmers Market Special Purpose District.**SUMMARY:** The purpose of this request is to allow the applicant to operate a flea market [Sweet Harvest Produce] within a building utilized for warehouse uses. The applicant has operated a flea market within the Farmers Market, but had to relocate as a result of the redevelopment. Planned Development District No. 357 defines a Farmers Market as a publicly owned or operated area for the sale and exchange of agricultural produce, food, and general merchandise by vendors. A Flea Market is defined as a privately owned and operated area for the sale and exchange of food and general merchandise by vendors.**CPC RECOMMENDATION:** Approval for a ten-year period with eligibility for automatic renewals for additional ten-year periods, subject to a site plan and conditions.**STAFF RECOMMENDATION:** Approval for a three-year period with eligibility for automatic renewals for additional three-year periods, subject to a site plan and conditions.

GUIDING CRITERIA FOR RECOMMENDATION:

The following factors are listed in Chapter 51A of the Dallas Development Code to guide the determination as to whether or not an SUP shall be granted. Staff has listed its findings based upon each component below:

1. *Compatibility with surrounding uses and community facilities* – On the northeast side of South Harwood is the redevelopment of the Farmer's Market with multifamily residential. Warehouse uses are developed on the southwest side of South Harwood Street, where the subject site is located. There is a concern that there could be a conflict with pedestrian activity from the flea market. As a result, there is a condition that a sidewalk must be delineated through the driveway aisles. Further, the time period will allow staff an opportunity to evaluate the compatibility.
2. *Contribution to, enhancement, or promoting the welfare of the area of request and adjacent properties* – The flea market will contribute to the welfare of the area by providing an additional location where the surrounding neighborhood can purchase goods and services. The SUP has conditions that are associated with the use. An SUP can have a reduced time period or not be renewed if the applicant does not comply or the use is found to be incompatible.
3. *Not a detriment to the public health, safety, or general welfare* – The use is not anticipated to be a detriment to the public welfare.
4. *Conforms in all other respects to all applicable zoning regulations and standards* – The request conforms to all applicable zoning regulations and standards. However, an SUP is required for a flea market use.

BACKGROUND INFORMATION:

- In January, 2013, the Farmers Market TIF District Board of Directors approved an amendment to the Project and Reinvestment Zone Financing Plan for the Farmers Market TIF District to: 1) increase the geographic area of the Farmers Market TIF District, 2) extend the term of the Farmers Market TIF District, 3) increase the total Farmers Market TIF District budget, 4) decrease the percentage of tax increment contributed by the City of Dallas, and 5) allow direct sales to implement the plan.
- In February 2013, the City Council approved a resolution authorizing a Master Agreement for the Redevelopment of the Dallas Farmers Market with a developer which outlined a redevelopment plan for the Dallas Farmers Market as well as the signage.

Zoning History: There has been one zoning case in the area over the past five years.

1. **Z123-317** On November 12, 2013, the City Council approved the creation of a new subdistrict for mixed uses within Planned Development District No. 357, the Farmer's Market Special Purpose District generally bounded by Marilla

Street, S. Ceasar Chavez Boulevard, East R.L. Thornton Freeway and South Harwood Street.

Thoroughfares/Streets:

Thoroughfare/Street	Type	Existing ROW	Proposed ROW
South Hardwood Street	Collector	80 ft.	80 ft.
St. Louis Street	Local	66 ft.	66 ft.

Traffic: The Engineering Section of the Department of Sustainable Development and Construction has reviewed the request and determined that it will not have a detrimental impact on the surrounding street system.

STAFF ANALYSIS:

COMPREHENSIVE PLAN: The *fowardDallas! Comprehensive Plan* was adopted by the City Council in June 2006. The *fowardDallas! Comprehensive Plan* outlines several goals and policies which can serve as a framework for assisting in evaluating the applicant's request. The Plan identifies the request site being within the Downtown Building Block.

The Downtown is a centrally located hub that provides high intensity, concentrated regional job and commercial activity supported by high-density housing. A Downtown includes pedestrian-oriented and mixed-use development and offers multiple transportation options. Ground floors of tall buildings feature shops with many windows for visual interest and safety while the streetscape incorporates trees for shade, wide sidewalks and easy-to-use signs for finding points of interest. Civic and open spaces provide an inviting atmosphere for pedestrians as well as a diversity of uses, generating activity throughout the day and evening.

The Downtown Building Block is an important transit hub for commuter rail, light rail, bus and local systems. This area warrants significant public investment to sustain itself as the signature address in the North Central Texas region and to continually reinvent itself to maintain its competitive advantage.

Land Use:

	Zoning	Land Use
Site	PDD No.357, Subdistrict 2	Warehouse
North	PDD No.357, Subdistrict 1A	Multifamily, retail uses under construction
South	PDD No.357, Subdistrict 2	Warehouse
East	PDD No.357, Subdistrict 1A	Multifamily, retail uses under construction
West	PDD No.357, Subdistrict 2	Warehouse/food processing

Land Use Compatibility: The request site is developed with a one story, 7,580 square foot building. The certificate of occupancy is for a warehouse. However, the appropriate use for the operation is a flea market. The applicant will use approximately 4,420 square feet of the structure for the flea market and the remaining 3,366 square feet will be used for indoor storage for cold products.

The surrounding land uses consist of a warehouse, warehouse distribution and processing uses. East of the site, across South Hardwood Street, is under construction with the development of multifamily and retail uses [Farmers Market].

The general provisions for a Specific Use Permit in Section 51A-4.219 of the Dallas Development Code specifically state: (1) The SUP provides a means for developing certain uses in a manner in which the specific use will be consistent with the character of the neighborhood; (2) Each SUP application must be evaluated as to its probable effect on the adjacent property and the community welfare and may be approved or denied as the findings indicate appropriate; (3) The city council shall not grant an SUP for a use except upon a finding that the use will: (A) complement or be compatible with the surrounding uses and community facilities; (B) contribute to, enhance, or promote the welfare of the area of request and adjacent properties; (C) not be detrimental to the public health, safety, or general welfare; and (D) conform in all other respects to all applicable zoning regulations and standards. The request does not appear to have an adverse impact on the surrounding zoning and land uses.

Landscaping: Landscaping must be in accordance with the landscaping requirements in Planned Development District No. 357, the Farmers Market Special Purpose District. The request site will not trigger any landscaping because there is no increase in the total floor area or impervious surface.

Parking: The existing warehouse structure was built prior to the approval of PDD No. 357, the Farmers Market Special Purpose District. The off-street parking requirement for a warehouse use under the CA-2(A) District states that “no off-street parking is required for a building with 5,000 square feet or less of floor area.” The applicant will use only 4,420 square feet of floor of the warehouse for the flea market use. This area

Z145-361(WE)

will be delineated on the site plan. The remaining 3,366 square feet of floor area will be used for the storage of cold products.

It is anticipated that the flea market will receive additional pedestrians from the new residences that are being constructed. However, there will be additional off-street parking on St. Louis Street and from the new development in the Farmers Market.

CPC Action (December 3, 2015)

Note: The Commission considered this item individually.

Motion: It was moved to recommend **approval** of a Specific Use Permit for a flea market for a ten-year period with eligibility for automatic renewals for additional ten-year periods, subject to a revised site plan and conditions on property zoned Subdistrict 2 of Planned Development District No. 357, the Farmers Market Special Purpose District on the south corner of St. Louis Street and South Harwood Street.

Maker: Emmons
Second: Abtahi
Result: Carried: 14 to 0

For: 14 - Anglin, Emmons, Houston, Davis, Shidid,
Abtahi, Haney, Jung, Housewright, Schultz, Peadon,
Murphy, Ridley, Tarpley

Against: 0
Absent: 1 - Anantasomboon
Vacancy: 0

Notices: Area: 200 Mailed: 6
Replies: For: 0 Against: 0

Speakers: For (Did not speak): Audra Buckley, 416 S. Ervay St.,
Dallas, TX, 75201
Against: None

LIST OF OFFICERS

Dallas DSL Property Management, LP

Pro Deuce Holding LLC

- James L. Ingendorf, Managing Member
- Leslie Ingendorf, Managing Member
- Doris B. Rodgers, Member
- Rodgers Family Trust A, Member

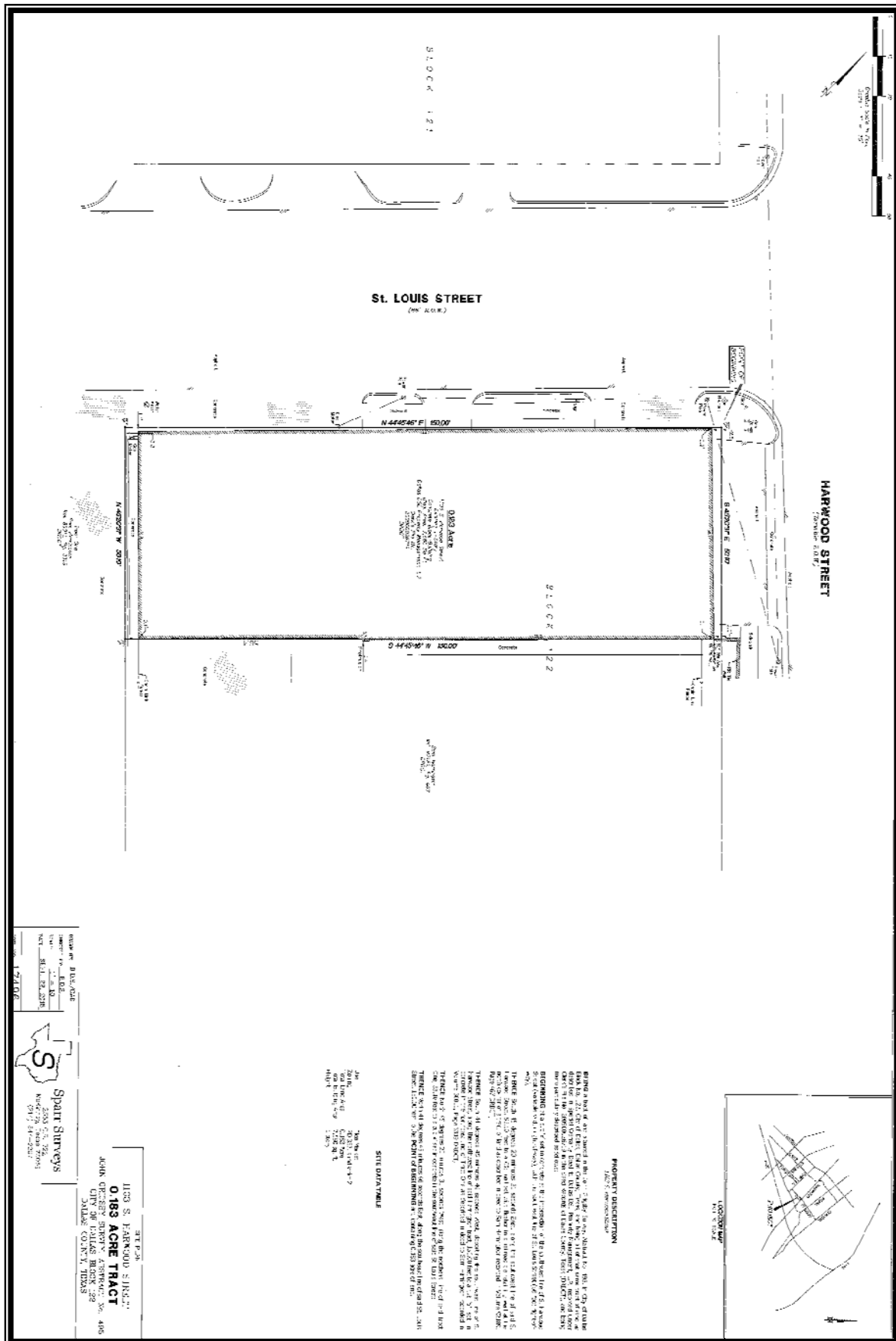
Applicant

- Benny Rubio
- Gloria Rubio

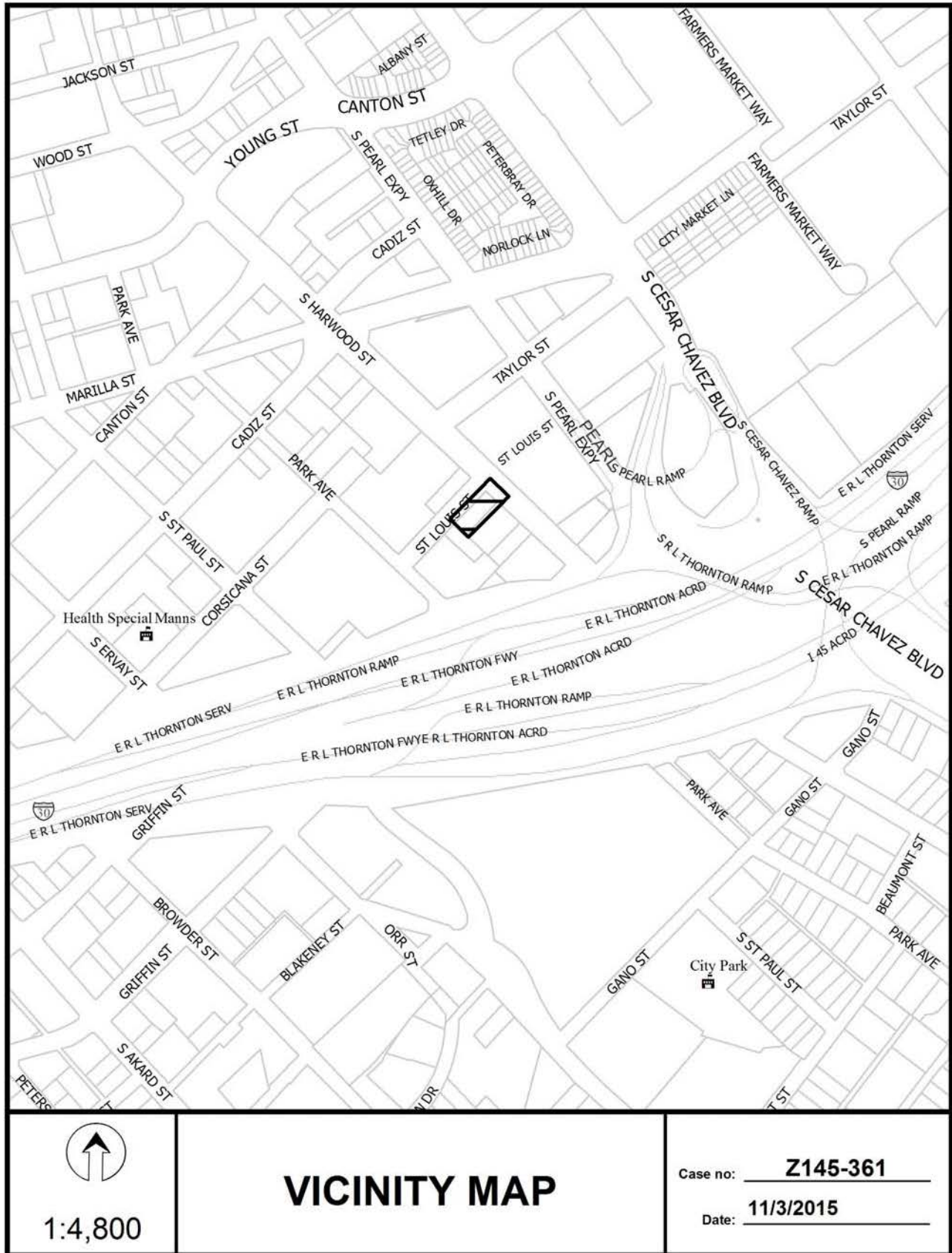
CPC PROPOSED SUP CONDITIONS

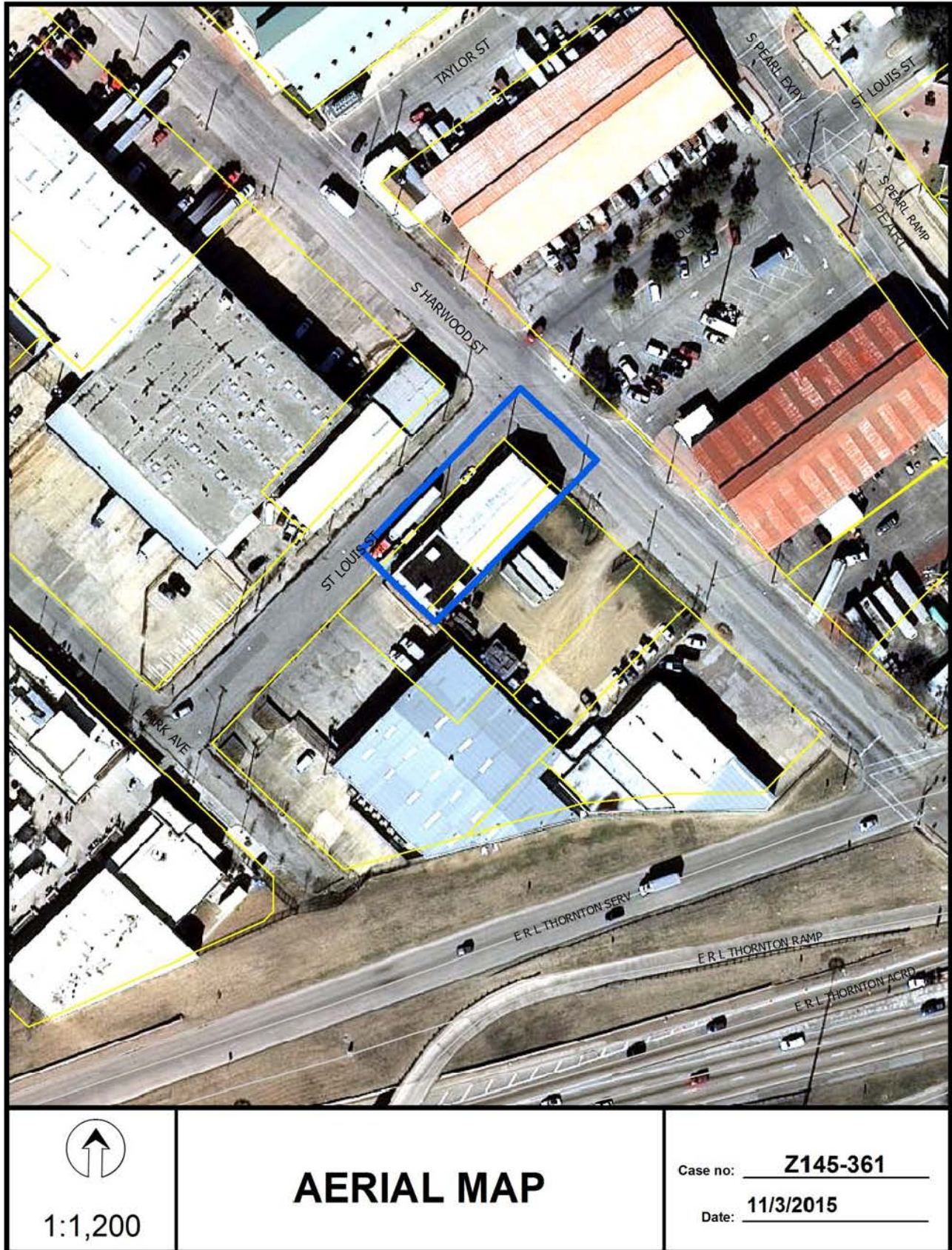
1. USE: The only use authorized by this specific use permit is a flea market.
2. SITE PLAN: Use and development of the Property must comply with the attached site plan.
3. TIME LIMIT: This specific use permit expires on_____, (ten-year period from the passage of this ordinance), but is eligible for automatic renewals for additional ten-year periods, pursuant to Section 51A-4.219 of Chapter 51A of the Dallas City Code, as amended. For automatic renewal to occur, the Property owner must file a complete application for automatic renewal with the director before the expiration of the current period. Failure to timely file a complete application will render this specific use permit ineligible for automatic renewal. (Note: The Code currently provides that applications for automatic renewal must be filed after the 180th but before the 120th day before the expiration of the current specific use permit period. The Property owner is responsible for checking the Code for possible revisions to this provision. The deadline for applications for automatic renewal is strictly enforced).
4. FLEA MARKET. The maximum floor area is of 4,420 square feet of floor area.
5. INGRESS AND EGRESS: Ingress and egress must be provided in the locations shown on the attached site plan. No other ingress or egress is permitted.
6. SIDEWALKS: Sidewalks must be delineated through the existing driveway approaches on South Harwood Street and St. Louis Street.
7. MAINTENANCE: The Property must be properly maintained in a state of good repair and neat appearance.
8. GENERAL REQUIREMENTS: Use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the City of Dallas.

PROPOSED SITE PLAN



Z145-361(WE)

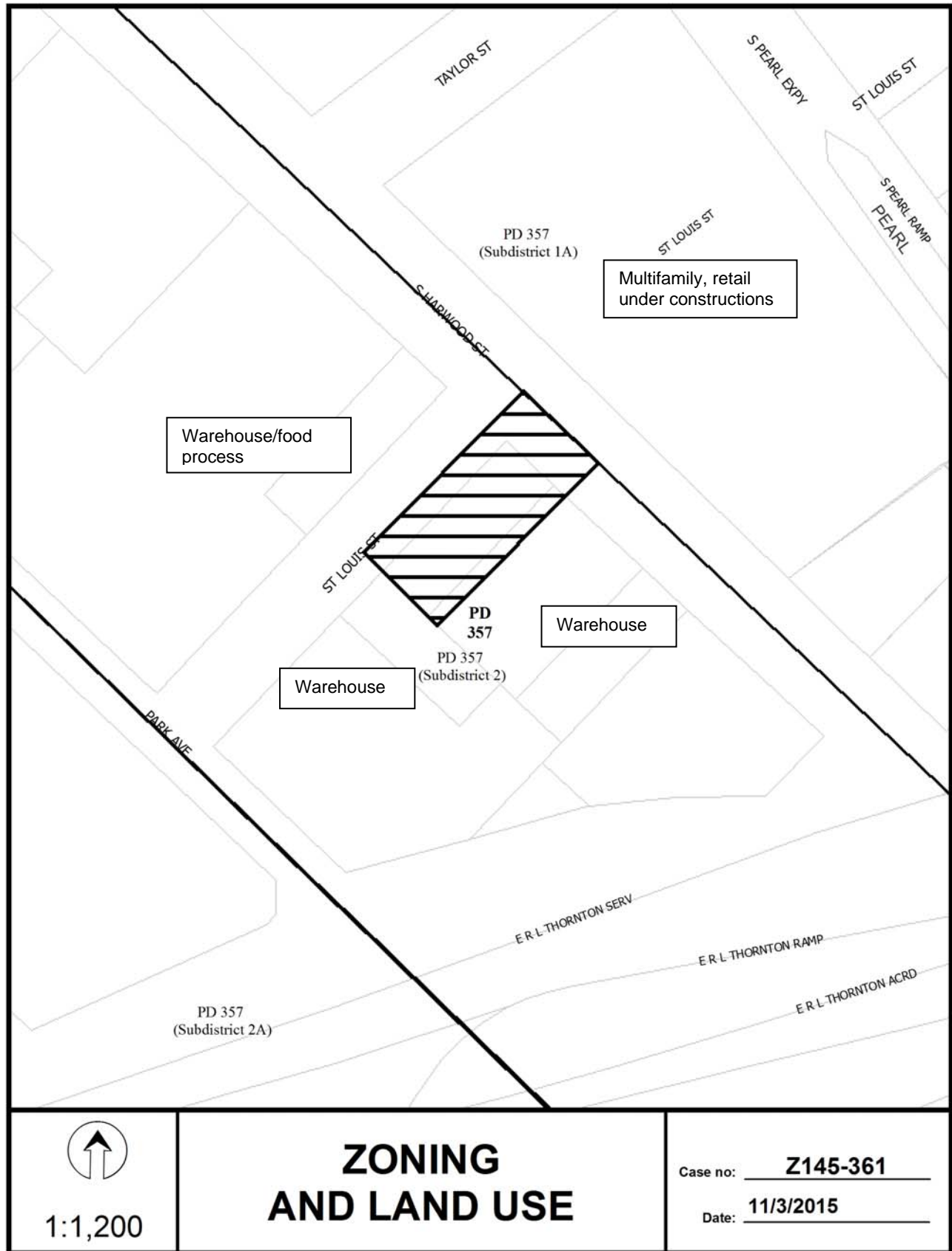


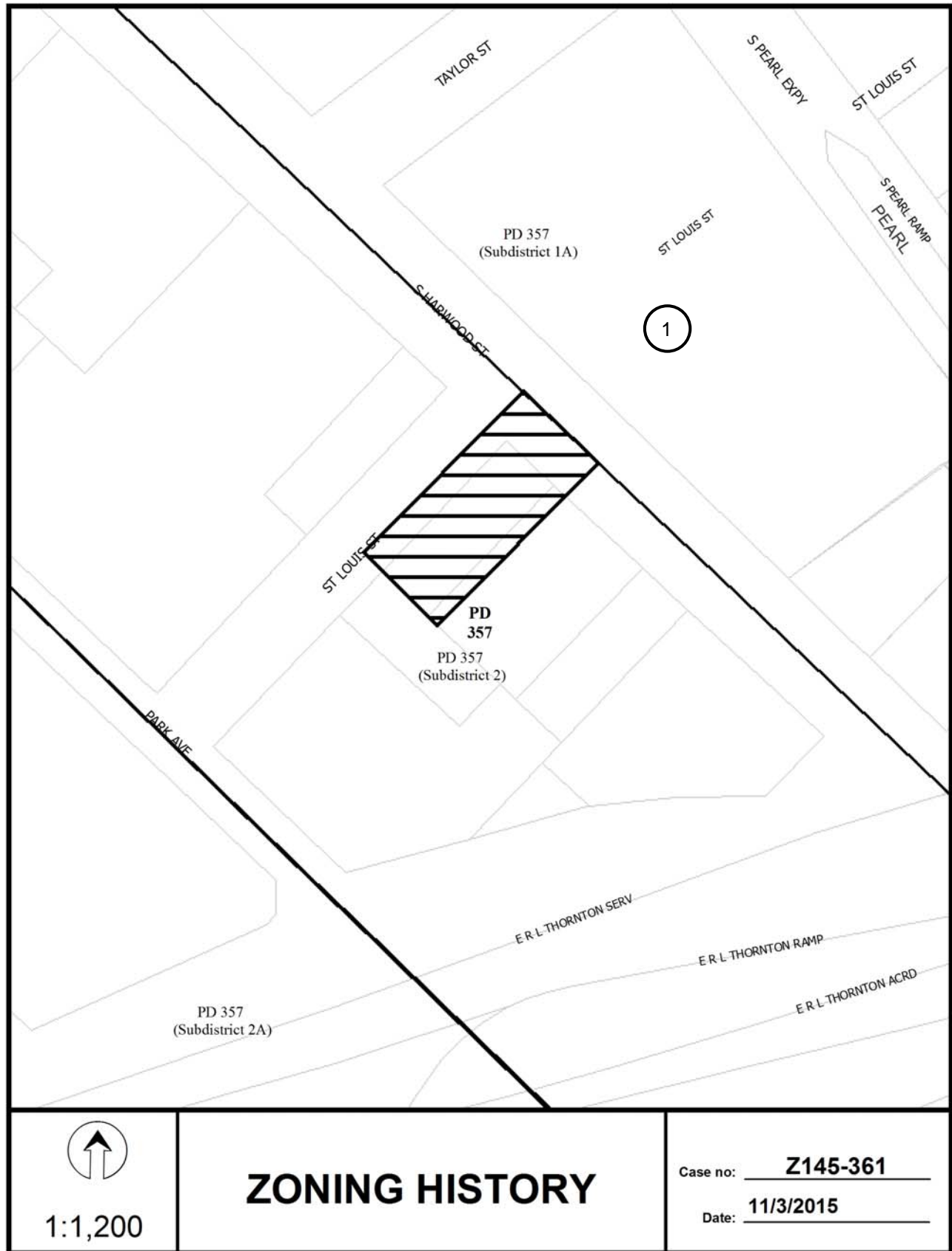


AERIAL MAP

Case no: **Z145-361**

Date: **11/3/2015**







Notification List of Property Owners

Z145-361

6 Property Owners Notified 0 Property Owners in Favor 0 Property Owners Opposed

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
1	1912	ST LOUIS ST	HARRINGTON SAM
2	1017	HARWOOD ST	DALLAS DSL PPTY MANAGEMENT
3	1015	HARWOOD ST	DALLAS DSL PPTY MANAGEMENT LP
4	1916	ST LOUIS ST	HARRINGTON SAMUEL M
5	1012	HARWOOD ST	FM HARVEST LTD
6	1207	HARWOOD ST	HARRINGTON SAMUEL

AGENDA ITEM # 52

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 3

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 64 V

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a Specific Use Permit for an open-enrollment charter school on property zoned an RR Regional Retail District, north of West Camp Wisdom Road and west of Interstate 35E Freeway

Recommendation of Staff and CPC: Approval for a five-year period with eligibility for automatic renewals for additional ten-year periods, subject to a site plan, traffic management plan and conditions

Z145-269(AF)

Note: This item was considered by the City Council at a public hearing on January 13, 2016, and was held under advisement until January 27, 2016, with the public hearing open

FILE NUMBER: Z145-269(AF)

DATE FILED: May 27, 2015

LOCATION: North of West Camp Wisdom Road and west of Interstate 35E Freeway

COUNCIL DISTRICT: 3

MAPSCO: 64V

SIZE OF REQUEST: Approx. 17.97 acres

CENSUS TRACT: 111.03

APPLICANT: Uplift Education

REPRESENTATIVE: Brian Nelson, HKS Architects

OWNER: Ravic Investment Co.

REQUEST: An application for a Specific Use Permit for an Open-enrollment charter school on property zoned an RR Regional Retail District.

SUMMARY: It is the applicant's intention to build a 169,000-square-foot Open-enrollment charter school with 66 total classrooms. These classrooms would encompass 24 elementary classrooms, 30 middle school classrooms, and 12 high school classrooms.

CPC RECOMMENDATION: **Approval** for a five-year period with eligibility for automatic renewals for additional ten-year periods, subject to a site plan, traffic management plan and conditions.

STAFF RECOMMENDATION: **Approval** for a five-year period with eligibility for automatic renewals for additional ten-year periods, subject to a site plan, traffic management plan and conditions.

GUIDING CRITERIA FOR STAFF RECOMMENDTION:

Staff recommends approval of the request based upon:

1. *Compatibility with surrounding uses and community facilities* – The proposed buildings and use are compatible in scale with the surrounding area. The school will have a maximum height of 36 feet except where it is restricted by residential proximity slope standards. The site’s residential adjacency will dictate added development standards that serve to mitigate any potential negative impacts. These standards include an increased side and rear setback of 20 feet, residential proximity slope restrictions with a 1 to 3 slope angle of projection restricting higher portions of the building farther away from the perimeter, and added screening and visual intrusion provisions.
2. *Contribution to, enhancement, or promoting the welfare of the area of request and adjacent properties* – The proposed open-enrollment charter school will provide a valuable service to this area of the city.
3. *Not a detriment to the public health, safety, or general welfare* – The proposed use will not be a detriment to the public health, safety, or general welfare. Infrastructure/street improvements around the site to further minimize the impact on the surrounding neighborhood.
4. *Conforms in all other respects to all applicable zoning regulations and standards* – Based on information depicted on the site plan, the request will comply with all applicable zoning regulations and standards. No variances or exceptions are proposed. All proposed improvements will require compliance with the building code and require final inspection compliance with the site plan.

Zoning History: There have not been any recent zoning changes requested in the area within the last five years.

Thoroughfares/Streets:

Thoroughfare/Street	Type	Existing Dimension
West Camp Wisdom	Major Arterial	100'
US Highway 35	Freeway	Variable

Traffic:

The Engineering Section of the Department of Sustainable Development and Construction has reviewed the requested amendment and determined that it will significantly impact the current state of the surrounding roadway system. Traffic circulation will be regulated through the traffic management plan included in the conditions. The Engineering Section has recommended street/infrastructure improvements to mitigate the impact on the surrounding neighborhood. These recommendations include new driveways southbound IH 35 E frontage road and Camp Wisdom Road, median modification for the eastbound left turn into the school campus, and right turn deceleration lanes for the approaches to two proposed driveways. It should be noted that any other development on this property would likely have triggered roadway improvements at permitting, as well.

Comprehensive Plan: The *forwardDallas! Comprehensive Plan* was adopted by the City Council in June 2006. The *forwardDallas! Comprehensive Plan* outlines several goals and policies which can serve as a framework for assisting in evaluating the applicant's request. The Plan identifies the request site as being in a Residential Neighborhood Building Block.

This Building Block represents the life-blood of Dallas, the traditional neighborhood of single-family detached homes. Dallas has many neighborhoods that match this description, including Winnetka Heights, Preston Hollow, Lakewood and Wheatley Place. Single-family dwellings are the dominate land use in these areas. Some shops, restaurants or institutional land uses such as schools and religious centers that serve neighborhood residents may be located at the edges or at key intersections. Neighborhood "pocket parks" provide open space for families. Individual lot size, front yard and side yard setbacks, garage orientation and scale of the home varies from neighborhood to neighborhood. These areas rely primarily on cars for access, although traffic on neighborhood streets is expected to be low. Cutthrough traffic or spill over from commercial areas will be strongly discouraged. While public transit may be available, typically it involves longer walks to bus stops or the need to drive to park-and-ride facilities. Newly developed neighborhoods may provide better pedestrian access to community services through shorter block lengths, narrower streets, sidewalks and greenbelts with hike and bike trails and might also provide improved access to transit service. Public investment will focus on protecting quality of life by providing amenities such as parks, trails, road improvements and strong code enforcement.

STAFF ANALYSIS:**Land Use:**

	Zoning	Land Use
Site	RR	Vacant/Undeveloped
North	R-7.5 (A)	Single Family
South	R-7.5 (A)	Single Family
East	RR	Restaurant with Drive-Thru
West	R-7.5 (A)	Single Family

Land Use Compatibility: The site is located within a predominately low density residential neighborhood on the northwest of the intersection of West Camp Wisdom Road and I35E. A variety of more intense uses are located at each intersection. Because the site is adjacent to or directly across an alley from a single family residential district, additional code regulations are triggered. These limitations dictate an increased side and rear setback of 20 feet, residential proximity slope restrictions with a 1 to 3 slope angle of projection restricting higher portions of the building farther away from the perimeter, and added screening and visual intrusion provisions.

The general provisions for a Specific Use Permit in Section 51A-4.219 of the Dallas Development Code specifically state: (1) The SUP provides a means for developing certain uses in a manner in which the specific use will be consistent with the character of the neighborhood; (2) Each SUP application must be evaluated as to its probable effect on the adjacent property and the community welfare and may be approved or denied as the findings indicate appropriate; (3) The city council shall not grant an SUP for a use except upon a finding that the use will: (A) complement or be compatible with the surrounding uses and community facilities; (B) contribute to, enhance, or promote the welfare of the area of request and adjacent properties; (C) not be detrimental to the public health, safety, or general welfare; and (D) conform in all other respects to all applicable zoning regulations and standards. The regulations in this chapter have been established in accordance with a comprehensive plan for the purpose of promoting the health, safety, morals, and general welfare of the city.

The applicant's request conforms with the applicable zoning regulations and standards and is consistent with the intent of the Dallas Development Code. Therefore, staff recommends approval subject to a site plan and conditions.

Development Standards:

Current Development Standards							
DISTRICT	Setbacks		Density	Height	Lot Coverage	Special Standards	Primary Uses
	Front	Side/Rear					
RR Regional Retail	15'	20' adjacent to residential OTHER: No Min.	1.5 FAR overall 0.5 office	70' 5 stories	80%	Proximity Slope Visual Intrusion	Retail & personal service, office

Proposed Development Standards for Open-enrollment charter school							
DISTRICT	Setbacks		Density	Height	Lot Coverage	Special Standards	Primary Uses
	Front	Side/Rear					
RR Regional Retail	15'	20' adjacent to residential OTHER: No Min.	0.21 FAR	36' 2 Stories	14%	Proximity Slope Visual Intrusion	School

Landscaping: The request will trigger any landscape changes. All development on the property will require landscaping per Article X of the Dallas Development Code.

Parking: Pursuant to §51A-4.204 of the Dallas Development Code, schools require one and one-half parking spaces for each kindergarten/elementary school classroom along with three and one half parking spaces for every junior high/middle school classrooms and nine and one half spaces for each senior high school classroom. The proposed school is projected to have 24 elementary, 30 middle, and 12 high school classrooms, which would require a total of 255 parking spaces. The applicant is proposing 300 parking spaces.

CPC Action – November 19, 2015

Z145-269(AF)

Planner: Aldo Fritz

Motion: It was moved to recommend **approval** of a Specific Use Permit for an Open-enrollment charter school for a five-year period with eligibility for automatic renewals for additional ten-year periods, subject to a revised site plan (36 feet maximum height), traffic management plan and conditions on property zoned an RR Regional Retail District, north of West Camp Wisdom Road and west of Interstate 35E Freeway.

Maker: Houston
Second: Anglin
Result: Carried: 15 to 0

For: 15 - Anglin, Emmons, Houston, Davis, Shidid,
Anantasomboon, Abtahi, Haney, Jung,
Housewright, Schultz, Peadon, Murphy, Ridley,
Tarpley

Against: 0
Absent: 0
Vacancy: 0

Notices: Area: 400 Mailed: 145
Replies: For: 10 Against: 6

Speakers: For: Eric Goodloe, 3807 Cypress Point Cove, Round Rock, TX, 78664
Yasmin Bhatia, 4230 Beechwood Ln., Dallas, TX, 75221
Bessie Record, 726 Edgemont Ave., Dallas, TX, 75216
Tiffany Serrano, 735 Owensons Dr., Dallas, TX, 75224
Patricia Durham, 735 Owensons Dr., Dallas, TX, 75224
Brian Nelson, 3206 Waldrop Dr., Dallas, TX, 75229
Against: None

List of Partners

Raymond D. Nasher Irrevocable Management Trust, Bank of America, N.A., Trustee, Dallas, TX
Emily L. Hexter Irrevocable Trust, Louis J. Hexter, II, Trustee, Mill Valley, CA
Louis J. Hexter, II, Mill Valley, CA
Victoria Hexter, Lake Oswego, OR
Holly Hexter, Washington, DC

Tony Dona*

Chairman of the Board
Partner, Thackeray Partners

Melissa McNeil*

Vice Chair
Community Volunteer

W. Carey Carter*

Retired Partner, Deloitte & Touche

Angie Dickson

Community Volunteer

Angela Farley

Vice President of Public Policy and Education,
Dallas Regional Chamber of Commerce

Ossa Fisher

Senior Vice President, Strategy & Analytics
Match.com

Daniel Flaherty

President, Gemmy Industries

Michael Giles

President, Right Management

Mary Ellen Weber

Managing Director, Stellar Strategies, LLC

Gilbert Prado

Dallas Independent School District

Randall Ray*

Partner, Munck Wilson & Mandala LLP

Josh Terry*

Managing Director, Highland Capital

Donell Wiggins*

Community Volunteer

Srikanth Srinivas

SVP Solution Innovation, Medecision

Dustin Marshall *

CEO, Hazels Hot Shot

Marnie Wildenthal

Community Volunteer

Richard Frapart

CFO, Mount Kellett Capital Management

John Beckert

Operating Partner, Highland Partners

Kay Allen

Certified Financial Planner, Aspen Wealth
Management

Abigail Williams

Community Volunteer

Alan LeBlanc

Vice President of Sales and Marketing, RAM
Surgical Supplies

Ardo Fuentes

Financial Advisor, Merrill Lynch & Co.

Catherine Estrada

Community Volunteer

Kevin Bryant

Chief Counsel, Crow Holdings

Ryan Moss

Partner, Perella Weinberg Partners

Cristina Barbosa

Community Volunteer

Dawn Mann

Founding Principal, Break of Day Design

Proposed Conditions

1. **USE:** The only use authorized by this specific use permit is an open-enrollment charter school.
2. **SITE PLAN:** Use and development of the Property must comply with the attached site plan.
3. **TIME LIMIT:** This specific use permit expires on (five years), but is eligible for automatic renewal for additional ten-year periods pursuant to Section 51A-4.219 of Chapter 51A of the Dallas City Code, as amended. For automatic renewal to occur, the Property owner must file a complete application for automatic renewal with the director before the expiration of the current period. Failure to timely file a complete application will render this specific use permit ineligible for automatic renewal. (Note: The Code currently provides that applications for automatic renewal must be filed after the 180th but before the 120th day before the expiration of the current specific use permit period. The Property owner is responsible for checking the Code for possible revisions to this provision. The deadline for applications for automatic renewal is strictly enforced.)
4. **CLASSROOMS:** The maximum number of classrooms is 66.
 - a. 24 Elementary classrooms
 - b. 30 middle school (MS) classrooms
 - c. 12 high school (HS) classrooms
5. **PLAYING FIELDS:** Use of the playing fields as shown on the site plan is prohibited between the hours of 8:00 p.m. and 6:00 a.m.
6. **TRAFFIC MANAGEMENT PLAN:**
 - (a) **In general.** Operation of the open-enrollment charter school must comply with the attached traffic management plan.
 - (b) **Queuing.** Queuing is only permitted inside the Property. Student drop-off and pick-up are not permitted within city rights-of-way.
 - (c) **Traffic study.**
 - (1) The Property owner or operator shall prepare a traffic study evaluating the sufficiency of the traffic management plan. The initial traffic study must be submitted to the Director by November 1, 2016. After the initial traffic study, the Property owner or operator shall submit annual updates of the traffic study to the Director by November 1st of each even-numbered year.
 - (2) The traffic study must be in writing, performed by a licensed engineer, based on a minimum of four samples taken on different school days at different drop-off and pick-up times over a two-week period, and must contain an analysis of the following:

- (A) ingress and egress points;
- (B) queue lengths;
- (C) number and location of personnel assisting with loading and unloading of students;
- (D) drop-off and pick-up locations;
- (E) drop-off and pick-up hours for each grade level;
- (F) hours for each grade level; and
- (G) circulation.

(3) Within 30 days after submission of a traffic study, the Director shall determine if the current traffic management plan is sufficient.

(A) If the Director determines that the current traffic management plan is sufficient, the director shall notify the applicant in writing.

(B) If the Director determines that the current traffic management plan results in traffic hazards or traffic congestion, the Director shall require the Property owner to submit an amended traffic management plan. If the Property owner fails to submit an amended traffic management plan within 30 days, the Director shall notify the city plan commission.

(d) Amendment process.

(1) A traffic management plan may be amended using minor plan amendment fee and public hearing process in Section 51A-1.105(k)(3) of Chapter 51A of the Dallas City Code.

(2) The city plan commission shall authorize changes in a traffic management plan if the proposed amendments improve queuing or traffic circulation; eliminate traffic hazards; or decrease traffic congestion.

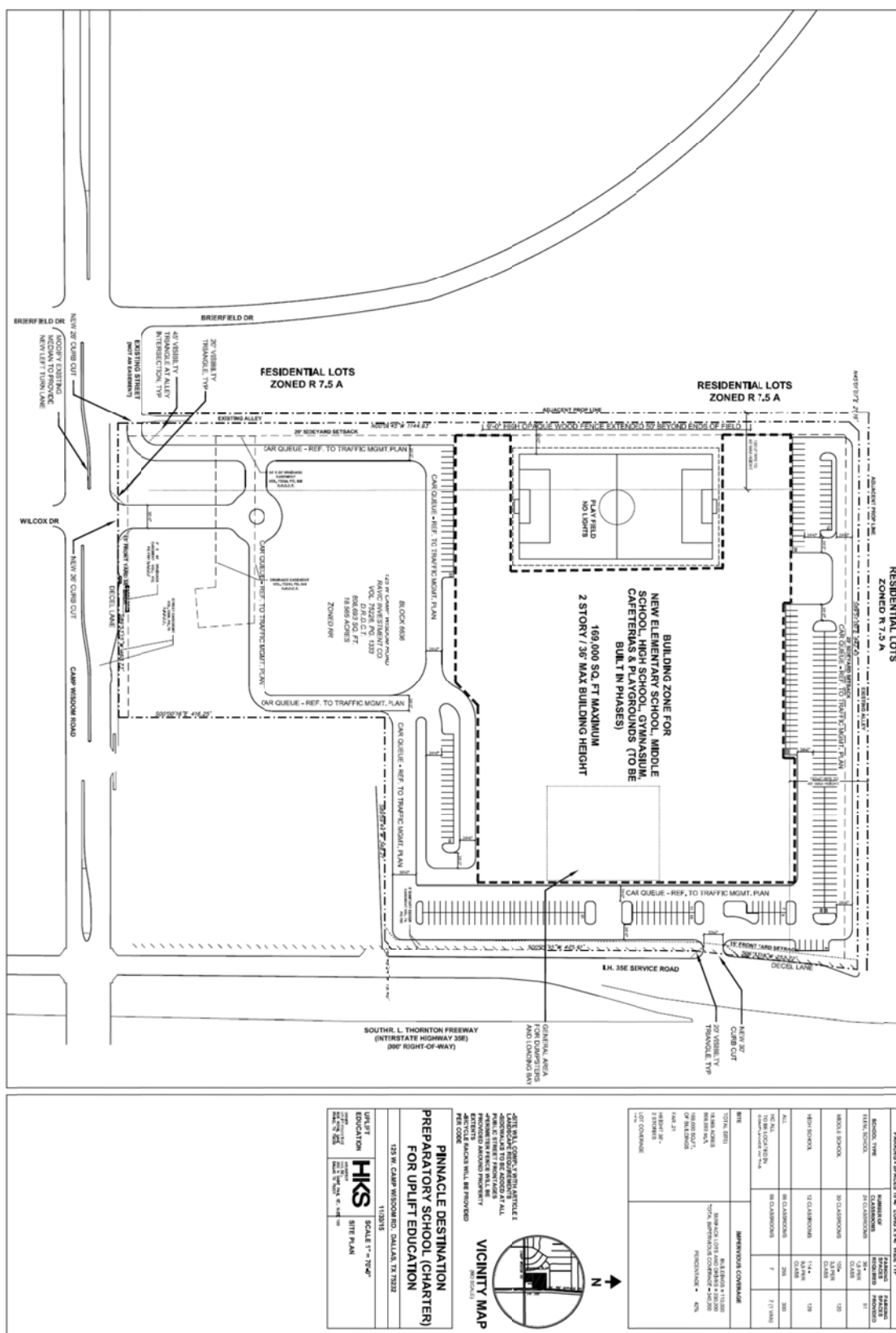
7. PARKING: Parking must be located as shown on the attached site plan.

8. LANDSCAPING: Landscaping must be provided and maintained in accordance with Article X of the Dallas Development Code, as amended.

9. MAINTENANCE: The Property must be properly maintained in a state of good repair and neat appearance.

10. GENERAL REQUIREMENTS: Use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the City of Dallas.

Proposed Site Plan



Proposed Traffic Management Plan

Traffic Management Plan

Uplift Pinnacle Preparatory

Z145-269-AF

Dallas, Texas



Walter P. Moore and Associates, Inc.
TBPE Firm Registration No. 1856

Prepared for
Uplift Education

Prepared by
WALTER P MOORE

Walter P. Moore and Associates, Inc.
1845 Woodall Rodgers Freeway, Suite 1650
Dallas, Texas 75201
214-740-6200

August 2015

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LIST OF EXHIBITS

EXHIBIT 1: Traffic Management Plan

LIST OF TABLES

TABLE 1: Calculated Queuing Space for Uplift Pinnacle Preparatory – Full Build Enrollment

INTRODUCTION

Uplift Education is proposing to initiate the operations for a charter school at the northwest corner of IH 35E and Camp Wisdom Road in Dallas, Texas. The proposed school site is bounded by IH 35E Southbound Frontage Road (SBFR) on the east, Camp Wisdom on the south, single-family homes that front Brierfield Drive on the west and the alley that serves homes that front Cherry Point Drive on the north. During the Full Build enrollment year of the school, the school proposes to accommodate 600 students in the Elementary School and 1,176 students in the Secondary School. Uplift Education acquired the services of Walter P Moore (WPM) to address traffic and queuing concerns of the City of Dallas and provide a Traffic Management Plan for the proposed site.

QUEUING ANALYSIS

The queuing data collected at existing Uplift Education school campuses was evaluated. From the data available, overall averages of approximately 1 vehicle for every 6.8 elementary students and 1 vehicle for every 7.5 secondary school students were calculated for peak queuing conditions.

Utilizing these peak queuing ratios as planning guidelines for Full Build enrollment of the school, the calculated linear feet of queuing space for the school was developed as provided in **TABLE 1**. The provided queuing space is also shown in **TABLE 1**.

TABLE 1: Calculated Queuing Space for Uplift Pinnacle Preparatory – Full Build Enrollment

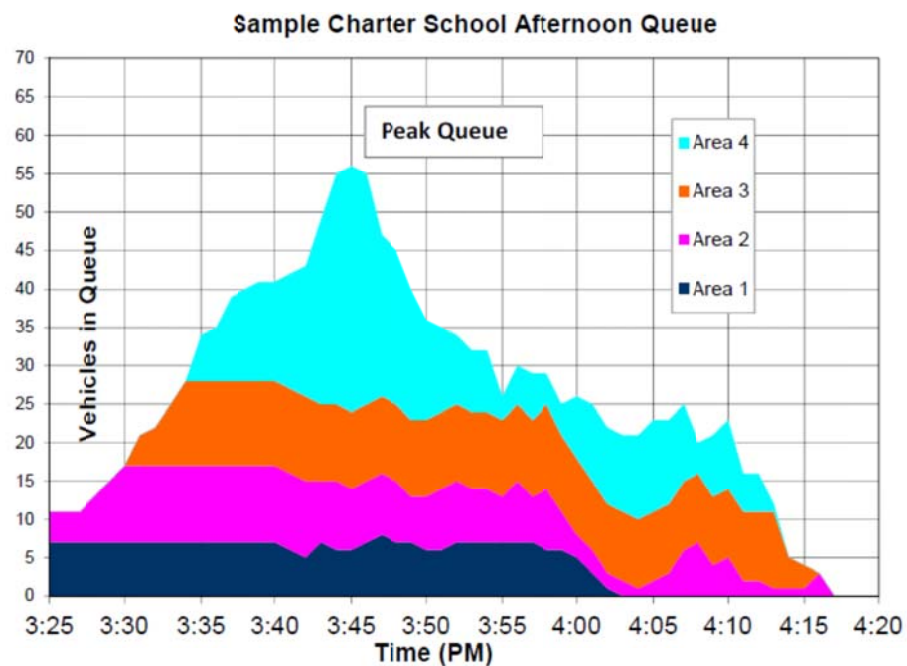
School Level	Grades	Number of Students- Full Build-out	Ratio: Number of Students per Peak Queued Vehicle*	Number of Peak Queued Vehicles-	Calculated Queuing Space- Linear Feet**	Provided Queuing Space – Linear Feet
Elementary School	K - 5	600	6.8:1	88	2,200	2,852
Middle School	6 - 8	504	7.5:1	67	1,675	2,969
High School	9 - 12	672	7.5:1	90	2,250	2,852***

*Average peak queuing rates obtained from existing school queuing data

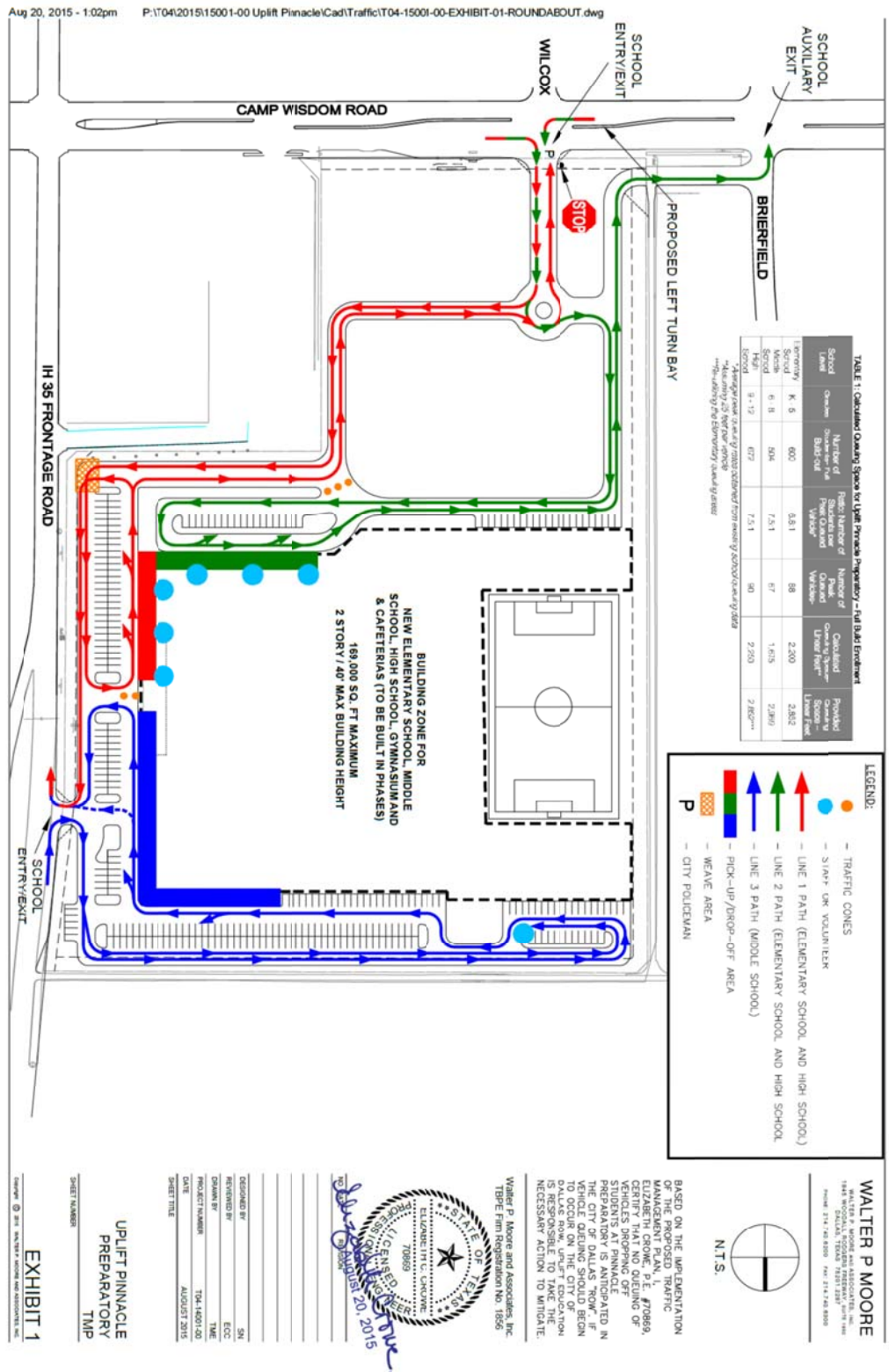
**Assuming 25 feet per vehicle

***Re-utilizing the Elementary queuing areas

The following figure illustrates a typical accumulation of queued vehicles leading up to the dismissal time and after the students have been released. As the figure indicates, the duration of the "Peak Queue" during the PM peak typically lasts for a minute or two before quickly diminishing. The Areas 1 through 4 represent the different queuing areas inside the school (curb lines, parking lots, etc.). This particular school campus has two dismissal times as is indicated by the peak at 3:45 PM as well as just after 4:05 PM.



The on-site queuing space for the Pinnacle Preparatory campus illustrated in **EXHIBIT 1** measures 1,580 linear feet for the red pick-up area, 1,272 linear feet for the green pick-up area and 2,969 linear feet for the blue pick-up area for a total single-use length of 5,821 linear feet. During the initiation of the school operations, it is recommended to monitor the drop off and pick up operations and make adjustments to the procedures as necessary. The site plan is anticipated to sufficiently serve the Full Build enrollment plan of the school campus with implementation of the Traffic Management Plan as further described.



TRAFFIC MANAGEMENT PLAN

The Traffic Management Plan was designed to create a vehicular circulation route for parents to drop-off and pick-up their children during the critical morning and afternoon times associated with the beginning and dismissal of school. It is designed to maintain all queued vehicles within the school property during both the AM and PM peak periods and not impede the flow of traffic on adjacent streets. Elements of the proposed traffic management plan are provided.

Family Identification Numbers

It is recommended to provide a carpool identification card for each elementary school family. The parents should be instructed to have the identification card visible for the administrator collecting this information to relay the specific parent's arrival information to the staff to alert the elementary students.

Ingress & Egress

Three driveways will provide access to the site as illustrated in **EXHIBIT 1**. The driveway along IH 35 SBFR on the northeast side of the school will primarily provide entry and exit access to the Secondary School carline. The driveway on the southern end of the campus at Camp Wisdom Road will primarily serve entry and exit for the Elementary School carline. Modification to the existing median opening will be necessary for the eastbound left turn movement from Camp Wisdom Road.

The driveway that runs parallel to Camp Wisdom Road along Brierfield Drive on the south side of the proposed school site will provide auxiliary access from the Elementary School carline. As the school population grows, it is recommended to utilize the Elementary School access for a portion of the Secondary School, potentially the High School.

Drop-off/Pick-up Locations

As depicted in **EXHIBIT 1**, there are three proposed student Drop-off/Pick-up locations provided on the school campus. Two areas are located on the east side of the school buildings parallel to the IH 35 frontage road and one is located on the south side of the elementary building parallel to Camp Wisdom Road. As illustrated in **EXHIBIT 1**, the Elementary School carline is proposed to utilize the two areas on the south side of the campus. The Middle School is proposed to utilize the area on the north side of the campus. The High School operations would initially operate at the loading area on the north side of the campus and may adjust to the south side of the campus as necessary to

respond to the changing needs and loading/queuing demands of the campus as the enrollment increases.

Circulation

Vehicles for the Elementary School drop-off/pickups will enter the south side driveway from Camp Wisdom Road with either a left turn or a right turn into the school campus. After entering the school site, the motorists will follow either the red path or the green path as illustrated in **EXHIBIT 1**. Motorists in the red line will proceed to the red pick-up area and either exit the site via the school drive to Camp Wisdom Road or from the driveway along IH 35 SBFR. As the need and/or demand warrants, the Elementary School also has the opportunity to utilize the green path as indicated on **EXHIBIT 1** with motorists exiting to Brierfield Drive. Depending on the number of students living in the neighborhood with access to Brierfield Drive, this auxiliary access point may be utilized for entry to the school site.

The vehicles for Secondary School drop-off/pickup operations will enter the site through the north side driveway along IH 35 SBFR with a southbound right-turn. The motorist carline will proceed counterclockwise around the parking lots north of the Secondary School building, as shown in **EXHIBIT 1**. There is potential for double stacking of the carline during the Full Build enrollment of the school.

Since it is assumed that the Elementary School pickup operations and Secondary School pickup operations will be staggered by at least a period of 20 minutes, it is recommended that a portion of the Secondary School (potentially the High School) utilize the Elementary School queuing space and circulation plan.

Release Time

Other Uplift Education campuses typically stagger the release times between the Elementary School (grades Kindergarten through fifth grade) at 3:30 PM and the Upper School (Grades 6 -12) at 4:00 PM. It is assumed that the Elementary School pickup operations and Secondary School pickup operations will be staggered similarly by a minimum of 20 minutes.

Administrative Officials

School staff and/or volunteers will be necessary to assist with both morning drop-off and afternoon pick-up operations at the Elementary School.

Each school campus has unique carpool operation conditions with its number of staff, students and parents. Monitoring of the proposed Traffic Management Plan should be performed at the onset of the school's opening to ensure that it is operating efficiently. As this campus will be a new location, the school should be prepared to provide additional staff at the onset of the school's opening and adjust based on the demand both in the morning and in the afternoon. With full enrollment, it is proposed that a number of staff or volunteers assist during the afternoon pick-up operations. There are six potential staff or volunteer locations indicated for the Elementary School loading areas located at on **EXHIBIT 1**. One potential staff member or volunteer is also illustrated on **EXHIBIT 1** at the location where two queuing lines would merge into one line.

An additional administrator can be utilized near the Elementary entry to alert the staff at the pick-up areas of the names of students that are to be picked-up so that they are queued up and ready to load into the vehicles. Police assistance should be anticipated to be needed at the stop-controlled exit to Camp Wisdom Road across from Wilcox Drive.

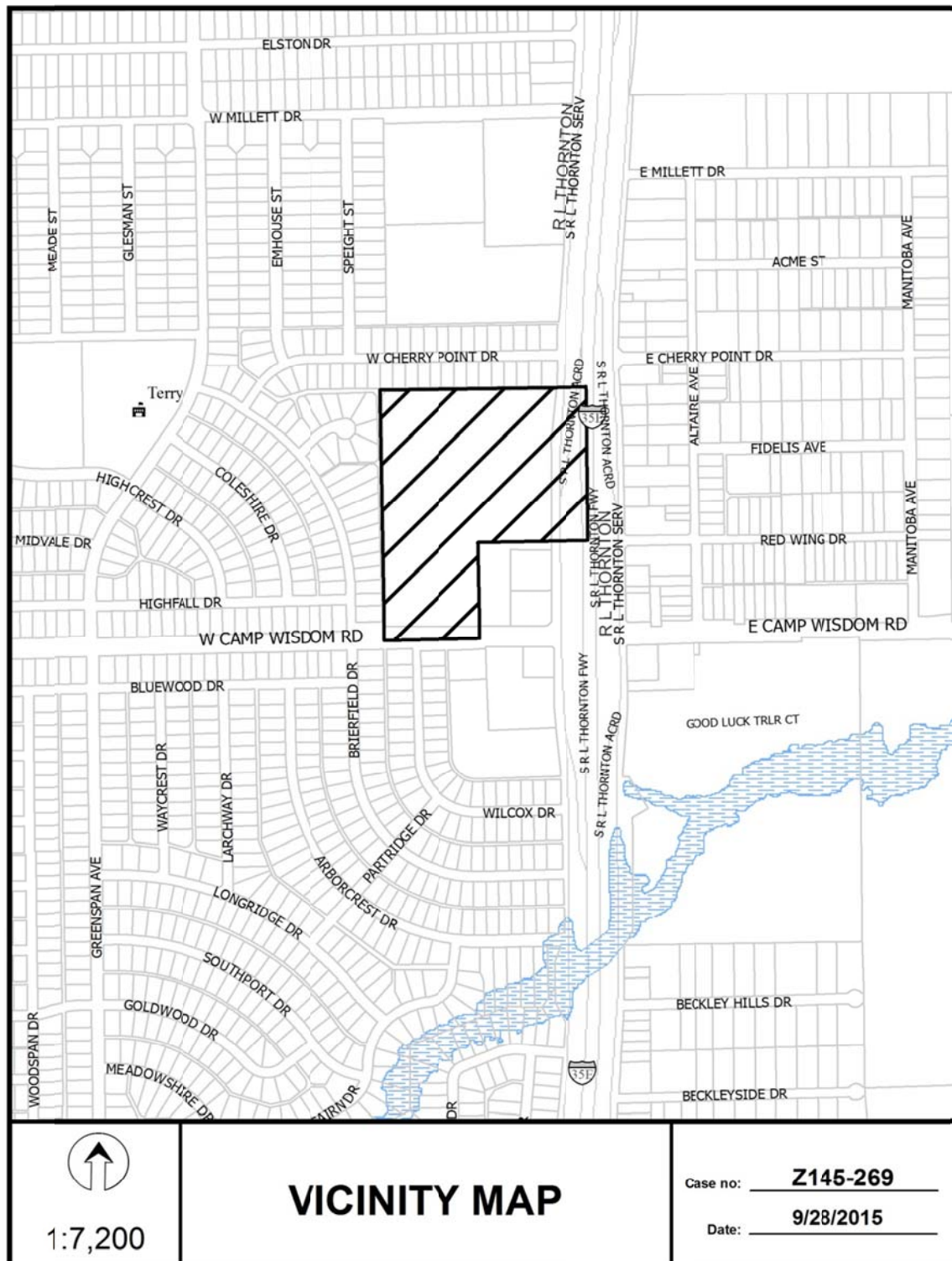
Summary

EXHIBIT 1 illustrates the Traffic Management Plan for Uplift Pinnacle Preparatory. New driveways are requested along southbound IH 35 E frontage road and Camp Wisdom Road. Median modification for the eastbound left turn into the school campus from Camp Wisdom Road is also requested. The City of Dallas has also requested right turn deceleration lanes for the approaches to the two proposed driveways as reflected on **EXHIBIT 1**.

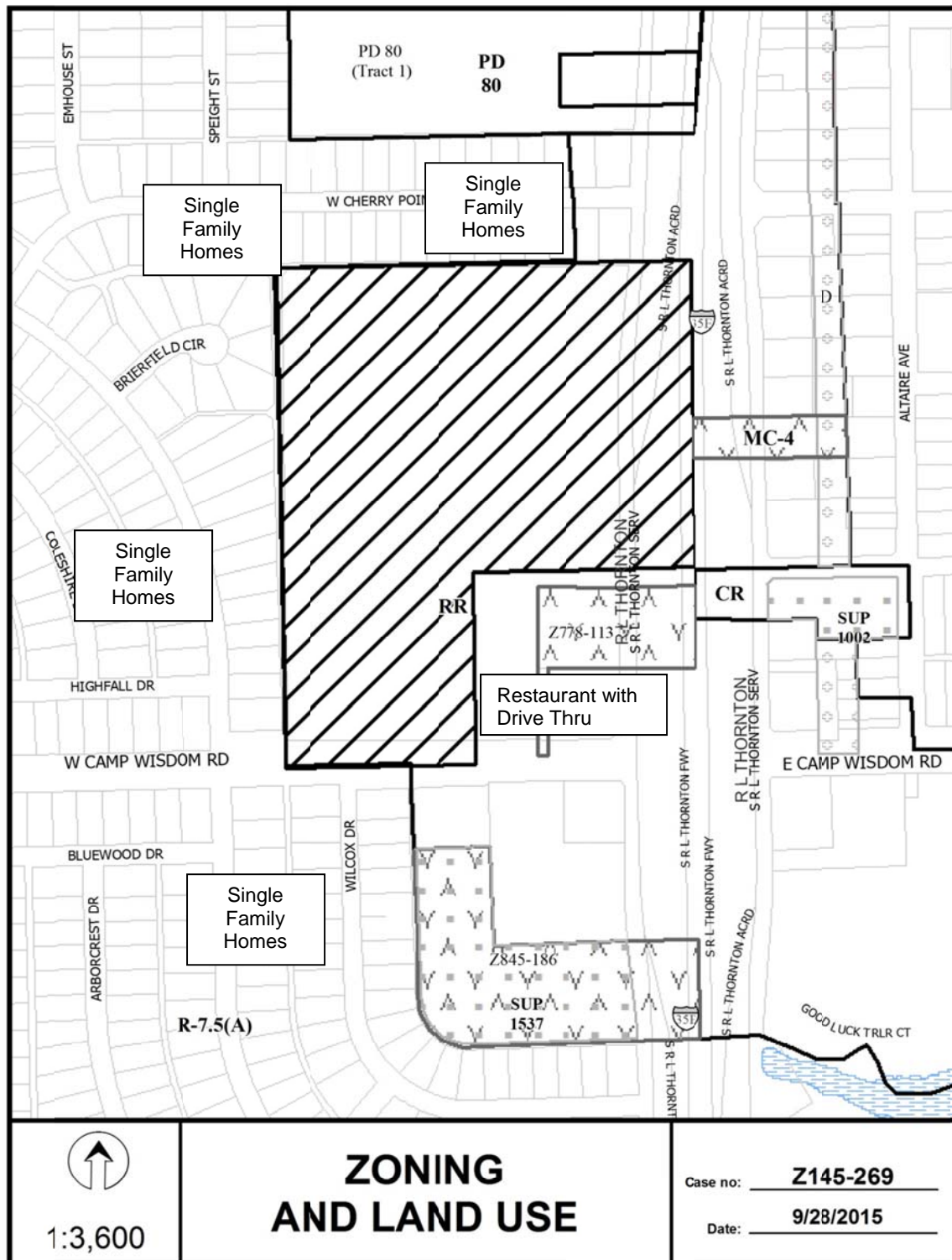
During the initiation of the school operations, it is recommended to monitor the drop off and pick up operations and make adjustments to the procedures as necessary. It is the responsibility of the Uplift Pinnacle Preparatory administrators to implement the plan. Based on the information provided by the school representatives and best planning practices, the provided circulation and queuing plan should be sufficient to accommodate the peak vehicular queue on site. No queuing will be allowed on the street right-of-way and administrators are responsible for immediately implementing any correction deemed necessary by City staff to correct any spill of queuing in the City of Dallas Right of Way. Strategies to minimize peak queuing conditions on school campuses include:

- Provide several opportunities for the parents, students and staff to learn any new carpool circulation plan; meetings, e-mails, posting on the school website, etc.,
- Instruct parents not to arrive at the school too early,

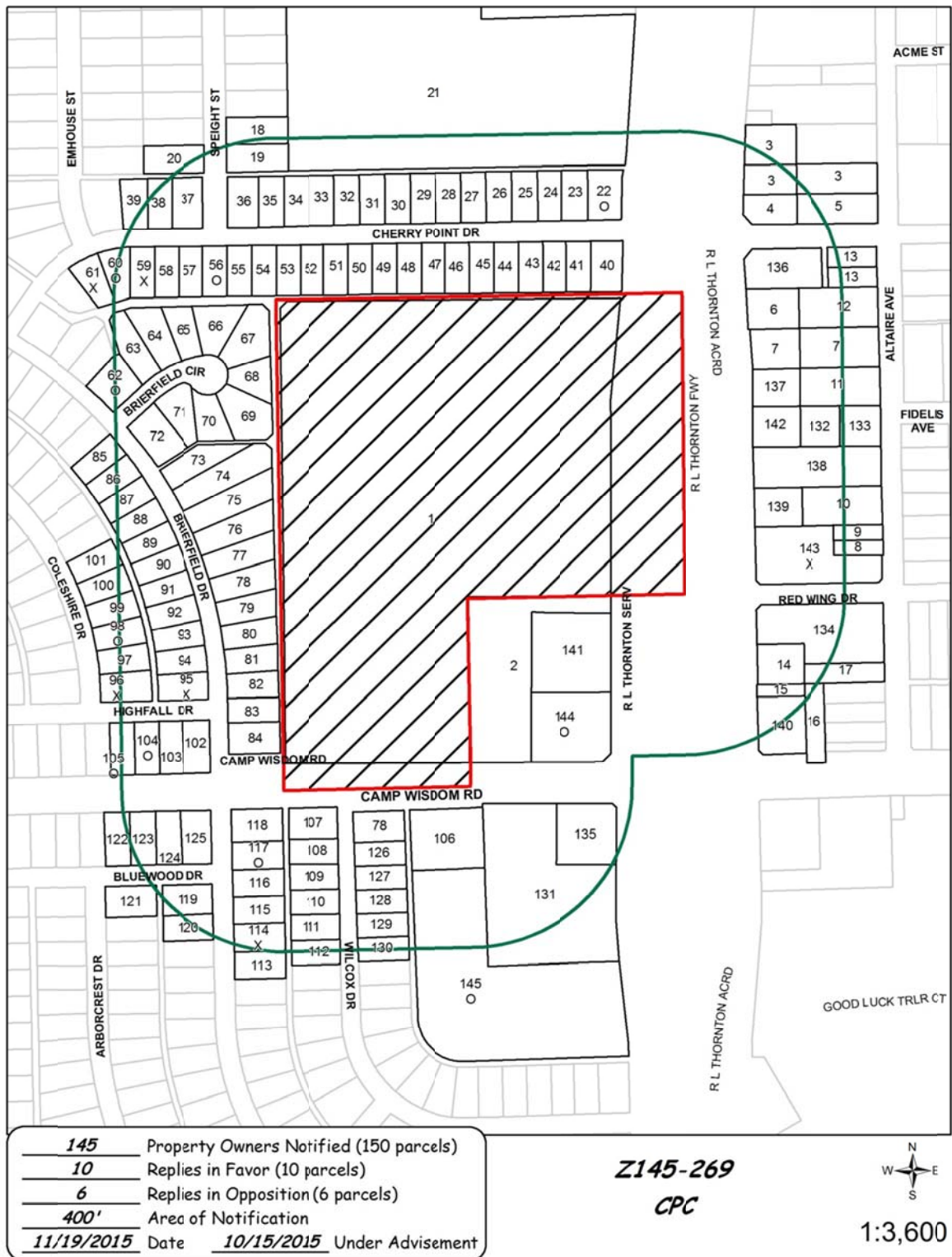
- Encourage carpooling among families,
- Have parents visibly display their family name or carpool number for easy identification,
- Encourage the parents to queue up closely to the vehicle in front of them,
- Provide additional staff members to assist with unloading/loading the Elementary School students, and/or
- Stagger release times for different grade levels.







CPC Responses



11/18/2015

Reply List of Property Owners***Z145-269******145 Property Owners Notified 10 Property Owners in Favor 6 Property Owners Opposed***

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	1	125 CAMP WISDOM RD	RAVIC INV CO
	2	125 CAMP WISDOM RD	MCDONALDS CORP 042 0425
	3	6718 R L THORNTON FWY	TRUE VINE OF HOLINESS
	4	6734 R L THORNTON FWY	TRUE VINE OF HOLINESS MISSIONARY BAPTIST
	5	6735 ALTAIRE AVE	TRUE VINE BAPTIST CHURCH
	6	6810 R L THORNTON FWY	MAGEE A INV INC
	7	6822 R L THORNTON FWY	DELEON THOMAS
	8	6929 ALTAIRE AVE	DAVIS FAMILY LIVING TRUST
	9	6925 ALTAIRE AVE	DUNCAN MRS MARGIE
	10	6923 ALTAIRE AVE	JORDAN WILLIE L
	11	6831 ALTAIRE AVE	VELASQUEZ TOBIAS &
	12	6815 ALTAIRE AVE	VELASQUEZ TOBIAS ETUX
	13	6807 ALTAIRE AVE	MAGEE A INV INC
	14	7010 R L THORNTON FWY	HOWARD MARSHALL &
	15	7014 R L THORNTON FWY	SPEED ED REVOCABLE LIV TR
	16	117 CAMP WISDOM RD	BOSCHERT DANIEL S KAREN S REV LIVING TRUST &
	17	7015 ALTAIRE AVE	BOSCHERT DANIEL S & KAREN S REV LIVING TRUST
	18	6632 SPEIGHT ST	LONGORIA ROBERTO &
	19	6636 SPEIGHT ST	MORIN SAN JUANA DIAZ
	20	6637 SPEIGHT ST	GORDON CLAY
	21	6601 R L THORNTON FWY	INSPIRING BODY OF CHRIST
O	22	105 CHERRY POINT DR	CALDWELL VICKI
	23	109 CHERRY POINT DR	RAMOS ALEX TOLY &
	24	115 CHERRY POINT DR	PEREZ MARGARITA
	25	119 CHERRY POINT DR	ERWIN DOROTHY EST OF
	26	125 CHERRY POINT DR	READY MORTGAGE CORP

11/18/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	27	129 CHERRY POINT DR	FALLS TERRANCE ANINE &
	28	135 CHERRY POINT DR	MATHIS WILLIE F
	29	139 CHERRY POINT DR	PFEIL RICHARD A
	30	205 CHERRY POINT DR	HERNANDEZ JOSE LUIS
	31	209 CHERRY POINT DR	WOODS LORENE
	32	215 CHERRY POINT DR	CORTEZ ARMANDO
	33	219 CHERRY POINT DR	JONES AMOS D
	34	223 CHERRY POINT DR	MENDEZ JOSE D
	35	227 CHERRY POINT DR	HOLLINS DANIEL SR
	36	235 CHERRY POINT DR	FINN SR LARRY LEROY
	37	305 CHERRY POINT DR	DENNIS DEBORAH
	38	311 CHERRY POINT DR	PAULIN SALVADOR & MA JEROMNIMA
	39	317 CHERRY POINT DR	CHILDERS ROY L
	40	106 CHERRY POINT DR	BRIONES GREGORIO T
	41	110 CHERRY POINT DR	DUARTE TERESO
	42	116 CHERRY POINT DR	WORTHAM MISHA L &
	43	120 CHERRY POINT DR	SKINNER CHARLES RAY
	44	126 CHERRY POINT DR	HOUSTON VERNON LLC
	45	130 CHERRY POINT DR	BLOSSER MARK & DEBORAH
	46	136 CHERRY POINT DR	BLOSSER MARK
	47	140 CHERRY POINT DR	COLEMAN MICHAL DEWYAN
	48	204 CHERRY POINT DR	IBARRA ROSA M &
	49	208 CHERRY POINT DR	PFEIL DICK
	50	214 CHERRY POINT DR	KING CHARLENE
	51	218 CHERRY POINT DR	COLEMAN JAMES
	52	224 CHERRY POINT DR	GREEN MARIE R
	53	228 CHERRY POINT DR	LEWIS ANDREA
	54	234 CHERRY POINT DR	STEEN ISAAC A & RHONDA
	55	238 CHERRY POINT DR	OLIVE PIT LLC
O	56	244 CHERRY POINT DR	COLEMAN ROMMIE
	57	304 CHERRY POINT DR	GUILLEN ANTONIO &

11/18/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	58	310 CHERRY POINT DR	MARTINEZ MARIA L & NICANOR HERNANDEZ
X	59	316 CHERRY POINT DR	MARTIN ALFRED H
O	60	320 CHERRY POINT DR	PRICE NOMOREE
X	61	326 CHERRY POINT DR	WILSON JERRY D
O	62	6822 BRIERFIELD DR	LAGARD BRENDA S
	63	6828 BRIERFIELD CIR	MARTINEZ LUIS E & CARMELA
	64	6832 BRIERFIELD CIR	BANKS JAMES & THELMA
	65	6836 BRIERFIELD CIR	BANKS JAMES & THELMA
	66	6840 BRIERFIELD CIR	JJB III INVESTMENTS INC
	67	6844 BRIERFIELD CIR	CLARK RANDY
	68	6848 BRIERFIELD CIR	LONDON RODERICK L MAR
	69	6852 BRIERFIELD CIR	PEREZ MIGUEL
	70	6856 BRIERFIELD CIR	SANTIBANEZ RENE & CONSTANCIA
	71	6860 BRIERFIELD CIR	PAULIN ADRIAN
	72	6864 BRIERFIELD CIR	LEWIS ROBERT B JR
	73	6914 BRIERFIELD DR	DUARTE MARTIN &
	74	6920 BRIERFIELD DR	PFEIL RICHARD A & ALANA P
	75	6924 BRIERFIELD DR	PFEIL RICHARD ALAN
	76	6930 BRIERFIELD DR	HERNANDEZ JOSE &
	77	6934 BRIERFIELD DR	BROOKS FRANCES B
	78	6940 BRIERFIELD DR	CLARK FREEMAN CO LLC
	79	7004 BRIERFIELD DR	WILLIAMS LEWILBURNE &
	80	7010 BRIERFIELD DR	AUSTIN BILLY
	81	7014 BRIERFIELD DR	ANGLEFORGE PROPERTIES LLC
	82	7020 BRIERFIELD DR	BROWN EMMETT D JR ETAL
	83	7024 BRIERFIELD DR	DAWKINS HANTOYO
	84	7030 BRIERFIELD DR	ANDREWS RUBY EST
	85	6903 BRIERFIELD DR	CONTRERAS KENNEDY A
	86	6909 BRIERFIELD DR	DANIELS H PAUL
	87	6915 BRIERFIELD DR	BUSBY WANDA
	88	6919 BRIERFIELD DR	CURRY MELVIN L

11/18/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	89	6923 BRIERFIELD DR	ORTEGA LEOPOLDO & MARIA LOURDES
	90	6929 BRIERFIELD DR	ESCUADRA JAVIER T.
	91	6933 BRIERFIELD DR	VILLAFRANCO ANA &
	92	7005 BRIERFIELD DR	KELLEY OSCAR JR
	93	7011 BRIERFIELD DR	UNGER DONALD G & RHONDA S
	94	7015 BRIERFIELD DR	OAK CLIFF RENTALS & RESTORATION PROJECTS LLC
X	95	7019 BRIERFIELD DR	BATTS SHARON
X	96	7028 COLESHIRE DR	HARPER JERRY D
	97	7022 COLESHIRE DR	POSADA FRANCISCO J &
O	98	7018 COLESHIRE DR	ALLISON BRIAN
	99	7012 COLESHIRE DR	TAYLOR AMOS JR &
	100	7008 COLESHIRE DR	YOUNG ROBERT
	101	7002 COLESHIRE DR	MITCHELL LOUIS H &
	102	406 HIGHFALL DR	HILL LEWIS L
	103	410 HIGHFALL DR	JEFFERSON WILLIE L ESTATE
O	104	416 HIGHFALL DR	BROWN H L
O	105	420 HIGHFALL DR	ALLEN DOROTHY
	106	110 CAMP WISDOM RD	HINDI AMGAD HAMID
	107	7105 WILCOX DR	WRIGHT LEWIS W &
	108	7111 WILCOX DR	WHISNANT J CLIFTON
	109	7115 WILCOX DR	VILLEGAS JESUS
	110	7121 WILCOX DR	JOHNSON ROYDELL
	111	7125 WILCOX DR	FIELDS BETTYE J
	112	7131 WILCOX DR	ROSENBOROUGH MICHAEL
	113	7130 BRIERFIELD DR	IRVIN WALTER L
X	114	7126 BRIERFIELD DR	MCCLENDON BERTHA L
	115	7120 BRIERFIELD DR	SCARBER FRED
	116	7116 BRIERFIELD DR	BUTLER TIMMIE D &
O	117	7110 BRIERFIELD DR	ESSLINGER INVESTMENTS IV LLC
	118	7106 BRIERFIELD DR	VILLAREAL RENE
	119	7119 BRIERFIELD DR	GARDNER WILLIAM & LUCY

11/18/2015

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	120	7125 BRIERFIELD DR	FONTENOT ROOSEVELT JR &
	121	7104 ARBORCREST DR	HILL TERESA A
	122	421 BLUEWOOD DR	WAKEFIELD WALKER &
	123	417 BLUEWOOD DR	VALIENTE DANIELA
	124	411 BLUEWOOD DR	BROWN KENNARDS C &
	125	405 BLUEWOOD DR	ESPINOZA ROBERTO M &
	126	7110 WILCOX DR	PALMER GLADYS M
	127	7114 WILCOX DR	HAL LOUISE
	128	7120 WILCOX DR	TIMOTHY PROPERTIES LLC
	129	7124 WILCOX DR	MAIZE VIDA
	130	7130 WILCOX DR	CONNER WILLIAM C
	131	220 CAMP WISDOM RD	GOKUL GROUP CORPORATION
	132	6906 R L THORNTON FWY	VELASQUEZ TOBIAS & ABELINA
	133	6907 ALTAIRE AVE	VELASQUEZ TOBIAS & ABELINA
	134	7006 R L THORNTON FWY	RAM LAXMAN INC
	135	7107 R L THORNTON FWY	RL THORNTON FUEL CNT LP
	136	6802 R L THORNTON FWY	MAGEE A INV INC
	137	6830 R L THORNTON FWY	VELASQUEZ TOBIAS &
	138	6910 R L THORNTON FWY	ROSTON CHARLOTTE N NAULLS &
	139	6918 R L THORNTON FWY	JORDAN WILLIE L
	140	7026 R L THORNTON FWY	BOSCHERT DANIEL S & KAREN SPEED BOSCHERT T
	141	7015 R L THORNTON FWY	AMERCO REAL ESTATE CO
	142	6906 R L THORNTON FWY	VELASQUEZ TOBIAS & ABELINA
X	143	6928 R L THORNTON FWY	DAVIS WM B & CONSTANCE TR
O	144	7027 R L THORNTON FWY	GHALA PETROLEUM INC
O	145	7227 R L THORNTON FWY	I 35 WAREHOUSE ASSOC LTD

AGENDA ITEM # 53

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 7

DEPARTMENT: Sustainable Development and Construction

CMO: Ryan S. Evans, 671-9837

MAPSCO: 59 B

SUBJECT

A public hearing to receive comments regarding an application for and an ordinance granting a Planned Development District for an Open-enrollment charter school and CR Community Retail District Uses on property zoned an MF-2(A) Multifamily District and a CR Community Retail District with the D-1 Liquor Control Overlay, on the east line of St. Augustine Drive, north of Bruton Road

Recommendation of Staff and CPC: Approval, subject to a development plan, traffic management plan and conditions

Z156-104(RB)

Note: This item was considered by the City Council at a public hearing on January 13, 2016, and was deferred until January 27, 2016

FILE NUMBER: Z156-104(RB)

DATE FILED: October 6, 2015

LOCATION: East line of St. Augustine Drive, north of Bruton Road

COUNCIL DISTRICT: 7

MAPSCO: 59 B

SIZE OF REQUEST: Approx. 5 Acres

CENSUS TRACT: 120.00

APPLICANT: TA II Acquisition, LLC

REPRESENTATIVES: Tommy Mann and Laura Hoffman

OWNER: Iglesia Nueva Vision, Inc.

REQUEST: An application for a Planned Development District for an Open-enrollment charter school and CR Community Retail District Uses on property zoned an MF-2(A) Multifamily District and a CR Community Retail District with the D-1 Liquor Control Overlay.

SUMMARY: The applicant proposes to develop the property with an open-enrollment charter school with a maximum of 48 classrooms for elementary and junior high/middle school aged children.

CPC RECOMMENDATION: **Approval**, subject to a development plan, traffic management plan, and conditions.

STAFF RECOMMENDATION: **Approval**, subject to a development plan, traffic management plan, and conditions.

GUIDING CRITERIA FOR RECOMMENATION:

Staff recommends approval of the request, subject to a development plan, traffic management plan, and staff's recommended conditions based upon:

1. *Performance impacts upon surrounding property* – The physical location of improvements as well as anticipated operational characteristics associated with such a use will not impact adjacent uses, It should be noted that staff is recommending conditions that will ensure the location of the illuminated outdoor athletic field does not impact adjacent residential uses to the north and east.
2. *Traffic impact* – As the site is exclusively accessed from a thoroughfare, impact on adjacent local streets that serve area residents will be minimal. Furthermore, the attached traffic management plan will ensure queuing for unloading/loading operations are confined to the applicant's site.
3. *Comprehensive Plan or Area Plan Conformance* – The request is in compliance with the designated Building Block for the area.
4. While the applicant has worked with adjacent property owners to address all aspects of the school's operation, staff's support is solely contingent on the attached recommended amending conditions.

Zoning History: There has been no zoning activity in the immediate area within the past five years.

Traffic: The Engineering Section of the Department of Sustainable Development and Construction has reviewed the request and traffic management plan and determined that it has not significantly impacted the surrounding roadway system. Furthermore, staff's recommended conditions will require scheduled updates of the TMP to ensure all queuing for unloading/loading is contained on the property.

Thoroughfare

St. Augustine Drive

Designation; Existing & Proposed ROW

Collector; 60' & 60' ROW

STAFF ANALYSIS:

Comprehensive Plan: The request site is located in an area considered a Residential Neighborhood Building Block. This Building Block represents the life-blood of Dallas, the traditional neighborhood of single-family detached homes. Dallas has many neighborhoods that match this description, including Winnetka Heights, Preston Hollow, Lakewood and Wheatley Place. Single-family dwellings are the dominate land use in these areas. Some shops, restaurants or institutional land uses such as schools and

religious centers that serve neighborhood residents may be located at the edges or at key intersections.

Land Use Compatibility: The request site is undeveloped and possesses significant topography, rising approximately 20 feet across 540 feet, from west to east. The applicant proposes to develop the site with an open-enrollment charter school, providing for 48 classrooms serving elementary and junior high/middle school students. Additional improvements will include surface parking areas, adequate circulation to accommodate unloading/loading of students, and an athletic field within the eastern third of the site. A PDD is being requested for the following reasons: 1) establish a unified zoning base [northern approx. one acre is zoned MF-2(A) and would trigger certain standards for building mass developed on the balance of the site, zoned a CR District); 2) permit the open-enrollment charter school by right, in lieu of a Specific Use Permit. The property will be developed in three phases, with the third phase being a gymnasium within the southeastern quadrant of the site.

Surrounding land use transitions from retail uses (south to north from the intersection of St. Augustine Drive and Bruton Road) to residential uses (multifamily and single family structures). It should be noted that construction of multifamily uses to the west/southwest will incorporate a retail component (oriented at the intersection of St. Augustine Drive and Bruton Road). In addition SUP Nos. 717 (Fire Station). 883 (Private recreation area). and 1573 (Tower, antenna for cellular communication) exists in close proximity to the site. Lastly, a public elementary school (Edward Titcher Elementary) is located in the area, approximately 1,200 feet to the northwest.

The applicant has reached out to area property owners to discuss the vision for the school operation. Staff and the applicant have been able to work through the majority of the request, however there remains concern as to the proximity, and operational flexibility, associated with the athletic field. Specific recommendations are noted in the attached conditions. With regard to the attached TMP, staff is recommending annual updates for the first five years to better measure the impact of initial and increased enrollment (360 to 1,119 within five years), with biennial updates after this period.

As this school is anticipated to accommodate enrollments as noted above, staff is recommending a requirement that the applicant make application of a school zone plan to the Streets Department prior to the issuance of a certificate of occupancy.

As a result of this analysis, staff is supportive of the request, subject to the attached development plan, traffic management plan, and staff's recommended conditions.

Landscaping: The site possesses native vegetation (limited to the center of the site and along the eastern property line) and will be shown on a tree survey prior to issuance

Z156-104(RB)

of any building permits. The applicant has proposed compliance with Article X landscaping. As such, a landscape plan (that works within the confines of the attached development plan) will be provided to the building official during permit review.

CPC ACTION

(December 3, 2015)

Motion: It was moved to recommend **approval** of a Planned Development District for an Open-enrollment charter school and CR Community Retail District Uses, subject to a revised development plan to include the following modification: 1) Prohibit solid screen on North St. Augustine, 2) Establish and indicate maximum number of gates per property frontage, 3) Maximum gate height of 6ft., and 4) Prohibit outdoor lighting on the field, traffic management plan and revised staff's recommended conditions on property zoned an MF-2(A) Multifamily District and a CR Community Retail District with the D-1 Liquor Control Overlay, on property on the east line of St. Augustine Drive, north of Bruton Road.

Maker: Abtahi
Second: Ridley
Result: Carried: 14 to 0

For: 14 - Anglin, Emmons, Houston, Davis, Shidid,
Abtahi, Haney, Jung, Housewright, Schultz,
Peadon, Murphy, Ridley, Tarpley

Against: 0
Absent: 1 - Anantasomboon
Vacancy: 0

Notices: Area: 300 Mailed: 14
Replies: For: 1 Against: 0

Speakers: For: Tommy Mann, 500 Winstead Bldg., Dallas, TX
Against: None

OFFICERS AND DIRECTORS

Owner: **IGLESIA NUEVA VISION, INC.**

Director:	Victor Manuel Gomez
Director:	Rosa Imelda Gomez
Director:	Carlos Gomez
Senior Pastor:	Victor Manuel Gomez
Secretary:	Rosa Imelda Gomez
Coordinator:	Carlos Gomez

OFFICERS AND DIRECTORS

Applicant: **TA II ACQUISITION LLC**

Vice President:

Daniel Millman

Vice President:

Bari Cooper Sherman

President:

K. Robert Turner

**CPC RECOMMENDED CONDITIONS FOR A PLANNED DEVELOPMENT
DISTRICT**

SEC. 51P ____ .101. LEGISLATIVE HISTORY.

PD _ was established by Ordinance No. __, passed by the Dallas City Council on _.

SEC. 51P ____ .102. PROPERTY LOCATION AND SIZE.

PD ____ is established on property generally located on the east line of St. Augustine Drive, north of Bruton Road. The size of PD ____ is approximately 5 acres.

SEC. 51P- ____ .103. DEFINITIONS AND INTERPRETATIONS.

(a) Unless otherwise stated, the definitions and interpretations in Chapter 51A apply to this article.

(b) Unless otherwise stated, all references to articles, divisions, or sections in this article are to articles, divisions, or sections in Chapter 51A.

(c) This district is considered to be a nonresidential zoning district.

SEC. 51P- ____ .104. EXHIBITS.

The following exhibits are incorporated into this article:

- (1) Exhibit __A: development plan.
- (2) Exhibit __B: traffic management plan.

SEC. 51P- ____ .105. DEVELOPMENT PLAN.

(a) For an open-enrollment charter school, development and use of the Property must comply with the development plan (Exhibit __A). If there is a conflict between the text of this article and the development plan, the text of this article controls.

(b) For all other uses, no development plan is required, and the provisions of Section 51A-4.702 regarding submission of or amendments to a development plan, site analysis plan, conceptual plan, development schedule, and landscape plan do not apply.

SEC. 51P- _____.106. MAIN USES PERMITTED.

(a) Except as provided in this section, the only main uses permitted are those main uses permitted in the CR Community Retail District, subject to the same conditions applicable in the CR Community Retail District, as set out in Chapter 51A. For example, a use permitted in the CR Community Retail District only by specific use permit (SUP) is permitted in this district only by SUP; a use subject to development impact review (DIR) in the CR Community Retail District is subject to DIR in this district, etc.

(b) An open-enrollment charter school is permitted by right.

SEC. 51P- _____.107. ACCESSORY USES.

As a general rule, an accessory use is permitted in any district in which the main use is permitted. Some specific accessory uses, however, due to their unique nature, are subject to additional regulations contained in Section 51A-4.217. For more information regarding accessory uses, consult Section 51A-4.217.

SEC. 51P- _____.108. YARD, LOT, AND SPACE REGULATIONS.

(Note: The yard, lot, and space regulations in this section must be read together with the yard, lot, and space regulations in Division 51A-4.400. If there is a conflict between this section and Division 51A-4.400, this section controls).

(a) In general. Except as provided in this section, the yard, lot, and space regulations for the CR Community Retail District apply.

(b) Retaining walls. Retaining walls a maximum of six feet in height may encroach into a required setback.

SEC. 51P- _____.109. OFF-STREET PARKING AND LOADING.

Consult the use regulations in Division 51A-4.200 for the specific off-street parking and loading requirements for each use.

SEC. 51P- _____.110. TRAFFIC MANAGEMENT PLAN.

(a) In general. The operation of an open-enrollment charter school must comply with the traffic management plan (Exhibit ____B).

(b) Queuing. Queuing is only permitted inside the Property. Student drop-off and pick-up are not permitted within city rights-of-way.

(c) Traffic study.

(1) The Property owner or operator shall prepare a traffic study evaluating the sufficiency of the traffic management plan. The initial traffic study must be submitted to the director by November 1, 2017. After the initial traffic study, the Property owner or operator shall submit updates of the traffic study to the director by November 1 of each year for the first five years (2018, 2019, 2020, 2021, and 2012) and then by November 1 of each even-numbered year.

(2) The traffic study must be in writing, performed by a licensed engineer, based on a minimum of four samples taken on different school days at different drop-off and pick-up times over a two-week period, and must contain an analysis of the following:

- (A) ingress and egress points;
- (B) queue lengths;
- (C) number and location of personnel assisting with loading and unloading of students;
- (D) drop-off and pick-up locations;
- (E) drop-off and pick-up hours for each grade level;
- (F) hours for each grade level; and
- (G) circulation.

(3) Within 30 days after submission of a traffic study, the director shall determine if the current traffic management plan is sufficient.

(A) If the director determines that the current traffic management plan is sufficient, the director shall notify the applicant in writing.

(B) If the director determines that the current traffic management plan results in traffic hazards or traffic congestion, the director shall require the Property owner to submit an amended traffic management plan. If the Property owner fails to submit an amended traffic management plan within 30 days, the director shall notify the city plan commission.

(d) Amendment process.

(1) A traffic management plan may be amended using the minor plan amendment fee and public hearing process in Section 51A-1.105(k)(3).

(2) The city plan commission shall authorize changes in a traffic management plan if the proposed amendments improve queuing or traffic circulation; eliminate traffic hazards; or decrease traffic congestion.

SEC. 51P- ____ .112. ENVIRONMENTAL PERFORMANCE STANDARDS.

See Article VI.

SEC. 51P- ____ .113. LANDSCAPING.

Landscaping must be provided in accordance with Article X.

SEC. 51P- ____ .114. FENCES FOR AN OPEN-ENROLLMENT CHARTER SCHOOL.

(a) Fencing along the perimeter of the property must be provided in the location as shown on the development plan.

(b) Access gates may be provided along each respective Property boundary in the general location as shown on the development plan, with no more than the following per fence section:

- (1) North Property line: A maximum of ____ access gates.
- (2) East Property line: A maximum of ____ access gates.
- (3) South Property line. A maximum of ____ access gates.

(c) Final location of the access gates may vary along each respective Property line and will not require a minor amendment to the development plan.

(d) Access gates may be locked.

SEC. 51P- ____ .115. SIGNS.

Signs must comply with the provisions for business zoning districts in Article VII.

SEC. 51P- _____.116.

ADDITIONAL PROVISIONS.

- (a) Open-enrollment charter school.
 - (1) The maximum number of classrooms is 48.
 - (2) An open-enrollment charter school may only operate between 7:00 a.m. and 6:00 p.m., Monday through Friday.
 - (3) The gymnasium may be used for recreational use during non-operating hours. Use of the soccer field is prohibited before 9:00 a.m., Monday through Sunday.
 - (4) Prior to the issuance of a certificate of occupancy, the Property owner must submit an application for a school zone plan to the Streets Department, inclusive of the financial responsibility for the improvements, with final design and construction approved by the department.
 - (5) Prior to the issuance of a certificate of occupancy, a minimum of 7,582 square feet of outdoor play area must be provided in the locations as shown on the development plan.
 - (6) Illumination of the soccer field is prohibited.
- (b) The Property must be properly maintained in a state of good repair and neat appearance.
- (c) Development and use of the Property must comply with all federal and state laws and regulations, and with all ordinances, rules, and regulations of the city.

SEC. 51P- _____.117.

COMPLIANCE WITH CONDITIONS.

- (a) All paved areas, permanent drives, streets, and drainage structures, if any, must be constructed in accordance with standard city specifications, and completed to the satisfaction of the director of the city.
- (b) The building official shall not issue a building permit to authorize work, or a certificate of occupancy to authorize the operation of a use, in this district until there has been full compliance with this article, the Dallas Development Code, the construction codes, and all other ordinances, rules, and regulations of the city.

Traffic Management Plan and Queuing Analysis
Proposed KIPP Pleasant Grove Elementary School ZXXX-XXX
2200 N. St. Augustine Drive, Dallas, TX
October 6, 2015

Introduction:

The proposed public charter school campus is a development of a vacant parcel of land located on Saint Augustine Drive north of the intersection with Bruton Road.

The school will start with approximately 360 students (Kindergarten – 2nd Grade) for the 2016-2017 school year, and may ultimately grow to up to 1,119 students after five years. The building has a total of 48 classrooms. The following table shows the proposed distribution of classrooms and students at buildout, actual student distribution may vary by year:

Grade	Classrooms	Students	Drop-Off Time	Dismissal Time
Pre-K	3	60	7:15-7:30 AM	3:30-3:50 PM
Kindergarten	33	120	7:15-7:30 AM	3:30-3:50 PM
1 st Grade		120	7:15-7:30 AM	3:30-3:50 PM
2 nd Grade		120	7:15-7:30 AM	3:30-3:50 PM
3 rd Grade		120	7:15-7:30 AM	3:30-3:50 PM
4 th Grade		120	7:15-7:30 AM	3:30-3:50 PM
5 th Grade		120	7:10-7:30 AM	4:30-5:00 PM
6 th Grade	12	116	7:10-7:30 AM	4:30-5:00 PM
7 th Grade		113	7:10-7:30 AM	4:30-5:00 PM
8 th Grade		110	7:10-7:30 AM	4:30-5:00 PM
Total	48	1,119		

Morning drop-off is from 7:15-7:30 AM for students from Pre-K to 4th grade and from 7:10-7:30 AM for students from 5th to 8th grade, with school starting at 7:30 AM. Afternoon pick-up is between 3:30-3:50 PM for students from Pre-K to 4th grade and 4:30-5:00 PM for students from 5th to 8th grade. The pick-up and drop-off times can be modified to match the school operations. When the student population using the pick-up loading areas grows beyond a certain point, separate dismissals will be required. The pick-up time periods can be modified as long as pick-ups are divided into groups that are separated by at least 15 minutes.

Public charter schools can achieve bus usage of around 50%. While a similar percentage is possible at this campus, a bus usage of 20% for Pre-K – Grade 1 and 40% for Grades 2 – 8 is assumed for the queue calculations in order to ensure a conservative analysis of the queuing demand. The TMP is expandable to handle all 1,119 students in the unlikely event that buses are not used.

Proposed TMP Operation:

The proposed TMP has two loading areas which will operate the same in both the drop-off and pick-up time periods.

The Bus Loading Area is entered from Saint Augustine Drive at the northern driveway, Drive 1. Buses will enter the right lane, separate from vehicles in the left lane, and circulate through a separate loop around the school. Just after the loading zone, a staffer will manage the crossing between buses and vehicles. Buses will exit the site first, then vehicles will be allowed to queue into the overlapping zone. Buses will continue through the parking lot and exit via the southern driveway, Drive 2, connection to Saint Augustine Drive. Separate left- and right-turn lanes are available for outbound movements at the intersection of Drive 2 and Saint Augustine Drive. Space is available for temporary bus stacking if necessary, but no buses will be stored on the site overnight. The Bus Loading Area can accommodate multiple buses loading or unloading at the same time.

The Loading Area is entered from Saint Augustine Drive at the northern driveway, Drive 1. Vehicles will enter the left lane, separate from buses. Starting from the entrance of the school to right before the loading area, vehicles will be queued in a single-stack manner. While the buses are loading, vehicles will be held at stop line in front of the gymnasium. After the buses are done loading and have exited the Bus Loading Area, the vehicles will be allowed to move to the loading zone. The Loading Area has approximately 1,460' (73 vehicles) of queuing distance available within the site. Staff members will assist with loading and unloading. Once loaded or unloaded, the vehicles will continue through the parking lot, merge with the buses, and exit via the southern driveway, Drive 2, connection to Saint Augustine Drive. Just after the loading zone, a staffer will manage the merging between buses and vehicles. Separate left- and right-turn lanes are available for outbound movements at the intersection of Drive 2 and Saint Augustine Drive.

At all locations loading is performed on the passenger side, and students have to cross the bus loading zone via a crosswalk to reach the loading zone. For the afternoon pick-up time period, arriving vehicles will display a sticker or placard indicating which student(s) they are picking up. The traffic administrator posted in advance of the loading stations looks for this information and calls ahead to the loading station so that the appropriate students are waiting at the correct loading station when the vehicle arrives. When the vehicles have come to a stop at the loading station, the students are loaded into the vehicles with the assistance of the staff member at each loading station.

The school will use multiple pick-up groups depending on the number of students. The school can accommodate up to 365 students with a single dismissal time. When the student population using one dismissal time grows above 365, the students will be split in two groups with dismissals separated by at least 15 minutes. In the initial year after the busing assumptions, the elementary school of 360 students would have a parent pick-up of 264 students, which can be accommodated to one dismissal period. The elementary school will eventually grow to have a parent pick-up of 456 students at buildout, which will require two dismissal times.

At buildout the middle school (grades 5-8) will have 459 students being dismissed at 4:30-5:00PM, which is 275 students after the busing assumptions. This number of students can be accommodated within one dismissal time.

Even if there is no busing from the school, the full 1,119 students can be accommodated with four dismissal times (two for elementary school, two for middle school).

Queuing Analysis:

Based on observations of queuing at other public charter schools in the DFW area, KHA uses a design standard for projecting queue demands at similar sites. The expected maximum queue in vehicles is equal to 20% of the largest number of students dismissed at one time. Students using buses or walking/biking are deducted from the student number since they do not attract personal vehicles to the campus. This method accounts for the differences in how schools divide up the pick-up time period, as some dismiss all students in one group and therefore have higher vehicle demands in a short time period, while some spread out the dismissals over two or more groups. The projected queue formula can be stated as:

$$(\text{Students dismissed in time period} - \text{Students using other modes}) * 0.20 = \text{Number of vehicle in queue}$$

By design of the TMP, the largest single dismissal that would be the middle school grades 5 – 8, with 459 students at buildout. Therefore, the projected maximum queue length is:

$$(459 \text{ students dismissed} - 184 \text{ Students using buses}) * 0.20 = 55 \text{ vehicles in queue}$$

The projected queue of 55 vehicles translates to 1,100' of queuing distance. This distance is well in excess of the recommended values for equivalent Texas schools found in the Texas Transportation Institute (TTI) research report 0-4286 *Operations and Safety Around Schools* published in January 2004. The more conservative (longer queues) KHA method reflects the nature of charter schools which draw from a large area and tend to have fewer students arriving as pedestrians or by bicycle than traditional public schools.

The projected maximum queue demand of 1,100' can easily be accommodated within the queue distance available in the Loading Area. The Loading Area has 1,460' of distance available in the TMP, which is approximately 360' of queuing distance available in excess of the demand.

Projected Queue Demand:	1,100'	55 Vehicles
Available Queuing Distance:	1,460'	73 Vehicles
Surplus (Deficiency):	360'	18 Vehicles

While the TMP allows the school flexibility of schedule and dismissals within the TMP limits of 340 students picked up by parents per dismissal period, the following table shows the projected conditions at school buildout. As noted, the elementary dismissals are divided in two groups, while the grades 5-8 can be dismissed at one time. All dismissal groups have a comfortable amount of unused space in the available queue.

Queuing Summary - School Buildout Example							
Group	Dismissal Time	Students Dismissed	Bus / Bike / Walk	Parent Pickup	Maximum Queue	Available Queue	Surplus (Deficiency)
Elementary 1 PreK, K, 1	3:30 PM	300	60	240	48 Vehicles 960'	73 Vehicles 1,460'	25 Vehicles 500'
Elementary 2 2, 3, 4	3:50 PM	360	144	216	43 Vehicles 860'	73 Vehicles 1,460'	30 Vehicles 600'
Middle Sch. 1 5, 6, 7, 8	4:30 PM	459	184	275	55 Vehicles 1,100'	73 Vehicles 1,460'	18 Vehicles 360'

Parking:

The KIPP school property contains 109 parking spaces. The school use, with 32 elementary classrooms at 1.5 spaces each and 16 middle school (grades 6-8 or specialized) classroom at 3.5 spaces each, will require 109 parking spaces at buildout per Dallas City Code §51A-4.202(17)(C).

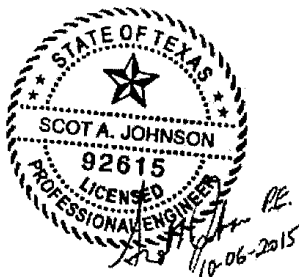
Summary:

This TMP and the attached TMP plan define the drop-off and pick-up procedures for the KIPP Truth Academy at 2200 N. Saint Augustine Drive with a maximum of 1,119 students. The TMP vehicle routes provide an available queue distance within the site that is greater than the projected maximum expected queue for the school's operations. The school traffic will never be allowed to queue vehicles in the ROW of any City street or alley, nor will the traffic on any City street be stopped or diverted. The property owner/school administrator is responsible for the administration of the TMP and minimizing the impact of the vehicle queue on the City streets. Only uniformed police officers should be allowed to direct and control traffic operating within the public right-of-way.

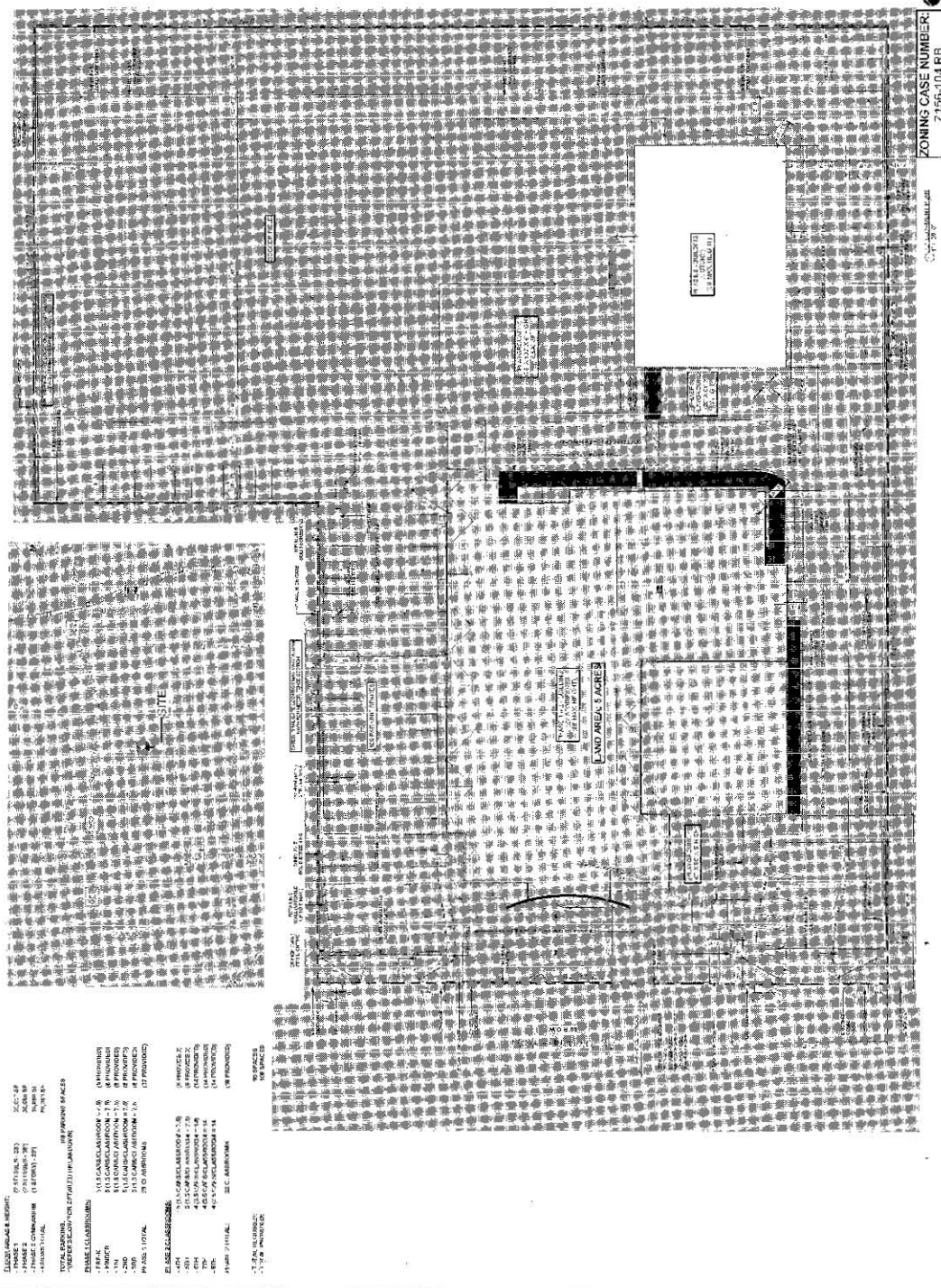
Based on the vehicle queuing analysis conducted and the resulting Traffic Management Plan, I, Scot A. Johnson, P.E. #92615, certify that the results indicate that no queuing of vehicles dropping off or picking up students at 2220 N. Saint Augustine Drive will extend onto City of Dallas rights-of-way as a result of internal queuing constraints.

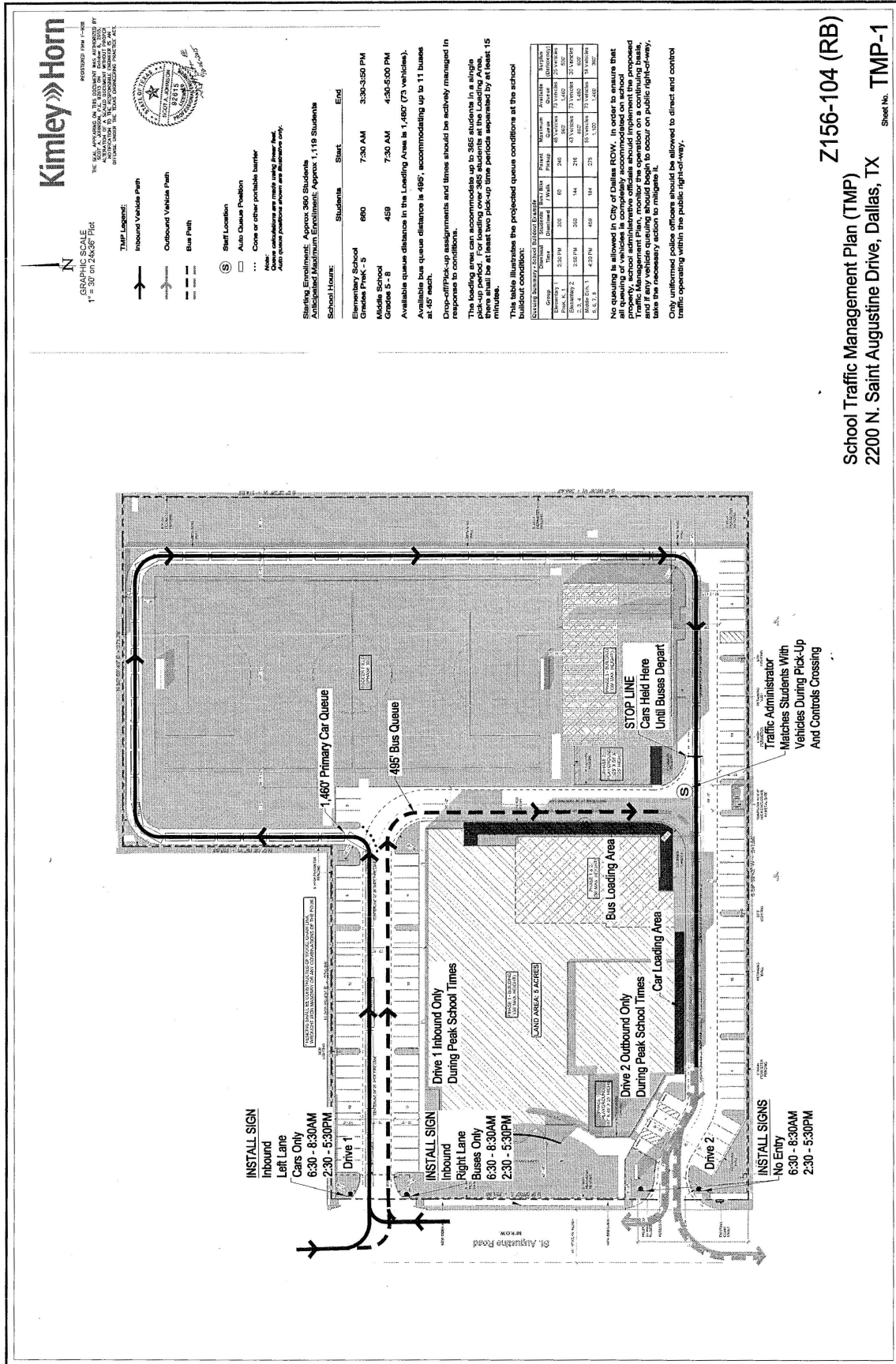
In order to ensure that all queuing of vehicles is completely accommodated on school property, the school administrative officials should implement the proposed Traffic Management Plan, monitor the operation on a continuing basis, and if any vehicle queuing should begin to occur on public right-of-way, take the necessary action to mitigate it.

Prepared by:
Kimley-Horn and Associates, Inc.
Scot A. Johnson, P.E., PTOE
12750 Merit Drive, Suite 1000
Dallas, TX 75251
(972) 770-1300

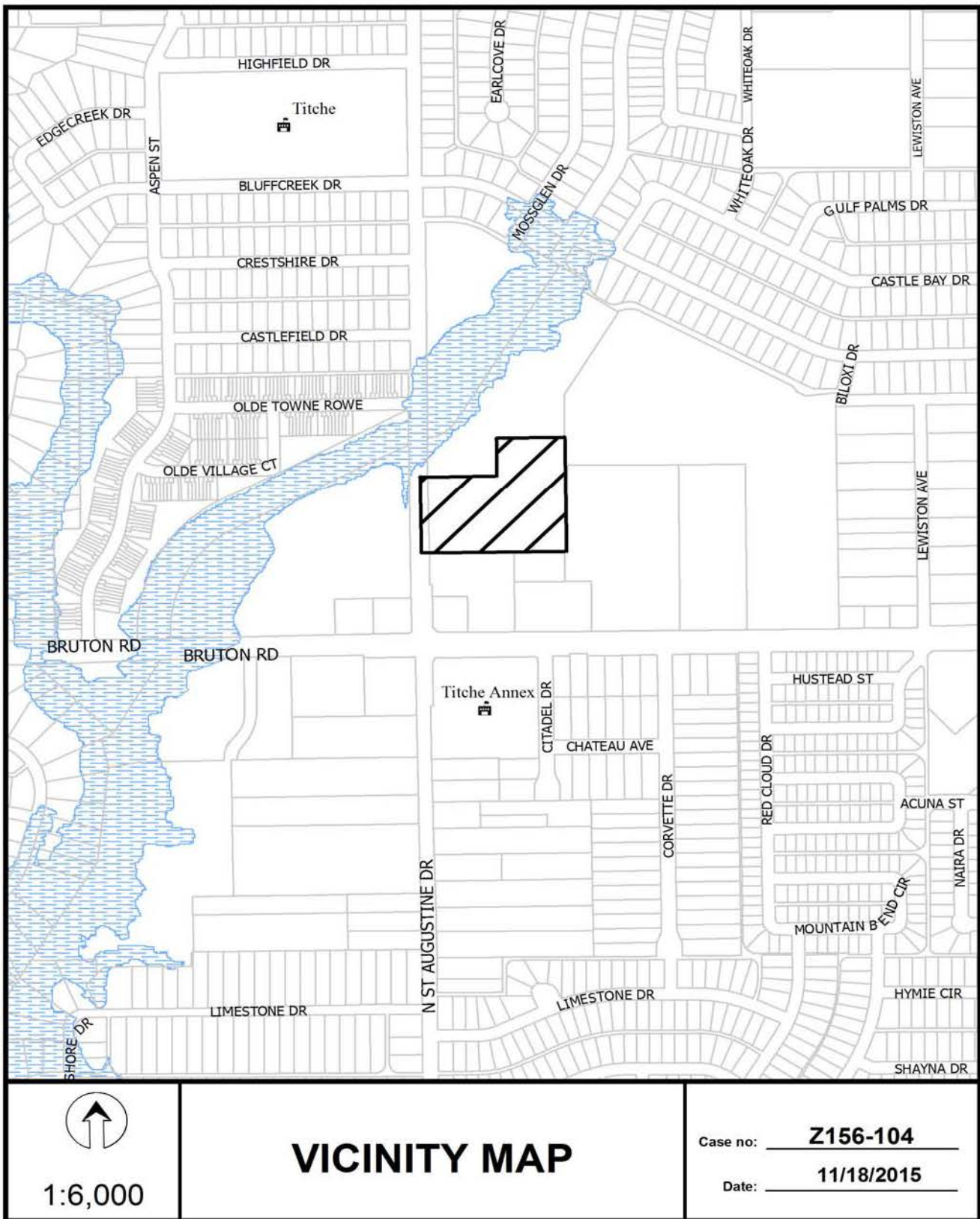


KIPP: PLEASANT GROVE ELEMENTARY SCHOOL
2200 N St. Augustine Dr. - Dallas, TX



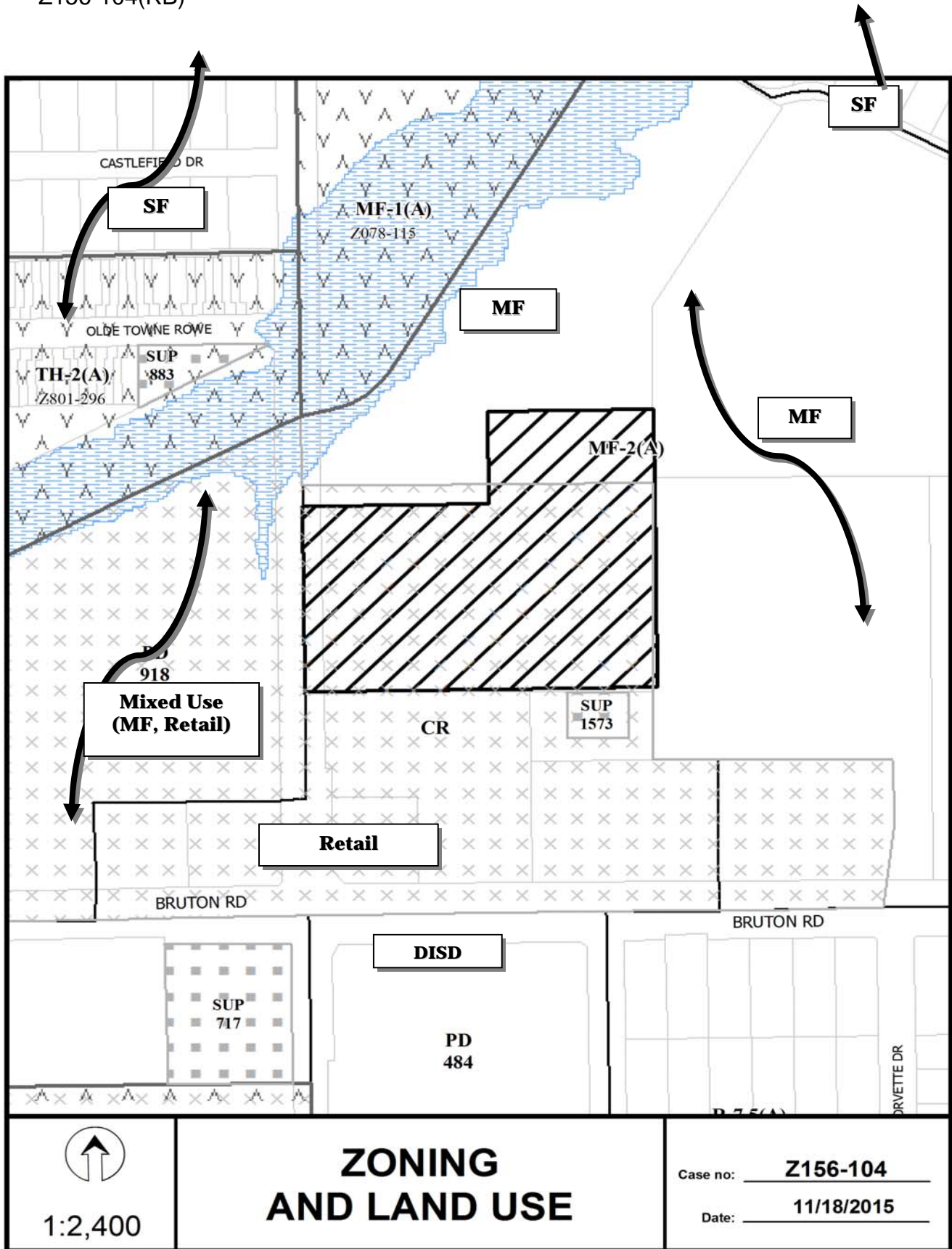


Z156-104(RB)

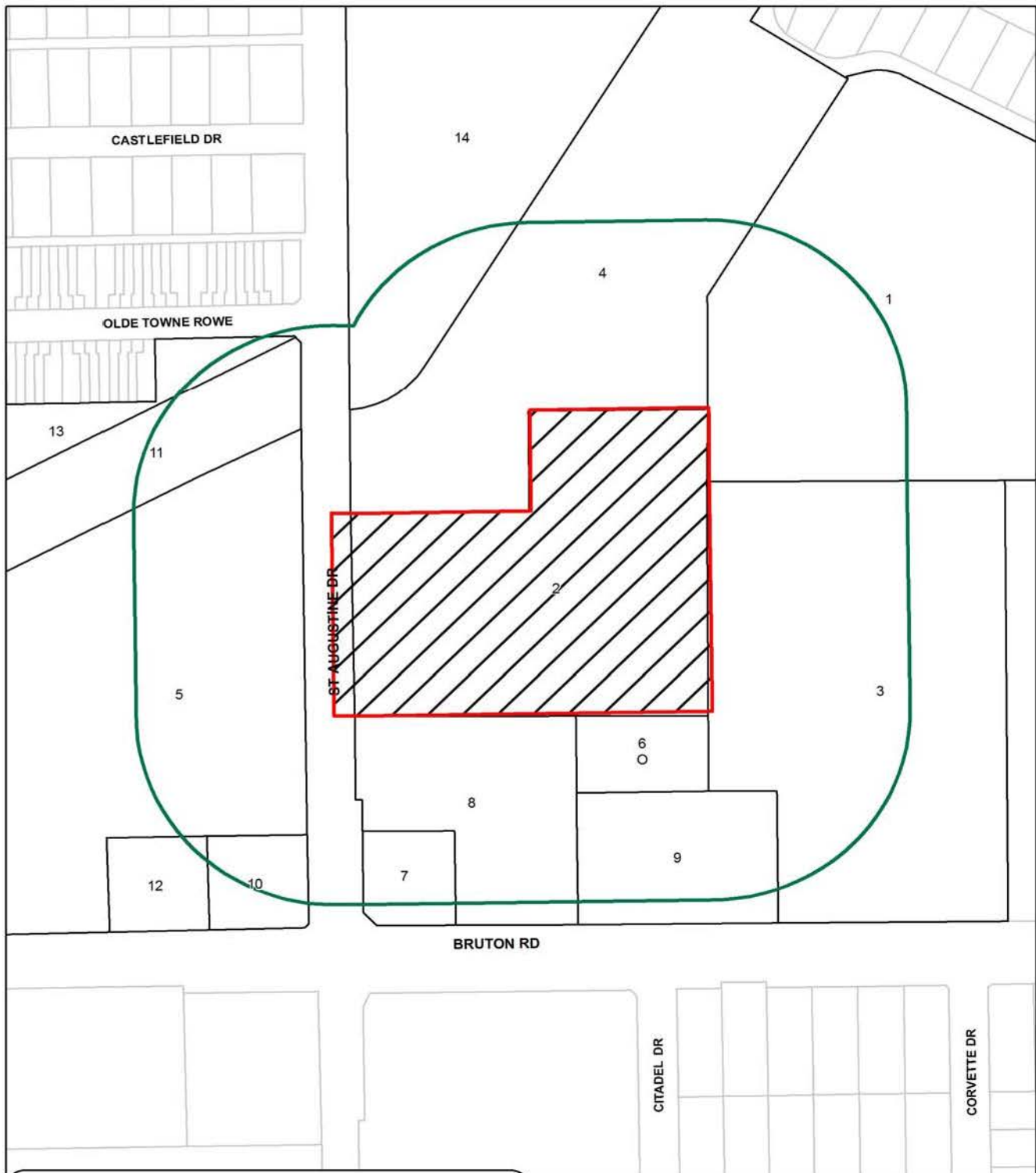




Z156-104(RB)



CPC RESPONSES



<u>14</u>	Property Owners Notified (14 parcels)
<u>1</u>	Replies in Favor (1 parcels)
<u>0</u>	Replies in Opposition (0 parcels)
<u>300'</u>	Area of Notification
<u>12/3/2015</u>	Date

Z156-104
CPC



1:2,400

12/02/2015

Reply List of Property Owners***Z156-104******14 Property Owners Notified******1 Property Owners in Favor******0 Property Owners Opposed***

<i>Reply</i>	<i>Label #</i>	<i>Address</i>	<i>Owner</i>
	1	9901 BRUTON RD	EL RANCHO APARTMENTS LLC
	2	2200 N ST AUGUSTINE DR	IGLESIA NUEVA VISION INC
	3	9797 BRUTON RD	OMNI AG THE FALLS LLC
	4	2222 N ST AUGUSTINE DR	ST AUGUSTINE ESTATE APTS
	5	9415 BRUTON RD	CITY OF DALLAS HSING FINANCE CORP
O	6	2300 N ST AUGUSTINE DR	SANDVIC INV LTD &
	7	9607 BRUTON RD	SILWAD INC
	8	2120 N ST AUGUSTINE DR	MAHAL PROPERTIES LTD
	9	9709 BRUTON RD	DALLAS MEDICAL HOLDINGS
	10	9535 BRUTON RD	VU TRAM PHUONG
	11	9203 BRUTON RD	VLG BY CREEK HOMEOWNERS
	12	9513 BRUTON RD	AKITA PARTNERS LLC
	13	9999 OLDE TOWNE ROWE	VILLAGE BY THE CREEK HOA
	14	2314 N ST AUGUSTINE DR	CRESTSHIRE VILLAGE LTD

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2

DEPARTMENT: Planning and Urban Design

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: 45Q, U, V

SUBJECT

A public hearing to receive comments to amend the City of Dallas Thoroughfare Plan to change the dimensional classification of Akard Street from IH-30 to Corinth Street from a standard four-lane undivided (S-4-U) roadway within 60-feet of right-of-way to a special two-lane undivided (SPCL 2U) roadway with bicycle facilities within 60-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

BACKGROUND

The City of Dallas' Office of Economic Development is working with Hamilton Properties Corporation to redevelop the hotel located at 1011 South Akard Street. Together they are requesting an amendment to the City of Dallas Thoroughfare Plan to change the designation of Akard Street from IH-30 to Griffin Street West. Staff looked at the area comprehensively and recommends amending Akard Street from IH-30 to Corinth Street. The proposed complete street design will enhance multi-modal connectivity from the DART Cedars Station and improve connectivity from the Cedars District to the downtown core. Although the proposed complete street cross section reduces auto capacity, it can accommodate existing and projected traffic volumes; it also adds bicycle facilities and maintains some parking.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

The City Plan Commission Transportation Committee acted on this item on November 19, 2015, and followed staff recommendation of approval.

The City Plan Commission acted on this item on December 3, 2015, and followed staff recommendation of approval.

FISCAL INFORMATION

No cost consideration to the City

MAP

Attached

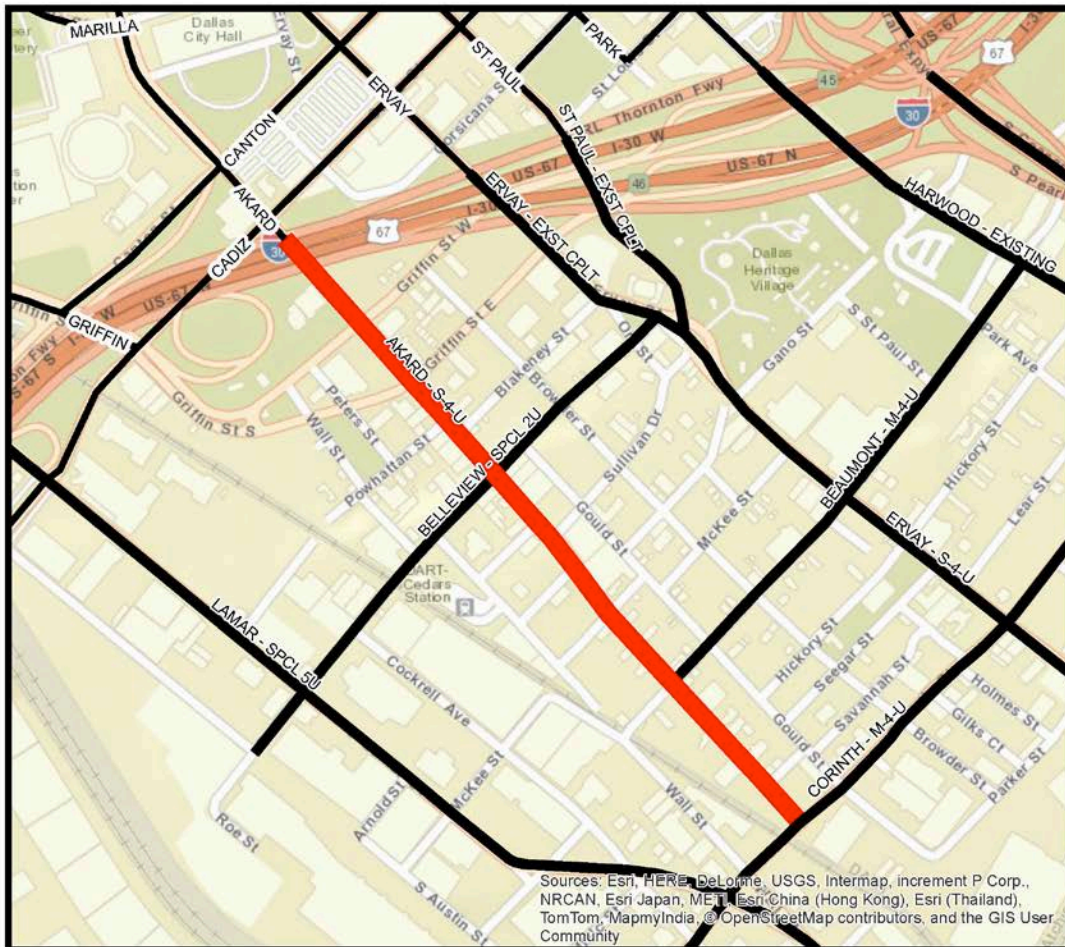
Akard Street

IH-30 to Corinth Street

Council District: 2
MAPSCO: 45Q,U,V



Thoroughfare Plan Amendment Map



Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), TomTom, MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 3

DEPARTMENT: Planning and Urban Design

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: 61A-X Y 71A-B

SUBJECT

A public hearing to receive comments to amend the City of Dallas Thoroughfare Plan to change the dimensional classification of Camp Wisdom Road from FM 1382 to the Grand Prairie City Limits from a standard six-lane divided (S-6-D) roadway within 107-feet of right-of-way to a special four-lane undivided (SPCL 4U) roadway with bicycle facilities within 100-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

BACKGROUND

The City of Dallas' Public Works Department is requesting an amendment to the City of Dallas Thoroughfare Plan to change the dimensional classification of Camp Wisdom Road from FM 1382 to the Grand Prairie City Limits. This amendment is to facilitate a joint project between the City of Dallas and Dallas County to reconstruct this segment of roadway. This complete street project design will provide multi-modal connectivity for pedestrians, cyclists and automotive users. The cross section can be accommodated within the existing right-of-way.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

The City Plan Commission Transportation Committee acted on this item on November 19, 2015, and followed staff recommendation of approval.

The City Plan Commission acted on this item on December 3, 2015, and followed staff recommendation of approval.

FISCAL INFORMATION

No cost consideration to the City

MAP

Attached

Camp Wisdom

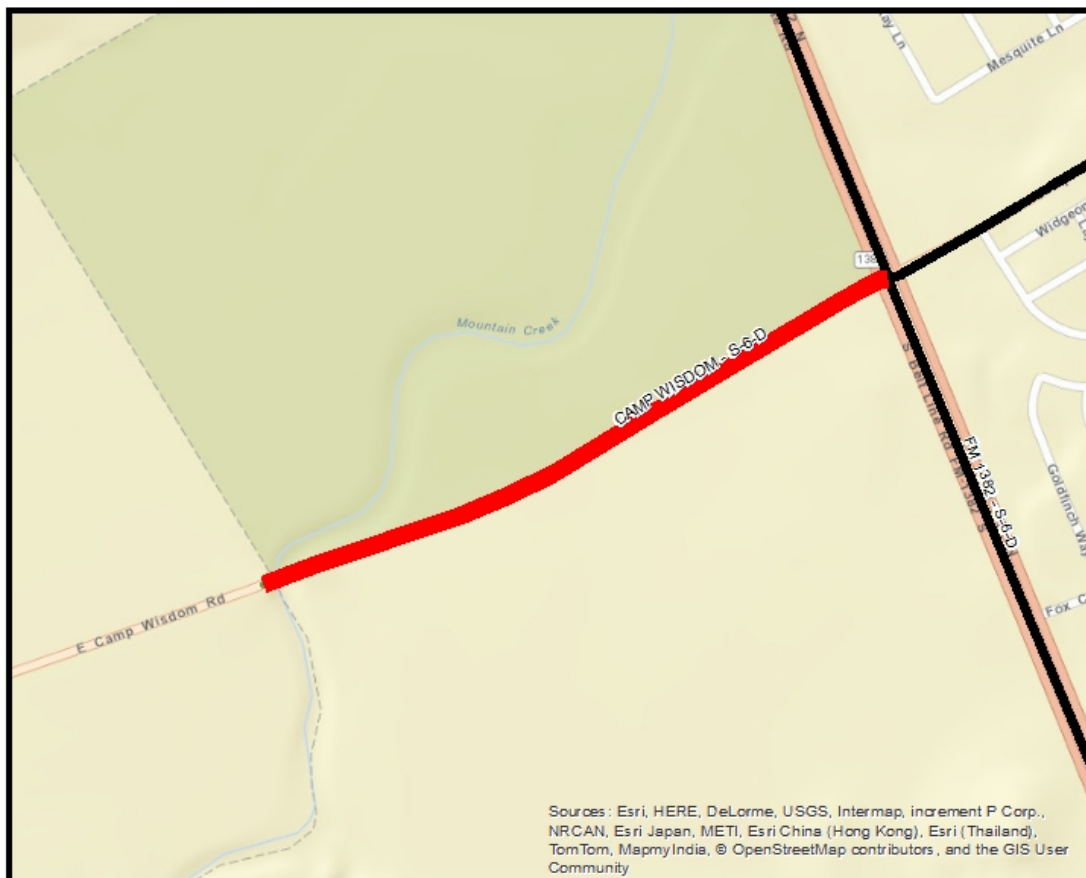
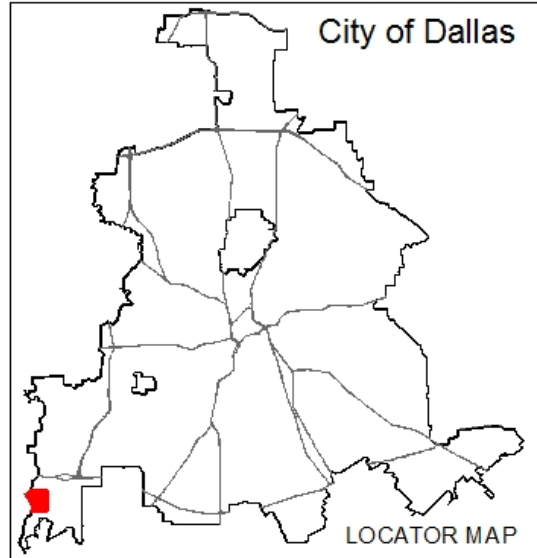
Grand Prairie City Limits to FM 1362

Council District: 3

MAPSCO: 61A-X,Y; 71A-B



Thoroughfare Plan Amendment Map



Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp.,
NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand),
TomTom, MapmyIndia, © OpenStreetMap contributors, and the GIS User
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KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: January 27, 2016

COUNCIL DISTRICT(S): 2

DEPARTMENT: Planning and Urban Design

CMO: Alan Sims, Chief of Neighborhood Plus, 670-1611

MAPSCO: 45P

SUBJECT

A public hearing to receive comments to amend the City of Dallas Central Business District Streets and Vehicular Circulation Plan to change the proposed rights-of-way on **(1)** Record Street from Wood Street to Young Street from 80-feet to 64-feet of right-of-way; and **(2)** Market Street from Wood Street to Young Street from 80-feet to 67-feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

BACKGROUND

400 South Record Street, LLC is requesting amendments to change the proposed rights-of-way on **(1)** Record Street from Wood Street to Young Street from 80-feet to 64-feet of right-of-way; and **(2)** Market Street from Wood Street to Young Street from 80-feet to 67-feet of right-of-way. The rights-of-way on both streets were abandoned by the City of Dallas in 1982, Ordinance No. 17423, however, amendments to the Central Business District Streets and Vehicular Circulation Plan were not processed. The applicant has submitted plans to improve the property bound by Record Street, Market Street, Young Street, and Wood Street. Amendments to the Central Business District Streets and Vehicular Circulation Plan are required to move the project forward. A reduction in the rights-of-way will not change the operation of Record Street or Market Street.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

The City Plan Commission Transportation Committee acted on this item on November 19, 2015, and followed staff recommendation of approval.

The City Plan Commission acted on this item on December 3, 2015, and followed staff recommendation of approval.

FISCAL INFORMATION

No cost consideration to the City

MAP

Attached

Record Street

Wood Street to Young Street

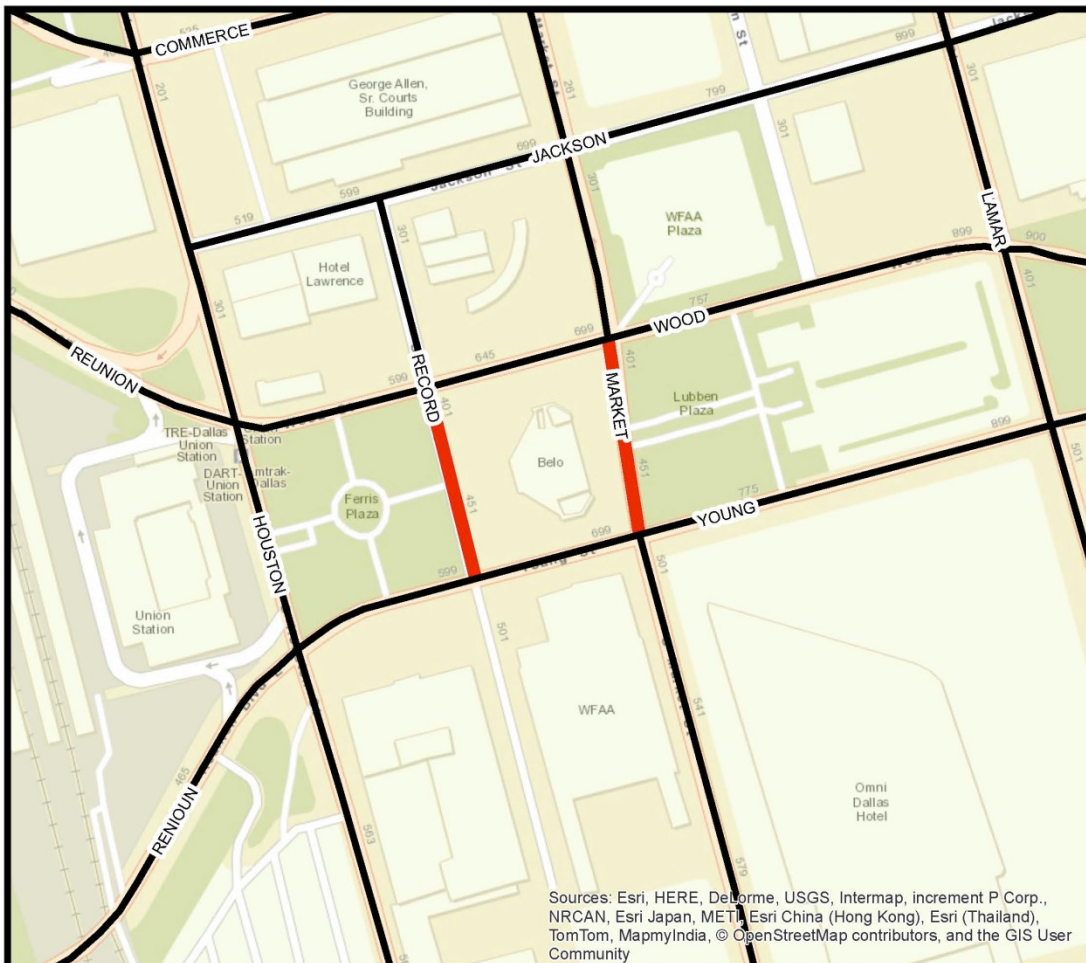
Market Street

Wood Street to Young Street

Council District: 2

MAPSCO: 45P

Central Business District Vehicle Circulation Plan Amendment Map



Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp.,
NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand),
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